June 24, 2025 7:00 p.m.

#### **Agenda**

#### I. Public Meeting Agenda:

- 1. Pledge of Allegiance
- 2. Approval of the Minutes from the Meeting of May 27, 2025.
- Preliminary Subdivision and Land Development application for Edna J. Bialobrzeski at 65
  Newville Road, Chalfont PA (TMP 26-003-026) to maintain the original house on one lot and
  subdivide one new building lot.
- 4. Conditional Use application for Celek Chalfont LLC to propose an L2 Outdoor Storage Use and Display per 27-1201.c (Uses permitted by Conditional Use in the C-1 District) and 27-305.L.L2 (Use Regulations for Outdoor Storage and Display) incidental to a Tractor Supply Retail Use at a commercial property located at 241 West Butler Avenue (TMP #26-013-032).
- 5. Preliminary Subdivision and Land Development application for Flagship Pennsylvania Propco LLC at 545 West Butler Avenue (TMP #26-006-101-001) proposing Use J19 Carwash ("Spotless Carwash").
- 6. Public Comment

#### **II. Other Business**

#### III. Adjournment

The next meeting of the New Britain Township Planning Commission is scheduled to take place on Tuesday July 22, 2025 at 7:00 p.m. at the New Britain Township Building, 207 Park Avenue, Chalfont, PA.

#### MEETING MINUTES May 27, 2025 7:00 PM

A meeting of the New Britain Township Planning Commission was held on May 27, 2025, at the Township Administration Building, 207 Park Avenue, New Britain Township, PA beginning at 7:00 p.m. In attendance were Chair Marco Tustanowsky, Vice Chair Jim Scanzillo, and members Deborah Rendon, Jeff Berman, and Theresa Rizzo-Grimes. Robert Livesay as well as Board Liaison Stephanie Shortall, Zoning Officer Doreen Curtin, and Township Engineer Craig Kennard, P.E.

- I. Pledge of Allegiance
- II. Public Meeting Agenda
  - a. Approval of the Minutes from the Meeting of March 25, 2025.

Robert Livesay made a motion to approve the March 25, 2025, meeting minutes, seconded by Jim Scanzillo with the following edit that Robert Livesay was in attendance.

- b. Applicants for the Conditional Use application for Celek Chalfont LLC to propose an L2 Outdoor Storage use and Display per 27-1201.c and 27-305.L.L2 incidental to a Tractor Supply Retain use at a commercial property located at 241 West Butler Ave (TMP 26-0130032) was tabled and rescheduled to the June 24, 2025, meeting agenda.
- c. Review of a Preliminary Subdivision and Land Development application for Flagship Pennsylvania Propco LLC at 545 West Butler Avenue (TMP 26-006-101-001) proposing aa Use J19 Carwash (Spotless Carwash) was tabled and rescheduled to the June 24, 2025, meeting agenda.
- d. 226 Chapman Road Rezone from Watershed Zoning District to C/R Conservation and Recreation District

Dan Grieser presented the request to rezone the parcel at 226 Chapman Road from the Watershed District to the Conservation and Recreation District to support the expansion of educational programs in the park. The C/R District permits public recreational programs while the WS residential district does not. He discussed the plans to widen the road going to the nature center all to better serve the public.

Township Engineer Mr. Kennard, P.E., noted that the rezoning would be more advantageous than a variance as it would allow for one contiguous zoning district.

It was recommended that the rest of Chapman Road be vacated.

Ms. Redon had questions regarding road widening and a discussion ensued. Mr. Grieser noted that stormwater management would be done. A discussion ensued regarding traffic concerns and some comments were noted regarding preliminary design ideas.

A discussion ensued regarding impervious coverage, and it was noted that there were no plans for additional coverage for a parking lot expansion. There was a small discussion regarding the use of the existing building.

Carol Young asked about the stormwater management and the existing parking during public comment.

Deborah Rendon made the motion to rezone the Zoning District from Watershed to Conservation/Recreation and to consider approving the draft ordinance and zoning map for the rezoning of the district. Jeff Berman seconded the motion, and the motion carried unanimously.

e. Review and make recommendations regarding proposed comprehensive amendments to the New Briain Township Zoning and Subdivision and Land Development Ordinances. Mr. Kennard recapped the development of the revisions noting that the Board of Supervisor concerns and the Planning Commission concerns were addressed in the revisions. He discussed the changed in general terms and noted that the revisions needed to go before the Bucks County Planning Commission for review and comment prior to submission to the Board of Supervisors for adoption.

He recommended that the motion be made to approve for recommendation to the Board of Supervisors, both ordinances as amended.

Jim Scanzillo made the motion as stated, Deborah Rendon seconded the motion, and the motion carried unanimously.

#### III. Public Comment

There was no additional public comment

#### IV. Other Business

There was no additional business.

#### V. Adjournment

Upon motion made by Jeff Berman, seconded by Jim Scanzillo, the meeting was adjourned at 7:20pm.

Respectfully Submitted,	
Marco Tustanowsky, Chair	Daniel Fox, Township Manager



May 30, 2025

File No. 2500540

Doreen Curtin, Zoning Officer New Britain Township 207 Park Avenue Chalfont. PA 18914

Reference: 65 Newville Road, Subdivision Review 1

TMP #s 26-003-026

#### Dear Doreen:

Pursuant to your request, Gilmore & Associates, Inc. has reviewed the Minor Subdivision Plan for the above-referenced project. We offer the following comments for consideration by New Britain Township:

#### I. Submission

- A. Minor Subdivision Plans prepared for Edna J. Bialobrzeski, as prepared by Cowan Associates, Inc., consisting of two (2) sheets, dated April 9, 2025.
- B. Waiver Requests letter for Edna J. Bialobrzeski Minor Subdivision, as prepared by Cowan Associates, Inc. dated April 22, 2025.

#### II. General Information

The 12.6-acre tract is located at 65 Newville Road (T-320), between Curley Mill and New Galena Roads which contains an existing single-family dwelling in the WS Watershed Zoning District. Existing natural resources include a watercourse, riparian buffer, floodplain soils, steep slopes, woodlands, and agricultural soils. The Applicant proposes to subdivide the property into two lots for presumably a future B1 single-family dwelling, which is permitted by right. Lot 1 will be 6.42 acres and contain the existing dwelling, accessory structures, on-lot well and on-lot septic system. Lot 2 will be 5.93 acres and there are currently no proposed improvements.

#### III. Review Comments

#### A. Zoning Ordinance

- 1. The following comments related to the dimensional standards shall be addressed:
  - a. §27-502.b.1.(b) The minimum lot width shall be measured at the minimum required building setback line. Though it appears to comply, the lot width for Lot 2 shall be dimensioned at the building setback line.
  - b. §27-502.b.1.(f) The building envelope, by definition (§27-201), shall exclude the area of any required setbacks, buffer yards, open space, easements, natural resources with 100% protection standard and the portion of those natural features that may not be developed or intruded upon as specified and identified by this chapter. The building envelope shall be revised accordingly and the area noted in the Zoning Information table.

65 East Butler Avenue | Suite 100 | New Britain, PA 18901 | Phone: 215-345-4330 | Fax: 215-345-8606

- c. §27-502.b.1.(g&h) The ratio base site area (RBSA) is used to calculate the impervious surface ratio and building coverage ratio and subtracts 100% protected resource areas. The ratio base site area calculation shall be provided on the plan and the coverage ratios revised as necessary. An impervious surface tabulation should be provided for the existing impervious for reference.
- 2. §27-502.b.3 & 2401 All natural resource protection land shall be protected on each lot with a conservation easement in accordance with §27-505 and §27-2400 of this Chapter. The watercourse easement shall be revised to a "conservation easement". The easement shall be expanded to include the full extent of all required protected natural resource land.
- 3. §27-505 & 2402 The Site Capacity Calculations shall include those resources with 100% protection set forth in § 27-2402, i.e. floodplain soils, riparian buffer, etc.
- 4. §27-2400.d Wetland boundaries shall be delineated through an on-site assessment conducted by a professional soil scientist. The National Wetlands Inventory shows potential wetlands along the watercourse.

#### B. <u>Subdivision and Land Development Ordinance Waivers</u>

The following waivers from the Subdivision and Land Development Ordinance have been formally requested by the Applicant:

- §22-703.4.A.(4) From the requirement that the depth of any proposed lot shall not exceed three times its width. Lots 1 has a depth to width ratio of approximately 4 to 1 and Lot 2 has a depth to width ratio of approximately 7 to 1 at the shortest point of the lot width. The existing lot has a depth-to-width ratio of 2.5 to 1 and any subdivision of the lot would exceed this requirement.
- 2. §22-706.1 & 2 From the requirement to provide curb and sidewalk along the property frontage. We note that no curb or sidewalk currently exists along Newville Road. Therefore, we have no objection to this waiver.
- 3. §22-711 From the requirement to provide erosion controls, since the minor subdivision plan does not propose any disturbance at this time. Erosion controls shall be deferred until the time of development of Lot 2.
- 4. §22-712 From the requirements for stormwater facilities and stormwater management since no improvements are currently proposed. When subdivision and/or land development plans are submitted for approval in phases, a complete stormwater management design for the entire subdivision and/or land development shall be submitted in the application for the first phase. We have no objection to stormwater facilities being deferred until the time of development of Lot 2. A note to this effect shall be added to the Record Plan.
- 5. §22-713.4.A From the requirement to provide street trees. The Applicant has requested a deferral from this requirement until the new house is constructed for Lot 2. We note that this requirement requires street trees on both lots. While there are sufficient street trees on Lot 2, based on 300 linear feet of frontage on Lot 1, 4 additional street trees are required. We recommend they be installed prior to the plans being recorded. If granted, section §22-713.4.A shall be specified. A note shall be added to the Record Plan requiring the street trees for Lot 2 if the existing trees are removed for future development of the property.
- 6. In addition, if public improvement waivers are granted, Township Resolution 2007-12 requires a contribution towards future improvements based on 50% of the total estimated construction cost. An estimated cost of construction of any waived improvements shall be provided. The Board may adjust the amount based on onsite and offsite improvements not required by Ordinance at their discretion.

#### C. Subdivision and Land Development Ordinance

- 1. §22-401.6 Subdivisions creating a new building lot shall be considered a major subdivision and the application shall establish a plan for all required improvements where there is additional vacant land for future subdivision of additional lots. Therefore, Site Capacity calculations shall be provided in accordance with §27-503 & 2402 based on minimum lot area.
  - We note that due to natural resources onsite, required stormwater improvements, and the lot being in the on-lot well and sewage service area, we recommend a concept plan be prepared by a licensed engineer to confirm that the lot can be developed as shown.
- 2. §22-405.2 Applications that qualify as minor plans may be submitted for concurrent preliminary and final plan approval. Though the plan doesn't quality as a minor plan, we would support a waiver to process the plan as preliminary/final with the plan approval being conditioned on the Applicant providing the final plan submission fee.
- 3. §22-406.1.I The Applicant shall prepare legal descriptions for the proposed lots, ultimate rights-of-way, conservation easements, backup septic, etc. as required.
- 4. §22-502.1.B(20) A note shall be added to the plan offering the ultimate right-of-way of Newville Road to New Britain Township.
- 5. §22-502.1.B.(21) The following comments regarding the plans and notes shall be addressed:
  - a. The distance between the title line and the ultimate right-of-way line shall be provided at the northwest corner of Lot 1 and southeast corner of Lot 2.
  - b. The Proposed Lot Line dimensions 999.56 feet to the ultimate right-of-way line and 1,029.09 feet to the title line. This is not consistent with the distance of 29.65 feet between the title line and ultimate right-of-way line. This discrepancy should be resolved.
  - c. The Bucks County Planning Commission no longer physically signs and dates the plans. The signature line and certification date can be removed. However, the Bucks County Planning Commission file number shall be added to the plan once received.
- 6. We offer the following comments regarding the road improvements required:
  - a. §22-705.3.A & C Newville Road is considered a minor collector street which requires a 60-foot ultimate right-of-way and 36-foot cartway. The plan appears to show the ultimate right-of-way 25 feet from the centerline of the street. The ultimate right-of-way shall be revised to provide the required 30-foot half width and the building setback line revised accordingly. The lot area calculations shall also be revised accordingly.
  - b. It appears that the lane width is 8 feet and therefore 10 feet of road widening is required. We would support a waiver from road widening.
  - c. We recommend that the Township consider having the Applicant remove any vegetation along Newville Road within the Township's right-of-way that is dead or interfering with the overhead wires as a condition of any waivers per the request of Township Public Works.
- 7. §22-705.3.G Where a subdivision and/or land development abuts or contains an existing street, the applicant shall be required to mill and overlay the entire width of the roadway a depth of 1 1/2 inches. We would support a waiver from milling and overlay.
- 8. §22-712.13.D The Township requires a Stormwater fee to maintain existing facilities. The fee applies to all existing and proposed storm sewers located in public rights-of-way and is based on \$2.50/LF of existing roads. Based on 414.38' of road frontage, a fee of \$165.75 is required.
- 9. §22-715 Land is required to be dedicated to the Township for park and recreation unless a fee in lieu of dedication is provided in the amount of \$2,500 per dwelling unit.

- 10. §22-716 The following comments related to monumentation shall be addressed:
  - a. A concrete monument shall be accurately placed at all outbound existing property corners and at all proposed lot corners, including all changes in direction of boundary.
  - b. A concrete monument shall be placed at the corner of Lot 1 and 2 and the street right-of-way in lieu of a pin.
  - c. Survey monuments and markers shall be set at the beginning and end of all easements, including changes in direction of easement. Proposed monumentation shall be shown at the corners of the required conservation easement.
  - d. A written certification letter from a professional land surveyor shall be provided once the plans are approved and prior to the recording of the plan indicating that the monumentation has been installed per the plan.
- 11. §22-719.4, 5 & §26-403.2 No building permit for a new residential building which is to be served by a new well, shall be issued unless the well intended to serve the building has been drilled, tested and certified by a licensed state laboratory to be in compliance with Chapter §26, Part 4. Wells are required to be constructed and tested to confirm there is adequate supply of water for the proposed use considering quality and quantity and/or that there are no adverse affects to nearby wells or watercourses prior to a permit for a building. Though no conceptual design is currently proposed, we recommend before the subdivision is approved, that the well be installed and tested to determine if suitable water can be provided for a future dwelling in accordance with the Township's Well Ordinance.
- 12. <u>§22-721</u> Though no improvements are proposed, we offer the following comments regarding on-lot septic systems:
  - a. §22-721.3 A completed PADEP Sewage Facilities Planning Module shall be submitted with all preliminary plan applications submitted for subdivision and/or land development with on-lot sewage disposal systems.
  - b. §22-721.5 Final plans shall not be recorded until a permit from the Bucks County Department of Health is issued for each proposed on-lot sewage disposal system, indicating the lot proposed for subdivision and/or land development is suitable for the type of on-lot sewage disposal proposed by the applicant.
  - c. §22-721.6 If the property being subdivided or developed contains an existing on-site sewage disposal system, the applicant shall submit to the Township acknowledgment from the Bucks County Department of Health or other qualified, responsible inspector, indicating that the existing system has been inspected and is functioning properly.
  - d. §22-721.7 All lots shall also be tested to identify a suitable replacement area in the event the primary disposal system fails. The on-lot sewage replacement area shall be identified for each lot on the plans and the soil testing shall be approved by the Township and Bucks County Department of Health. An easement deed-restricting the sewage replacement area from being built upon shall be provided and shown on the site plan. The sewage replacement area shall be located at least 25 feet from the primary system and shall not be located directly down slope of the primary system, or within any well isolation area.

If you have any questions regarding the above, please contact this office.

Sincerely,

Janene Marchand, P.E. Project Engineer Gilmore & Associates, Inc.

Janune Manchamd

#### JM/tw

cc: Daniel Fox, Township Manager

Alexandria Mullin, Assistant to Manager Randy Teschner, Building Code Official Scott Holbert, Esquire, Flager & Associates Scott P. McMakin, P.E., Cowan Associates, Inc.

Edna J. Bialobrzeski, Applicant, 65 Newville Road/jcroissette@remax440.com

Craig D. Kennard, P.E., E.V.P., Gilmore & Associates, Inc.

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### **NEW BRITAIN TOWNSHIP**

207 Park Avenue • Chalfont, PA 18914 • Telephone: (215) 822-1391

### SUBDIVISION & LAND DEVELOPMENT APPLICATION

1.	Date of Application:	April 25, 2025	TOWNSHIP USE ONLY		
2.	Date of Plan or Revision:	April 9, 2025	Date Recieved:		
3.	Application for:	Minor Subdivision	Payment: Check #:		
4.	Name of Subdivision or Land Development:	Edna J. Bialobrzeski	Receipt #: Escrow Acc. #:		
5.	Location:	65 Newville Road	Looi ow 7 too. H.		
6.	Tax Map Parcel #: 26- 003-026	Total Acreage: Gross 12.63	Base Site Area 12.34		
7.	Net Buildable Site Area (from Section 2401):	-			
8.	Zoning Requirements:	mum Lot Size 80,000 SF	Maximum Density		
	Front Yard 100' Side	Yard30'	Rear Yard60'		
9.	Number of Lots or Dwelling Units:	2			
10.	Equitable Owner of Record of Land:	Edna J. Bialobrzeski			
	Address:	65 Newville Road			
		Chalfont, PA 18914			
	Phone: 215-872-4966 E-r	nail: jcroissette@remax440.com			
11.	Applicant:	Same as Owner	_		
	Address:				
	Phone: E-r	mail:			
12.	Registered Engineer or Surveyor:	Scott P. McMackin, P.E.,	Cowan Associates, Inc.		
	Address:	120 Penn Am Drive			
		Quakertown, PA 18951			
	Phone: 215-536-7075, ext 132 E-n	nail: <u>spm@cowanassociates.</u>	<u>co</u> m		
13.	Type of Water & Sewer: ☐ Public Water ☐ Private Water ☐ Public Sewer ☐ Private Sewer				
14.	Proposed Use:	B-1 Single Family Detac	<u>che</u> d		
	is to certify that I have read Article V of the New the accompanying plan meets the requirements of the New Article V of the New the Article V of the New the Article V of the New the New the Article V of the New the New the Article V of the New t	of that Article to the best of my kno			
	Signature of Applicant	Signature of Re	gistered Engineer or Surveyor		

		SUBMISSION CHECKLIST				
Date of	f Application:	April 25, 2025				
		Edna J. Bialobrzeski				
	abdivision/Ednid Development Name.					
	s of Property:					
	wwner(s) Name: Edna J. Bialobrzeski					
	ant(s) Name:	Same as Owner 26-003-026				
Tax Ma	ap Parcel Number:	20-003-020				
Plan S	Sets for New Britain Township - Fold	ed to 8 1/2 x 11: (9 Full Size, 5 11x17)				
	NBT Board of Supervisors (Full Size) - 5 Copi	ies				
$\square$	Township Engineer (Full Size) - 1 Copy	✓ NBT File (Full Size) - 2 Copies				
	Fire Marshal (Full Size) - 1 Copy	☑ Digital Submission - Email or File Sharing Service				
Applia	cation Forms & Fees to be Submitte	d to the Township: All applicants include these items or the				
applica		omplete and returned to the applicant. Please also submit				
$\square$	New Britain Township Subdivision and Land I	Development Application (1 Signed Original)				
abla	Filing Fee according to the most current Fee	Schedule adopted by Resolution				
$\square$	Escrow Fee according to the most current Fe	e Schedule adopted by Resolution				
	Contract for Professional Services Agreemen	t (3 Signed Originals) - To be provided by Borough Solicitor				
	Proof of Submission to Bucks County Plannin	, , ,				
N/A □	Proof of Submission to Bucks County Conser	vation District				
N/A □	Proof of Submission to Water Authority (North	n Penn/North Wales/Aqua), if applicable				
	Proof of Submission to Chalfont New Britain	Joint Sewer Authority <b>OR</b> Buck County Department of Health				
N/A □	Stomwater Management Report (2 Copies) (I	Digital Acceptable)				
	PADEP Sewage Facilities Planning Module A	pplication/Mailer approved by Sewer Authority or SEO (Original & 2 Copies)				
√A □	Traffic Impact Study, if applicable (2 Copies)	(Digital Acceptable)				
N/A □	PADOT Highway Occupancy Permit Plan/App	olication, if applicable (2 Copies)				
√A □	Community Impact Assessment Report, if applicable (4 Copies)					
	Affidavit of Mailing Notification, Mailing Conte	nt & Post Property of SLD Application (§ 22-401.8 General Procedures)				
√A □	Township Road Opening Permit, if applicable					
N/A □	Water Resource Impact Study, if applicable (2 Copies) (Digital Acceptable)					
	Waiver Request Letter with Justification					
Revie	wed By:	Date:				
1 10 110	Township Representative	<del></del>				

<sup>\*\*</sup>The attached checklist is provided for the applicant as a guideline to assist in the submission process. All applicants must include these items or the application will be considered administratively incomplete and returned to the applicant.\*\*



<sup>\*</sup>All fees or contributions in lieu of shall be payable to New Britain Township. All plan sets, applications and forms shall be submitted directly to New Britain Township.

## NEW BRITAIN TOWNSHIP SUBDIVISION & LAND DEVELOPMENT PROCEDURES INFORMATION FOR APPLICANTS

#### **Applications:**

All formal applications must be submitted to the Township Zoning Officer during regular office hours.

All required plans, fees and accompanying documentation must be submitted to the Township in accordance with the New Britain Township Plan Submission Checklist (attached). No application shall be considered complete without this information.

If the application is found to be incomplete, the Zoning Officer will advise the applicant, in writing, of the deficiencies within five (5) business days.

If the application is found to be complete, the application will be date-stamped and a letter will be forwarded to the applicant with the anticipated application processing dates and scheduled Planning Commission and Board of Supervisor's meeting dates. The application will then be distributed for review to the Township professional staff, boards, commissions and professional consultants.

If for any reason the Planning Commission does not meet within 30 days of the initial application filing approval date of the Township Zoning Officer, the Municipalities Planning Code (MPC) timetable for plan review processing will then automatically begin, unless waived by the applicant.

#### **Review Process:**

Upon receipt of the Township Engineer's review letter, it is recommended that the applicant conference with Township staff and/or the Township Engineer prior to the scheduled Planning Commission meeting when the Planning Commission would consider the application for a recommendation. During the conference, if it is determined that the application will require substantial revisions for compliance with Township ordinances, the Township staff and/or engineer will advise the applicant of an appropriate course of action for revising the application. An extension/waiver to the 90-day limit (attached to SALDO Application), as set forth by the MPC, may be necessary to allow sufficient time for the applicant to make necessary plan revisions and for Township staff and its engineer to review the revised application.

All revised applications must be resubmitted directly to the Township Zoning Officer. It shall be the responsibility of the applicant to submit the same number of revised plans to the Township as with the original application.

#### **Recommendations and Plan Decisions:**

The applicant is responsible for posting the property, notifying property owners within 1000ft (or adjoining owners for minor plans), and submitting an affidavit of mailing & the mailings content to the Township five days prior to the plans appearing before the Township Planning Commission (§ 22-401.8 General Procedures).

The Planning Commission in its review shall consider the comments, if submitted, of the Bucks County Planning Commission, Fire Marshal, Bucks County Conservation District and Township professional staff and consultants. At their anticipated monthly meeting, the Planning Commission may do any of the following:

- Recommend that the applicant address plan deficiencies or unresolved planning issues with Township staff.
- Recommend to the Board of Supervisors that the plan be approved with conditions and specifically list such conditions.
- Recommend to the Board of Supervisors that the plan be denied and specifically list the reasons for such a recommendation.

The Board of Supervisors shall take official action on all applications, after receiving the Township engineer's report and the recommendation by the Planning Commission and within the 90-day time limit, unless the applicant grants a time extension/waiver. The action of the Board of Supervisors shall be in writing and communicated to the applicant by mail within five (5) days following the date of the official action.



#### **Record Plans:**

Final record plans to be recorded must be submitted *directly to the Township Engineer* for review and execution prior to approval by the Board of Supervisors. The plans must include four (4) paper copies of the record plan(s). All record plans must bear the original seal and signature and title of the professional engineer or land surveyor responsible for preparation of the plans and bear the notarized signature of the land owner(s). At the time of record plan submission, five (5) complete paper sets of construction drawings should be submitted to the Township Engineer to be stamped approved for construction.

No final record plans shall be recorded for any project unless all of the following have been satisfied:

- All conditions of final approval have been met (including water and sewer approvals/agreement) and approved by Township Solicitor and Engineer.
- All required outside agency approvals/permits have been obtained.
- All Township legal, engineering, planning and administrative costs relative to the plan submission have been paid in full.
- The required financial security and developer's agreements have been executed and the construction escrow has been posted with the Township.

The Township Solicitor will record all final record plans at the Bucks County Courthouse. The Township will notify the applicant when the copies of the recorded plans are available to be picked up at the Township Zoning Office.

#### **Contract for Professional Services Agreement and Professional Escrows:**

The applicant shall be responsible for maintaining the original escrow balance relative to the escrow account established with the Township for the payment of Township incurred costs and fees on behalf of the applicant per the Contract for Professional Services Agreement. The Township shall forward an invoice to the applicant setting forth the amount deducted from said escrow account for payment of costs and fees. Within fourteen (14) days of the date of the invoice, the applicant shall remit the amount of the invoice to the Township, thereby bringing the balance of the escrow back to the original amount. In the event the applicant's escrow account balance falls below the original amount, the Township may direct its professionals and consultants to cease work on the applicant's submission until such time as the escrow account balance is restored to its original amount.

Any applicant who desires to meet with the Township's professionals or consultants prior to submitting a plan application shall execute a Contract for Professional Services Agreement with the Township and post an escrow amount to be determined therein to cover any costs incurred by the Township associated with the aforementioned consultations.

The applicant should also be advised that the Township has enacted a "Disclosure Ordinance No. 2001-09-01" which mandates that the seller of any lot divulge certain information to the purchaser of the lot prior to settlement.

\*These guidelines may be amended from time to time, as needed.



#### NEW BRITAIN TOWNSHIP 90-DAY REVIEW PERIOD WAIVER

I/We, the applicant, understand that the time necessary for adequate review of this application and plans for compliance with current Zoning and Subdivision & Land Development Ordinances, including reviews of any revised plans, may exceed the 90-day review period stipulated under the Municipalities Planning Code.

In recognition of the above, I/we hereby waive the 90-day review period, with the understanding that I/we may revoke this waiver at any time in the future, upon 30-day written notice to the New Britain Township Zoning Officer.

Name of Subdivision/Land Development:	Edna J. Bialobrzes	ski	
Tax Map Parcel Number(s):	26-003-026		
Signature of Applicant: Atimas. B. L.	alobyesi	Date:	4/24/2025
Signature of Applicant:		Date:	-



THIS DEED, made this

6 day of Warel , 2004

**BETWEEN** EDNA BIALOBRZESKI, Executrix of the Estate of Edna E. Bialobrzeski, party of the first part,

and EDNA J. BIALOBRZESKI, singlewoman, of the County of Bucks and Commonwealth of Pennsylvania, party of the second part:

WITNESSETH, That the said party of the first part, for and in consideration of the sum of ONE (\$1.00) DOLLAR and other good and valuable consideration, lawful money of the United States of America, well and truly paid by the said party of the second part to the said party of the first part, at and before the ensealing and delivery of these presents, the receipt whereof is hereby acknowledged, has granted, bargained, sold, aliened, enfeoffed, released, conveyed and confirmed and by these presents does grant, bargain, sell, alien, enfeoff, release, convey and confirm unto the said party of the second part, her heirs and assigns.

ALL THAT CERTAIN lot or piece of ground SITUATE in the Township of New Britain, County of Bucks and Commonwealth of Pennsylvania, described according to a survey and plan thereof, made the 27<sup>th</sup> day of December, A. D., 1954, by Russell S. Lyman, registered Professional Engineer, Huntingdon Valley, Pennsylvania, as follows, to wit:

BEGINNING at a point in the center line of School House Lane (33 feet wide) at a corner of land now or late of John Moore; thence extending along the center line of said School House Lane North Fifty-two Degrees,

Thirty Minutes West Four hundred and fourteen and thirty-five onehundredths feet to a point; thence extending along the property line of land now or late of Adam McLean, of which this was a part, crossing the Northeasterly side of said School House Lane North thirty-nine Degrees, Thirty Minutes East One Thousand twenty-nine and forty one-hundredths feet to a point; thence along the line of land now or late of Jesse Moore South Fifty-two Degrees, Thirty-three Minutes, Forty Seconds East Six hundred and forty-five and fifty-seven one-hundredths feet to a point; thence along the line of land now or late of John Moore the three following courses and distances: (1) South Sixty-three Degrees, Twelve Minutes, Fifty Seconds West Five hundred and fifty and eighty-two one-hundredths feet to an iron pipe; thence (2) South Twenty-eight Degrees, Twenty-three Minutes, Forty Seconds East Two hundred and forty-seven and thirty-nine one-hundredths feet to an iron pipe; thence (3) South Sixty-seven Degrees, Fifty-six Minutes, Twenty Seconds West Five hundred and one and twentynine one-hundredths feet recrossing the Northeasterly side of School House Lane to the point and place of beginning.

**CONTAINING 12. 632 Acres.** 

**BUCKS COUNTY TAX PARCEL NO. 26-003-026.** 

BEING THE SAME PREMISES which Adam McLean and Lulu C. McLean, his wife, by Deed of Indenture bearing date June 6, 1955 and recorded in the Office for the Recording of Deeds in and for the County of Bucks in Deed Book 1238 page 0124 &c., granted and conveyed unto Alex Bialobrzeski and Edna E. Bialobrzeski, his wife, as tenants by the entireties.

AND THE SAID Alex Bialobrzeski died on September 18, 1997, whereupon by virtue of the laws of the Commonwealth of Pennsylvania relating to tenancy by the entireties, Edna E. Bialobrzeski, as surviving spouse, became seised of the whole.

> nt-15-04 11120C004 TAX EXEMPT \$0.00

\$0.00

AND THE SAID Edna E. Bialobrzeski, being seised of the property described herein, died on July 4, 2003, leaving a Last Will and Testament dated May 6, 1983; which Last Will and Testament was probated with the Register of Wills of Bucks County, Pennsylvania under File No. 9-2003-1357.

UNDER Item IV of the Last Will and Testament, the real property was specifically devised unto Edna Bialobrzeski.

THIS conveyance is excluded from realty transfer tax under 72 P.S. Section 8102.

TOGETHER with all and singular the buildings and improvements, woods, ways, rights, liberties, privileges, hereditaments and appurtenances, to the same belonging, or in any wise appertaining, and reversion and reversions; remainder and remainders, rents, issues, and profits thereof, and of every part and parcel thereof:

AND ALSO, all the estate, right, title, interest, property, possession, claim and demand whatsoever, both in law and equity, of the said party of the first part, of, in, and to the said premises, with the appurtenances:

TO HAVE AND TO HOLD the said premises, with all and singular the appurtenances unto the said party of the second part, her heirs and assigns forever.

AND the said party of the first part, for herself, her heirs, executors, and administrators does by these presents, covenant, grant, and agree, to and with the said party of the second part, her heirs and assigns forever, that she the said party of the first part and her heirs, all and singular the hereditaments and premises herein above described and granted, or mentioned and intended so to be, with the appurtenances, unto the said party of the second part, her heirs and assigns, against her the said party of the first part and her heirs, and against all and every other person or persons, whomsoever lawfully claiming or to claim the same or any part

thereof, by, from or under them, SHALL and WILL BY THESE PRESENTS WARRANT and forever DEFEND.

IN WITNESS WHEREOF, the said party of the first part to these presents has hereunto set her hand and seal.

Signed, Sealed and Delivered

EDNA BIALOBRZESKI

Executrix of the Estate of Edna E. Bialobrzeski

**COMMONWEALTH OF PENNSYLVANIA** 

COUNTY OF BUCKS

On this (day of Man, 2004, before me, the undersigned officer, personally appeared EDNA BIALOBRZESKI, known to me (or satisfactorily proven) to be the person whose name is subscribed to the within instrument and acknowledged that she executed the same for the purposes therein contained.

IN WITNESS WHEREOF, I hereunto set my hand and official seal.

**Notary Public** 

I hereby certify that the Address of the Grantee is: 65 Newville Road

Chalfont, PA 18914

NEW BRITAIN TOWNSHIP DOES NOT CUNFIRM
THE ACCURACY OF THE METES AND BOUNDS

Deed Registration

New Britain Township Signature

Oate.

nuco

Commonwealth Of Pennsylvania

Notarial Seal Stephen A. Shelly, Notary Public Quakertown Boro, Bucks County My Commission Expires July 14, 2007

Member, Pennsylvania Association Of Notaries



B.C.B.O.A Registry

BK3882PG0032



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Serving Business, Municipalities & Industry Since 1958

Building engineering; construction quality control; environmental engineering, exploration, testing and inspection; forensic engineering; individual sewage disposal design; land development engineering; landscape architecture; municipal engineering; structural engineering; surveying

Richard S. Cowan, PE (1910-1997)
James R. Leister, PE/PLS (1936-2006)
William D. Kee, PE (Retired)
Johann F. Szautner, PE/PLS (Retired)
Todd R. Myers, PLS (Semiretired)
Scott P. McMackin, PE
Charles R. Tomko, PE

COWAN ASSOCIATES, INC.

Michael R. Smith, PE Wayne V. Doyle, PE

To:	New Britain Township		Date:	April 25, 2025	
	207 Park Ave.		Job No.:	71864.01	
	Chalfont, PA 18914		Re:	Edna J. Bialobrzeski	
				Minor Subdivision	
				Parcel ID 26-003-026	
Tran	smitted to you by emailx	by n	nail	by messenger, are the following:	

Copies	Date	No.	Description	
9	4/9/2025	SP-1 to SP-2	Plan set entitled "Edna J. Bialobrzeski" (full size)	
5	4/9/2025	SP-1 to SP-2	Plan set entitled "Edna J. Bialobrzeski" (11x17)	
2	4/25/2025	-	New Britain Township Subdivision and Land	
			Development Application	
2	4/25/2025	-	Waiver Request Letter	
1	=	-	Deed for Subject Property	
1	4/22/2025	3050	\$600 Application Fee	
1	4/22/2025	3051	\$10,000 Escrow Fee	
	_			
	_			

Comments: Please review at your earliest convenience and place us on the next available Planning Commission Meeting agenda.

Very truly yours, COWAN ASSOCIATES, INC.

Scott P. McMackin, P.E. spm@cowanassociates.com



#### Providing a full range of Engineering & Surveying Services

Serving Business, Municipalities & Industry Since 1958

April 22, 2025

New Britain Township Board of Supervisors New Britain Township Planning Commission 207 Park Avenue Chalfont, PA 18914 Richard S. Cowan, PE (1910-1997)
James R. Leister, PE/PLS (1936-2006)
William D. Kee, PE (Retired)
Johann F. Szautner, PE/PLS (Retired)
Todd R. Myers, PLS (Semiretired)
Scott P. McMackin, PE
Charles R. Tomko, PE
Michael R. Smith, PE
Wayne V. Doyle, PE

Subject: Waiver Requests

Edna J. Bialobrzeski Minor Subdivision

65 Newville Road

New Britain Township, Bucks County

CAI 71864.01

Dear Board and Planning Commission Members:

Please accept the following waiver requests for the Edna J. Bialobrzeski Minor Subdivision:

1. <u>SALDO 22-703.4.A.(4)</u> regarding depth to width ratio.

Lot is currently deeper than wide and meets other requirements.

2. <u>SALDO 22-706.1</u> regarding curbing along streets.

The project is a minor subdivision, and no curbing exists on the road.

3. SALDO 22-706.2 regarding sidewalks along streets.

The project is a minor subdivision, and no curbing exists on the road.

SALDO 22-711 regarding grading and E&S.

Request for deferral to show proposed improvements on Lot 2 for buyer.

5. SALDO 22-712 regarding stormwater.

Request for deferral to show proposed improvements on Lot 2 for buyer.

SALDO 22-713 regarding landscaping.

Request for deferral to show proposed improvements on Lot 2 for buyer.

Given the minor nature of the project, the wavers are requested.

Very truly yours,

COWAN ASSOCIATES, INC.

Scott P. McMackin, P.E.

SPM:aew

V:\PROJECTS\71864.01 Bialobrzeski Minor Subdivision\Correspondence\Waiver Request 4-25-25.docx



May 19, 2023

File No. 2500567

Doreen Curtin, Zoning Officer New Britain Township 207 Park Avenue Chalfont, PA 18914

Reference: Celek Chalfont, LLC – Conditional Use Review 1

241 W. Butler Avenue TMP #26-013-032

#### Dear Doreen:

Pursuant to your request, Gilmore & Associates, Inc. has reviewed the Conditional Use Application for the above-referenced project which includes the following:

#### I. Submission

Application for Conditional Use Hearing dated June 2, 2025 including Conditional Use Plans by Howell Engineering, dated April 16, 2025

#### II. General Information

The Applicant requests a Conditional Use for an accessory use, Use L2 Outdoor Storage or Display proposed at 241 West Butler Avenue (S.R. 4202) within the Chalfont Village Shopping Center. The 7.957-acres site is within the New Britain Township's C-1 Commercial Zoning District and Butler Corridor Overlay District.

Based on the provided plans, the Applicant proposes to designate areas for two fenced outdoor displays (7,000 sf in the front of the building and 423 sf in the rear), sidewalk display areas along the front of the building (totaling approximately 1,500 sf), and a permanent equipment display area along the right side of the building (2,585 sf) associated with a future retail use. The total outdoor storage area would be approximately 11,500 sf based on our estimates. Other physical improvements include relocating and adding parking spaces, installing a knee wall, storm sewer modifications, and a new pylon sign. The Applicant proposes to increase the number of spaces from 211 spaces to 226 spaces with no increase to the area of building or impervious surfaces. The area of disturbance is all within impervious areas.

#### III. Review Comments

In accordance with §27-3008.b.1 of the Township's Zoning Ordinance, the Board of Supervisors shall grant a conditional use only if it finds adequate evidence that any proposed development submitted will meet the following general requirements as well as any specific requirements and standards listed [in this section] for the proposed use.

- In accordance with the Township Comprehensive Plan;
- In the best interests of the Township, the convenience of the community, the public welfare;
- Suitable for the property in question, and designed, constructed, operated and maintained so as to be
  in harmony with and appropriate in appearance to the existing or intended character of the general
  vicinity;

65 East Butler Avenue | Suite 100 | New Britain, PA 18901 | Phone: 215-345-4330 | Fax: 215-345-8606

- In conformance with all applicable requirements of this chapter and all Township ordinances;
- Suitable in terms of effect on highway traffic and safety with adequate access arrangements to protect streets from undue congestion and hazard;
- In accordance with sound standards of subdivision and land development practice where applicable;
   and
- In accordance with the specific standards and criteria of this chapter.

We offer the following comments regarding applicable Zoning Ordinance requirements as required for Conditional Uses:

- §27-305.L.L2. Outside storage areas shall be shielded from view from all public streets. The plans show the outdoor storage areas screened with 8' chain-link fence with privacy slats, while a 3'4" CMU knee wall is proposed parallel to the front façade to shield the sidewalk display area. We recommend potential renderings of these features be presented.
- 2. §27-2118. Within the Butler Avenue Corridor Overlay District (COD), where work on a nonresidential building requires a zoning or building permit, such work shall conform with all of the applicable requirements set forth in § 22-722 of Chapter 22, Subdivision and Land Development, of the Township Code, unless waived by the Board of Supervisors at a public meeting.
  - a. §22-722.3 Where no subdivision or land development approval is required but the proposed work is subject to a building or zoning permit, such as, but not limited to, facade changes/improvements, signage changes, or changes in use, the proposed design shall be reviewed by the Township as part of the permit review process and, at the request of the Township, by the New Britain Township Planning Commission, to ensure such proposed changes comply with the applicable provisions of this section. Design review consists of a systematic assessment of the three-dimensional configuration, design, and materials to be used to ensure that the proposed development meets the goals for the Corridor Overlay District. All applicants undergoing a design review shall submit, in addition to all other required information, the information and drawings listed below:
    - 1) Description of use or uses proposed;
      - We recommend the future building use be documented on the plans, the number of existing and proposed units be clarified, and the square footage of existing building space to be used by the proposed use clarified as well.
    - 2) Architectural drawings of proposed building(s) showing all sides of the proposed building(s), with information on building materials and colors; and
    - 3) Photographs of the original site on which the proposed development will occur, prior to any changes being made, showing surrounding areas, surrounding zoning districts, and how the new development will relate to the existing surroundings.
      - We recommend the above information be presented to the Planning Commission and Board of Supervisors for consideration and incorporate the building materials such as stone and brick similar to other recent projects within the COD.
  - b. §22-722.4.C.(7) Along every public street, a mixture of street trees shall be planted 50 feet on center between the curb and sidewalk. The street trees shall be planted in a proper location for the chosen species (i.e., the level of sunlight, type of soils, size of planting area, distances to buildings and utilities, etc.). For example, where there may be conflict with overhead utility wires, the shorter tree species should be used. We recommend street trees be planted between the sidewalk and parking lot along Butler Avenue due to overhead utilities and provided there are no conflicts with underground utilities.
  - c. §22-722.4.D.(2) –The parking layout shall provide for pedestrian circulation throughout the parking area. This pedestrian circulation system shall be illustrated on the site plan and shall connect to walkways within the right-of-way.
  - d. §22-722.4.D.(6) Bicycle parking shall be provided in an amount equal to 5% of the minimum required off-street parking for vehicles, or a minimum of two spaces, whichever is greater.

- e. §22-722.4.D.(7) The area between the cartway and the parking areas shall be devoted to required street trees, buffering, landscaping, and sidewalks. Where parking areas are located or are proposed to be located between a street and a principal building, a buffer fence shall be installed within five feet of the ultimate right-of-way line of the street between this parking area and the street. This buffer fence, along with associated piers and hedge, shall be constructed and installed in accordance with §22-722.4.D.(7).
- f. §22-722.4.E. & 27-2904.f. All parking areas, walkways, and passages shall be adequately illuminated with a lighting system designed to complement the general areas and the site of the proposed development and to prevent any off-site glare and spillover light onto adjacent properties. We recommend a lighting plan be developed and provided for review for the existing and any proposed lighting. If supplemental lighting is required to comply with the minimum and maximum lighting candles specified in the Ordinance, we recommend it be installed as part of this project. Additionally, in order to establish a consistent design scheme along the Corridor Overlay District, all freestanding fixture types shall be constructed of metal, with a black finish. The luminaire shall be in the style illustrated [in this section] from Philips [CityPost LED Post Top (TX1)] and shall be placed upon a P3165 pole from Philips, or the Board of Supervisors may approve an appropriate alternative.
- 3. §27-2401.a.3 To meet the natural resource protection standards, calculations shall be provided which indicate the area of the site with natural resources and the area of natural resources that would be disturbed or encroached upon.
- 4. §27-2800 We recommend the condition of the existing property line buffer be assessed, maintained and supplemental plantings provided if there are gaps along the property lines shared with residential properties.
- 5. §27-2901.J. The plans shall document the total number of parking spaces required for the site including the proposed use in addition to the existing uses.
- 6. §27-2904.h. A maximum of 15 consecutive and contiguous parking spaces in a row shall be allowed without being separated by a landscaped area. Eighteen spaces are proposed behind the building on existing asphalt.
- 7. §27-2904.j.2.(d) The plans shall demonstrate that the proposed accessible parking spaces and accessible walkway are a maximum 2% in all directions.
- 8. §27-3008.b.5. & 27-2500 The proposed use shall be suitable in terms of effect on highway traffic and safety with adequate access arrangements to protect streets from undue congestion and hazard. A traffic impact study is required as a prerequisite for a zoning permit for any use expected to generate greater than 100 new trips inbound to the site or out bound from the site in site peak hour traffic, or 1,000 trips per day. We recommend the number of anticipated new trips and daily trips be presented and a traffic impact study provided, if required, in accordance with §27-2501.
- 9. §27-3008.e. The applicant is required to submit an Environmental Impact Statement (EIS) in support of the conditional use application as specified in Part 2, § 27-201, Appendix A. We note that Sheet 1 of the plan set references a title report which notes that there is an environmental covenant with the property as well as a history of hazardous material. We recommend that the status of the environmental covenant and hazardous material be discussed in the EIS as well as the future intent for existing monitoring wells to remain.

If you have any questions regarding the above, please contact this office.

Sincerely.

Craig D. Kennard, P.E. Executive Vice President Gilmore & Associates, Inc.

**Township Engineers** 

#### CDK/jm

cc: Daniel Fox, Township Manager
Alexandria Mullin, Assistant to the Manager
Randy Teschner, Code Enforcement Officer/Fire Marshall
Scott Hobert, Esq., Flager & Associates, PC, Township Solicitors
Matthew W. Indorante, MSP Development, LLC
Julie L. VonSprecklesen, Esq., Eastburn and Gray
Janene Marchand, P.E., Gilmore & Associates, Inc.



#### Matthew Indorante, Esquire

PO Box 2280 Cranberry Twp, PA 16066 MSP Development, LLC Sr. Director of Real Estate Development Cell: (724)-469-0569 Matthew@mspdevelopment.com

April 23, 2025

#### VIA EMAIL & FEDEX-DELIVERY

Dan Fox Township Manager New Britain Township 207 Park Avenue Chalfont, PA 18914

Re: Conditional Use Hearing Application of Celek Chalfont, LLC Properties: 241 W Butler Ave

#### Dear Dan:

On behalf of Celek Chalfont, LLC, enclosed are the following documents and supporting materials constituting a conditional use hearing application:

- 1. An original and seven (7) copies of the completed and executed New Britain Township Conditional Use Hearing Application;
- 2. Seven (7) 11"x 17" size copies of the conditional use plans consisting of two sheets entitled "Existing Conditions & Demolition Plan" and "Site Improvements Plan" prepared by Howell Engineering, dated April 16, 2025;
- 3. One (1) executed copy of the New Britain Township Contract for Professional Services;

Dan Fox April 22, 2025 Page 2

- 4. One (1) copy of the deed dated February 28, 2025, between Chalfont Plaza Associates, L.P. ("Grantor") and Celek Chalfont, LLC ("Grantee") recorded on March 7, 2025 with the Bucks County Recorder of Deeds, instrument number 2025009563
- 5. the application fee in the amount of \$2,500.00 check made payable to New Britain Township; and
- 6. the escrow fee in the amount of \$2,500.00 check made payable to New Britain Township.

We request a hearing on this application.

#### Please:

- confirm the date this application will be scheduled for a hearing before the Board of Supervisors;
- provide me with copies of any correspondence, reviews, and comments sent or received by the Township with regard to this application; and
- provide me with advance notice of any meeting of a Township board, commission, agency, or committee which intends to discuss or consider this application.

Thank you for your consideration.

Very truly yours,

Matthew W Indorante
Matthew Indorante

#### **Enclosures**

cc: Julie L. Von Spreckelsen (w/ enclosures)
Nick Urbanowicz (w/o enclosures)
Denny Howell III (w/o enclosures)

#### **BUCKS COUNTY RECORDER OF DEEDS**

55 East Court Street
Doylestown, Pennsylvania 18901
(215) 348-6209

Instrument Number - 2025009563 Recorded On 3/7/2025 At 2:12:51 PM

\* Total Pages - 6

- \* Instrument Type DEED
  - Invoice Number 1419374 User ATV
- \* Grantor CHALFONT PLAZA ASSOCIATES L P
- \* Grantee CELEK CHALFONT LLC
- \* Customer SIMPLIFILE LC E-RECORDING
- \* FEES

STATE TRANSFER TAX \$47,000.00
RECORDING FEES \$85.75
CENTRAL BUCKS \$23,500.00
SCHOOL DISTRICT REALTY TAX
NEW BRITAIN \$23,500.00
TOWNSHIP
TOTAL PAID \$94,085.75

Bucks County UPI Certification On March 6, 2025 By SHY

This is a certification page

#### DO NOT DETACH

This page is now part of this legal document.

RETURN DOCUMENT TO: FIRST AMERICAN TITLE INSURANCE COMPANY NCS NEW YORK NATIONAL 666 3RD AVE FL 5 NEW YORK, NY 10017-4126

I hereby CERTIFY that this document is recorded in the Recorder of Deeds Office of Bucks County, Pennsylvania.



Daniel J. McPhillips Recorder of Deeds

David J. Millips

\* - Information denoted by an asterisk may change during the verification process and may not be reflected on this page.

1CF1B9

CERTIFIED PROPERTY IDENTIFICATION NUMBERS
26-013-032- - N BRITAIN TWP
CERTIFIED 03/06/2025 BY SHY

#### Prepared by and record and return to:

Kennedy Kelly LLP One West 3rd Street, Suite 213 Media, PA 19063 Attn: James I. Kennedy

Tax Parcel No. 26-013-032

THIS INDENTURE, made the day of February, 2025 and effective as of the day of February, 2025.

BETWEEN CHALFONT PLAZA ASSOCIATES, L.P., a Delaware limited partnership, having an address at c/o Brixmor Property Group, Inc., 200 Ridge Pike, Suite 100, Conshohocken, Pennsylvania 19428 (hereinafter called the "Grantor"), of the one part, and CELEK CHALFONT, LLC, a Pennsylvania limited liability company, having an address at One West 3rd Street, Suite 108, Media, Pennsylvania 19063 (hereinafter called the "Grantee"), of the other part,

WITNESSETH, that the said Grantor for and in consideration of the sum of Four Million Seven Hundred Thousand and 00/100 Dollars (\$4,700,000.00) lawful money of the United States of America, unto it well and truly paid by the said Grantee at or before the sealing and delivery hereof, the receipt whereof is hereby acknowledged, has granted and conveyed, bargained and sold, released and confirmed, and by these presents does grant and convey, bargain and sell, release and confirm unto the said Grantee,

ALL THAT CERTAIN real property which is more particularly described on Exhibit A attached hereto and made a part hereof.

TOGETHER WITH all and singular the buildings and improvements, ways, streets, alleys, passages, waters, water-courses, rights, liberties, privileges, hereditaments and appurtenances, whatsoever unto the hereby granted premises belonging, or in anywise appertaining, and the reversions and remainders, rents, issues, and profits thereof; and all the estate, right, title, interest, property, claim and demand whatsoever of it, the said Grantor, as well at law as in equity, of, in and to the same.

TO HAVE AND TO HOLD the said lot or piece of ground above described, with the buildings and improvements thereon erected, hereditaments and premises hereby granted, or mentioned and intended so to be, with the appurtenances, unto the said Grantee, its successors

and assigns, to and for the only proper use and behoof of the said Grantee, its successors and assigns, forever.

**SUBJECT TO** (i) restrictions, easements, covenants and other matters of record, if any, to the extent same are in force and effect, (ii) such discrepancies, conflicts in boundary lines, shortages in area, or any other facts which a current survey of the premises conveyed hereby would disclose, (iii) the right of tenants under recorded and unrecorded leases and (iv) real estate taxes and assessment not yet due and payable.

**GRANTEE** is hereby notified that the real property described herein is subject to the activity and use limitations described in that certain Environmental Covenant dated April 9, 2018, recorded on May 9,2018, in the Recorder of Deeds Office of Bucks County, Pennsylvania as Instrument Number 2018024111.

AND the said Grantor does covenant, promise and agree, to and with the said Grantee, its successors and assigns, by these presents, that the said Grantor shall and will WARRANT and forever DEFEND title to property conveyed herein against any person or party lawfully claiming by, under or through Grantor, except as hereinbefore mentioned.

[NO FURTHER TEXT ON THIS PAGE]

IN WITNESS WHEREOF, the party of the first part has caused these presents to be executed by its duly authorized officer. This Indenture is dated the day and year first above written.

#### **GRANTOR:**

CHALFONT PLAZA ASSOCIATES, L.P., a Delaware limited partnership

By: Chalfont Plaza LVC, a Delaware limited liability company, its general partner

By: Name: Steven Stegel

Title: Executive Vice President

STATE OF NEW YORK ) SS: COUNTY OF NEW YORK )

On this, the 15th day of February, 2025, before me, a Notary Public for the State of New York, the undersigned officer, personally appeared Steven Siegel who acknowledged himself to be the Executive Vice President of Chalfont Plaza LLC, the general partner of Chalfont Plaza Associates, L.P. and that he as such officer, being authorized to do so, executed the foregoing instrument for the purposes therein contained by signing the name of the limited liability company as the general partner of said limited partnership, by himself as such officer.

In Witness Whereof, I hereunto set my hand and official seal.

Notary/Public

My commission expires: Nov. 4, 2027

[Notarial Seal]

S MALIK BANKS
Notary Public, State of New York
Reg. No. 01BA6399959
Qualified in New York County
Commission Expires November 04, 2027

The precise residence and the complete post office address of the above named Grantee is:

One West 3<sup>rd</sup> Steet Media, Pennsylvania 19063

On behalf of the Grantee

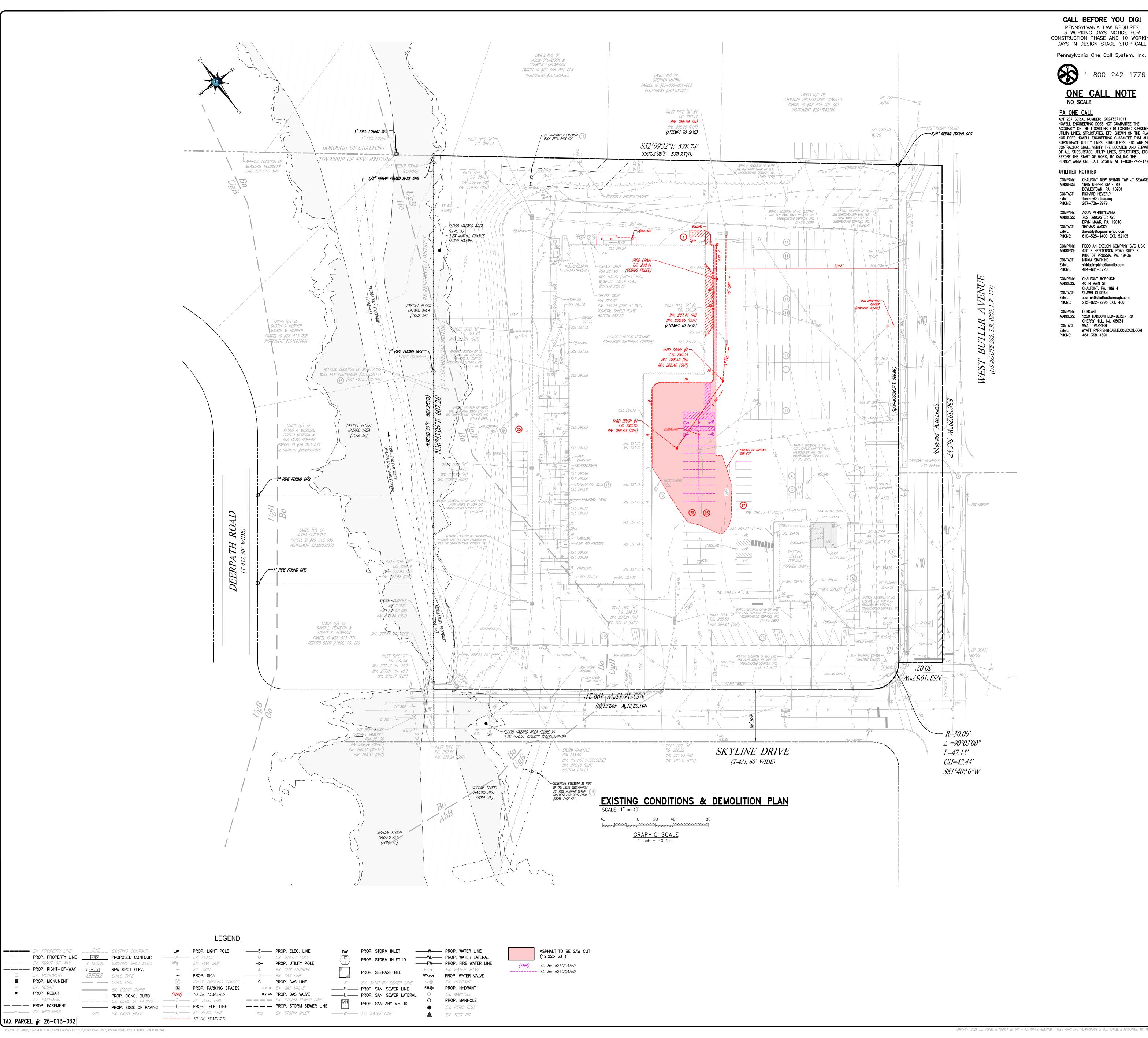
#### **EXHIBIT A**

#### LEGAL DESCRIPTION

ALL THAT CERTAIN tract or parcel of land with the buildings or improvements thereon erected, situate in the Township of New Britain, County of Bucks, and Commonwealth of Pennsylvania, as shown on the As Built Survey of Chalfont Village Shopping Center, dated August 8, 1973 and last revised December 30, 1974, made by Richard S. Cowan & Associates, Inc., Consulting Engineers, bounded and described as follows, to wit:

BEGINNING at a point in the centerline of U. S. Route 202, said point being located North 38 degrees, 47 minutes, 01 seconds East, a distance of 60.08 feet measured along the centerline of said U.S. Route 202 from its intersection with the centerline of Skyline Drive (60 fect wide); THENCE (1) leaving the centerline of said U.S. Route 202, North 51 degrees, 12 minutes, 59 seconds West, a distance of 50.01 feet to a point of curvature on the ultimate right of way of said U.S. Route 202: THENCE (2) along an arc of a curve to the right in a Morthwesterly direction having an arc length of 47.15 feet and a radius of 30 feet to a point of tangency on the Northeasterly side of Skyline Drive; THENCE (3) along the said side of Skyline Drive North 51 degrees, 09 minutes, 21 seconds West, a distance of 499.21 feet to a point, a corner of Lot No. 14, Oxbow Neadows, Section No. 2; THENCS (4) leaving said side of Skyline Drive and passing along Lots No. 14, 15, 16 and 17 of Oxbow Meadows, Section No. 2, North 38 degrees, 50 minutes, 30 seconds East, a distance of 607.26 feet to an iron pin in line of Oxbow Meadows, Section No. 1; THENCE (5) passing along Oxbow Meadows, Section No. 1, South 50 degrees, 02 minutes, 08 seconds East, crossing the ultimate right of way a distance of 578.74 feet to a point on the centerline of the aforementioned U.S. Route 202; THENCE (6) along said centerline South 38 degrees, 47 minutes, 01 seconds West, a distance of 565.89 feet to the place of beginning.

BEING the same premises which Chalfont Associates, L.P., a Delaware limited partnership, by Deed dated September 16, 1999 and recorded September 27, 1999 in Bucks County at Land Record Book 1932 Page 1730, granted and conveyed unto Chalfont Plaza Associates, L.P., a Delaware limited partnership, in fee.



CALL BEFORE YOU DIG! PENNSYLVANIA LAW REQUIRES 3 WORKING DAYS NOTICE FOR CONSTRUCTION PHASE AND 10 WORKING

Pennsylvania One Call System, Inc.

## ONE CALL NOTE

HOWELL ENGINEERING DOES NOT GUARANTEE THE ACCURACY OF THE LOCATIONS FOR EXISTING SUBSURFACE UTILITY LINES, STRUCTURES, ETC. SHOWN ON THE PLANS, NOR DOES HOWELL ENGINEERING GUARANTEE THAT ALL SUBSURFACE UTILITY LINES, STRUCTURES, ETC. ARE SHOWN CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATIONS OF ALL SUBSURFACE UTILITY LINES, STRUCTURES, ETC. BEFORE THE START OF WORK, BY CALLING THE

PENNSYLVANIA ONE CALL SYSTEM AT 1-800-242-1776. COMPANY: CHALFONT NEW BRITAIN TWP JT SEWAGE AUTH

COMPANY: PECO AN EXELON COMPANY C/O USIC ADDRESS: 450 S HENDERSON ROAD SUITE B

ADDRESS: 1250 HADDONFIELD-BERLIN RD WYATT\_PARRISH@CABLE.COMCAST.COM Patriot Place Chalfont Brittany Farms

> LOCATION MAP SCALE: 1" = 2,000'

**GENERAL NOTES** 1. RECORD OWNER/MAILING ADDRESS: CELEK CHALFONT, LLC

P.O. BOX 2280 CRANBERRY TOWNSHIP, PA. 16066

Village

237 W. BUTLER AVE. CHALFONT, PA 18914

3. TAX PARCEL #: 26-013-032

4. SOURCE OF TITLE: INSTRUMENT TYPE - DEED INSTRUMENT NUMBER - 2025009563

5. LOT AREA: 7.306 ACRES

6. BOUNDARY, TOPOGRAPHY AND PHYSICAL IMPROVEMENTS SHOWN ARE FROM FIELD SURVEY BY HOWELL SURVEYING, LLC. PERFORMED ON 11/26/2024 & 11/27/2024.

Trace Park Brittany

Farms-the

7. CONTOURS PLOTTED FROM FIELD RUN TOPOGRAPHIC SURVEY BY HOWELL SURVEYING, L.L.C.. DATUM: NAVD 88 (COMPUTED USING GEOID18) & NAD 83 (2011) (EPOCH:2010.0000) (AS DETERMINED BY GPS OBSERVATION), SITE BENCH = SANITARY AS-BUILT ON SKYLINE DRIVE, ELEVATION= 281.06', CONTOUR

8. UNDERGROUND UTILITIES SHOWN WERE PLOTTED FROM OBSERVABLE EVIDENCE AT THE TIME OF SURVEY AND INFORMATION FROM PLANS SUPPLIED BY UTILITY COMPANIES AS WELL AS MARKED BY A UNDERGROUND UTILITY LOCATOR. NO GUARANTEE IS MADE THAT UNDERGROUND UTILITIES ARE ACCURATELY OR COMPLETELY SHOWN HEREON.

9. THIS PROPERTY IS LOCATED WITHIN FLOOD HAZARD ZONE X, AN AREA OF MINIMAL FLOOD HAZARD PER THE NATIONAL FLOOD INSURANCE RATE MAP FOR BUCKS COUNTY, PA. MAP NO. 42017C0289K, PANEL 289 OF 532, DATED MARCH 21, 2017. FLOOD PLAIN BOUNDARIES SHOWN HEREON WERE SCALED FROM

10. THERE WAS NO EVIDENCE OF ANY EARTH MOVING WORK, BUILDING CONSTRUCTION, OR BUILDING ADDITIONS OBSERVED IN THE PROCESS OF CONDUCTING THE FIELDWORK. 11. PARKING COUNT: 201 REGULAR SPACES

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TOGETHER WITH the easement appurtenant to the Land as set forth in that certain Agreement recorded May 14, 1973 in Deed Book 2085 Page 524. BEING PARCEL NO. 26-013-032 BEING the same premises which Chalfont Associates, L.P., a Delaware limited partnership, by Deed

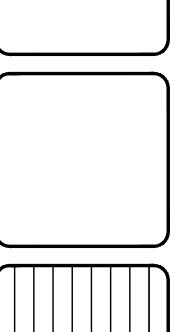
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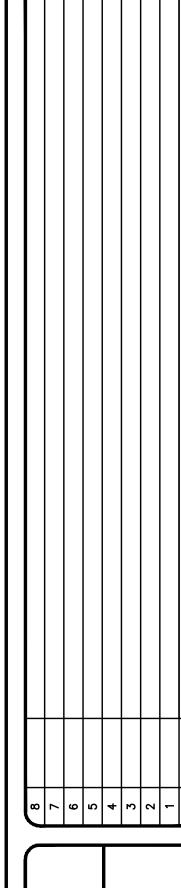
Abb - Abbottstown Silt Loam, 3 to 8 percent slopes BOWMANSVILLE-KNAUERS SILT LOAMS UgB - URBAN LAND-ABBOTTSTOWN COMPLEX

### APPLICANT/EQUITABLE OWNER

CELEK CHALFONT, LLC P.O. BOX 2280 CRANBERRY TOWNSHIP, PA. 16066





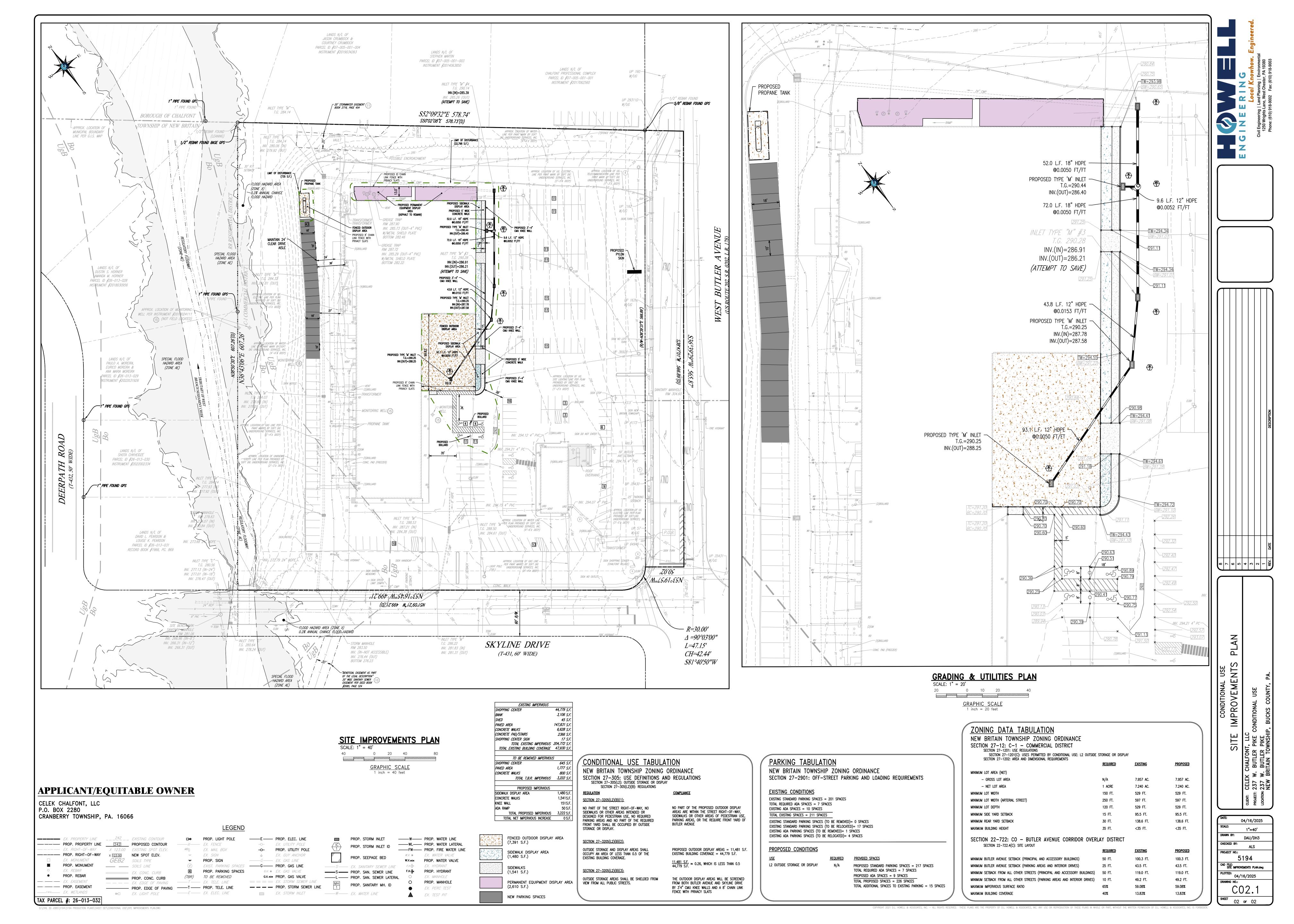


TION N O EXIST CELEK C 237 W. 237 W. NEW BRI

04/16/2025 1"=40' CHECKED BY: CAD FILE: EXISTING CONDITIONS & DEMOLITION PLAN.du PLOTTED: 04/16/2025

C01.1

01 of 02





# **NEW BRITAIN TOWNSHIP**

207 Park Avenue · Chalfont, PA 18914 · Telephone: (215) 822-1391

### CONDITIONAL USE HEARING APPLICATION

leas	e Note:					TOWNSHIP USE ONLY
ie th	a annlin	ant'e roenoneihility t	n complete all	pertinent sections of this form.	Please	Application #:
		the state of the second	THE RESERVE THE PERSON NAMED IN	ou need any assistance.	ricase	Date Filed:
-		Anna Canada para a				Payment:
1.	Date:		4/24/25		7 (1)	Check #:
2.	Applica	nt:				Treceipt #.
	(a)	Name:	Celek Chalfon	t LLC_		
	(b)	Mailing address:	Po Box 2280			
			Cranberry Twp PA			
	(c)	Telephone number:	412-780-8399	7		
	(d)	State whether owner Legal Title	r of legal title, owr	ner of equitable title, or tenant with the	permission of ow	ner of legal title:
3.	Applica	nt's attorney, if any:				
	(a)	Name:	Matthew Indor	rante		
	(b)	Mailing Address:	Po Box 2280			
			Cranberry Twp PA	District Control		
	(c)	Telephone number:	724-469-0569			
	(d)	Fax Number:				
4.	Propert	y;				
	(a)	Present Zoning Use	Classification:	C-1 & Butler Ave Corridor Overlay Distri	t is	
	(b)	Tax Parcel Number:		26-013-032		
	(c)	The state of the s		tersections or prominent features): tersection of W Butler Ave		
5.	Propose	ed use.				
	Пороз	L2 Outdoor storage		Section 27-1201.c & Section 27-305.L. play area, sidewalk display area & trailer	L2	
		& equipment display are	a, as necessary but incl	idental to the Tractor Supply retail store use		
6.	Has any	y previous zoning applie None Known	cations been filed	concerning this property? If yes, spe	cify:	
(We)	herehy re	ertify that the above info	rmation is true ar	nd correct to the best of my (our) know	ledge information	or belief
(110)	noroby oc	and a service above mic	and the state of	in solitoria die bost of thy (out) know	inogo, information	O. DONOI
					11 11-	
					Signature	
Notes:						

(1) One copy of plans (if size 8 1/2" x 11") or seven copies (if larger than size 8 1/2" x 11") must be attached to the application. The plan or plans must be prepared by a professional engineer or surveyor. The plan or plans must contain all information relevant to the application, including but not limited to, the following: the property related to a street, the dimensions and area of the lot, the dimensions and location of existing buildings or improvements, the dimensions and locations of proposed uses, buildings or improvements.

Filing fee, which must accompany this application, and which is not returnable once the application is accepted. (2)Conditional Use Application Fee: \$2,500.00, plus Professional Services Agreement and escrow.

#### NEW BRITAIN TOWNSHIP CONTRACT FOR PROFESSIONAL SERVICES

THISAGREEMENT made this 23d day of April , 2025 by and between New Britain Township, Bucks County, Pennsylvania, with offices located at 207 Park Avenue, Chalfont, PA 18914 (hereinafter "Township") and Celek Chalfont LLC with an address of P.O. Box 2280, Cranberry Township, PA 16066 (hereinafter "Developer").

#### WITNESSETH:

WHEREAS, the Developer is the legal or equitable owner of certain real estate located at or near 241 West Butler Avenue, New Britain Township, Bucks County, Pennsylvania further identified as Bucks County Tax Map Parcel No. 26-013-032 (hereinafter "Property"); and

WHEREAS, the Developer is proceeding to develop, consolidate and/ or construct improvements on the Property (hereinafter "Project"); and

WHEREAS, the Developer has requested the Township provide administrative, engineering and legal services regarding the review and processing of its informal and/ or formal application to the Township related to the development of the Project; and

WHEREAS, the Township is willing to authorize its professional staff to review said plans upon execution of this Agreement and upon deposit of an escrow account in accordance with Township Ordinances and the Township Fee Schedule.

NOW, THEREFORE, the parties agree as follows:

1. The Developer and Township hereby authorize and direct the Township's consultants, including, but not limited to the Township Engineer, the Township Traffic Engineer, the Township Solicitor, Landscape Architect, Lighting Consultant, Planner and any other professional (hereinafter referred to as "Consultants"") to review the request, application and plans and to make such

recommendations and specifications as may be necessary with respect to such plans and to make any and all engineering inspections as required by the Township pursuant to its ordinances or codes which in the Consultant's opinion are required.

- The Developer and Township acknowledge that the Township will incur additional engineering, legal and other costs and fees relating to the development of the subject site and review and approval of Developer's proposed plan or project.
- 3. The Developer shall pay: (a) the Township Engineer, Traffic Engineer, Landscape Architect, Lighting Consultant, Solicitor, Planner, and associated Township administrative charges and fees for review of and/or preparation of any Plans or development proposals and all subsequent inspections, monitoring or testing performed in order to insure compliance with all applicable ordinances of the Township or other rules, regulations or statutes; and (b) legal fees for review by the Township Solicitor of any and all plans, documents, correspondence or other materials and matters or issues related to the Developer's Plan or proposal as well as preparation of documents.

All charges and fees shall be paid by the Developer as required by the Township and in accordance with Paragraph 4 below. It is understood by its executing this Agreement that the Developer specifically accepts the fee schedules currently in effect.

4. The Developer hereby agrees to deposit with the Township the required escrow in an initial amount of Two Thousand Five Hundred Dollars (\$2,500.00) and to increase the escrow deposit upon submission of a land development application to meet the minimum escrow requirement as set forth in the current Township fee schedule as security for the payment of all costs and expenses, charges and fees as set forth in Paragraph 3 above, upon execution of this Agreement, which shall be held in a non-interest-bearing account by the Township.

In the event that the above deposited escrow fund shall fall below the

original deposit, the Developer shall immediately, upon receipt of written notice from the Township or its agent(s), deposit sums with the Township necessary to replenish the account to its original balance. In the event that this is insufficient to pay current Township incurred expenses, Developer agrees to pay the total amount currently due for Township incurred expenses without delay in addition to reestablishing the base escrow account balance. The Township will use its best efforts to advise the Developer of the impending likelihood that its costs have exceeded the required escrow account sums as described above.

Developer and Township agree that upon completion of the proposed development and/or upon completion of Township's review of Developer's plan or proposal, all unused portions of the escrow account as described above shall be returned to the applicant upon written request to the Township Manager and in accordance with the instructions, if any, with said written request.

5. Developer and Township acknowledge that the New Britain Township Subdivision and Land Development Ordinance requires Developer to pay Township's professional services relating to this plan or project and in the event that Developer fails to provide sufficient funds in the above-described revolving escrow account upon fifteen (15) days written notice to the Developer or make the initial deposit payment described above within five (5) days of the date of this Agreement, Developer shall be in default of this Agreement and in violation of the Subdivision and Land Development Ordinance if Developer's plan or permit application constitutes a subdivision or land development as defined by the Municipalities Planning Code.

In the event of Developer's default as described above, the Township may refuse to issue any permit or grant any approval necessary to further improve or develop the subject site until such time as the terms of this Agreement are strictly met by Developer.

6. The Developer and the Township further agree that all fees or costs arising out of this Agreement shall be paid prior to the issuance of any permit, occupancy or otherwise, for the use, improvement or construction of the buildings

as proposed on the Developer's final Plan or project. The Developer agrees and acknowledges that no permit, occupancy or otherwise, shall be issued until all outstanding fees and costs due the Township as of the date of the requested Occupancy Permit have been paid and Developer is not in default under this Agreement.

- 7. The Developer may at any time terminate all further obligations under this Agreement by giving fifteen (15) days written notice to the Township that it does not desire to proceed with the development as set forth on the Plan and upon receipt of such written notice by the Developer to the Township, the Developer shall be liable to the Township for its costs and expenses incurred to the date and time of its receipt of the notice, plus the applicable administrative costs and expenses as outlined in Paragraph 3 above.
- 8. The Developer and the Township further agree that the Township shall have the right and privilege to sue the Developer or then property owner in assumpsit for reimbursement or to lien the property or both in its sole discretion for any expense in excess of the then current balance of funds on deposit with the Township in accordance with this Agreement incurred by the Township by reason of any review, supervision and inspection of Developer's project by its professionals including, but not limited to, the Township Engineer, Traffic Engineer and Solicitor. The Township's election of its remedies under this paragraph shall not constitute a waiver of any other remedies the Township may have.
- 9. The Developer and the Township intend to execute Development and Financial Security Agreements if the project receives a subdivision or land development approval under Township Ordinances. Any such Development and Financial Security Agreements may incorporate or replace the parties agreement and Escrow Fund established under this contract.
- 10. By execution of this Agreement, the Developer acknowledges and agrees that Township employees and professional staff, including Planning Commission members and members of the Board of Supervisors, may enter upon and inspect the property which is the subject of this application in order to

determine compliance with Township Ordinances and to facilitate appropriate planning for the project.

11. This Agreement shall be binding on and inure to the benefit of the successors and assigns of Developer. The Township shall receive thirty (30) days advance written notice from Developer of any proposed assignment of Developer's rights and responsibilities under this Agreement.

In witness whereof, and intending to be legally bound, the parties have caused their signatures to be affixed and have affixed their hand and seals the day and year first above written.

ATTEST:

NEW BRITAIN TOWNSHIP

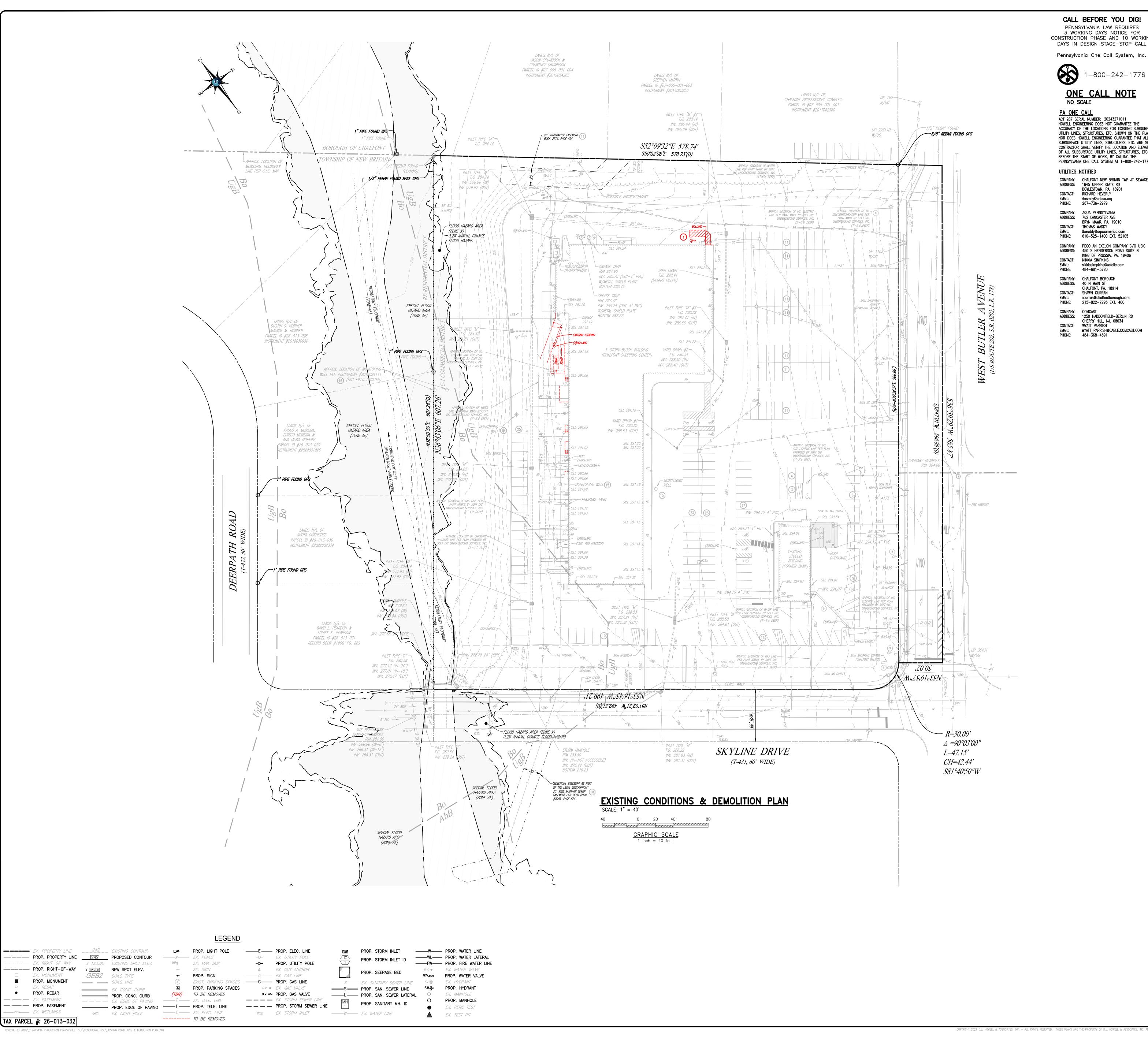
Dan Fox

**Township Manager** 

ATTEST:

CELEK CHALFONT LLC

**Authorized Representative** 



CALL BEFORE YOU DIG! PENNSYLVANIA LAW REQUIRES 3 WORKING DAYS NOTICE FOR CONSTRUCTION PHASE AND 10 WORKING

Pennsylvania One Call System, Inc.

# ONE CALL NOTE

ACT 287 SERIAL NUMBER: 20243271011 HOWELL ENGINEERING DOES NOT GUARANTEE THE ACCURACY OF THE LOCATIONS FOR EXISTING SUBSURFACE UTILITY LINES, STRUCTURES, ETC. SHOWN ON THE PLANS, NOR DOES HOWELL ENGINEERING GUARANTEE THAT ALL SUBSURFACE UTILITY LINES, STRUCTURES, ETC. ARE SHOWN CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATIONS OF ALL SUBSURFACE UTILITY LINES, STRUCTURES, ETC. BEFORE THE START OF WORK, BY CALLING THE

PENNSYLVANIA ONE CALL SYSTEM AT 1-800-242-1776. COMPANY: CHALFONT NEW BRITAIN TWP JT SEWAGE AUTH ADDRESS: 1645 UPPER STATE RD

tbwaddy@aquaamerica.com 610-525-1400 EXT. 52105 COMPANY: PECO AN EXELON COMPANY C/O USIC ADDRESS: 450 S HENDERSON ROAD SUITE B

nikkiasimpkins@usicllc.com

scurran@chalfontborough.con 215-822-7295 EXT. 400

CHERRY HILL, NJ. 08034 WYATT\_PARRISH@CABLE.COMCAST.COM Patriot Place Chalfont Brittany Farms

> LOCATION MAP SCALE: 1" = 2,000'

1. RECORD OWNER/MAILING ADDRESS: CELEK CHALFÓNT, LLC P.O. BOX 2280

Village

CRANBERRY TOWNSHIP, PA. 16066

237 W. BUTLER AVE.

**GENERAL NOTES** 

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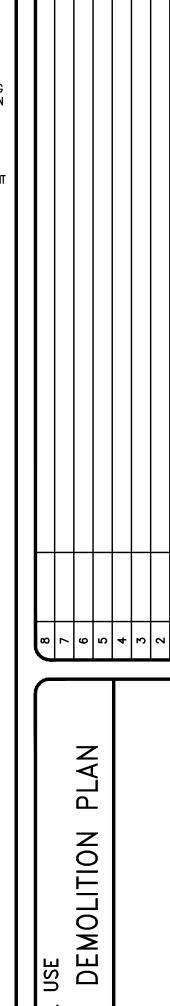
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NOI.

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EXIST CELEK C 237 W. 237 W. NEW BRI

04/16/2025

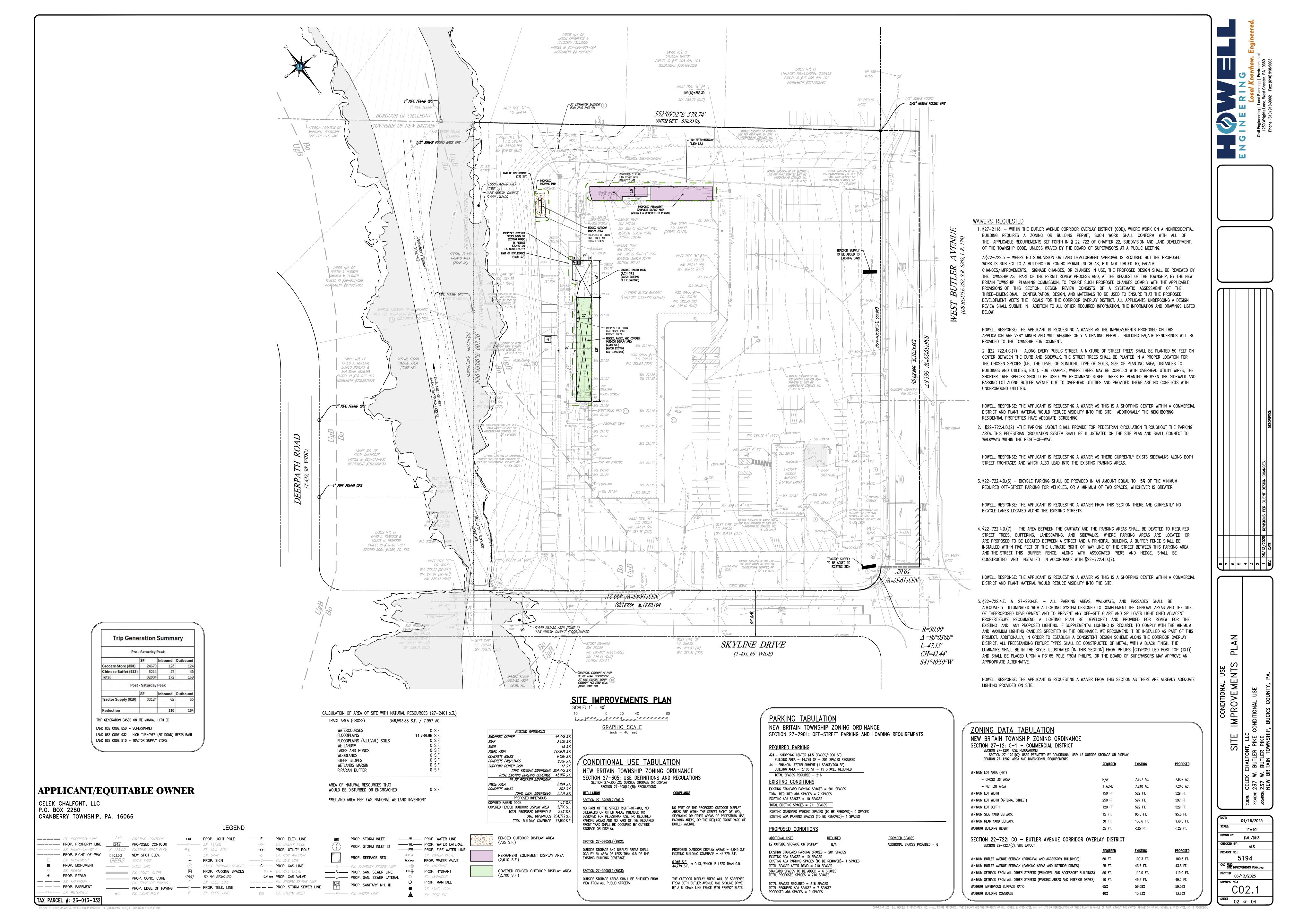
1"=40'

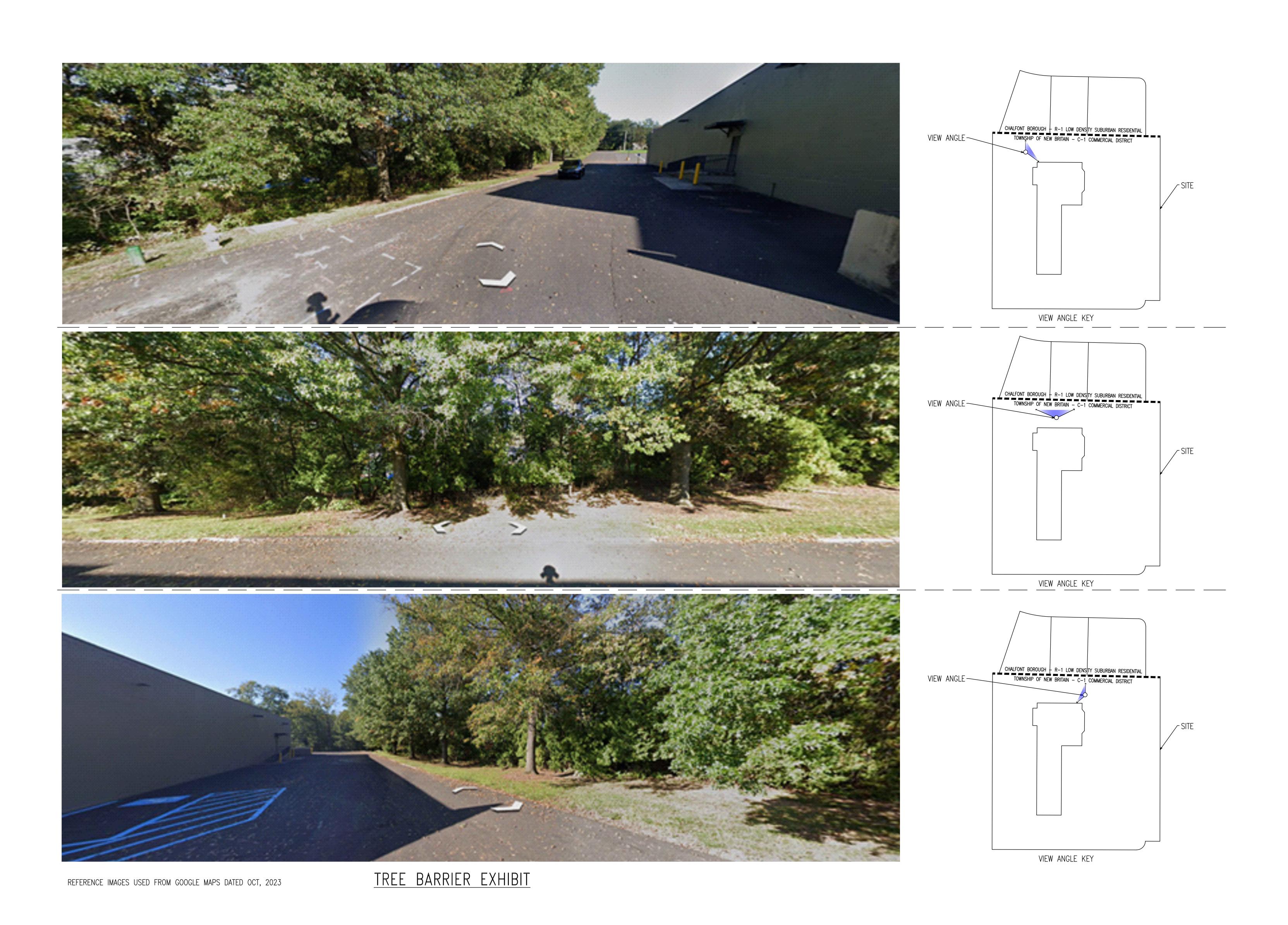
CAD FILE: EXISTING CONDITIONS & DEMOLITION PLAN.du

01 of 04

PLOTTED: 06/13/2025

CHECKED BY:





TREE BARRIER EXHIBIT

TREE BARRIER EXHIBIT

STATE

TO ST

DATE: 04/16/2025

SCALE: N.T.S.

DRAWN BY: DAU/DH3

CHECKED BY: ALS

PROJECT NO.:

5194

DRAWN BY:
DAU/DH3
CHECKED BY:
ALS
PROJECT NO.:
5194

CAD FILE:
TREE BARRIER EXHIBIT.dwg
PLOTTED:
06/13/2025
DRAWING NO.:
CO3.1

SHEET
03 OF 04



REFERENCE IMAGES USED FROM GOOGLE MAPS DATED OCT, 2023

TREE BARRIER EXHIBIT



June 13, 2025

New Britain Township / Craig D. Kennard, P.E. New Britain Township / Gilmore & Associates, Inc. 207 Park Aven Chalfont, PA 18914

Re: Celek Chalfont, LLC – Conditional Use Review 1

241 W. Butler Avenue TMP #26-013-032

Dear Mr. Craig D. Kennard, P.E.:

Attached please find revised plans and associated documents for your review. The plans and documents have been revised in accordance with review letter dated May 19, 2025. The following explains how each comment has been addressed:

# Comments regarding applicable Zoning Ordinance requirements as required for Conditional Uses:

1. <u>Comment:</u> §27-305.L.L2. — Outside storage areas shall be shielded from view from all public streets. The plans show the outdoor storage areas screened with 8' chain-link fence with privacy slats, while a 3'4" CMU knee wall is proposed parallel to the front façade to shield the sidewalk display area. We recommend potential renderings of these features be presented.

<u>Howell Response:</u> Careful consideration has been given to the design to shield visibility from the public. We acknowledge the recommendation to include renderings of the 8' chain-link fence with privacy slats and 3'4" knee wall or any other features to shield outside areas from view from all streets and these renderings will be provided at the Planning Commission meeting.

2. <u>Comment:</u> §27-2118. – Within the Butler Avenue Corridor Overlay District (COD), where work on a nonresidential building requires a zoning or building permit, such work shall conform with all of the applicable requirements set forth in § 22-722 of Chapter 22, Subdivision and Land Development, of the Township Code, unless waived by the Board of Supervisors at a public meeting.

development approval is required but the proposed work is subject to a building or zoning permit, such as, but not limited to, facade changes/improvements, signage changes, or changes in use, the proposed design shall be reviewed by the Township as part of the permit review process and, at the request of the Township, by the New Britain Township Planning Commission, to ensure such proposed changes comply with the



applicable provisions of this section. Design review consists of a systematic assessment of the three-dimensional configuration, design, and materials to be used to ensure that the proposed development meets the goals for the Corridor Overlay District. All applicants undergoing a design review shall submit, in addition to all other required information, the information and drawings listed below:

#### 1) Description of use or uses proposed;

We recommend the future building use be documented on the plans, the number of existing and proposed units be clarified, and the square footage of existing building space to be used by the proposed use clarified as well.

- 2) Architectural drawings of proposed building(s) showing all sides of the proposed building(s), with information on building materials and colors; and
- 3) Photographs of the original site on which the proposed development will occur, prior to any changes being made, showing surrounding areas, surrounding zoning districts, and how the new development will relate to the existing surroundings.

  We recommend the above information be presented to the Planning Commission and Board of Supervisors for consideration and incorporate the building materials such as stone and brick similar to other recent projects within the COD.
- b. §22-722.4.C.(7) Along every public street, a mixture of street trees shall be planted 50 feet on center between the curb and sidewalk. The street trees shall be planted in a proper location for the chosen species (i.e., the level of sunlight, type of soils, size of planting area, distances to buildings and utilities, etc.). For example, where there may be conflict with overhead utility wires, the shorter tree species should be used. We recommend street trees be planted between the sidewalk and parking lot along Butler Avenue due to overhead utilities and provided there are no conflicts with underground utilities.
- c. §22-722.4.D.(2) –The parking layout shall provide for pedestrian circulation throughout the parking area. This pedestrian circulation system shall be illustrated on the site plan and shall connect to walkways within the right-of-way.
- d. §22-722.4.D.(6) Bicycle parking shall be provided in an amount equal to 5% of the minimum required off-street parking for vehicles, or a minimum of two spaces, whichever is greater.
- e. §22-722.4.D.(7) The area between the cartway and the parking areas shall be devoted to required street trees, buffering, landscaping, and sidewalks. Where



parking areas are located or are proposed to be located between a street and a principal building, a buffer fence shall be installed within five feet of the ultimate right-of-way line of the street between this parking area and the street. This buffer fence, along with associated piers and hedge, shall be constructed and installed in accordance with §22-722.4.D.(7).

f. §22-722.4.E. & 27-2904.f. – All parking areas, walkways, and passages shall be adequately illuminated with a lighting system designed to complement the general areas and the site of the

proposed development and to prevent any off-site glare and spillover light onto adjacent properties.

We recommend a lighting plan be developed and provided for review for the existing and any proposed lighting. If supplemental lighting is required to comply with the minimum and maximum lighting candles specified in the Ordinance, we recommend it be installed as part of this project. Additionally, in order to establish a consistent design scheme along the Corridor Overlay District, all freestanding fixture types shall be constructed of metal, with a black finish. The luminaire shall be in the style illustrated [in this section] from Philips [CityPost LED Post Top (TX1)] and shall be placed upon a P3165 pole from Philips, or the Board of Supervisors may approve an appropriate alternative.

<u>Howell Response:</u> The Property has been developed with a shopping center since 1973 and has legally existing nonconformities that are being maintained. The only change to this long-established development and use is the addition of a small area of outside storage and display on existing impervious coverage, which has no parking requirement. Thus, the applicant is requesting waivers from these SLDO sections. See the waiver request letter.

- 3. <u>Comment:</u> §27-2401.a.3 To meet the natural resource protection standards, calculations shall be provided which indicate the area of the site with natural resources and the area of natural resources that would be disturbed or encroached upon.
  - <u>Howell Response:</u> In accordance with section §27-2401.a.3, calculations that indicate the area of the site with natural resources have been included in the Site Improvements Plan Sheet. The proposed small area of outside storage and display does not disturb or encroach into the area of natural resources which is composed of one floodplain region and has been delineated on the plan.
- 4. <u>Comment:</u> §27-2800 We recommend the condition of the existing property line buffer be assessed, maintained and supplemental plantings provided if there are gaps along the property lines shared with residential properties.



<u>Howell Response:</u> The condition of the existing property line buffer between commercial and residential districts has been assessed and deemed that the current vegetation effectively serves the purpose of a visibility shield between the underlying districts. In response, Howell has prepared sheets C03.0 and C04.0 showing the view towards the residential properties.

- 5. <u>Comment:</u> §27-2901.J. The plans shall document the total number of parking spaces required for the site including the proposed use in addition to the existing uses.
  - <u>Howell Response:</u> The plans have been revised to retain the existing parking except for the removal of one ADA space. A tabulation has been provided on Sheet 2 of the Plan Set, which shows that 7 ADA spaces are required and even though one ADA space is proposed to be removed, 9 ADA spaces will remain. Also, six additional parking spaces are being proposed on existing impervious coverage in order to achieve the required number of parking spaces, which is currently a legally existing nonconforming condition.
- 6. <u>Comment:</u> §27-2904.h. A maximum of 15 consecutive and contiguous parking spaces in a row shall be allowed without being separated by a landscaped area. Eighteen spaces are proposed behind the building on existing asphalt.
  - <u>Howell Response:</u> The eighteen parking spaces that were initially proposed behind the building on existing asphalt have been reduced to 6.
- 7. <u>Comment:</u> §27-2904.j.2.(d) The plans shall demonstrate that the proposed accessible parking spaces and accessible walkway are a maximum 2% in all directions.
  - <u>Howell Response:</u> The existing ADA spaces are no longer being relocated; thus, no new ADA Spaces are proposed.
- 8. <u>Comment:</u> §27-3008.b.5. & 27-2500 The proposed use shall be suitable in terms of effect on highway traffic and safety with adequate access arrangements to protect streets from undue congestion and hazard. A traffic impact study is required as a prerequisite for a zoning permit for any use expected to generate greater than 100 new trips inbound to the site or out bound from the site in site peak hour traffic, or 1,000 trips per day. We recommend the number of anticipated new trips and daily trips be presented and a traffic impact study provided, if required, in accordance with §27-2501.

<u>Howell Response</u>: It is anticipated that there will be a net decrease in the total trips generated by the retail use per ITE Trip Generation Manual 11<sup>th</sup> Ed. See Sheet 2 of the Plan Set. This does not meet the warrant requirement for a traffic study because less than 100 new trips inbound or outbound from the site during peak hour traffic is contemplated per the ITE Trip Generation Manual.



9. <u>Comment:</u> §27-3008.e. – The applicant is required to submit an Environmental Impact Statement (EIS) in support of the conditional use application as specified in Part 2, § 27-201, Appendix A. We note that Sheet 1 of the plan set references a title report which notes that there is an environmental covenant with the property as well as a history of hazardous material. We recommend that the status of the environmental covenant and hazardous material be discussed in the EIS as well as the future intent for existing monitoring wells to remain.

<u>Howell Response:</u> The applicant understands that there is an environmental covenant on the property. After review of the environmental covenant, the use limitations described do not restrict the minor improvements shown on this conditional use plan. Further an EIS should not be required because this Property has been fully developed for over 50 years and only a small area of outdoor storage and display is proposed on existing impervious coverage.

We trust that the information provided above, and the revised plans satisfactorily address the concerns as stated in the review letter. If you have any questions or require any additional information, please contact me directly.

Sincerely,

**HOWELL ENGINEERING** 

Demy Howell III

Denny L Howell III EIT



June 13, 2025

Doreen Curtin, Zoning Officer New Britain Township 207 Park Aven Chalfont, PA 18914

Re: Celek Chalfont, LLC – Conditional Use Review 1

241 W. Butler Avenue TMP #26-013-032

Dear Mr. Doreen Curtin, Zoning Officer:

This letter is a formal request for relief from the sections outlined below related to the Subdivision and Land Development of the New Britain Township. The comments correspond with review letter dated May 19, 2025. The following explains how each comment has been addressed and why relief is needed:

<u>Comments regarding applicable Zoning Ordinance requirements as required for Conditional Uses:</u>

- 1. <u>Comment:</u> §27-2118. Within the Butler Avenue Corridor Overlay District (COD), where work on a nonresidential building requires a zoning or building permit, such work shall conform with all of the applicable requirements set forth in § 22-722 of Chapter 22, Subdivision and Land Development, of the Township Code, unless waived by the Board of Supervisors at a public meeting.
  - a. §22-722.3 Where no subdivision or land development approval is required but the proposed work is subject to a building or zoning permit, such as, but not limited to, facade changes/improvements, signage changes, or changes in use, the proposed design shall be reviewed by the Township as part of the permit review process and, at the request of the Township, by the New Britain Township Planning Commission, to ensure such proposed changes comply with the applicable provisions of this section. Design review consists of a systematic assessment of the three-dimensional configuration, design, and materials to be used to ensure that the proposed development meets the goals for the Corridor Overlay District. All applicants undergoing a design review shall submit, in addition to all other required information, the information and drawings listed below:

We recommend the future building use be documented on the plans, the number of existing and proposed units be clarified, and the square footage of existing building space to be used by the proposed use clarified as well.



- 2) Architectural drawings of proposed building(s) showing all sides of the proposed building(s), with information on building materials and colors; and
- 3) Photographs of the original site on which the proposed development will occur, prior to any changes being made, showing surrounding areas, surrounding zoning districts, and how the new development will relate to the existing surroundings.

We recommend the above information be presented to the Planning Commission and Board of Supervisors for consideration and incorporate the building materials such as stone and brick similar to other recent projects within the COD.

Howell Response: The Property has been developed with a shopping center since 1973 and has legally existing nonconformities that are being maintained. The only change to this long-established development and use is the addition of a small area of outside storage and display, on existing impervious coverage, which has no parking requirement. The applicant is requesting a waiver as the improvements proposed are very minor and will require only a grading permit. Building façade renderings will be provided to the Township for comment.

b. §22-722.4.C.(7) – Along every public street, a mixture of street trees shall be planted 50 feet on center between the curb and sidewalk. The street trees shall be planted in a proper location for the chosen species (i.e., the level of sunlight, type of soils, size of planting area, distances to buildings and utilities, etc.). For example, where there may be conflict with overhead utility wires, the shorter tree species should be used. We recommend street trees be planted between the sidewalk and parking lot along Butler Avenue due to overhead utilities and provided there are no conflicts with underground utilities.

Howell Response: The applicant is requesting a waiver as this is a decades old developed shopping center within a commercial district and plant material would reduce visibility into and from the site.

c. §22-722.4.D.(2) –The parking layout shall provide for pedestrian circulation throughout the parking area. This pedestrian circulation system shall be illustrated on the site plan and shall connect to walkways within the right-of-way.

Howell Response: The applicant is requesting a waiver as there currently exists sidewalks along both street frontages and which also lead into the existing parking areas. The parking layout has existed for decades and is not being modified.



d. §22-722.4.D.(6) – Bicycle parking shall be provided in an amount equal to 5% of the minimum required off-street parking for vehicles, or a minimum of two spaces, whichever is greater.

Howell Response: The applicant is requesting a waiver from this section there are currently no bicycle lanes located along the existing streets. Additionally, the proposed use is not anticipated to generate any bicycle traffic.

e. §22-722.4.D.(7) – The area between the cartway and the parking areas shall be devoted to required street trees, buffering, landscaping, and sidewalks. Where parking areas are located or are proposed to be located between a street and a principal building, a buffer fence shall be installed within five feet of the ultimate right-of-way line of the street between this parking area and the street. This buffer fence, along with associated piers and hedge, shall be constructed and installed in accordance with §22-722.4.D.(7).

Howell Response: The applicant is requesting a waiver as this is a decades old developed shopping center within a commercial district and plant material and fencing would reduce visibility into and from the site.

f. §22-722.4.E. & 27-2904.f. – All parking areas, walkways, and passages shall be adequately illuminated with a lighting system designed to complement the general areas and the site of the

proposed development and to prevent any off-site glare and spillover light onto adjacent properties.

We recommend a lighting plan be developed and provided for review for the existing and any proposed lighting. If supplemental lighting is required to comply with the minimum and maximum lighting candles specified in the Ordinance, we recommend it be installed as part of this project. Additionally, in order to establish a consistent design scheme along the Corridor Overlay District, all freestanding fixture types shall be constructed of metal, with a black finish. The luminaire shall be in the style illustrated [in this section] from Philips [CityPost LED Post Top (TX1)] and shall be placed upon a P3165 pole from Philips, or the Board of Supervisors may approve an appropriate alternative.

Howell Response: The Property has been fully developed and the shopping center use existed for decades. The applicant is requesting a waiver from this section as there is already adequate lighting provided on site.



We trust that the information provided above is sufficient to justify the need for relief from the Subdivision and Land Development section of the New Britain Township. If you have any questions or require any additional information, please contact me directly.

Sincerely,

HOWELL ENGINEERING

Demy Howell III

Denny L Howell III EIT

June 18, 2025

File No. 2500567

Doreen Curtin, Zoning Officer New Britain Township 207 Park Avenue Chalfont, PA 18914

Reference: Celek Chalfont, LLC – Conditional Use Review 2

241 W. Butler Avenue, TMP #26-013-032

#### Dear Doreen:

Pursuant to your request, Gilmore & Associates, Inc. has reviewed the revised Conditional Use Application for the above-referenced project which includes the following:

#### I. Submission

- A. Application for Conditional Use Hearing dated June 2, 2025 including Conditional Use Plans by Howell Engineering, consisting of four (4) sheets, dated April 16, 2025, last revised June 13, 2025.
- B. Response letter by Howell Engineering dated June 13, 2025
- C. Waiver request by Howell Engineering dated June 13, 2025

#### II. General Information

The Applicant requests a Conditional Use for an accessory use, Use L2 Outdoor Storage or Display proposed at 241 West Butler Avenue (S.R. 4202) within the Chalfont Village Shopping Center. The 7.96-acre site is within the New Britain Township's C-1 Commercial Zoning and Butler Corridor Overlay District. The area of disturbance is all within impervious areas. The plans have been revised to propose the following:

- 2,700-sf permanent equipment display area on the right side of the building
- 2,700-sf fenced, raised, and covered outdoor display along the rear of the building
- 400-sf fenced area for a propane tank behind the building
- 1,030-sf covered, raised loading dock area
- The striping of six (6) parking spaces at the rear of the building

#### III. Review Comments

In accordance with  $\S 27-3008.b.1$  of the Township's Zoning Ordinance, the Supervisors shall grant a conditional use only if it finds adequate evidence that any proposed development submitted will meet the following requirements as well as any specific requirements and standards listed for the proposed use:

- In accordance with the Township Comprehensive Plan;
- In the best interests of the Township, the convenience of the community, the public welfare;
- Suitable for the property in question, and designed, constructed, operated and maintained so as to be in harmony with and appropriate in appearance to the existing or intended character of the vicinity;
- In conformance with all applicable requirements of this chapter and all Township ordinances;
- Suitable in terms of effect on highway traffic and safety with adequate access arrangements to protect streets from undue congestion and hazard;
- In accordance with sound standards of subdivision and land development practice where applicable;
- In accordance with the specific standards and criteria of this chapter.

We offer the following comments regarding the proposed Conditional Use Application:

65 East Butler Avenue | Suite 100 | New Britain, PA 18901 | Phone: 215-345-4330 | Fax: 215-345-8606

- 1. §27-305.L.L2. Outside storage areas shall be shielded from view from all public streets. The plans show the outdoor storage areas screened with 8' chain-link fence with privacy slats. The Applicant has indicated that they will present renderings of the fencing to the Township.
- 2. §27-2118. Within the Butler Avenue Corridor Overlay District (COD), where work on a nonresidential building requires a zoning or building permit, such work shall conform with all of the applicable requirements set forth in §22-722 of Chapter 22, Subdivision and Land Development, unless waived by the Board of Supervisors at a public meeting.
  - a. §22-722-3 In the COD, a design review is required for any project with façade improvements or changes in use to ensure the goals of the COD are being met. The Applicant is requesting a waiver from the COD review process including the assessment of the configuration, design, and materials based on the scope of work, because the property was developed in 1973 with existing nonconformities proposed to remain.
  - b. §22-722.4.C.(7) Along every public street, a mixture of street trees shall be planted 50 feet on center between the curb and sidewalk. The Applicant is requesting a waiver from street trees since current shopping center does not have street trees and to maintain visibility from West Butler Avenue.
  - c. §22-722.4.D.(2) The parking layout shall provide for pedestrian circulation throughout the parking area. Per the Waiver Request letter dated June 13, 2025, the Applicant is requesting a waiver since the parking area is not being modified with the exception of pavement striping for 6 spaces.
  - d. §22-722.4.D.(6) Bicycle parking shall be provided. Per the Waiver Request letter dated June 13, 2025, the Applicant is requesting a waiver, as there is no bicycle lane along W. Butler Avenue, and the proposed use does not generate bicycle traffic.
  - e. §22-722.4.D.(7) The area between the cartway and the parking areas shall be devoted to required landscaping and buffering (fencing with piers and hedges). The Applicant is requesting a waiver from these requirements to maintain visibility from West Butler Avenue.
- 3. §22-722.4.E. All parking areas, walkways, and passages shall be adequately illuminated with a lighting system. The Applicant has requested a waiver from this requirement, stating the lighting is an existing non-conformity and due to the scope of work. We recommend that adequate lighting be provided for all outdoor storage area accessible to the public.
- 4. §27-2401.a.3 The Natural Resource table on Sheet 2 shall be revised to document the woodlands area at the rear of the property.
- 5. §27-3008.e. The applicant is required to submit an Environmental Impact Statement (EIS) in support of the conditional use application as specified in Part 2, § 27-201, Appendix A. The Applicant indicated in the Response letter dated June 13, 2025, that an EIS should not be required based on the scope of work. In addition, the Response letter dated June 13, 2025, the Applicant understands that the proposed improvements are in accordance with an existing environmental covenant over the property.

If you have any questions regarding the above, please contact this office.

Sincerely.

Craig D. Kennard, P.E., E.V.P. Gilmore & Associates, Inc.

Township Engineers and Assistant Zoning Officer

CDK/jm

cc: Daniel Fox, Township Manager
Alexandria Mullin, Assistant to the Manager
Randy Teschner, Code Enforcement Officer/Fire Marshall
Scott Hobert, Esq., Flager & Associates, PC, Township Solicitors
Matthew W. Indorante, MSP Development, LLC
Julie L. VonSprecklesen, Esq., Eastburn and Gray
Janene Marchand, P.E., Gilmore & Associates, Inc.

#### **BUCKS COUNTY RECORDER OF DEEDS**

55 East Court Street
Doylestown, Pennsylvania 18901
(215) 348-6209

Instrument Number - 2025004671 Recorded On 2/3/2025 At 8:31:48 AM

\* Total Pages - 6

\* Instrument Type - DEED

Invoice Number - 1414801 User - ATV

- \* Grantor CHALFONT HOLDINGS LLC
- \* Grantee FLAGSHIP PENNSYLVANIA PROPCO LLC
- \* Customer SIMPLIFILE LC E-RECORDING
- \* FEES

STATE TRANSFER TAX \$20,000.00
RECORDING FEES \$85.75
CENTRAL BUCKS \$10,000.00
SCHOOL DISTRICT REALTY TAX
NEW BRITAIN \$10,000.00
TOWNSHIP
TOTAL PAID \$40,085.75

Bucks County UPI Certification On January 31, 2025 By LMC

This is a certification page

#### DO NOT DETACH

This page is now part of this legal document.

RETURN DOCUMENT TO: FIRST AMERICAN TITLE INSURANCE COMPANY -NCS WASHINGTON DC

1850 K STREET NW SUITE 1050 WASHINGTON, DC 20006

I hereby CERTIFY that this document is recorded in the Recorder of Deeds Office of Bucks County, Pennsylvania.



Daniel J. McPhillips Recorder of Deeds

\* - Information denoted by an asterisk may change during the verification process and may not be reflected on this page.



Prepared by and Return to:

CERTIFIED PROPERTY IDENTIFICATION NUMBERS
26-006-101--001 - N BRITAIN TWP
CERTIFIED 01/31/2025 BY LMC

First American Title Insurance Company National Commercial Services 250 West Pratt Street, Suite 650 Baltimore, Maryland 21201 Attn: Michael Leeb

UPI: 26-006-101-001

Street Address: 545 W. Butler Ave., Chalfont PA 18914

#### SPECIAL WARRANTY DEED

THIS INDENTURE, made the 27th day of January, 2025, and effective the 30th day of January, 2025, between Chalfont Holdings LLC, a Pennsylvania limited liability company, with an address at 2200 Michener Street, Suite 4, Philadelphia, PA 19115 (hereinafter called the Grantor), of the one part, and Flagship Pennsylvania Propco, LLC, a Pennsylvania limited liability company, with an address at 2 Mid America Plaza, Suite 450, Oakbrook Terrace, IL 60181, Attention: Real Estate Operations (hereinafter called the Grantee), of the other part.

WITNESSETH, that the said Grantor for and in consideration of the sum of Two Million and 00/100 Dollars (\$2,000,000.00) lawful money of the United States of America and other good and valuable consideration, unto it well and truly paid by the said Grantee, at or before the sealing and delivery hereof, the receipt whereof is hereby acknowledged, has granted, bargained and sold, conveyed, released and confirmed, and by these presents does grant, bargain and sell, convey, release and confirm unto the said Grantee, its successors and assigns, in fee simple, all of Grantor's right, title and interest in and to:

ALL THAT CERTAIN lot or piece of ground with the buildings and improvements thereon erected, located in the Township of New Britain, County of Bucks and Commonwealth of Pennsylvania, as more fully described by metes and bounds on Exhibit A attached hereto and made a part hereof (the "Property").

**UNDER AND SUBJECT TO** all conditions, easements, rights of way, agreements, covenants, reservations, exceptions, restrictions and other encumbrances of record, insofar as the same are note in force and burden and affect the Property and without reimposing the same.

**TOGETHER WITH** all and singular the buildings and improvements, ways, streets, alleys, driveways, passages, waters, water-courses, rights, liberties, privileges, hereditaments and appurtenances, whatsoever unto the hereby granted Property belonging, or in anywise appertaining, and the reversions and remainders, rents, issues, and profits thereof; and all the estate, right, title, interest, property, claim and demand whatsoever of it, the said Grantor, as well as at law as in equity, of, in and to the same.

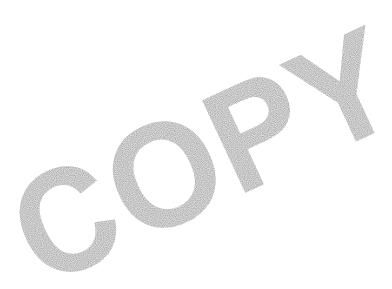
AND ESPECIALLY TOGETHER WITH (i) any and all easements, rights-of-way, agreements, privileges, appurtenances, riparian rights, licenses and other rights and benefits belonging to or running with the Property including, without limitation, easements for lateral connections and/or surface flow from the Property to the regional stormwater management ponds and/or stormwater management facilities, and any and all development rights, air rights, water rights, mineral rights and timber rights; and (ii) all of Grantor's right, title

and interest in and to any and all land lying in the bed of any street, road or highway, opened or proposed, in front of or abutting or adjoining the Property.

TO HAVE AND TO HOLD the said Property, with the buildings and improvements thereon erected, hereditaments and premises hereby granted, or mentioned and intended so to be, with the appurtenances, unto the said Grantee, its successors and assigns, to and for the only proper use and behoof of the said Grantee, its successors and assigns, forever.

AND the said Grantor, for itself and its successors and assigns, does, by these presents, covenant, grant and agree, to and with the said Grantee, its successors and assigns, that it, the said Grantor, and its successors and assigns, all and singular the hereditaments and premises herein described and granted, or mentioned and intended so to be, with the appurtenances, unto the said Grantee, its successors and assigns, against it, the said Grantor, and its successors and assigns, and against all and every other person or persons whomsoever lawfully claiming or to claim the same or any part thereof, by, from, or under it, shall and will SPECIALLY WARRANT and forever DEFEND.

(signature page follows)



IN WITNESS WHEREOF, the Grantor has hereunto set its hand and seal dated the day and year first above written.

Chalfont Holdings LLC,

a Pennsylvania limited liability company

By:

Name: Mital Rao

Title: Managing Member

COMMONWEALTH OF PENNSYLVANIA

COUNTY OF \_

On this 27 day of January, 2025 before me, the undersigned notary public, personally appeared Mital Rao, who acknowledged himself to be the Managing Member of Chalfont Holdings LLC, a Pennsylvania limited liability company, and that as such officer, being authorized to do so, executed the foregoing instrument for the purposes therein contained.

My commission expires:

EditionWealth of Pennsylvania - Notary Seel ROSEMARY A GORESKI - MOTARY PUBLIC Montgomery County My Cuttituissium Expires November 9, 2027

Cummission Number 1060732

(Grantee acknowledgement page follows)

The precise residence and the complete post office address of the above-named Grantee is:

2 Mid America Plaza, Suite 450 Oakbrook Terrace, IL 60181 Attention: Real Estate Operations

On behalf of the Grantee:

Flagship Pennsylvania Propco, LLC, a Pennsylvania limited liability company

By:

Name: David Kelly

Title: Vice-President

#### **EXHIBIT "A"**

#### TO SPECIAL WARRANTY DEED

#### **Legal Description**

All that certain tract or parcel of land and premises, situate, lying and being in the Township of New Britain, County of Bucks and Commonwealth of Pennsylvania, bounded and described according to a Plan thereof made by John E. Burkhardt and Associates, dated August 22, 1956, as follows to wit:

Beginning at a point formed by the intersection of the center line of County Line Road (State Highway Rural Route No. 09033) (33 feet wide), with the center line of Doylestown Pike (Legislative Route No. 178) (40 feet wide); thence extending from said Point of Beginning, North 49 degrees 44 minutes West along the center line of County Line Road, 201.01 feet to a point; thence extending, North 41 degrees 26 minutes East, along lands now or late of Ira M. Shaffer and Louise D. Shaffer, crossing the Northeasterly side of County Line Road, 250.01 feet to a monument, a corner; thence extending, South 49 degrees 44 minutes East, still by lands of the aforementioned Shaffer, crossing the Northwesterly side of Doylestown Pike, 201.01 feet to a spike on the center line of Doylestown Pike; thence extending, South 41 degrees 26 minutes West along the center line of Doylestown Pike, 250.01 feet to the first mentioned Point and Place of Beginning.

Excepting Thereout and Therefrom: All that certain Fee Simple Deed conveyed by County Line/New Britain, L.P. to the Commonwealth of Pennsylvania, Department of Transportation, dated February 04, 2010 and recorded March 12, 2010 in Land Record Book 6356 Page 1264.

BEING PARCEL NO. 26-006-101-001

BEING the same premises which County Line/New Britain Realty L.P., by Deed dated August 24, 2022 and recorded September 02, 2022 in Bucks County at Instrument No. 2022054655, granted and conveyed unto Chalfont Holdings LLC, a Pennsylvania limited liability company, in fee.



April 3, 2025

Dan Fox Township Manager New Britain Township 207 Park Avenue Chalfont, PA 18914

RE: Flagship Car Wash (Spotless Brands)

545 West Butler Avenue, Chalfont, PA 18914

VCEA No. 24-02-NBR

Dear Mr. Fox:

In conjunction with the Land Development Application for the property (PARID No. 26-006-101-001) address is 545 West Butler Avenue, Chalfont, PA 18914; Please consider the below written waiver requests from requirements in the New Britain Township Subdivision/Land Development Ordinance (SALDO).

#### **WAIVER REQUESTS:**

1. SALDO §22-403 - Preliminary Plan Submission and Review Procedure

The applicant is proposing to combine the Preliminary and Final plan stages therefore shortening the review period required.

2. <u>SALDO §22-706.1.D & SALDO §22-708.6.1</u> – Requiring all proposed curbs shall have a minimum radius of five (5) feet.

In lieu of pavement gore stripping curbing was utilized to delineate the parking area uses, utilizing curb radii less than the required 5 feet.

3. <u>SALDO §22-708.2.F.</u> – Requiring a six (6) foot minimum width sidewalk or walkway when adjacent to parking stalls.

Due to the lot size and configuration of the site, a sidewalk of 4.33 feet was provided along the front of the proposed building in lieu of the required 6 feet width.

4. <u>SALDO §22-711.4.C.</u> – Requiring the top of bottom edge of slopes shall be a minimum of five (5) feet from property lines in order to permit normal rounding of the edge without encroaching onto the abutting property.

Due to the lot size and configuration of the site, the grading associated with the site features extends to the adjoiner property lines.

#### **OFFICE LOCATIONS**

#### www.vancleefengineering.com



5. <u>SALDO §22-712.5.E.</u> – Requiring storm sewers to be reinforced concrete pipe with a minimum diameter of 15 inches or equivalent. Storm sewer located outside a public ultimate right-of-way or easement shall be high density corrugated polyethylene.

Due to the lot size and anticipated stormwater runoff, only a maximum pipe size of 12 inches is necessary to convey stormwater for the site.

6. SALDO §22-722.4.C.(1)(a)&(b), and (2) – Requiring setbacks from Butler Avenue to be 50 feet from principal and accessory buildings. 25 feet from surface parking areas and interior drives and setbacks from all other street lines shall be 50 feet except parking areas and interior drives may be located up to 10 feet from all other street lines.

Due to the lot size and configuration of the site, the interior drive along Bulter Avenue are setback 8 feet from the property line.

7. SALDO §22-722.4.D.(6) – Requiring bicycle parking.

Due to the nature of the business, we do not believe bicycle parking is necessary.

# PRELIMINARY / FINAL LAND DEVELOPMENT PLANS

**FOR** 

# FLAGSHIP CARWASH

PREPARED FOR

# SPOTLESS BRANDS

SITUATED IN

# NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA

**OWNER OF RECORD:** 

FLAGSHIP PENNSYLVANIA PROPCO, LLC

SITE ADDRESS: 545 WEST BUTLER AVENUE CHALFONT, PA 18914

PARID 26-006-101-001 INSTRUMENT NUMBER 2025004671

28,414 SQ. FT. OR 0.652 ACRES

**SITE AREA:** 

SPUR ROAD

OPONIESTOM ROAD

BUTLER AVENUE

OP

RR

OP

RR

OP

C-2

OP

C-3

MANOR DRIVE

LOCATION MAP

SCALE: 1"=1000'

GRAPHIC SCALE
0 500 1000 2000
1 INCH = 1000 FT

**DATE: APRIL 3, 2025** 



VAN CLEEF ENGINEERING ASSOCIATES, LLC 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM PHONE (215) 345-1876 **INDEX OF SHEETS** 

SHEET No. TITLE

SITE PLAN

**COVER SHEET** 

**EXISTING FEATURES PLAN** 

4 AERIAL PLAN

5 GRADING PLAN

6 UTILITY PLAN

7 LANDSCAPE PLAN

8 LIGHTING PLAN
9 EROSION AND SEDIMENTATION CONTROL PLAN

10 CONSTRUCTION DETAILS

11 UTILITY DETAILS

12 EROSION AND SEDIMENTATION CONTROL NOTES

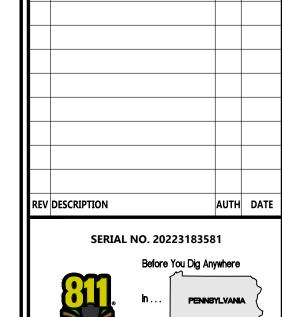
13 EROSION AND SEDIMENTATION CONTROL DETAILS

14 CONSTRUCTION DETAILS15 CONSTRUCTION DETAILS

16 ADA DETAILS

ENGINEERING WITH FOCUS

Bridges/Highways
Construction Inspection
Runicipal Engineering
Environmental
Site Development



PLAN NOTATION

ONLY THOSE PLANS WHICH CONTAIN A DIGITAL, IMPRESSED, OR COLORIZED INK SEAL OF THE RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERED VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARED FOR THE OWNER DESIGNATED HEREON. ANY MODIFICATION, REVISION, DUPLICATION OR USE WITHOUT THE WRITTEN CONSENT OF VAN CLEEF ENGINEERING ASSOCIATES IS PROHIBITED. RELIANCE ON THIS PLAN FOR ANY PURPOSE OTHER THAN THAT WHICH IS INTENDED SHALL BE AT THE SOLE DISCRETION AND LIABILITY OF THE



SEAN DAVID McGRANAHAN
PENNSYLVANIA PROFESSIONAL
ENGINEER NUMBER 085090



FIIONE (213) 343-1070

COVER SHEET

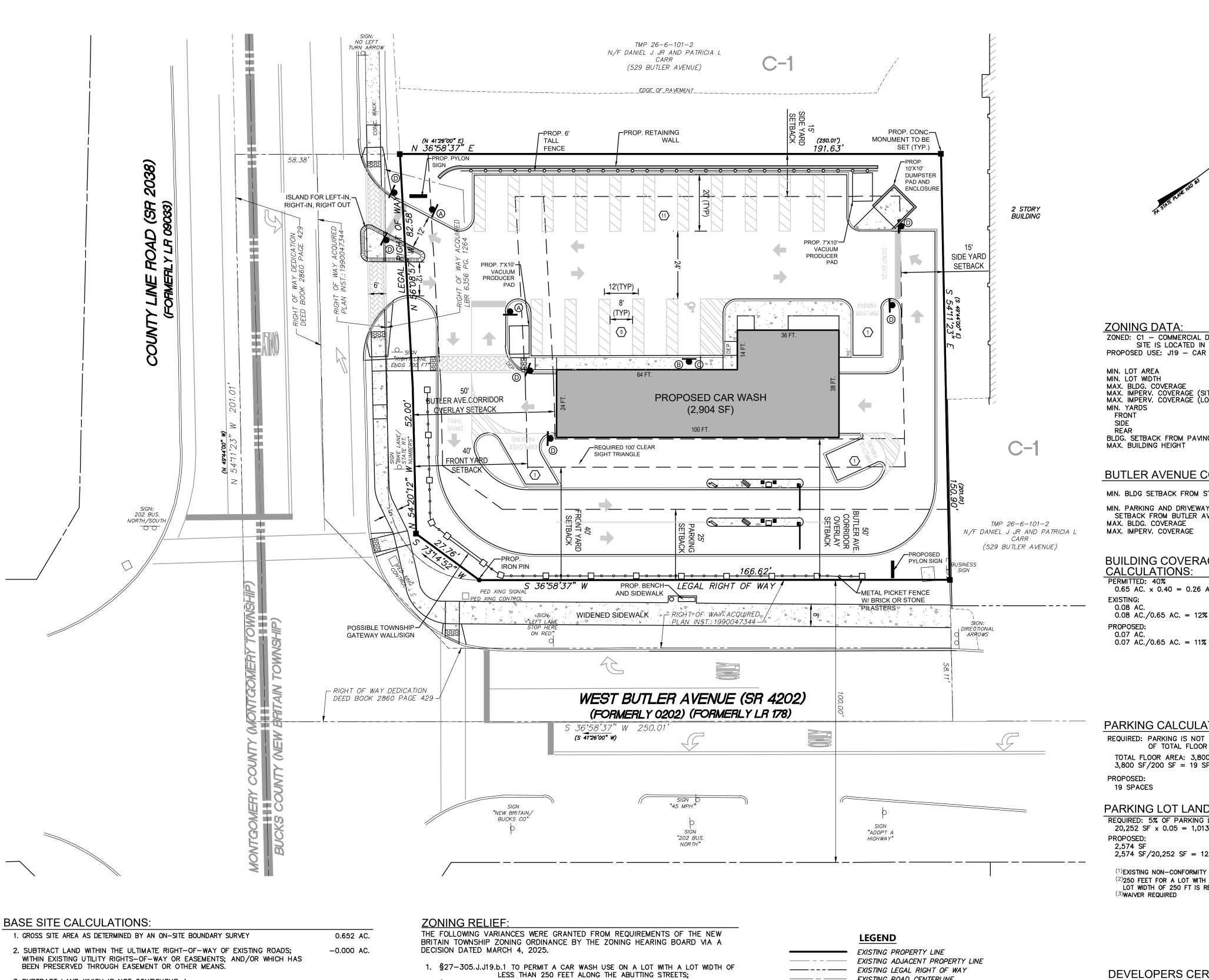
FOR

FLAGSHIP CARWASH

DATE: APRIL 3, 20:
SCALE: AS NOTE
DESIGNED BY: LDM, SD
DRAWN BY: R.
CHECKED BY: SI
JOB NUMBER: 2402-NE

TMP'S 26-006-101-001

NEW BRTIAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA



#### 3. SUBTRACT LAND WHICH IS NOT CONTIGUOUS, i.e.: (a) A SEPARATE PARCEL WHICH DOES NOT ABUT OR ADJOIN, NOR SHARE -0.000 AC. COMMON BOUNDARIES WITH, THE REST OF THE DEVELOPMENT; AND/OR (b) LAND WHICH IS CUT OFF FROM THE MAIN PARCEL BY A ROAD, RAILROAD. -0.000 AC. EXISTING LAND USE, AND/OR MAJOR STREAM, SO AS TO SERVE AS A MAJOR BARRIER TO COMMON USE AND/OR SO THAT IT IS ISOLATED AND UNAVAILABLE FOR BUILDING PURPOSES. 4. SUBTRACT LAND WHICH, IN A PREVIOUSLY APPROVED SUBDIVISION, WAS SET -0.000 AC. ASIDE, RESERVED, AND/OR RESTRICTED FOR OPEN SPACE, NATURAL RESOURCE PROTECTION, AND/OR RECREATION PURPOSES. 5. SUBTRACT LAND USED FOR ANOTHER USE (i.e. LAND WHICH IS USED OR TO BE -0.000 AC. USED, FOR COMMERCIAL OR INDUSTRIAL USES IN A RESIDENTIAL DEVELOPMENT) OR LOCATED IN A DIFFERENT ZONING DISTRICT THAN THE REST OF THE DEVELOPMENT. BASE SITE AREA 0.652 AC. NATURAL RESOURCE PROTECTION AREA (AC) PROTECTION 0.000 RIPARIAN BUFFER 0.000 FLOODPLAIN 0.000 0.000 FLOODPLAIN (ALLUVIAL) SOILS 1.00 0.000 0.000 0.000 0.000 0.000 0.000 0.000 0.000 WETLANDS 0.000 LAKES OR PONDS STEEP SLOPES (8%-15%) 0.000

0.000

0.000

0.000

NET BUILDABLE SITE AREA = BASE AREA - OPEN SPACE AREA = 0.625 AC - 0.00 = 0.625 AC

0.000

0.000

STEEP SLOPES (15%-25%)

STEEP SLOPES (25%+)

AGRICULTURAL SOILS

- 2. §27-1202.b TO PERMIT A SIDE YARD SETBACK OF 10 FEET IN THE AREA OF THE VACUUM STATIONS;
- 3. \$27-2901.J.J19.b and \$27-2904.a.7 TO PERMIT 16 OF THE OFF-STREET PARKING SPACES TO BE JOINTLY USED AS STALLS FOR VEHICLE VACUUMING; AND
- 4. §27-2904.q.5 TO PERMIT A PAVED AREA BE 0 FEET FROM THE EXTERIOR STRUCTURAL WALL OF THE PROPOSED BUILDING.

LAND TO BE DEDICATED FOR PARKS & RECREATION:

2.500 S.F. OF P&R SPACE PER 4.000 S.F. OF BUILDING AREA REQUIRED. BUILDING AREA = 2,904 S.F.=1,1815 S.F. PARKS & RECREATION SPACE REQUIRED.

1. CONTRIBUTION SHALL BE MADE BASED ON THE CURRENT TOWNSHIP FEE SCHEDULE.

15% MIN. SITE AREA TO BE DESIGNED AS GREEN SPACE:

 $0.65 \text{ ACRES } \times 43,560 \text{ SF} = 28,314 \text{ SF} \times 0.15 = 4,247 \text{ SF GREEN SPACE REQUIRED.}$ GREEN SPACE AROUND BENCH, GATEWAY MONUMENT SIGN, AND TRASH RECEPTACLE OPEN GREEN AREA = 4,482 SF PROVIDED.

## NOTE:

0.000

0.000

1. ROTATING OR REVOLVING SIGNS, ELECTRONIC MESSAGE BOARDS, PENNANTS, FLAGS, BANNERS, BALLOONS, AND OTHER OBJECTS AND ITEMS (SUCH AS TIRES, AUTOMOBILES, FOOD PRODUCTS, ETC.) THAT ARE DESIGNED TO DRAW ATTENTION TO THE PRODUCT OR BUSINESS ARE ALL PROHIBITED IN THE CORRIDOR OVERLAY DISTRICT.

----- EXISTING ROAD CENTERLINE

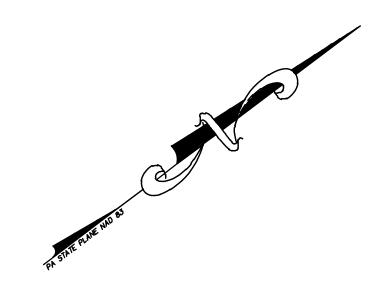
----- EXISTING EASEMENT EXISTING ROAD ====== EXISTING CURB EXISTING EDGE OF DRIVE EXISTING CONCRETE MONUMENT EXISTING PIN

# ZONING CLASSIFICATION

PROPOSED MONUMENT TO BE SET PROPOSED IRON PIN TO BE SET ------ PROPOSED ROAD CENTERLINE PROPOSED CURB

PROPOSED SIDEWALK PROPOSED RIGHT OF WAY — — PROPOSED BUILDING SETBACK

----- PROPOSED EASEMENT ----- PROPOSED BUFFER PROPOSED CLEAR SIGHT TRIANGLE



# **ZONING DATA:**

ZONED: C1 - COMMERCIAL DISTRICT SITE IS LOCATED IN THE BUTLER AVENUE CORRIDOR OVERLAY DISTRICT PROPOSED USE: J19 - CAR WASH (PERMITTED)

MIN. LOT AREA MIN. LOT WIDTH MAX. BLDG. COVERAGE MAX. IMPERV. COVERAGE (SITE) MAX. IMPERV. COVERAGE (LOT) MIN. YARDS	REQUIRED 1 AC. 150 FT.(2) 40% 70% 75%	EXISTING 0.65 AC.(1) 150 FT.(1)(2) 12% 75%(1) 75%	PROPOSED 0.65 AC(1) 150 FT.(1)(2) 11% 75%(1) 75%
FRONT SIDE REAR BLDG. SETBACK FROM PAVING MAX. BUILDING HEIGHT	40 FT.	61 FT.	50 FT.
	15 FT.	10 FT. <sup>(1)</sup>	10 FT. <sup>(1)</sup>
	30 FT.	N/A	N/A
	20 FT.	3.4 FT. <sup>(1)</sup>	0 FT. <sup>(1)</sup>
	35 FT.	<35 FT.	<35 FT.

# BUTLER AVENUE CORRIDOR OVERLAY REQUIREMENTS:

MIN. BLDG SETBACK FROM STREETS	REQUIRED 50 FT.	EXSITING 61 FT.	PROPOSED 50 FT. ALONG BUTLER AVE 50 FT. ALONG CNTY.LINE R
MIN. PARKING AND DRIVEWAY SETBACK FROM BUTLER AVE MAX. BLDG. COVERAGE	25 FT. 40%	20 FT. <sup>(1)</sup>	8 FT. <sup>(3)</sup>

#### **BUILDING COVERAGE** CALCULATIONS:

PERMITTED: 40%  $0.65 \text{ AC.} \times 0.40 = 0.26 \text{ AC.}$ EXISTING: O.OB. AC.

0.08 AC. / 0.65 AC. = 12%PROPOSED:

#### BUILDING SIDEWALKS PARKING $0.49 \text{ AC.}/0.65 \text{ AC.} = 75\%^{(1)}$

**EXISTING:** 

PROPOSED: BUILDING SIDEWALKS, WALLS PARKING

SITE IMPERVIOUS

PERMITTED: 70%

SURFACE CALCULATIONS:

0.02 AC.

0.03 AC

 $0.65 \text{ AC.} \times 0.70 = 0.46 \text{ AC.}$ 

 $0.49 \text{ AC.}/0.65 \text{ AC.} = 75\%^{(1)}$ 

PARKING CALCULATIONS:

REQUIRED: PARKING IS NOT REQUIRED FOR A CAR WASH OR 1 SPACE PER 200 SF OF TOTAL FLOOR AREA.

TOTAL FLOOR AREA: 3,800 SF MAX (FIRST FLOOR: 2,904 SF + SECOND FLOOR: 896 SF) 3,800 SF/200 SF = 19 SPACESPROPOSED: 19 SPACES

#### PARKING LOT LANDSCAPE CALCULATIONS: REQUIRED: 5% OF PARKING LOTS MUST BE LANDSCAPE AREA

 $20,252 \text{ SF } \times 0.05 = 1,013 \text{ SF}$ PROPOSED: 2,574 SF

2,574 SF/20,252 SF = 12.7%

(1)EXISTING NON-CONFORMITY (2)250 FEET FOR A LOT WITH A DRIVEWAY ENTERING ONTO AN ARTERIAL STREET. ADDITIONALLY, A LOT WIDTH OF 250 FT IS REQUIRED PURSUANT TO J-19 CAR WASH REQUIREMENTS) (3)WAIVER REQUIRED

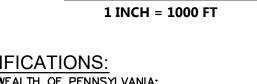
## **DEVELOPERS CERTIFICATION:**

I/WE ACKNOWLEDGE THAT THE STORMWATER FACILITIES AND BMP'S ARE FIXTURES THAT CAN BE ALTERED OR REMOVED ONLY AFTER APPROVAL OF NEW BRITAIN TOWNSHIP.

## SPOTLESS BRANDS

## PROPOSED SIGNAGE SYMBOL LEGEND

- PROPOSED "STOP" SIGN (R1-1) PROPOSED "RESERVED PARKING" SIGN (R7-8) &
- "RESERVED PARKING PENALTIES" SIGN (R7-8F)
- PROPOSED "VAN ACCESSIBLE" SIGN (R7-8B) PROPOSED "DO NOT ENTER" SIGN (R5-1)



500 1000

# **CERTIFICATIONS:**

COMMONWEALTH OF PENNSYLVANIA:

#### COUNTY OF BUCKS

OWNERSHIP, ACKNOWLEDGEMENT OF PLAN AND OFFER OF DEDICATION WE HAVE LAID OUT UPON OUR LAND, SITUATED IN THE TOWNSHIP OF NEW BRITAIN, COUNTY OF BUCKS, COMMONWEALTH OF PENNSYLVANIA, LOTS AND STREETS ACCORDING TO THIS PLAN WHICH IS INTENDED TO BE RECORDED. WITNESS MY HAND AND SEAL THIS \_\_\_\_\_ DAY OF

**GRAPHIC SCALE** 

#### SPOTLESS BRANDS

NOTARY PUBLIC IN AND FOR THE COMMONWEALTH OF PENNSYLVANIA, THE UNDERSIGNED OFFICER, PERSONALLY APPEARED REPRESENTING

KNOWN TO ME. OR SATISFACTORILY PROVEN TO ME, TO BE THE PERSON WHOSE NAME IS SUBSCRIBED TO THE WITHIN INSTRUMENT AS \_\_ OF SAID \_ APPEARED BEFORE ME IN PERSON AND ACKNOWLEDGED THAT HE SIGNED AND DELIVERED THE SAID INSTRUMENT AS HIS OWN FREE AND VOLUNTARY ACT AND AS THE FREE AND VOLUNTARY ACT OF SAID COMPANY FOR THE USES AND PURPOSES THEREIN SET FORTH.

#### MY COMMISSION EXPIRES NOTARY PUBLIC SIGNATURE

BOARD OF SUPERVISORS APPROVED BY THE NEW BRITAIN TOWNSHIP BOARD OF SUPERVISORS

# THIS \_\_\_\_\_ DAY OF \_\_\_\_\_ 20\_\_\_\_.

DAY OF \_\_\_\_\_\_ 20\_\_\_\_.

TOWNSHIP ENGINEER APPROVED BY THE NEW BRITAIN TOWNSHIP ENGINEER THIS \_\_\_\_\_\_

# TOWNSHIP ENGINEER

RECORDER OF DEEDS

RECORDED IN THE OFFICE FOR THE RECORDING OF DEEDS, ETC, IN AND FOR THE COUNTY OF BUCKS AT DOYLESTOWN, PENNSYLVANIA IN PLAN BOOK \_\_\_\_\_ PAGE \_\_\_\_ ON \_\_\_\_

## RECORDER OF DEEDS

## ENGINEER'S CERTIFICATION

THIS IS TO CERTIFY THAT I HAVE READ ARTICLE V OF THE NEW BRITAIN TOWNSHIP SUBDIVISION AND LAND DEVELOPMENT ORDINANCE, AND THAT THE ACCOMPANYING PLAN MEETS THE REQUIREMENTS OF THAT ARTICLE TO THE BEST OF MY KNOWLEDGE.

4/3/25 SEAN DAVID McGRANAHAN, P.E. (PA PE-085090)

## SURVEYOR'S CERTIFICATION

THIS IS TO CERTIFY THAT I HAVE READ ARTICLE V OF THE NEW BRITAIN TOWNSHIP SUBDIVISION AND LAND DEVELOPMENT ORDINANCE, AND THAT THE ACCOMPANYING PLAN MEETS THE REQUIREMENTS OF THAT ARTICLE TO THE BEST OF MY KNOWLEDGE.

## DAVID H. ARTMAN, P.L.S. (PA SU051039-E)

OWNER OF RECORD: FLAGSHIP PENNSYLVANIA PROPCO, LLC

**SITE ADDRESS: 545 WEST BUTLER AVENUE** CHALFONT, PA 18914

PARID 26-006-101-001 **INSTRUMENT NUMBER 2025004671** 

SITE AREA:

OAVID H. ARTMAN 28,414 SQ. FT. OR 0.652 ACRES ENNSYLVANIA PROFESSIONAL AND SURVEYOR NUMBER SU-051039-

REV DESCRIPTION

**SERIAL NO. 20223183581** 



PA ONE CALL SYSTEM, INC. **GRAPHIC SCALE** 10 20

1 INCH = 20 FT

PLAN NOTATION ONLY THOSE PLANS WHICH CONTAIN A DIGIT. IMPRESSED, OR COLORIZED INK SEAL OF T RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERED VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPAREI FOR THE OWNER DESIGNATED HEREON. ANY MODIFICATION, REVISION, DUPLICATION OR USE WITHOUT THE WRITTEN CONSENT OF VAN CLEEF ENGINEERING ASSOCIATES IS PROHIBITE RELIANCE ON THIS PLAN FOR ANY PURPO OTHER THAN THAT WHICH IS INTENDED SHALL AT THE SOLE DISCRETION AND LIABILITY OF T APPLICABLE PARTY.



EAN DAVID McGRANAHAN



VAN CLEEF ENGINEERING ASSOCIATES, LLC 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM PHONE (215) 345-1876

SITE PLAN (SHEET 1 OF 1 TO BE RECORDED)

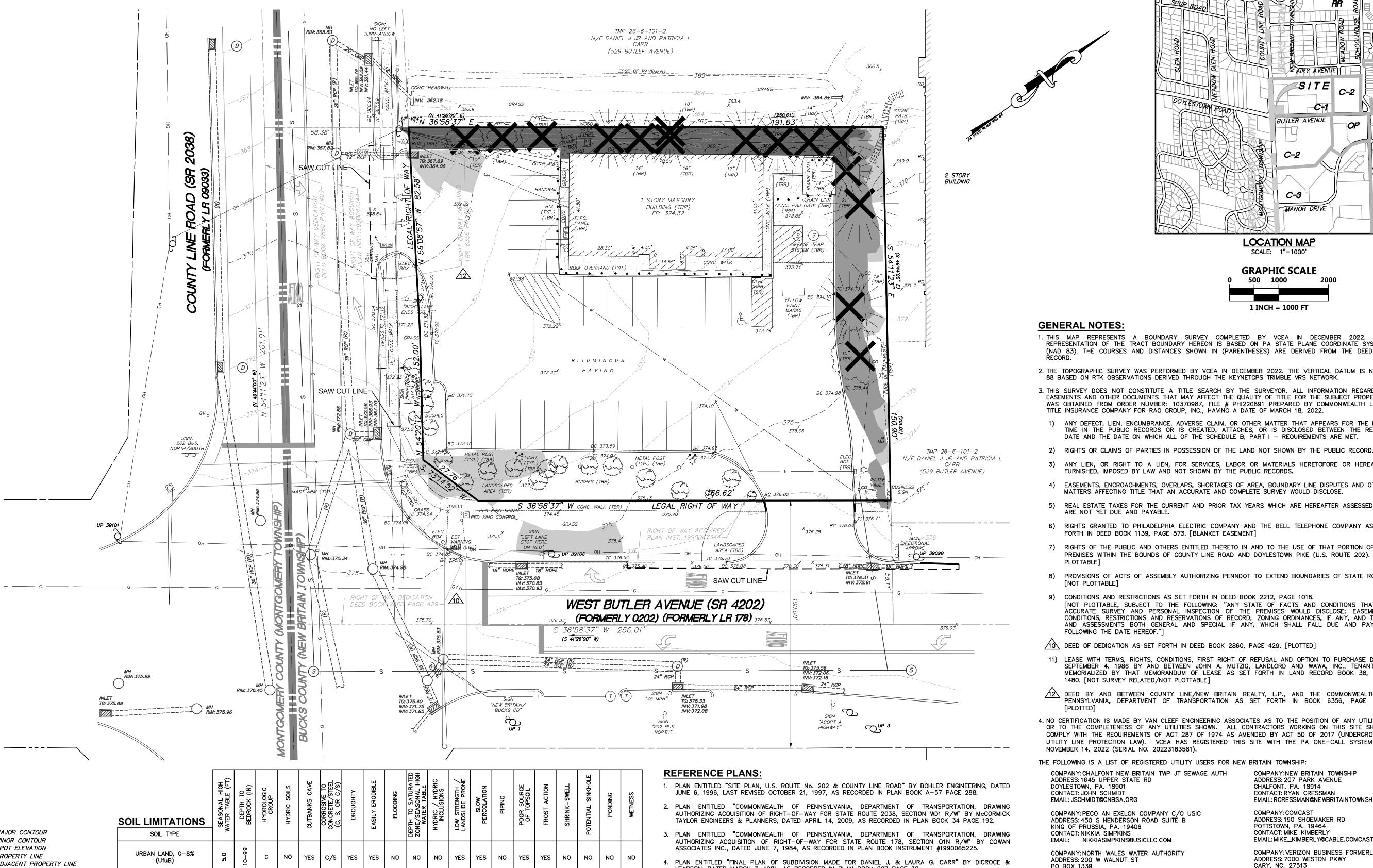
# FOR

**FLAGSHIP CARWASH** 

DESIGNED BY RAWN BY HECKED BY B NUMBER

TMP'S 26-006-101-001

**NEW BRTIAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 



# **LEGEND**

---- EXISTING MAJOR CONTOUR EXISTING MINOR CONTOUR EXISTING SPOT ELEVATION EXISTING PROPERTY LINE EXISTING ADJACENT PROPERTY LINE ---- EXISTING LEGAL RIGHT OF WAY EXISTING EASEMENT ------ EXISTING ROAD CENTERLINE EXISTING ROAD EXISTING CURB ----- EXISTING EDGE OF DRIVE EXISTING SIGN EXISTING SOILS LIMIT AND SOILS TYPE EXISTING TREES EXISTING UTILITY POLES EXISTING OVERHEAD ELECTRIC — — T — EXISTING TELEPHONE — — G — EXISTING GAS MAIN ===== EXISTING STORM SEWER EXISTING SANITARY SEWER

EXISTING WATER MAIN

SAW CUT LINE

EXISTING WATER VALVE

EXISTING TREE TO BE REMOVED

EXISTING ITEM TO BE REMOVED

## **GENERAL NOTES:**

1. SOILS SHOWN HEREON ARE AS MAPPED BY THE NATURAL RESOURCES CONSERVATION SERVICES (NRCS) WEBSITE FOR BUCKS COUNTY ON SEPTEMBER 26, 2023. 2. THE SITE FLOWS DRAIN TO THE WEST BRANCH OF THE NESHAMINY CREEK (WWF, MF) WITHIN THE NESHAMINY CREEK WATERSHED. THE WATERS DESIGNATED/EXISTING USES ARE CLASSIFIED BY THE PA CODE CHAPTER 93.

# **SOIL RESOLUTIONS:**

WHEN THE ABOVE SOILS LIMITATIONS ARE ENCOUNTERED, UTILIZE THE FOLLOWING SOILS RESOLUTIONS:

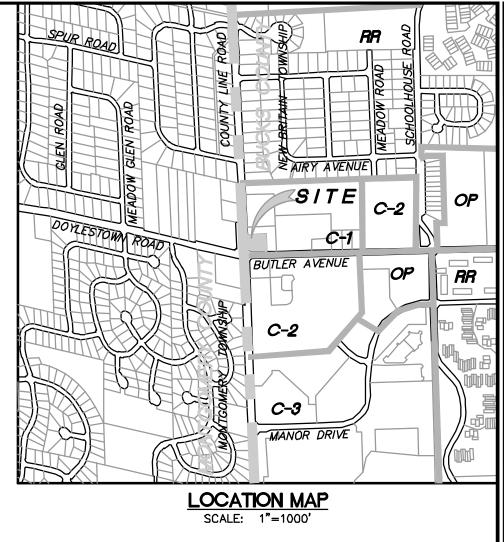
- DEPTH TO BEDROCK WHEN DEPTH TO BEDROCK IS ENCOUNTERED, CONTRACTOR MAY BLAST IF NECESSARY.
- CUTBANKS CAVE ALL APPLICABLE OSHA STANDARDS AND REGULATIONS WITH REGARD TO UTILITY AND RETAINING WALL CONSTRUCTION MUST BE IMPLEMENTED AT ALL TIMES. LIMIT SLOPES TO 3:1 MAXIMUM.
- CORROSIVE TO CONCRETE/STEEL ALL UNDERGROUND CONCRETE AND/OR STEEL MATERIALS TO BE BACKFILLED WITH APPROPRIATE MATERIAL WHEN CORROSIVENESS IS LIMITED. FOR BUILDINGS, CONTRACTOR TO COMPLY WITH THE INTERNAL RESIDENTIAL CODE (IRC) BUILDING CODE.
- . DROUGHTY PROVIDE WATER TRUCK FOR IRRIGATION IN NEEDED. WHERE LANDSCAPING IS PROPOSED WITHIN DROUGHTY SOILS, APPLYING COMPOST MAY BE NECESSARY TO HELP
- EASILY ERODIBLE SOILS TO BE STABILIZED WITH APPROPRIATE COVER AND/OR MATTING.
- LOW STRENGTH/LANDSLIDE PRONE PRECAUTIONS SHOULD BE TAKEN TO PREVENT SLOPE FAILURES DUE TO IMPROPER CONSTRUCTION PRACTICES, SUCH AS OVER-STEEPENING AND OVERLOADING OF SLOPES, REMOVAL OF LATERAL SUPPORT, AND FAILURE TO PREVENT SATURATION OF SLOPES. ROAD FILL MATERIAL MAY NEED TO BE IMPORTED IN AREAS WHERE SOILS HAVE LOW STRENGTH.
- SLOW PERCOLATION UTILIZE STRUCTURAL BMP'S THAT DO NOT REQUIRE INFILTRATION (I.E. AMENDED SOILS, SLOW RELEASE BASIN). NO INFILTRATION BMP'S ARE PROPOSED. • POOR SOURCE OF TOPSOIL - CONTRACTOR TO SUPPLY TOPSOIL AS NEEDED.
- FROST ACTION PRECAUTIONS ARE NEEDED TO PREVENT DAMAGE, ESPECIALLY TO ROADWAYS.

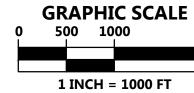
- 4. PLAN ENTITLED "FINAL PLAN OF SUBDIVISION MADE FOR DANIEL J, & LAURA G. CARR" BY DICROCE & LEAPSON, DATED MARCH 3, 1981, AS RECORDED IN PLAN BOOK 207 PAGE 32.
- 5. PLAN ENTITLED "PROPOSED WAWA FINAL LAND DEVELOPMENT PLANS DEVELOPED BY PROVCO PINEGOOD CHALFONT, LLC" BY BOHLER ENGINEERING, DATED MAY 18, 2018, LAST REVISED NOVEMBER 1, 2019, AS RECORDED IN PLAN BOOK INSTRUMENT # 2020034823.
- 6. PLAN ENTITLED "SHOWALTER SUBDIVISION MINOR SUBDIVISION PLAN" BY SHOWALTER & ASSOCIATES, DATED MARCH 26, 1984, LAST REVISED APRIL 10, 1990, AS RECORDED IN PLAN BOOK 256 PAGE 82.

# **SUMMARY OF ADVERSE FEE-SIMPLE CONVEYANCES:**

-12,566 S.F. DEED BK. 2860 PG. 429 - 8,050 S.F. PLAN INSTRUMENT NO. 1990047344 - 1,214 S.F. DEED LRB 6356 PG. 1264

50,244 S.F. DEED INSTRUMENT NO. 2022054655 28,414 S.F. CURRENT SURVEY





#### **GENERAL NOTES**

1. THIS MAP REPRESENTS A BOUNDARY SURVEY COMPLETED BY VCEA IN DECEMBER 2022. THE REPRESENTATION OF THE TRACT BOUNDARY HEREON IS BASED ON PA STATE PLANE COORDINATE SYSTEM (NAD 83). THE COURSES AND DISTANCES SHOWN IN (PARENTHESES) ARE DERIVED FROM THE DEED OF

- 2. THE TOPOGRAPHIC SURVEY WAS PERFORMED BY VCEA IN DECEMBER 2022. THE VERTICAL DATUM IS NAVD 88 BASED ON RTK OBSERVATIONS DERIVED THROUGH THE KEYNETGPS TRIMBLE VRS NETWORK.
- 3. THIS SURVEY DOES NOT CONSTITUTE A TITLE SEARCH BY THE SURVEYOR. ALL INFORMATION REGARDING EASEMENTS AND OTHER DOCUMENTS THAT MAY AFFECT THE QUALITY OF TITLE FOR THE SUBJECT PROPERTY WAS OBTAINED FROM ORDER NUMBER: 10370987, FILE # PHI220891 PREPARED BY COMMONWEALTH LAND TITLE INSURANCE COMPANY FOR RAO GROUP, INC., HAVING A DATE OF MARCH 18, 2022.
- ANY DEFECT, LIEN, ENCUMBRANCE, ADVERSE CLAIM, OR OTHER MATTER THAT APPEARS FOR THE FIRST TIME IN THE PUBLIC RECORDS OR IS CREATED, ATTACHES, OR IS DISCLOSED BETWEEN THE REPORT DATE AND THE DATE ON WHICH ALL OF THE SCHEDULE B, PART I - REQUIREMENTS ARE MET.
- ANY LIEN, OR RIGHT TO A LIEN, FOR SERVICES, LABOR OR MATERIALS HERETOFORE OR HEREAFTER
- FURNISHED, IMPOSED BY LAW AND NOT SHOWN BY THE PUBLIC RECORDS. 4) EASEMENTS, ENCROACHMENTS, OVERLAPS, SHORTAGES OF AREA, BOUNDARY LINE DISPUTES AND OTHER
- MATTERS AFFECTING TITLE THAT AN ACCURATE AND COMPLETE SURVEY WOULD DISCLOSE. 5) REAL ESTATE TAXES FOR THE CURRENT AND PRIOR TAX YEARS WHICH ARE HEREAFTER ASSESSED AND
- ARE NOT YET DUE AND PAYABLE. 6) RIGHTS GRANTED TO PHILADELPHIA ELECTRIC COMPANY AND THE BELL TELEPHONE COMPANY AS SET FORTH IN DEED BOOK 1139, PAGE 573. [BLANKET EASEMENT]
- 7) RIGHTS OF THE PUBLIC AND OTHERS ENTITLED THERETO IN AND TO THE USE OF THAT PORTION OF THE PREMISES WITHIN THE BOUNDS OF COUNTY LINE ROAD AND DOYLESTOWN PIKE (U.S. ROUTE 202). [NOT
- 8) PROVISIONS OF ACTS OF ASSEMBLY AUTHORIZING PENNDOT TO EXTEND BOUNDARIES OF STATE ROADS. [NOT PLOTTABLE]
- 9) CONDITIONS AND RESTRICTIONS AS SET FORTH IN DEED BOOK 2212, PAGE 1018. [NOT PLOTTABLE, SUBJECT TO THE FOLLOWING: "ANY STATE OF FACTS AND CONDITIONS THAT AN ACCURATE SURVEY AND PERSONAL INSPECTION OF THE PREMISES WOULD DISCLOSE; EASEMENTS, CONDITIONS, RESTRICTIONS AND RESERVATIONS OF RECORD; ZONING ORDINANCES, IF ANY, AND TAXES AND ASSESSMENTS BOTH GENERAL AND SPECIAL IF ANY, WHICH SHALL FALL DUE AND PAYABLE FOLLOWING THE DATE HEREOF."]

# 10 DEED OF DEDICATION AS SET FORTH IN DEED BOOK 2860, PAGE 429. [PLOTTED]

- 11) LEASE WITH TERMS, RIGHTS, CONDITIONS, FIRST RIGHT OF REFUSAL AND OPTION TO PURCHASE DATED SEPTEMBER 4. 1986 BY AND BETWEEN JOHN A. MUTZIG, LANDLORD AND WAWA, INC., TENANT, AS MEMORIALIZED BY THAT MEMORANDUM OF LEASE AS SET FORTH IN LAND RECORD BOOK 38, PAGE 1480. [NOT SURVEY RELATED/NOT PLOTTABLE]
- 12 DEED BY AND BETWEEN COUNTY LINE/NEW BRITAIN REALTY, L.P., AND THE COMMONWEALTH OF PENNSYLVANIA, DEPARTMENT OF TRANSPORTATION AS SET FORTH IN BOOK 6356, PAGE 1264.
- 4. NO CERTIFICATION IS MADE BY VAN CLEEF ENGINEERING ASSOCIATES AS TO THE POSITION OF ANY UTILITIES OR TO THE COMPLETENESS OF ANY UTILITIES SHOWN. ALL CONTRACTORS WORKING ON THIS SITE SHALL COMPLY WITH THE REQUIREMENTS OF ACT 287 OF 1974 AS AMENDED BY ACT 50 OF 2017 (UNDERGROUND UTILITY LINE PROTECTION LAW). VCEA HAS REGISTERED THIS SITE WITH THE PA ONE-CALL SYSTEM ON NOVEMBER 14, 2022 (SERIAL NO. 20223183581).
- THE FOLLOWING IS A LIST OF REGISTERED UTILITY USERS FOR NEW BRITAIN TOWNSHIP:

COMPANY: CHALFONT NEW BRITAIN TWP JT SEWAGE AUTH ADDRESS: 1645 UPPER STATE RD DOYLESTOWN, PA. 18901 CONTACT: JOHN SCHMIDT EMAIL: JSCHMIDT@CNBSA.ORG

COMPANY: PECO AN EXELON COMPANY C/O USIC ADDRESS: 450 S HENDERSON ROAD SUITÉ B KING OF PRUSSIA, PA. 19406 CONTACT: NIKKIA SIMPKINS EMAIL: NIKKIASIMPKINS@USICLLC.COM

COMPANY: NORTH WALES WATER AUTHORITY ADDRESS: 200 W WALNUT ST PO BOX 1339 NORTH WALES, PA. 19454

- CONTACT: JOSEPH MURPHY EMAIL: JMURPHY@NWWATER.COM
- 5. THIS SURVEY DOES NOT CERTIFY AS TO THE PRESENCE OR ABSENCE OF UNDERGROUND IMPROVEMENTS OR ENCROACHMENTS.
- 6. THIS SURVEY DOES NOT CERTIFY AS TO THE PRESENCE OR ABSENCE OF WETLANDS OR WATERS.
- 7. THE PARCEL SHOWN HEREON IS NOT LOCATED IN A SPECIAL FLOOD HAZARD AREA (SFHA), PER FLOOD INSURANCE RATE MAPS (FIRM) PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA), BEING PANEL 288 OF 532 MAP NUMBER 42017C0288K, EFFECTIVE DATE MARCH 21, 2017.

# OWNER OF RECORD:

FLAGSHIP PENNSYLVANIA PROPCO, LLC **SITE ADDRESS: 545 WEST BUTLER AVENUE** 

PARID 26-006-101-001 **INSTRUMENT NUMBER 2025004671** 

SITE AREA:

CHALFONT, PA 18914

28,414 SQ. FT. OR 0.652 ACRES

AVID H. ARTMAN

NSYLVANIA PROFESSIONA AND SURVEYOR NUMBER SU-051039

COMPANY: NEW BRITAIN TOWNSHIP ADDRESS: 207 PARK AVENUE

EMAIL: RCRESSMAN@NEWBRITAINTOWNSHIP.ORG

EMAIL: MIKE\_KIMBERLY@CABLE.COMCAST.COM

COMPANY: VERIZON BUSINESS FORMERLY MCI

CHALFONT, PA. 18914

POTTSTOWN, PA. 19464

CONTACT: MIKE KIMBERLY

COMPANY: COMCAST

CARY, NC. 27513

CONTACT: VICTOR WOOD

CONTACT: RYAN CRESSMAN

ADDRESS: 190 SHOEMAKER RD

ADDRESS: 7000 WESTON PKWY

EMAIL: VICTOR.S.WOOD@VERIZON.COM

SERIAL NO. 20223183581



PA ONE CALL SYSTEM, INC. **GRAPHIC SCALE** 10 20

1 INCH = 20 FT PLAN NOTATION ONLY THOSE PLANS WHICH CONTAIN A DIGIT IMPRESSED, OR COLORIZED INK SEAL OF MITTERSEED, ON COUNTED INN SEAL OF THE RESPONSIBLE PROFESSIONAL SHALL BE CONSIDEREL VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPAREL FOR THE OWNER DESIGNATED HEREON. ANY MODIFICATION, REVISION, DUPLICATION OR USE WITHOUT THE WRITTEN CONSENT OF VAN CLEEF ENGINEERING ASSOCIATES IS PROHIBITE RELIANCE ON THIS PLAN FOR ANY PURPO



EAN DAVID McGRANAHAN

IEER NUMBER 085090

**VAN CLEEF ENGINEERING ASSOCIATES, LL** 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM

PHONE (215) 345-1876

**EXISTING FEATURES DEMOLITION PLAN** 

FOR

**FLAGSHIP CARWASH** 

RAWN BY

HECKED BY

B NUMBER

TMP'S 26-006-101-001

**NEW BRTIAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 



ENGINEERING WITH FOCUS
Bridges/Highways
Construction Inspection
Environmental
Geotechnical/Dams
Surveying/Aerial Drones/GIS
Mater/Wastewater

SCRIPTION AUTH DAT

SERIAL NO. 20223183581

Before You Dig Anywhere

h ... PENNSYLVANA

t's below. STOPI CALL 1-800-242-1776
TOLL NEE
poe excepted

PA ONE CALL SYSTEM INC.

**GRAPHIC SCALE**0 25 50 10

1 INCH = 50 FT

PLAN NOTATION

ONLY THOSE PLANS WHICH CONTAIN A DIGITAL, IMPRESSED, OR COLORIZED INK SEAL OF THE RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERED VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARED FOR THE OWNER DESIGNATED HEREON. ANY MODIFICATION, REVISION, DUPLICATION OR USE WITHOUT THE WRITTEN CONSENT OF VAN CLEEF ENGINEERING ASSOCIATES IS PROHIBITED. RELIANCE ON THIS PLAN FOR ANY PURPOSE OTHER THAN THAT WHICH IS INTENDED SHALL BE AT THE SOLE DISCRETION AND LIABILITY OF THE APPLICABLE PARTY.

PROFESSIONAL

SEAN DAVID MCGRANAHAN

ENGINEER
No. PE085090

A/5/5

SEAN DAVID McGRANAHAN

DA

PENNSYLVANIA PROFESSIONAL
ENGINEER NUMBER 085090



VAN CLEEF ENGINEERING ASSOCIATES, LLC
501 NORTH MAIN STREET, DOYLESTOWN, PA 18901
WEB: WWW.VANCLEEFENGINEERING.COM
PHONE (215) 345-1876

**AERIAL PLAN** 

FLAGSHIP CARWASH

 DATE:
 APRIL 3, 2025

 SCALE:
 1"=50"

 DESIGNED BY:
 LDM, SDM

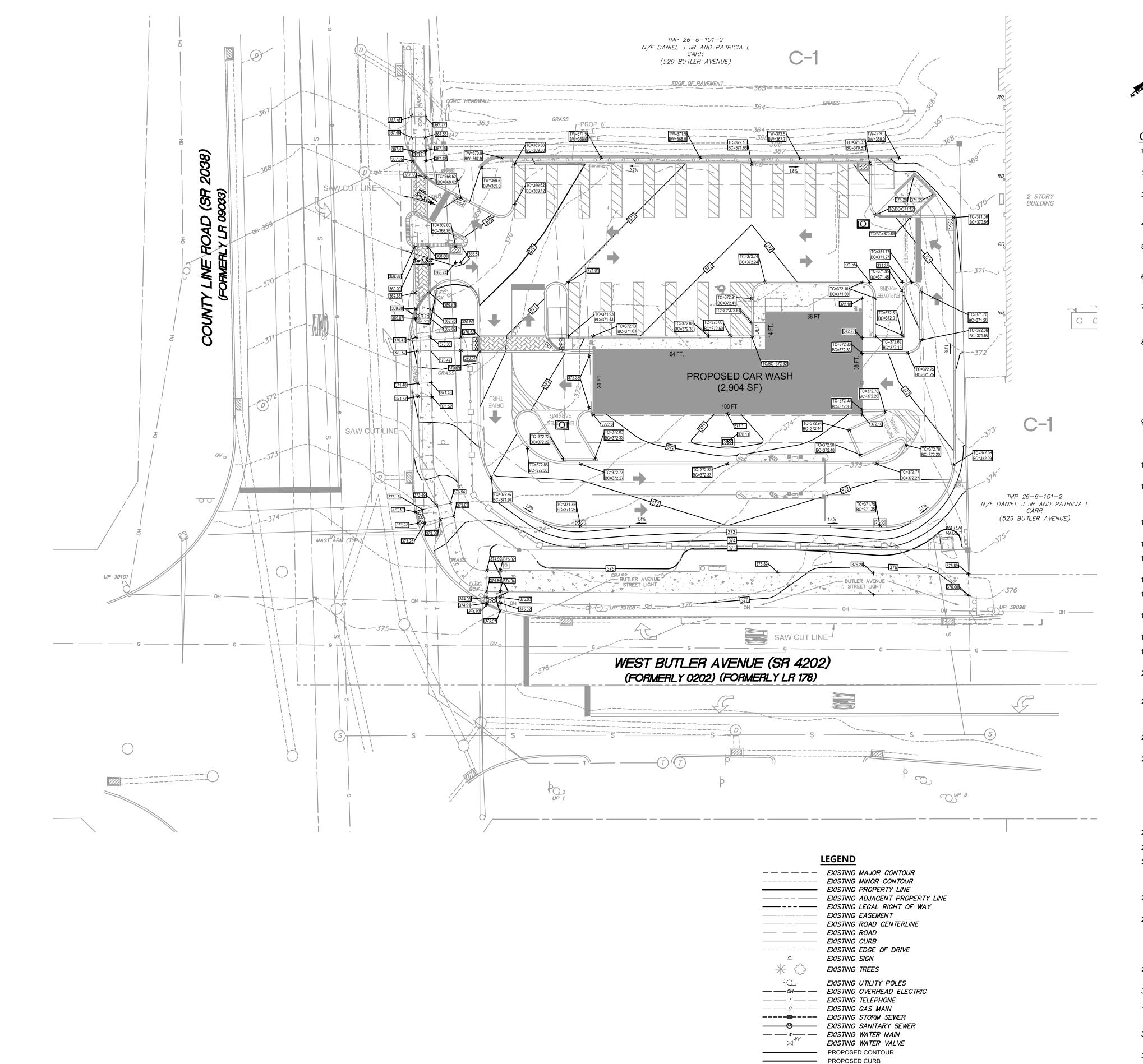
 DRAWN BY:
 RJN

 CHECKED BY:
 SDC

 JOB NUMBER:
 2402-NBR

TMP'S 26-006-101-001

NEW BRTIAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA



PROPOSED EASEMENT

— — PROPOSED SETBACK LINE

PROPOSED STORM SEWER

PROPOSED WATER LATERAL

507.30

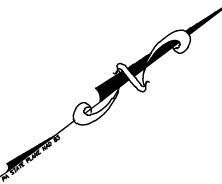
PROPOSED RIGHT OF WAY

PROPOSED SIDEWALK

PROPOSED SIGN

FINISHED FLOOR ELEVATION

PROPOSED SPOT ELEVATION



#### **GRADING NOTES:**

- 1. VERTICAL DATUM IS NAVD 1988 AND ESTABLISHED BY OBSERVATIONS REFERENCED TO THE KEYSTONE VRS
- 2. THE MINIMUM SLOPE IN GRASSED AREAS SHALL NOT BE LESS THAN 2.0% AND THE MINIMUM IN PAVED AREAS SHOULD NOT BE LESS THAN 1.0%.
- 3. PAVEMENT SHALL BE SAW CUT IN STRAIGHT LINES TO THE FULL DEPTH OF THE EXISTING PAVEMENT. ALL DEBRIS FROM REMOVAL OPERATIONS SHALL BE REMOVED FROM THE SITE AT THE TIME OF EXCAVATION. STOCKPILING OF DEBRIS WILL NOT BE PERMITTED.
- 4. IN CASE OF DISCREPANCIES BETWEEN PLANS, THE SITE / RECORD PLAN WILL SUPERSEDE IN ALL CASES. THE ENGINEER OF RECORD MUST BE IMMEDIATELY NOTIFIED IN WRITING OF ANY CONFLICTS.
- 5. THE CONTRACTOR SHALL BE REQUIRED TO SECURE ALL NECESSARY PERMITS (INCLUDING DEP, ETC.) FOR ALL OFF-SITE HAUL AND/OR BORROW SITES. CONTRACTOR SHALL SUPPLY A COPY OF APPROVALS TO DESIGN ENGINEER AND OWNER PRIOR TO INITIATING WORK.
- 6. EXISTING INLETS AND STORM SEWER INDICATED AS FILLED W/DEBRIS SHALL BE CLEANED AND FLUSHED. NEW INLETS AND PIPES SHALL BE CHECKED FOR SILT/DEBRIS AFTER CONSTRUCTION AND FLUSHED/CLEANED
- 7. DEPTH OF EXISTING UTILITIES IN PORTIONS OF THE SITE ARE UNKNOWN. WHERE EXISTING UTILITIES ARE TO REMAIN AND ARE FOUND TO HAVE INADEQUATE GROUND COVER AFTER FINAL PROPOSED GRADES HAVE BEEN ESTABLISHED, THE DESIGN ENGINEER SHALL BE CONTACTED IMMEDIATELY AND PRIOR TO FURTHER CONSTRUCTION ACTIVITIES IN THE AREA OF SAID CONFLICT.
- 8. ALL DESIGNERS AND CONTRACTORS UTILIZING THIS PLAN AND THE INFORMATION CONTAINED THEREON ARE CAUTIONED TO COMPLY WITH THE REQUIREMENTS OF PENNSYLVANIA ACT 287. LOCATION OF EXISTING AND PROPOSED UNDERGROUND UTILITIES AND FACILITIES SHOWN ON THE DRAWINGS HAVE BEEN DEVELOPED FROM INFORMATION MADE AVAILABLE. COMPLETENESS AND ACCURACY OF LOCATION AND DEPTH OF UTILITIES AND FACILITIES CANNOT BE GUARANTEED. THE CONTRACTOR IS TO VERIFY THE DEPTH AND LOCATION OF ALL UTILITIES AND FACILITIES BEFORE THE START OF WORK. UTILIZE HAND EXCAVATION AS REQUIRED. WORK IS ALSO TO BE DONE IN ACCORDANCE WITH THE STANDARDS OF THE UTILITY COMPANIES WHOSE FACILITIES ARE IN THE PROXIMITY OF THE WORK. OTHER UTILITIES MAY BE REQUIREMENTS OF PENNSYLVANIA ACT 38 (1991), THE CONTRACTOR SHALL CONTACT THE PENNSYLVANIA ONE CALL SYSTEM AT 1—800—242—1776, AT LEAST 3 DAYS PRIOR TO EXCAVATION.
- 9. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE FEDERAL, STATE AND LOCAL CODES, AND ALL REGULATIONS APPURTENANT TO THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970. ALL WORK SHALL BE DONE IN ACCORDANCE WITH PROJECT SPECIFICATIONS INCLUDING CURRENT APPLICABLE STANDARDS AND REQUIREMENTS. WHERE ANY STANDARDS SEEM IN CONFLICT WITH THESE DRAWINGS, NOTIFY THE DESIGN ENGINEER AND CONSTRUCTION MANAGER FOR DIRECTION PRIOR TO PROCEEDING WITH WORK.
- 10. CONTRACTORS SHALL HAVE ALL REQUIRED SUBMITTAL APPROVALS PRIOR TO BEGINNING WORK OR ORDERING
- 11. CONTRACTORS SHALL VERIFY ALL DIMENSIONS, INVERTS, ELEVATIONS, AND EXISTING CONDITIONS PRIOR TO PROCEEDING WITH THE WORK OR PROCUREMENT OF MATERIALS. VARIATIONS BETWEEN DRAWINGS AND ACTUAL FIELD CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER AND RESOLVED PRIOR
- 12. CONTRACTOR SHALL BE RESPONSIBLE FOR SAFETY, PROCEDURES, MEANS AND METHODS, SEQUENCING, AND
- 13. ALL WORK SHALL BE PERFORMED BY QUALIFIED, EXPERIENCED PERSONNEL.
- 14. CONTRACTOR SHALL NOTIFY THE OWNER OF PREEXISTING CONDITIONS OF DETERIORATION IN AREAS OF WORK THAT ARE UNCOVERED OR EXPOSED DURING THE WORK.
- 15. FIELD CHANGES REQUIRE PRIOR DESIGN ENGINEERING REVIEW AND WRITTEN CONFIRMATION.
- 16. CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR TO ANY SIDEWALKS, LAWN, TREES, PAVING, AND OTHER IMPROVEMENTS DISTURBED OR DAMAGED BY DEMOLITION ACTIVITIES PROPOSED HEREIN.
- CONTRACTOR SHALL PROVIDE PROPER TEMPORARY BRACING AND SHORING OF ALL CONSTRUCTION TO REMAIN OR DEMOLITION WORK IN PROGRESS.
- 18. CONTRACTOR SHALL PROVIDE LAYOUT, LINE AND GRADE UNLESS OTHERWISE NOTED.
- 19. CONTRACTOR SHALL NOTIFY THE OWNER OF ANY DISCREPANCIES WITHIN THE DRAWINGS, SPECIFICATIONS, CODES OR STANDARDS FOR CORRECTIVE ACTION PRIOR TO START OF WORK.
- 20. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE AND PROTECTION OF VEHICULAR AND PEDESTRIAN TRAFFIC. ALL TRAFFIC CONTROL MEASURES SHALL BE IN ACCORDANCE WITH LOCAL, PENNDOT & OSHA REGULATIONS.
- 21. CONTRACTOR SHALL PROVIDE PROTECTION FOR EXISTING UTILITIES UNLESS OTHERWISE NOTED. THE CONTRACTOR SHALL IMMEDIATELY REPAIR ANY UTILITY LINE INTERRUPTION AT NO ADDITIONAL CONTRACT COST. THE CONTRACTOR SHALL PROVIDE ADEQUATE PROTECTION AND SUPPORT FOR ALL UTILITIES EXPOSED DURING THE WORK TO INSURE AGAINST DAMAGE AT NO ADDITIONAL COST.
- 22. CONTRACTOR SHALL PROVIDE TEMPORARY DEWATERING OF EXCAVATIONS THROUGHOUT THE DURATION OF CONTRACT AT NO ADDITIONAL COST.
- 23. EXISTING UTILITIES SHOWN ARE BASED ON AVAILABLE DATA. DUE TO THE POTENTIAL LACK OF COMPLETE OR ACCURATE DATA REGARDING EXISTING ONSITE AND OFFSITE UTILITIES, THE CONTRACTOR SHALL ASSESSAVAILABLE DATA, SHALL REQUEST UTILITY COMPANY MARKOUTS, SHALL COORDINATE WITH AFFECTED UTILITY COMPANIES, AND SHALL DIG TEST PITS AT ALL PROPOSED UTILITY CROSSING LOCATIONS SO THAT ELEVATIONS CAN BE TAKEN TO ASSESS POTENTIAL CONFLICTING PIPES/UTILITIES PRIOR TO ANY PROPOSED UTILITY CONSTRUCTION. IF UNEXPECTED UTILITY LOCATIONS OR ELEVATIONS OR PIPE CONFLICTS ARE ENCOUNTERED DURING CONSTRUCTION, OR IF TEST PITS REVEAL POTENTIAL CONFLICT, DESIGN ENGINEER SHALL BE NOTIFIED SO THAT ELEVATIONS AND LOCATIONS (WHERE NECESSARY) OF AFFECTED UTILITIES CAN BE OBTAINED TO FACILITATE NECESSARY DESIGN ADJUSTMENTS.
- 24. REMOVAL OF EXISTING UTILITIES SHALL BE COORDINATED WITH THE APPROPRIATE UTILITY COMPANIES.
- 25. REFER TO THE SITE / RECORD PLAN FOR ADDITIONAL NOTES.

BY THE SERVICING WATER AND SEWER AUTHORITY.

- 26. ALL SIDEWALKS, CROSSWALK, TRAILS, ENTRANCES, AND RAMPS TO BE BUILT IN ACCORDANCE WITH ADA STANDARDS. MÁXIMUM LONGITUDINAL SLOPE TO BE 5%. MAXIMUM CROSS SLOPE TO BE 2%. IT IS RECOMMENDED TO CONSTRUCT CROSS SLOPES AT 1.5% AND LONGITUDINAL SLOPES AT 4.9% TO ALLOW FOR CONSTRUCTION TOLERANCE.
- 27. REFER TO ADA CURB RAMP CONSTRUCTION DETAILS AND ADA ACCESSIBILITY PLAN FOR ADDITIONAL INFORMATION SPECIFIC TO CURB RAMP AND ACCESSIBLE ROUTE GRADING.
- 28. AFTER FINAL GRADING THERE SHALL BE A MINIMUM OF EIGHT INCHES OF TOPSOIL ON THE ENTIRE SITE OTHER THAN THAT PORTION OF THE SITE WHERE THERE ARE BUILDINGS OR OTHER IMPERVIOUS SURFACE COVERAGE. THE DEVELOPER IS NOT REQUIRED TO IMPORT TOPSOIL TO THE SITE IF AN EIGHT-INCH FINAL DEPTH CANNOT BE ACHIEVED. IN CASES WHERE THE ORIGINAL SITE HAS LESS THAN EIGHT INCHES OF TOPSOIL AT THE START OF GRADING ACTIVITIES, THE TOTAL AMOUNT AVAILABLE WILL BECOME THE MINIMUM TOPSOIL REQUIREMENT. NO TOPSOIL WILL BE ALLOWED TO BE RELEASED FROM THE SITE.
- 29. ALL AREAS SHALL BE PROPERLY GRADED TO ENSURE DRAINAGE FLOW AWAY FROM THE PROPOSED BUILDING WITHOUT PONDING OR OBSTRUCTION.
- 30. DURING GRADING OPERATIONS, NECESSARY MEASURES FOR DUST CONTROL SHALL BE EXERCISED.
- 31. THE CHALFONT NEW BRITAIN JOINT SEWER AUTHORITY IS TO BE CONTACTED ONE WEEK PRIOR TO SANITARY SEWER CONSTRUCTION, AND 72 HOURS PRIOR TO EXCAVATING NEAR, AND CONNECTING INTO ANY EXISTING SANITARY SEWER.
- 32. ALL SANITARY SEWER CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THE NORTH WALES WATER AUTHORITY SPECIFICATIONS.
- 33. PUBLIC WATER AND SEWER CURRENTLY SERVE THE EXISTING BUILDING. THE PROPOSED WATER SERVICE
- SHALL CONNECT TO THE EXISTING WATER VAULT.
- 34. PROPOSED HANDICAP PARKING SPACES SHALL BE PROVIDED WITH POST-MOUNTED SIGNS.

35. CONTRACTOR TO CONNECT NEW BUILDING UTILITY CONNECTIONS TO EXISTING WATER AND SEWER.

- 36. CONTRACTOR SHALL VERIFY COMPLIANCE WITH ADA STANDARDS WITH PROJECT ARCHITECT FOR RAMP AREAS PROPOSED ADJACENT TO DOORWAYS.
- 37. ALL UTILITY SERVICES TO THE PROPOSED BUILDING SHALL BE INSTALLED UNDERGROUND.
- 38. TOPSOIL MAY NOT BE REMOVED FROM THE SITE WITHOUT APPROVAL FROM THE TOWNSHIP. 39. FINAL DESIGN OF PROPOSED WATER AND SEWER FACILITIES ARE SUBJECT TO THE REVIEW AND APPROVAL



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**GRAPHIC SCALE** 10 20

1 INCH = 20 FT

PLAN NOTATION ONLY THOSE PLANS WHICH CONTAIN A DIGIT IMPRESSED, OR COLORIZED INK SEAL OF RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERED VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARED FOR THE OWNER DESIGNATED HEREON. ANY MODIFICATION, REVISION, DUPLICATION OR USE WITHOUT THE WRITTEN CONSENT OF VAN CLEEF

ENGINEERING ASSOCIATES IS PROHIBITE RELIANCE ON THIS PLAN FOR ANY PURPO

OTHER THAN THAT WHICH IS INTENDED SHALL AT THE SOLE DISCRETION AND LIABILITY OF T APPLICABLE PARTY. **PROFESSIONAL** EAN DAVID MCGRAN \ FNGINFFR

EAN DAVID McGRANAHAN



IEER NUMBER 085090

VAN CLEEF ENGINEERING ASSOCIATES, LLC 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM

PHONE (215) 345-1876

**GRADING PLAN** 

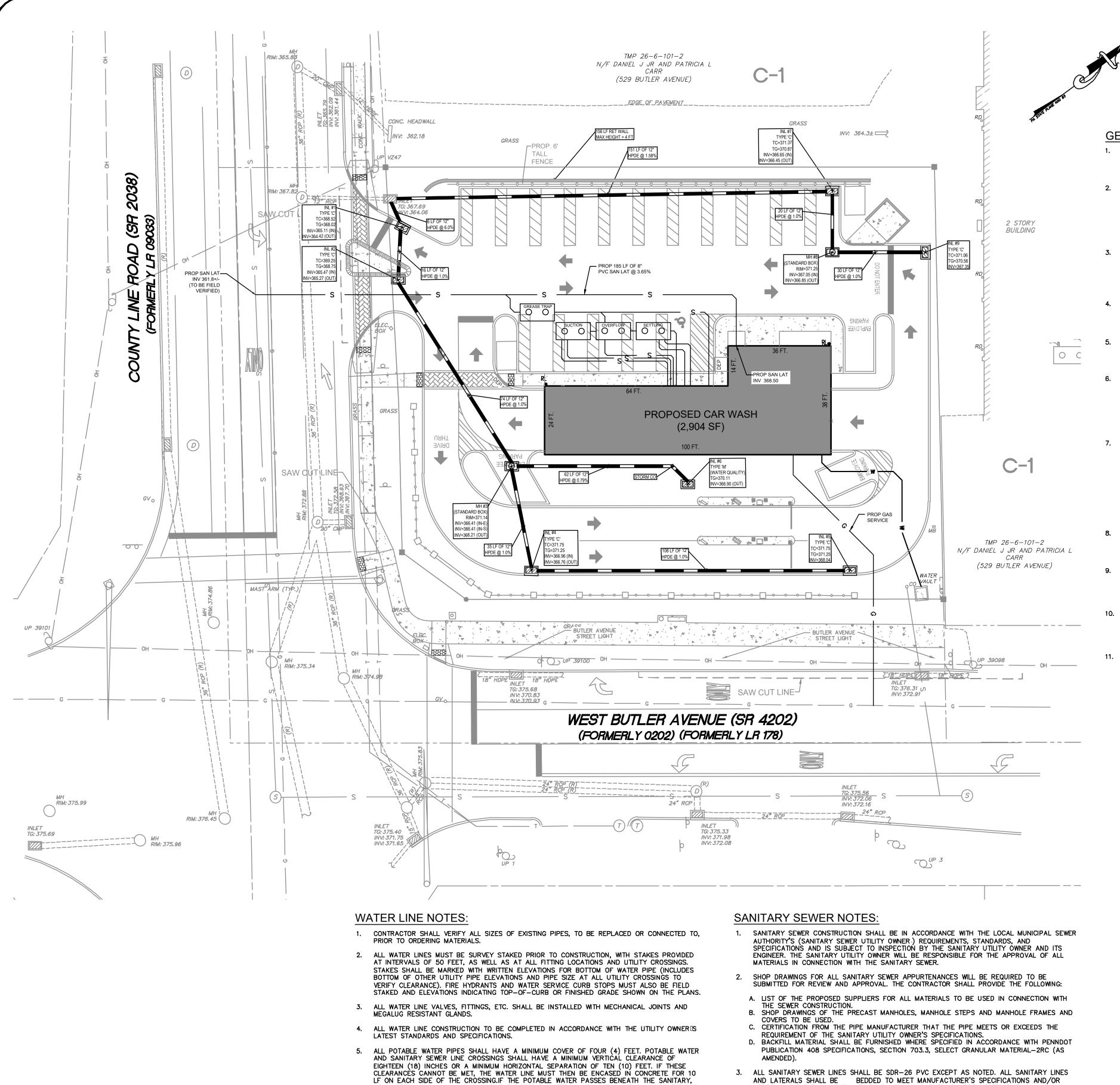
FOR

FLAGSHIP CARWASH

DESIGNED BY DRAWN BY CHECKED BY B NUMBER

TMP'S 26-006-101-001

**NEW BRTIAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 



- NO MATTER HOW MUCH CLEARANCE, THE POTABLE WATER MUST BE ENCASED IN CONCRETE FOR 10 LF ON EACH SIDE OF THE CROSSING.
- 6. ALL WATER LINES ARE TO BE PRESSURE TESTED TO 1.5 TIMES THE WORKING PRESSURE OR 150 P.S.I., WHICHEVER IS GREATER, IN SECTIONS BETWEEN VALVES. THE TEST PRESSURE SHALL BE MAINTAINED FOR A PERIOD NOT LESS THAN 2 HOURS. SHOULD ANY PRESSURE TEST DISCLOSE AN INABILITY TO HOLD THE TEST PRESSURE, THE CONTRACTOR SHALL LOCATE AND CORRECT DEFECTS AND RETEST TO THE SATISFACTION OF THE UTILITY OWNER.
- 7. THE INSTALLATION OR REPAIR OF ANY UNDERGROUND FACILITIES OR PIPING WHICH CONNECTS TO OR FURNISHES WATER FOR THE FIRE PROTECTION SPRINKLER SYSTEM SHALL BE PERFORMED ONLY BY A LICENSED UTILITY CONTRACTOR, FIRE PROTECTION SPRINKLER CONTRACTOR, OF LICENSED PLUMBER. A COPY OF THE LICENSE OR CERTIFICATE OF COMPETENCY SHALL BE PROVIDED TO THE INSPECTOR AT THE FINAL INSPECTION.

## STORM SEWER NOTES:

- ALL STORM CONVEYANCE PIPE SHALL BE SMOOTH LINED DOUBLE WALLED HIGH DENSITY POLYETHYLENE PIPE (HDPE), UNLESS OTHERWISE SPECIFIED. ANY ROOF LEADERS SHALL BE
- 2. IF A CONFLICT ARISES DURING THE INSTALLATION OF ANY PART OF THE STORM SEWER SYSTEM THE ENGINEER IS TO BE NOTIFIED IMMEDIATELY IN WRITING.
- 3. LANDSCAPING, FENCES AND STRUCTURES SHALL BE PLACED A MINIMUM OF 3 FEET AWAY FROM STORM SEWERS OUTSIDE OF THE RIGHT OF WAY OF STREETS.

- APPROVAL OF THE SANITARY UTILITY OWNER, WHICHEVER IS MORE RESTRICTIVE.
- 4. CONTRACTOR TO ENSURE THAT ALL SANITARY MAINS AND LATERALS HAVE A MINIMUM OF 18-INCH VERTICAL CLEARANCE OR 10 FT HORIZONTAL CLEARANCE BETWEEN ANY OTHER UTILITY CROSSING. OTHERWISE CONTRACTOR SHALL ENCASE MAIN OR LATERAL IN CONCRETE WITH A MINIMUM OF 10 LF ON EACH SIDE OF THE CROSSING. CLEANOUTS ARE TO BE PLACED AT MAXIMUM INTERVALS OF 100-FT AND AT ALL BENDS.
- 5. ALL SANITARY SEWER MANHOLES PROPOSED TO BE INSTALLED OR ADJUSTED SHALL BE INSTALLED WITH WATERTIGHT FRAMES AND COVERS. ANY MANHOLE TO BE INSTALLED OR ADJUSTED OUTSIDE OF PAVED AREAS SHALL BE INSTALLED A MINIMUM OF 6. ABOVE FINISH

6. CONTRACTOR SHALL VERIFY ALL SIZES, MATERIALS, AND CONDITION OF EXISTING PIPES, TO BE

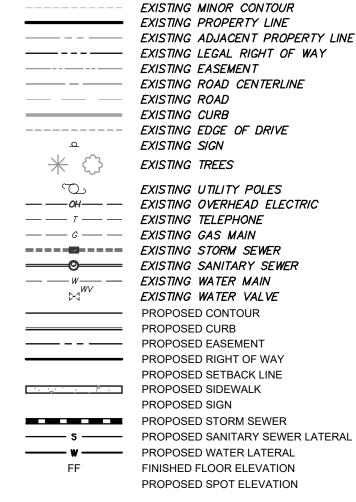
- REPLACED OR CONNECTED TO, PRIOR TO ORDERING MATERIALS.
- 7. ALL EXISTING MANHOLES SHALL BE TESTED PER SANITARY UTILITY OWNER STANDARDS TO ASSURE NO LEAKS EXIST WITHIN THE STRUCTURES.
- 8. ALL PROPOSED SANITARY SEWER FACILITIES WILL BE OWNED AND PERPETUALLY MAINTAINED BY THE PROPERTY OWNER IN PERPETUITY.
- 9. ALL MATERIALS USED, AND DETAILS OF CONSTRUCTION, INSTALLATION PROCEDURES, AND REQUIREMENTS WITH RESPECT TO THE SANITARY SEWERAGE FACILITIES SERVED BY THE LOCAL MUNICIPAL SEWER AUTHORITY'S (SANITARY SEWER UTILITY OWNER) SHALL BE IN ACCORDANCE WITH THE AUTHORITY'S STANDARD SPECIFICATIONS FOR CONSTRUCTION OF SANITARY SEWERS, FORCE MAINS. PUMPING STATIONS AND WASTEWATER TREATMENT.

#### **GENERAL UTILITIES NOTES:**

- 1. ALL NEW UTILITIES/SERVICES, INCLUDING ELECTRIC, TELEPHONE, CABLE TV, ETC. ARE TO BE INSTALLED UNDERGROUND. ALL NEW UTILITIES/SERVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE UTILITY/SERVICE PROVIDER INSTALLATION SPECIFICATIONS
- 2. THE LOCATION OF ALL EXISTING AND PROPOSED SERVICES ARE APPROXIMATE. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING WITH LOCAL UTILITY COMPANIES TO LOCATE ALL PUBLIC OR PRIVATE UTILITIES WHICH LIE IN OR ADJACENT TO THE CONSTRUCTION SITE PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION, REMOVAL OR EXCAVATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING, AT HIS EXPENSE, ALL EXISTING UTILITIES DAMAGED DURING CONSTRUCTION. SANITARY SEWER AND ALL OTHER UTILITY SERVICE CONNECTION POINT CONFLICTS SHALL BE REPORTED IMMEDIATELY IN WRITING TO THE ENGINEER.
- 3. ALL UTILITY EXTENSIONS FROM THE BUILDING BY OTHER TRADES SHALL I CONNECTED TO THE CONTINUATION UTILITY LINES ON THE SITE. THE GENERAL CONTRACTOR SHALL MAKE THE CONNECTIONS AT A POINT 5 FROM THE BUILDING WALL WHERE OTHER TRADES HAVE ENDED THEIR WORK. THE GC SHALL COORDINATE SCHEDULING OF THESE CONNECTIONS WITH OTHER TRADES TO ASSURE THE SYSTEM IS COMPLETE WHEN FLOW WILL BEGIN IN THE LINES.
- 4. CONSTRUCTION SHALL COMMENCE BEGINNING AT THE LOWEST INVERT (POINT OF CONNECTION) AND PROGRESS UP GRADIENT. PROPOSED INTERFACE POINTS (CROSSINGS) WITH EXISTING UNDERGROUND UTILITIES SHALL BE FIELD VERIFIED BY TEST PIT PRIOR TO COMMENCEMENT OF CONSTRUCTION.
- DEFINE AND LOCATE VERTICALLY AND HORIZONTALLY ALL ACTIVE UTILITY AND/OR SERVICE SYSTEMS THAT ARE NOT TO REMAIN. THE CONTRACTOR IS RESPONSIBLE TO PROTECT AND MAINTAIN ALL ACTIVE SYSTEMS THAT ARE NOT BEING REMOVED/RELOCATED DURING SITE ACTIVITY.
- 6. THE CONTRACTOR SHALL FAMILIARIZE THEMSELVES WITH THE APPLICABLE UTILITY SERVICE PROVIDER REQUIREMENTS AND IS RESPONSIBLE FOR COMPLIANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS, CODES AND REGULATIONS.THE CONTRACTOR IS ALSO RESPONSIBLE FOR ALL COORDINATION REGARDING UTILITY WORK AS IDENTIFIED OR REQUIRED FOR PROJECT. THE CONTRACTOR SHALL PROVIDE THE OWNER WRITTEN NOTIFICATIONS THAT THE EXISTING UTILITIES AND SERVICES HAVE BEEN TERMINATED AND ABANDONED IN ACCORDANCE WITH JURISDICTION AND UTILITY COMPANY
- 7. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATION OF SITE PLAN DOCUMENTS AND ARCHITECTURAL DESIGN FOR EXACT BUILDING UTILITY CONNECTION LOCATIONS, DOOR ACCESS, AND EXTERIOR GRADING. (THE UTILITY SERVICE SIZES ARE TO BE DETERMINED BY ARCHITECT.) THE CONTRACTOR SHALL COORDINATE INSTALLATION OF UTILITIES/SERVICES WITH THE INDIVIDUAL COMPANIES TO AVOID CONFLICTS AND ENSURE PROPER DEPTHS ARE ACHIEVED. IN ADDITION TO MEETING THE UTILITY OWNER REQUIREMENTS. THE UTILITY TIE-INS/CONNECTIONS SHALL BE COORDINATED. THIS SHALL BE COMPLETED PRIOR TO CONNECTING TO THE EXISTING UTILITY/SERVICE. WHERE CONFLICTS EXIST WITH THESE PLANS, THE ENGINEER OF RECORD SHALL BE NOTIFIED IN WRITING TO RESOLVE SAID CONFLICTS PRIOR TO
- 8. THE TOPS OF EXISTING MANHOLES, INLET STRUCTURES, AND SANITARY CLEANOUT TOPS SHALL BE ADJUSTED, IF REQUIRED, TO MATCH THE GRADING AND UTILITY PLAN(S). THE SANITARY CLEANOUT TOPS SHALL BE REINSTALLED IN THE SAME MANNER AS THE ORIGINAL CLEANOUTS.
- CONTRACTOR SHALL CONTACT THE LOCAL (I.E. TOWNSHIP, BOROUGH, ETC.) CODE ENFORCEMENT OFFICE WHEN UNDERGROUND PIPING IS BEING HYDROSTATICALLY TESTED ON SITE. APPLICANTS ARE ALSO REMINDED THAT FLUSHING OF THE UNDERGROUND PIPING SHALL BE WITNESSED BY A TOWNSHIP OFFICIAL PRIOR TO FINAL RISER CONNECTIONS, PER NFPA 13.
- UNDERGROUND UTILITY MARKOUTS BY THE UTILITY COMPANIES WERE ORDERED AND THE PHYSICAL LOCATION OF THESE MARKOUTS ARE SHOWN HEREON, HOWEVER, NO CERTIFICATION IS MADE BY VCEA AS TO THE ACTUAL UNDERGROUND POSITION OF ANY UTILITIES OR TO THE COMPLETENESS OR ACCURACY OF THE UTILITY MARKOUTS
- 11. IT IS IMPERATIVE THAT UTILITY COMPANIES ARE NOTIFIED PRIOR TO ANY EXCAVATION AND/OR CONSTRUCTION. CALL 1-800-242-1776 TO ORDER UTILITY MARKOUTS BY OTHERS.

## **LEGEND**

---- EXISTING MAJOR CONTOUR



- THE CONTRACTOR, DURING THE PERFORMANCE OF ALL WORK ASSOCIATED WITH THE CONSTRUCTION OF THE PROJECT, IS RESPONSIBLE FOR COMPLIANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS, CODES AND REGULATIONS.
- 13. ANY DISCREPANCIES FOUND BETWEEN THE DRAWINGS AND SITE CONDITIONS OR ANY INCONSISTENCIES OR AMBIGUITIES IN DRAWINGS SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER, IN WRITING, WHO SHALL PROMPTLY ADDRESS SUCH INCONSISTENCIES OR AMBIGUITIES. WORK DONE BY THE CONTRACTOR AFTER HIS DISCOVERY OF SUCH DISCREPANCIES, INCONSISTENCIES OR AMBIGUITIES SHALL BE DONE AT THE CONTRACTOR'S RISK.
- 14. THE CHALFONT NEW BRITAIN JOINT SEWER AUTHORITY IS TO BE CONTACTED ONE WEEK PRIOR TO SANITARY SEWER CONSTRUCTION, AND 72 HOURS PRIOR TO EXCAVATING NEAR, AND CONNECTING INTO ANY EXISTING SANITARY SEWER.
- 15. ALL SANITARY SEWER CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THE CHALFONT NEW BRITAIN JOINT SEWER AUTHORITY (CNBSA)
- a.) THE MINIMUM ALLOWABLE LATERAL SLOPE SHALL BE 0.02 FT/FT. b.) MAGNETIC - TYPE LOCATOR TAPE SHALL BE LAID DIRECTLY OVER THE TOP OF PVC PIPE, TWO FEET (2') BELOW GRADE.
- c.) CONTRACTOR SHALL MAKE TESTS AS DIRECTED BY THE ENGINEER TO ASCERTAIN IF THE PIPE IS PROPERLY ALIGNED AND THE JOINTS ARE LIGHT. ENGINEER MUST WITNESS ALL TESTS OR THE CONTRACTOR WILL NEED TO RETEST IN THE PRESENCE OF THE ENGINEER AT THE SOLE COST OF THE CONTRACTOR.
- CONTRACTOR TO CONNECT NEW BUILDING UTILITY CONNECTIONS TO EXISTING WATER AND SEWER.
- 17. FINAL DESIGN OF PROPOSED WATER AND SEWER FACILITIES ARE SUBJECT TO THE REVIEW AND APPROVAL BY THE SERVICING WATER AND SEWER AUTHORITY. ALL WATER MAIN CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH NORTH WALES WATER AUTHORITY SPECIFICATIONS.
- 18. LATERALS ARE TO BE LOCATED AS SHOWN ON THE PLAN.
- WATER SERVICE IS TO BE LOCATED A MIN. OF 1.5 FEET ABOVE THE SANITARY LATERAL WITH A MINIMUM COVER OF 48".
- NO GROUNDING OF ELECTRICAL SERVICE TO COPPER WATER SERVICE IS PERMITTED. CABLE AND TELECOMM UTILITIES TO FOLLOW ELECTRIC FACILITIES ALIGNMENTS,
- 18" VERTICAL CLEARANCE OR 10 FEET OF HORIZONTAL CLEARANCE SHALL BE PROVIDED BETWEEN THE SANITARY SEWER AND ALL OTHER UTILITIES. OTHERWISE, CONCRETE ENCASEMENT OF THE SANITARY SEWER SHALL BE PROVIDED.
- 22. ALL DOWNSPOUT LOCATIONS TO BE OUTFITTED WITH A OVERFLOW / CLEANOUT. OVERFLOWS SHALL HAVE A SPLASHBLOCK INSTALLED AT THEIR OUTFALL LOCATION.
- 23. PROPOSED ROOF DRAIN CONNECTIONS SHOWN SCHEMATICALLY, FINAL DESIGN TO BE PROVIDED BY ARCHITECT AT TIME OF BUILDING PERMITS.
- 24. MATERIALS SHOWN HEREON, SUCH AS ADS N-12 PIPE, OR APPROVED EQUAL SHALL
- 25. REFER TO ARCHITECTURAL, MECHANICAL, ELECTRICAL AND PLUMBING PLANS FOR ADDITIONAL COORDINATION W/ IN 5' OF BUILDINGS.
- 26. PUBLIC SEWER SERVICE WILL BE PROVIDED BY BCSWA, AND CONSTRUCTION OF ALL SANITARY SEWER IMPROVEMENTS SHALL COMPLY WITH AUTHORITY STANDARDS.
- 27. ALL BURIED GRAVITY SANITARY SEWER MAINS AND LATERALS SHALL BE CLASS 50 DUCTILE IRON PIPE OR PVC SDR-26 OR SDR-35.
- THRUST BLOCKS SHALL BE PROVIDED AT ALL BENDS AND TEES. 29. GENERAL CONTRACTOR (GC) TO COORDINATE LIGHT POLE LOCATIONS WITH TRADES.

## **GENERAL CONSTRUCTION NOTES**

1. NO CERTIFICATION IS MADE BY VAN CLEEF ENGINEERING ASSOCIATES AS TO THE POSITION OF ANY UTILITIES OR TO THE COMPLETENESS OF ANY UTILITIES SHOWN. ALL CONTRACTORS WORKING ON THIS SITE SHALL COMPLY WITH THE REQUIREMENTS OF ACT 287 OF 1974 AS AMENDED BY ACT 50 OF 2017 (UNDERGROUND UTILITY LINE PROTECTION LAW). VCEA HAS REGISTERED THIS SITE WITH THE PA ONE-CALL SYSTEM ON NOVEMBER 14, 2022 (SERIAL NO. 20223183581).

## THE FOLLOWING IS A LIST OF REGISTERED UTILITY USERS FOR NEW BRITAIN

COMPANY: CHALFONT NEW BRITAIN TWP JT SEWAGE AUTH ADDRESS: 1645 UPPER STATE RD CHALFONT, PA. 18914 CONTACT: RYAN CRESSMAN OYLESTOWN, PA. 18901 CONTACT: JOHN SCHMID EMAIL: JSCHMIDT@CNBSA.ORG EMAIL: RCRESSMAN@NEWBRITAINTOWNSHIP.ORG COMPANY: COMCAST COMPANY: PECO AN EXELON COMPANY C/O USIC ADDRESS: 190 SHOEMAKER RD POTTSTOWN, PA. 19464 ADDRESS: 450 S HENDERSON ROAD SUITE B KING OF PRUSSIA, PA. 19406 CONTACT: MIKE KIMBERLY EMAIL: MIKE\_KIMBERLY@CABLE.COMCAST.COM FMAIL: NIKKIASIMPKINS@USICLI.C.COM COMPANY: VERIZON BUSINESS FORMERLY MCI COMPANY: NORTH WALES WATER AUTHORITY ADDRESS: 200 W WALNUT ST ADDRESS: 7000 WESTON PKWY PO BOX 1339 NORTH WALES, PA. 19454

EMAIL: VICTOR.S.WOOD@VERIZON.COM

CONTACT: JOSEPH MURPHY
EMAIL: JMURPHY@NWWATER.COM 2. IT IS IMPERATIVE THAT UTILITY COMPANIES ARE NOTIFIED PRIOR TO ANY EXCAVATION AND/OR CONSTRUCTION. CALL 1-800-242-1776 TO ORDER UTILITY MARKOUTS BY OTHERS.

- 3. THE LOCATIONS OF EXISTING UTILITIES AS SHOWN ARE APPROXIMATE ONLY. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL PUBLIC OR PRIVATE UTILITIES WHICH LIE IN OR ADJACENT TO THE CONSTRUCTION SITE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING, AT HIS EXPENSE, ALL EXISTING UTILITIES DAMAGED DURING CONSTRUCTION.
- 4. THE CONTRACTOR, DURING THE PERFORMANCE OF ALL WORK ASSOCIATED WITH THE CONSTRUCTION OF THE PROJECT, IS RESPONSIBLE FOR COMPLIANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS, CODES AND REGULATIONS.
- 5. ANY DISCREPANCIES FOUND BETWEEN THE DRAWINGS AND SITE CONDITIONS OR ANY INCONSISTENCIES OR AMBIGUITIES IN DRAWINGS SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER, IN WRITING, WHO SHALL PROMPTLY ADDRESS SUCH INCONSISTENCIES OR AMBIGUITIES. WORK DONE BY THE CONTRACTOR AFTER HIS DISCOVERY OF SUCH DISCREPANCIES, INCONSISTENCIES OR AMBIGUITIES SHALL BE DONE AT THE CONTRACTOR'S RISK.
- 6. ALL MATERIAL USED FOR FILL OR BACKFILL SHALL BE FREE OF WOOD, ROOTS, ROCKS, BOULDERS OR ANY OTHER NON-COMPACTIBLE SOIL TYPE MATERIAL. UNSATISFACTORY MATERIALS ALSO INCLUDE MAN-MADE FILLS AND REFUSE DEBRIS DERIVED FROM ANY SOURCE.
- 7. ALL EXCAVATIONS, INCLUDING TRENCHES, SHALL BE KEPT DRY TO PROTECT THEIR INTEGRITY.
- 8. MATERIALS USED TO CONSTRUCT EMBANKMENTS FOR ANY PURPOSE, BACKFILL AROUND DRAINAGE STRUCTURES OR IN UTILITY TRENCHES OR ANY OTHER DEPRESSIONS REQUIRING FILL OR BACKFILL SHALL BE COMPACTED TO AT LEAST 95% OF MAXIMUM DENSITY AS DETERMINED BY THE STANDARD PROCTOR TEST AS SET OUT IN ASTM STANDARD D-698 UNLESS SPECIFIED ELSEWHERE WITHIN THE CONSTRUCTION PLANS OR IF SPECIFIED DIFFERENTLY BY THE TOWNSHIP ENGINEER. CONTRACTOR TO VERIFY COMPACTION REQUIREMENTS WITH TOWNSHIP ENGINEER'S OFFICE PRIOR TO EXCAVATION.
- 9. ALL CONSTRUCTION DEBRIS INCLUDING EXCESS EXCAVATED MATERIAL, SCRAP WOOD, BRICKS, BLOCKS, ETC. SHALL BE DISPOSED OF BY THE CONTRACTOR IN ACCORDANCE WITH FEDERAL, STATE AND LOCAL REQUIREMENTS.
- 10. ELEVATIONS, DIMENSIONS, AND THE LOCATIONS OF LINEAR FEATURES SHALL BE FIELD VERIFIED BY THE CONTRACTOR.
- 11. MINIMUM SLOPE OF ALL GRASS AREAS SHALL NOT BE LESS THAN 2%, AND FOR PAVED AREAS, NOT LESS THAN 1%.



Before You Dig Anywhere



PA ONE CALL SYSTEM, INC. **GRAPHIC SCALE** 10 20

1 INCH = 20 FT

PLAN NOTATION ONLY THOSE PLANS WHICH CONTAIN A DIGIT. MPRESSED, OR COLORIZED INK SEAL OF T RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERED VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARED FOR THE OWNER DESIGNATED HEREON. A MODIFICATION, REVISION, DUPLICATION OR U WITHOUT THE WRITTEN CONSENT OF VAN CLE ENGINEERING ASSOCIATES IS PROHIBITE RELIANCE ON THIS PLAN FOR ANY PURPO OTHER THAN THAT WHICH IS INTENDED SHALL AT THE SOLE DISCRETION AND LIABILITY OF



AN DAVID McGRANAHAN IEER NUMBER 085090



VAN CLEEF ENGINEERING ASSOCIATES, LLC 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM PHONE (215) 345-1876

UTILITY PLAN

FOR

FLAGSHIP CARWASH

DESIGNED BY LDM, SDN

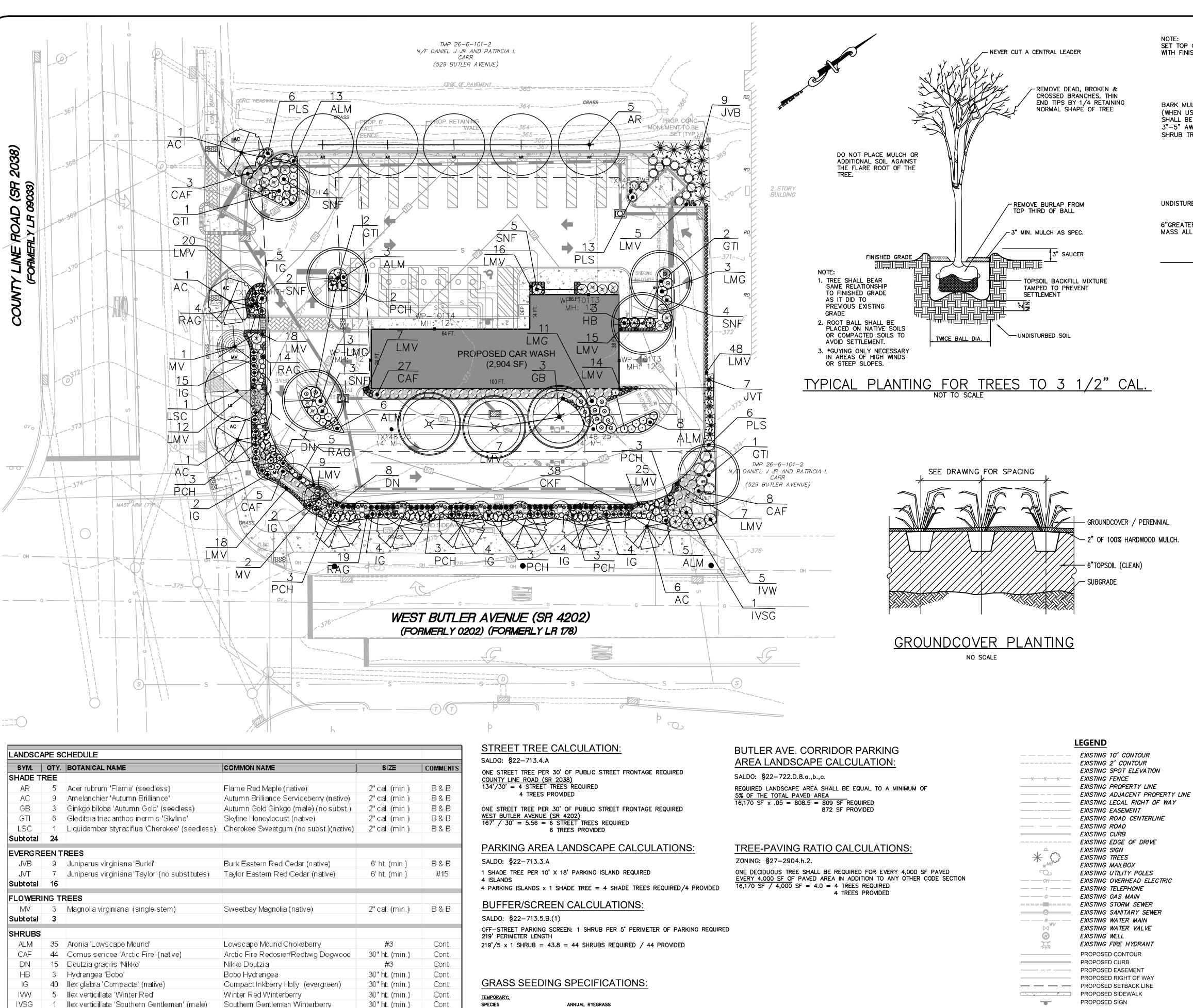
RAWN BY

CHECKED BY

B NUMBER

TMP'S 26-006-101-001

**NEW BRTIAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 



% PURE LIVE SEED

FERTILIZER TYPE

MULCH TYPE

MULCH RATE

PERMANENT:

MULCH TYPE

MULCH RATE

% pure live seed Application rate Fertilizer type

ANCHOR MATERIAL

ANCHORING METHOD

RATE OF ANCHOR MATERIAL APPL. N/A

FERTILIZER APPL, RATE

SPECIES

APPLICATION RATE

FERTILIZER APPL, RATE

87.0 LB/ACRE

1,100 LB/ACRE

STRAW

3 T/ACRE

RED FESCUE

1,100 LB/ACRE

2 T/ACRE HAY OR STRAW

3 T/ACRE

COMMERCIAL 10-20-10

41 / 20 / 14 / 20 LB/ACRE

COMMERCIAL 10-20-10

FESCUE / PERENNIAL RYEGRASS / CHEWNG'S

APRIL 15 - MAY 30, AUGUST 15 - OCTOBER 15

Cont.

Cont.

Cont.

Cont.

24" o.c

18" o.c

24" o.c

- 30" ht. (min.)

30" ht. (min.)

30" ht. (min.)

2 Gal.

1 Gal.

1 Gal.

Moss Green Prostrate Honeysuckle

|Schip / Skip Laurel (evergreen)

Karl Foerster Feather Reed Grass

Amer. Gold Rush Black-Eyed Susan

|Variegated Liriope\_(evergreen)

Neon Flash Spirea

|Chestnut Hill Cherry Laurel (evergreen) |

LMG

SNF

Subtotal 224

PERENNIALS

Subtotal 302

TOTAL 569

CKF

RAG

17 Lonicera pliata 'Moss Green'

= 38 |Calamagrostis 'Karl Foerster'

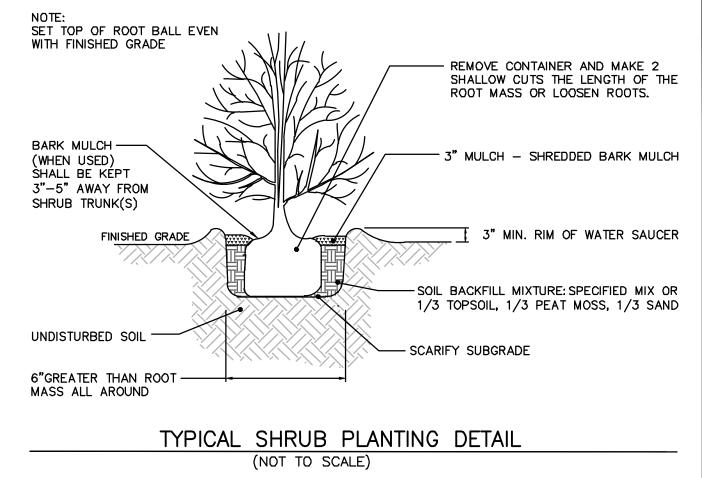
221 | Liriope muscari 'Variegata' |

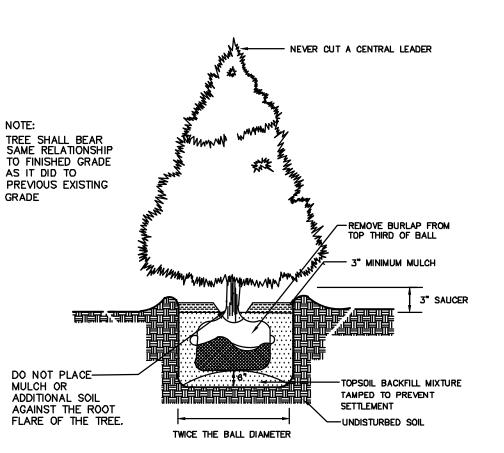
18 Spirea 'Neon Flash'

20 Prunus laurocerasus 'Chestnut Hill'

26 Prunus laurocerasus 'Schipkaensis'

43 Rudbeckia fulgida 'American Gold Rush'





EVERGREEN PLANTING DETAIL (NOT TO SCALE)

## GENERAL LANDSCAPING NOTES

- . ALL PLANTS SHALL BE TYPICAL OF THEIR SPECIES OR VARIETY: AND SHALL HAVE NORMA WELL-DEVELOPED BRANCHES AND MGOROUS FIBROUS ROOT SYSTEMS. ALL PLANTS SHALL BE NURSERY-GROWN UNLESS OTHERWISE STATED; THEY SHALL HAVE BEEN GROWING UNDER T SAME CLIMATE CONDITIONS AS THE MUNICIPALITY FOR AT LEAST TWO (2) YEARS PRIOR TO DATE OF PLANTING. ALL PLANTS WHICH ARE FOUND UNSUITABLE IN GROWTH OR CONDITION OR WHICH ARE NOT TRUE TO NAME SHALL BE REMOVED AND REPLACED WITH ACCEPTABLE PLANTS.
  ALL PLANT MATERIAL SHALL BE TWICE TRANSPLANTED, NURSERY-GROWN OF SPECIMEN QUALITY.
- 2. ALL PLANT MATERIAL SHALL BE TWICE TRANSPLANTED, NURSERT—GROWN OF SPECIMEN QUALITY.
  THEY SHALL BE OF SYMMETRICAL GROWTH OR TYPICAL OF THE VARIETY AND SUPPLIED FROM
  SOURCES IN THE SAME HARDINESS ZONE AS THE DEVELOPMENT IS LOCATED AND FREE OF INSECT
  AND DISEASE PROBLEMS OR OBJECTIONABLE DISFIGUREMENTS. ALL PLANT MATERIAL SHALL
  CONFORM TO THE STANDARDS OF THE AMERICAN ASSOCIATION OF NURSERYMEN.

  3. ALL PRECAUTIONS CUSTOMARY IN GOOD TRADE PRACTICE SHALL BE TAKEN IN PREPARING PLANTS
  FOR MOVING. ALL BALLED AND BURLAPPED PLANTS SHALL BE DUG TO MEET OR EXCEED THE "USDA STANDARDS FOR NURSERY STOCK".
  4. ALL PLANT MATERIAL SHALL MEET THE STANDARDS OF AMERICAN STANDARD FOR NURSERY
- STOCK BY THE AMERICAN ASSOCIATION OF NURSERYMEN (1994), OR MOST RECENT EDITION, AND THE HEIGHT, SPREAD AND/OR CALIPER FOR TREES AND SHRUBS 5. ALL PLANT MATERIAL SHALL BE INSTALLED IN ACCORDANCE WITH THE PLANTING PRACTICES STATED IN CHAPTER 3 OF TREE MAINTENANCE BY P. P. PIRONE, FIFTH OR MOST RECENT EDITION. 6. PLANTS SHALL BE PACKED, TRANSPORTED AND HANDLED WITH UTMOST CARE TO INSURE
- ADEQUATE PROTECTION AGAINST INJURY. 7. ALL PLANT MATERIALS SHALL BE QUARANTEED FOR A PERIOD OF 18 MONTHS FOLLOWING FINAL APPROVAL BY THE TOWNSHIP, EXCEPT WHERE OTHERWISE NOTED IN THIS SECTION. ANY PLANT MATERIAL THAT HAS 25% OR GREATER DEAD BRANCHES SHALL BE CONSIDERED DEAD. A TREE SHALL BE CONSIDERED DEAD WHEN THE MAIN LEADER HAS DIED OR 25% OF THE CROWN IS DEAD. ANY DEAD MATERIAL SHALL BE REPLACED AND INSTALLED ACCORDING TO APPROVED PLANTING
- 8. MAINTAIN A MINIMUM FIFTEEN (15) FOOT SEPARATION DISTANCE BETWEEN TREES AND UNDERGROUND UTILITIES. THIS MINIMUM MAY BE REDUCED TO TEN (10) FEET IN CERTAIN CIRCUMSTANCES WITH APPROVAL OF THE TOWNSHIP ENGINEER. HOWEVER, DURING CONSTRUCTION, IF IT IS DEEMED NECESSARY TO FIELD ADJUST THE LOCATIONS OF ANY PLANTINGS AND/OR UTILITY CONNECTIONS DUE TO THE NEED TO PROVIDE THE MINIMUM TEN FOOT SEPARATION BETWEEN THOSE PLAN ELEMENTS, THIS ADJUSTMENT SHOULD BE UNDERTAKEN IN CONSULTATION WITH NEW BRITAIN TOWNSHIP STAFF/PROFESSIONALS. 9. THE LOCATIONS OF ALL PLANT MATERIAL INDICATED ON THE LANDSCAPE PLANS ARE
- APPROXIMATE. THE FINAL LOCATION OF ALL PLANT MATERIAL AND PLANTING BED LINES SHALL BE DETERMINED IN THE FIELD UNDER THE DIRECTION OF THE LANDSCAPE ARCHITECT.

  10. THE PLANTING PLAN SHALL TAKE PRECEDENCE OVER THE PLANT SCHEDULE SHOULD ANY PLANT QUANTITY DISCREPANCIES OCCUR. 11. ALL SHADE TREES PLANTED NEAR PEDESTRIAN DR VEHICULAR ACCESS SHOULD BE CLEAR TRUNK

UP TO 7'-0" ABOVE GRADE. ALL SHRUBBERY MATERIAL LOCATED WITHIN SIGHT TRIANGLES

SHALL NOT EXCEED A MATURE HEIGHT OF 3' ABOVE THE ELEVATION OF THE ADJACENT CURB.

- ALL SHADE TREES PLANTED OR EXISTING IN SIGHT TRIANGLES SHALL BE PRUNED SO AS NOT TO HAVE BRANCHES BELOW 7'-0". 12. ALL PLANT MATERIAL SHALL BE PROPERLY GUYED, STAKED, WRAPPED AND PLANTED IN CONFORMANCE WITH THE TYPICAL PLANTING DETAILS. GUY WIRES SHALL BE ATTACHED TO THE TREE AT TWO—THIRDS THE HEIGHT OF THE TREE AND SHOULD BE LOCATED AT POINTS SO AS NOT TO SPLIT THE TRUNKS OF MULTI—STEMMED TREES. PROVIDE TWO TO THREE TREE STAKES PER TREE AS NOTED ON THE PLANS. INSTALL ALL PLANT MATERIAL ON UNDISTURBED GRADE. PROVIDE BURLAP WRAPPING WITH A 50% OVERLAP. CUT AND REMOVE BURLAP FROM TOP
- ONE—THIRD OF THE ROOT BALL.

  13. PROVIDE PLANTING PITS AS INDICATED ON PLANTING DETAILS. BACKFILL PLANTING PITS WITH ONE PART EACH OF TOPSOIL, PEAT MOSS AND PARENT MATERIAL. IF WET OR CLAY SOIL CONDITIONS EXIST, INSTALL A 4" PERFORATED DRAIN. RUN TO GRADE OR CATCH BASIN.
- . PLANT MATERIAL SHALL BEAR THE SAME RELATION TO FINISHED GRADE AS IT DID TO FXISTING GRADE. 15. NEWLY INSTALLED PLANT MATERIAL SHALL BE WATERED AT THE TIME OF INSTALLATION. REGULAR WATERING ALL PLANT MATERIAL SHALL BE PROVIDED TO ENSURE THE ESTABLISHMENT, GROWTH
- AND SURVIVAL OF ALL PLANTS.

  16. ALL DISTURBED LAWN AREAS SHALL BE STABILIZED WITH SEED AS INDICATED ON THE LANDSCAPE PLANS. SEED MIXTURE SHALL BE AS LISTED IN THE SEEDING SCHEDULE ON THE SOIL EROSION AND SEDIMENT CONTROL NARRATIVE. ALL DISTURBED LAWN AREAS SHALL BE TOPSOILED, LIMED,
- FERTILIZED AND FINE GRADED PRIOR TO LAWN INSTALLATION. 17. ALL PLANTING BEDS SHALL RECEIVE MINIMUM 3" OF SHREDDED HARDWOOD BARK.

  18. ALL SHRUB MASSES SHALL BE PLANTED IN CONTINUOUS MULCHED BEDS.

  19. ALL EXISTING TREES AND SHRUBS TO BE PRESERVED ON SITE SHALL BE PROTECTED AGAINST CONSTRUCTION DAMAGE BY SNOW FENCING. ALL FENCING SHALL BE PLACED A MINIMUM OF ONE
- (1) FOOT OUTSIDE THE INDIVIDUAL TREE DRIPLINE, OR AT THE SPECIFIC LOCATION DETERMINED APPROPRIATE BY THE LANDSCAPE ARCHITECT. ALL TREES TO REMAIN SHALL BE IDENTIFIED IN THE FIELD PRIOR TO COMMENCEMENT OF CONSTRUCTION, GRADING OR CLEARING. ALL EXISTING VEGETATION BEING PRESERVED AND LOCATED AT THE EDGE OF THE NEW TREELINE SHALL BE PRUNED AND TRIMMED TO REMOVE DEAD OR DAMAGED BRANCHES.

  20. THE QUANTITIES SHOWN ON THE PLAN SHALL HAVE PRECEDENCE OVER THE PLANT SCHEDULE IN THE EVENT OF ANY QUANTITY DISCREPANCIES.
- 21. A CLEAR—SIGHT TRIANGLE OF TEN FEET (10') MEASURED FROM THE POINT OF INTERSECTION OF THE STREET LINE AND THE EDGE OF THE ACCESSWAY SHALL BE MAINTAINED, WITHIN WHICH VEGETATION AND OTHER VISUAL OBSTRUCTIONS SHALL BE LIMITED TO A HEIGHT OF NOT MORE THAN TWO FEET (2') ABOVE THE STREET GRADE.

PROPOSED WATER LATERAL PROPOSED LIGHT

PROPOSED STORM SEWER

PROPOSED LANDSCAPING

PROPOSED SANITARY SEWER LATERAL

THIS PLAN IS FOR LANDSCAPE PURPOSES ONLY

**BUCKS COUNTY, PENNSYLVANIA** 

7 of 16

DESIGNED BY

HECKED BY

B NUMBER

RAWN BY

EV DESCRIPTION

**SERIAL NO. 20223183581** 

now what's below. STOP! CALL 1-800-242-1776

**GRAPHIC SCALE** 

1 INCH = 20 FT

ONLY THOSE PLANS WHICH CONTAIN A DIGIT. IMPRESSED, OR COLORIZED INK SEAL OF T

RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERED VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPAREI FOR THE OWNER DESIGNATED HEREON. ANY MODIFICATION, REVISION, DUPLICATION OR USE WITHOUT THE WRITTEN CONSENT OF VAN CLEEF

ENGINEERING ASSOCIATES IS PROHIBITE RELIANCE ON THIS PLAN FOR ANY PURPO

OTHER THAN THAT WHICH IS INTENDED SHALL AT THE SOLE DISCRETION AND LIABILITY OF

PROFESSIONAL

\ FNGINFFR

**VAN CLEEF ENGINEERING ASSOCIATES, LLC** 

501 NORTH MAIN STREET, DOYLESTOWN, PA 18901

WEB: WWW.VANCLEEFENGINEERING.COM PHONE (215) 345-1876

LANDSCAPE

**PLAN** 

FOR

FLAGSHIP CARWASH

TMP'S 26-006-101-001

**NEW BRTIAIN TOWNSHIP** 

SEAN DAVID MCGRAN

AN DAVID McGRANAHAN

INSYLVANIA PROFESSIONAL GINEER NUMBER 085090

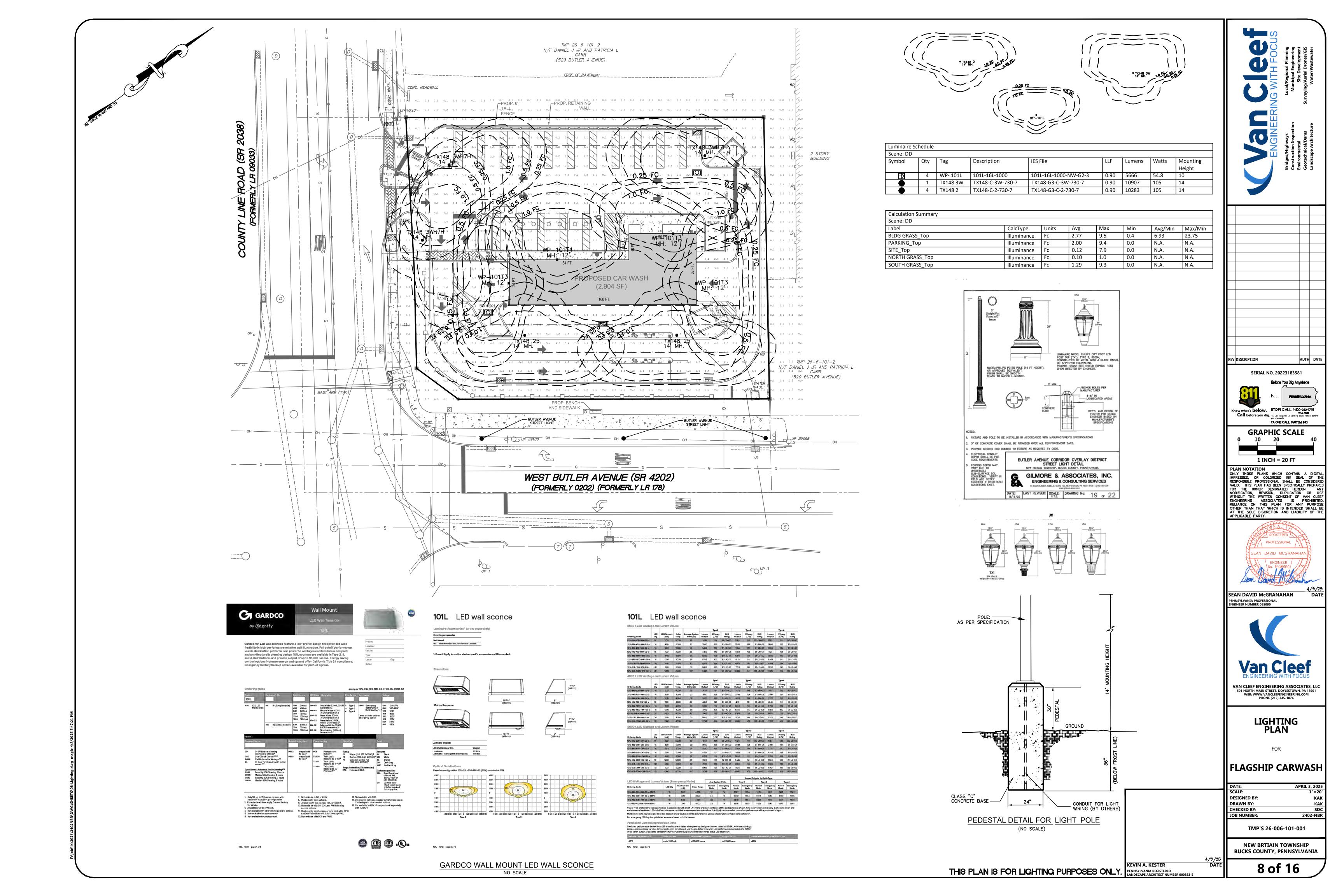
PA ONE CALL SYSTEM, INC.

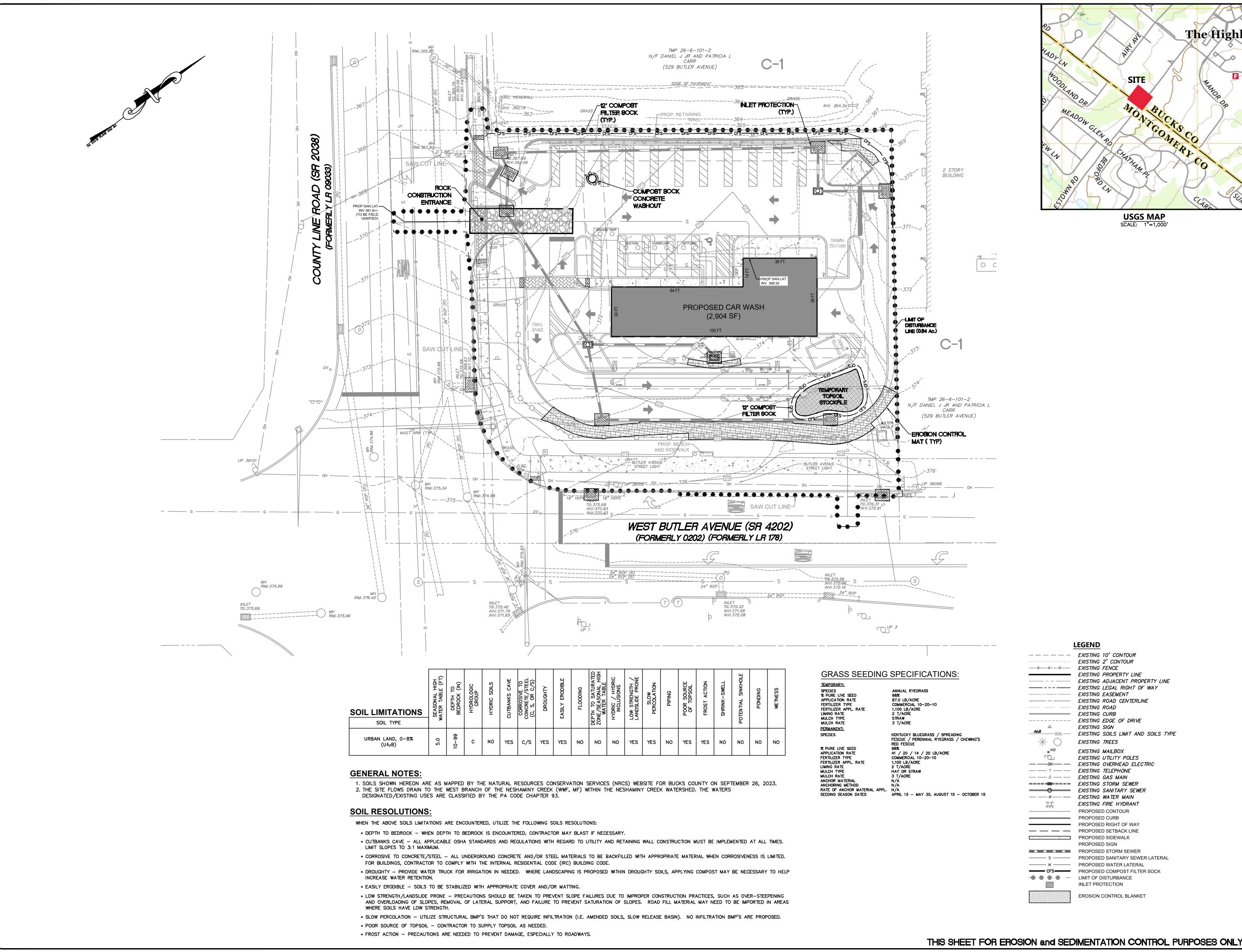
Call before you dig. PA Law requires 3 working days no you excavate

10 20

PLAN NOTATION

PPLICABLE PARTY.





REV DESCRIPTION SERIAL NO. 20223183581

(now what's below. STOP! CALL 1-800-242-1778 Call before you dig. PA Law requires 3 working days notice you exceed to PA ONE CALL SYSTEM, INC. **GRAPHIC SCALE** 

1 INCH = 20 FT

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EAN DAVID McGRANAHAN INSYLVANIA PROFESSIONAL GINEER NUMBER 085090



VAN CLEEF ENGINEERING ASSOCIATES, LLC 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM PHONE (215) 345-1876

**EROSION & SEDIMENTATION CONTROL PLAN** 

**FLAGSHIP CARWASH** 

DRAWN BY:

TMP'S 26-006-101-001

**NEW BRTIAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 

CHECKED BY

DB NUMBER:

### GENERAL EROSION & SEDIMENT CONTROL NOTES

- 1. ALL EARTH DISTURBANCES, INCLUDING CLEARING AND GRUBBING AS WELL AS CUTS AND FILLS SHALL BE DONE IN ACCORDANCE WITH THE APPROVED E&S PLAN. A COPY OF THE APPROVED DRAWINGS (STAMPED, SIGNED AND DATED BY THE REVIEWING AGENCY) MUST BE AVAILABLE AT THE PROJECT SITE AT ALL TIMES. THE REVIEWING AGENCY SHALL BE NOTIFIED OF ANY CHANGES TO THE APPROVED PLAN PRIOR TO IMPLEMENTATION OF THOSE CHANGES. THE REVIEWING AGENCY MAY REQUIRE A WRITTEN SUBMITTAL OF THOSE CHANGES FOR REVIEW AND APPROVAL AT ITS DISCRETION.
- 2. AT LEAST 7 DAYS PRIOR TO STARTING ANY EARTH DISTURBANCE ACTIVITIES, INCLUDING CLEARING AND GRUBBING, THE OWNER AND/OR OPERATOR SHALL INVITE ALL CONTRACTORS, THE LANDOWNER, APPROPRIATE MUNICIPAL OFFICIALS, THE E&S PLAN PREPARER, THE PCSM PLAN PREPARER, THE LICENSED PROFESSIONAL RESPONSIBLE FOR OVERSIGHT OF CRITICAL STAGES OF IMPLEMENTATION OF THE PCSM PLAN, AND A REPRESENTATIVE FROM THE LOCAL CONSERVATION DISTRICT TO AN ON-SITE PRECONSTRUCTION MEETING.
- 3. AT LEAST 3 DAYS PRIOR TO STARTING ANY EARTH DISTURBANCE ACTIVITIES, OR EXPANDING INTO AN AREA PREVIOUSLY UNMARKED, THE PENNSYLVANIA ONE CALL SYSTEM INC. SHALL BE NOTIFIED AT 1-800-242-1776 FOR THE LOCATION OF EXISTING UNDERGROUND UTILITIES.
- 4. ALL EARTH DISTURBANCE ACTIVITIES SHALL PROCEED IN ACCORDANCE WITH THE SEQUENCE PROVIDED ON THE PLAN DRAWINGS. DEVIATION FROM THAT SEQUENCE MUST BE APPROVED IN WRITING FROM THE LOCAL CONSERVATION DISTRICT OR BY THE DEPARTMENT PRIOR TO IMPLEMENTATION.
- 5. AREAS TO BE FILLED ARE TO BE CLEARED, GRUBBED, AND STRIPPED OF TOPSOIL TO REMOVE TREES, VEGETATION, ROOTS AND OTHER OBJECTIONABLE MATERIAL.
- 6. CLEARING, GRUBBING, AND TOPSOIL STRIPPING SHALL BE LIMITED TO THOSE AREAS DESCRIBED IN EACH STAGE OF THE CONSTRUCTION SEQUENCE. GENERAL SITE CLEARING, GRUBBING AND TOPSOIL STRIPPING MAY NOT COMMENCE IN ANY STAGE OR PHASE OF THE PROJECT UNTIL THE E&S BMPS SPECIFIED BY THE BMP SEQUENCE FOR THAT STAGE OR PHASE HAVE BEEN INSTALLED AND ARE FUNCTIONING AS DESCRIBED IN THIS E&S
- 7. AT NO TIME SHALL CONSTRUCTION VEHICLES BE ALLOWED TO ENTER AREAS OUTSIDE THE LIMIT OF DISTURBANCE BOUNDARIES SHOWN ON THE PLAN MAPS. THESE AREAS MUST BE CLEARLY MARKED AND FENCED OFF BEFORE CLEARING AND GRUBBING OPERATIONS BEGIN.
- 8. TOPSOIL REQUIRED FOR THE ESTABLISHMENT OF VEGETATION SHALL BE STOCKPILED AT THE LOCATION(S) SHOWN ON THE PLAN MAPS(S) IN THE AMOUNT NECESSARY TO COMPLETE THE FINISH GRADING OF ALL EXPOSED AREAS THAT ARE TO BE STABILIZED BY VEGETATION. EACH STOCKPILE SHALL BE PROTECTED IN THE MANNER SHOWN ON THE PLAN DRAWINGS. STOCKPILE HEIGHTS SHALL NOT EXCEED 35 FEET. STOCKPILE SLOPES SHALL BE 2H: 1V OR FLATTER.
- 9. IMMEDIATELY UPON DISCOVERING UNFORESEEN CIRCUMSTANCES POSING THE POTENTIAL FOR ACCELERATED EROSION AND/OR SEDIMENT POLLUTION, THE OPERATOR SHALL IMPLEMENT APPROPRIATE BEST MANAGEMENT PRACTICES TO ELIMINATE POTENTIAL FOR EROSION AND SEDIMENT POLLUTION AND NOTIFY THE LOCAL CONSERVATION DISTRICT AND/OR THE REGIONAL OFFICE OF THE DEPARTMENT.
- 10. ALL BUILDING MATERIAL AND WASTES MUST BE REMOVED FROM THE SITE AND RECYCLED IN ACCORDANCE WITH DEP'S SOLID WASTE REGULATIONS (25 PA CODE 260.1 ET SEQ., 271.1 ET SEQ., AND 287.1 ET SEQ.), AND/OR ANY ADDITIONAL LOCAL, STATE OR FEDERAL REGULATIONS. NO BUILDING MATERIALS (USED OR UNUSED) OR WASTE MATERIALS SHALL BE BURNED, BURIED, DUMPED OR DISCHARGED AT THE SITE.
- 11. ALL OFF-SITE WASTE AND BORROW AREAS MUST HAVE AN E&S PLAN APPROVED BY THE LOCAL CONSERVATION DISTRICT OR THE DEPARTMENT FULLY IMPLEMENTED PRIOR TO BEING ACTIVATED.
- 12. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT ANY MATERIAL BROUGHT ON SITE IS CLEAN FILL, FORM FP-001 MUST BE RETAINED BY THE PROPERTY OWNER FOR ANY FILL MATERIAL AFFECTED BY A SPILL OR RELEASE OF A REGULATED SUBSTANCE BUT QUALIFYING AS CLEAN FILL
- 13. ALL PUMPING OF WATER FROM ANY WORK AREA SHALL BE DONE ACCORDING TO THE PROCEDURE DESCRIBED IN THIS PLAN, OVER UNDISTURBED VEGETATED AREAS.
- 14. UNTIL THE SITE IS STABILIZED, ALL EROSION AND SEDIMENT BMPS SHALL BE MAINTAINED PROPERLY. MAINTENANCE SHALL INCLUDE INSPECTIONS OF ALL EROSION AND SEDIMENT BMPS AFTER EACH RUNOFF EVENT AND ON A WEEKLY BASIS, ALL PREVENTATIVE AND REMEDIAL MAINTENANCE WORK, INCLUDING CLEAN OUT, REPAIR, REPLACEMENT, REGRADING, RESEEDING, REMULCHING AND RENETTING MUST BE PERFORMED IMMEDIATELY. IF THE E&S BMPS FAIL TO PERFORM AS EXPECTED, REPLACEMENT BMPS, OR MODIFICATIONS OF THOSE INSTALLED WILL BE REQUIRED.
- 15. A LOG SHOWING DATES THAT E&S BMPS WERE INSPECTED AS WELL AS ANY DEFICIENCIES FOUND AND THE DATE THEY WERE CORRECTED SHA BE MAINTAINED ON THE SITE AND BE MADE AVAILABLE TO REGULATORY AGENCY OFFICIALS AT THE TIME OF INSPECTION. WRITTEN DOCUMENTATION OF INSPECTION AND REPLACEMENT OF BMP's BY CONTRACTOR.
- 16. SEDIMENT TRACKED ONTO ANY PUBLIC ROADWAY OR SIDEWALK SHALL BE IMMEDIATELY RETURNED TO THE CONSTRUCTION SITE AND DISPOSED IN THE MANNER DESCRIBED IN THIS PLAN. IN NO CASE SHALL THE SEDIMENT BE WASHED, SHOVELED, OR SWEPT INTO ANY ROADSIDE DITCH, STORM SEWER. OR SURFACE WATER.
- 17. ALL SEDIMENT REMOVED FROM BMPS SHALL BE DISPOSED OF IN THE MANNER DESCRIBED ON THE PLAN DRAWINGS.
- 18. AREAS WHICH ARE TO BE TOPSOILED SHALL BE SCARIFIED TO A MINIMUM DEPTH OF 8" OR TO THE DEPTH ENCOUNTERED WITHIN THE SITE PRIOR TO PLACEMENT OF TOPSOIL. AREAS TO BE VEGETATED SHALL HAVE A MINIMUM 8" OF TOPSOIL (OR TO THE DEPTH ENCOUNTERED WITHIN THE SITE, WHICHEVER IS GREATER) IN PLACE PRIOR TO SEEDING AND MULCHING.
- 19. ALL FILLS SHALL BE COMPACTED AS REQUIRED TO REDUCE EROSION, SLIPPAGE, SETTLEMENT, SUBSIDENCE OR OTHER RELATED PROBLEMS. FILL INTENDED TO SUPPORT BUILDINGS, STRUCTURES AND CONDUITS, ETC. SHALL BE COMPACTED IN ACCORDANCE WITH LOCAL REQUIREMENTS OR
- 20. ALL EARTHEN FILLS SHALL BE PLACED IN COMPACTED LAYERS NOT TO EXCEED 9 INCHES IN THICKNESS.
- 21. FILL MATERIALS SHALL BE FREE OF FROZEN PARTICLES, BRUSH, ROOTS, SOD, OR OTHER FOREIGN OR OBJECTIONABLE MATERIALS THAT WOULD INTERFERE WITH OR PREVENT CONSTRUCTION OF SATISFACTORY FILLS.
- 22. FROZEN MATERIALS OR SOFT, MUCKY, OR HIGHLY COMPRESSIBLE MATERIALS SHALL NOT BE INCORPORATED INTO FILLS.
- 23. FILL SHALL NOT BE PLACED ON SATURATED OR FROZEN SURFACES.
- 24. SEEPS OR SPRINGS ENCOUNTERED DURING CONSTRUCTION SHALL BE HANDLED IN ACCORDANCE WITH THE STANDARD AND SPECIFICATION FOR SUBSURFACE DRAIN OR OTHER APPROVED METHOD.
- 25. ALL GRADED AREAS SHALL BE PERMANENTLY STABILIZED IMMEDIATELY UPON REACHING FINISHED GRADE. CUT SLOPES IN COMPETENT BEDROCK AND ROCK FILLS NEED NOT BE VEGETATED. SEEDED AREAS WITHIN 50 FEET OF A SURFACE WATER, OR AS OTHERWISE SHOWN ON THE PLAN DRAWINGS, SHALL BE BLANKETED ACCORDING TO THE STANDARDS OF THIS PLAN.
- 26. IMMEDIATELY AFTER EARTH DISTURBANCE ACTIVITIES CEASE IN ANY AREA OR SUBAREA OF THE PROJECT, THE OPERATOR SHALL STABILIZE ALL DISTURBED AREAS. DURING NON-GERMINATING MONTHS, MULCH OR PROTECTIVE BLANKETING SHALL BE APPLIED AS DESCRIBED IN THE PLAN. AREAS NOT AT FINISHED GRADE, WHICH WILL BE REACTIVATED WITHIN 1 YEAR, MAY BE STABILIZED IN ACCORDANCE WITH THE TEMPORARY STABILIZATION SPECIFICATIONS. THOSE AREAS WHICH WILL NOT BE REACTIVATED WITHIN 1 YEAR SHALL BE STABILIZED IN ACCORDANCE WITH THE PERMANENT STABILIZATION SPECIFICATIONS.
- 27. PERMANENT STABILIZATION IS DEFINED AS A MINIMUM UNIFORM, PERENNIAL 70% VEGETATIVE COVER OR OTHER PERMANENT NON-VEGETATIVE COVER WITH A DENSITY SUFFICIENT TO RESIST ACCELERATED EROSION. CUT AND FILL SLOPES SHALL BE CAPABLE OF RESISTING FAILURE DUE TO
- 28. E&S BMPS SHALL REMAIN FUNCTIONAL AS SUCH UNTIL ALL AREAS TRIBUTARY TO THEM ARE PERMANENTLY STABILIZED OR UNTIL THEY ARE REPLACED BY ANOTHER BMP APPROVED BY THE LOCAL CONSERVATION DISTRICT OR THE DEPARTMENT.
- 29. UPON COMPLETION OF ALL EARTH DISTURBANCE ACTIVITIES AND PERMANENT STABILIZATION OF ALL DISTURBED AREAS, THE OWNER AND/OR OPERATOR SHALL CONTACT THE LOCAL CONSERVATION DISTRICT FOR AN INSPECTION PRIOR TO REMOVAL/CONVERSION OF THE E&S BMPS.
- 30. AFTER FINAL SITE STABILIZATION HAS BEEN ACHIEVED, TEMPORARY EROSION AND SEDIMENT BMPS MUST BE REMOVED OR CONVERTED TO PERMANENT POST CONSTRUCTION STORMWATER MANAGEMENT BMPS. AREAS DISTURBED DURING REMOVAL OR CONVERSION OF THE BMPS SHALL BE STABILIZED IMMEDIATELY. IN ORDER TO ENSURE RAPID REVEGETATION OF DISTURBED AREAS, SUCH REMOVAL/CONVERSIONS ARE TO BE DONE ONLY DURING THE GERMINATING SEASON.
- 31. FAILURE TO CORRECTLY INSTALL E&S BMPS, FAILURE TO PREVENT SEDIMENT-LADEN RUNOFF FROM LEAVING THE CONSTRUCTION SITE, OR FAILURE TO TAKE IMMEDIATE CORRECTIVE ACTION TO RESOLVE FAILURE OF E&S BMPS MAY RESULT IN ADMINISTRATIVE, CIVIL, AND/OR CRIMINAL PENALTIES BEING INSTITUTED BY THE DEPARTMENT AS DEFINED IN SECTION 602 OF THE PENNSYLVANIA CLEAN STREAMS LAW. THE CLEAN STREAMS LAW PROVIDES FOR UP TO \$10,000 PER DAY IN CIVIL PENALTIES, UP TO \$10,000 IN SUMMARY CRIMINAL PENALTIES, AND UP TO \$25,000 IN MISDEMEANOR CRIMINAL PENALTIES FOR EACH VIOLATION.
- 32. FILL MATERIAL FOR EMBANKMENTS SHALL BE FREE OF ROOTS, OR OTHER WOODY VEGETATION, ORGANIC MATERIAL, LARGE STONES, AND OTHER OBJECTIONABLE MATERIALS.
- 33. STOCKPILE HEIGHTS MUST NOT EXCEED 35 FEET; STOCKPILE SLOPES MUST NOT EXCEED 2.1.
- 34. THE OPERATOR/RESPONSIBLE PERSON (O/RP) ON SITE SHALL ASSURE THAT THE APPROVED EROSION AND SEDIMENT CONTROL PLAN IS PROPERLY AND COMPLETELY IMPLEMENTED.
- 35. THE O/RP SHALL ASSURE THAT AN EROSION AND SEDIMENT CONTROL PLAN HAS BEEN PREPARED, APPROVED BY THE BUCKS COUNTY CONSERVATION DISTRICT AND IS BEING IMPLEMENTED AND MAINTAINED FOR ALL SOIL AND/OR ROCK SPOIL AND BORROW AREAS REGARDLESS OF THEIR LOCATIONS.
- 36. ALL PUMPING OF SEDIMENT-LADEN WATER SHALL BE THROUGH A SEDIMENT CONTROL BMP SUCH AS A PUMPED WATER FILTER BAG DISCHARGING
- 37. A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN MUST BE AVAILABLE ON THE PROJECT SITE AT ALL TIMES.
- 38. EROSION AND SEDIMENT BMPS MUST BE CONSTRUCTED, STABILIZED AND FUNCTIONAL BEFORE SITE DISTURBANCE BEGINS WITHIN THE TRIBUTARY AREAS OF THOSE BMPS.
- 39. DISTURBED AREAS THAT ARE AT FINISHED GRADE OR WHICH WILL NOT BE RE-DISTURBED WITHIN ONE YEAR MUST BE STABILIZED IN ACCORDANCE WITH PERMANENT VEGETATIVE STABILIZATION SPECIFICATIONS.
- 40. AN AREA SHALL BE CONSIDERED TO HAVE ACHIEVED FINAL STABILIZATION WHEN IT HAS A MINIMUM UNIFORM 70% (PERCENT) VEGETATIVE OR OTHER PERMANENT NON-VEGETATIVE COVER WITH A DENSITY SUFFICIENT TO RESIST ACCELERATED SURFACE EROSION AND SUBSURFACE CHARACTERISTICS SUFFICIENT TO RESIST SLIDING AND OTHER MOVEMENTS.
- 41. SEDIMENT REMOVED FROM BMPS SHALL BE DISPOSED OF ON-SITE IN LANDSCAPED AREAS OUTSIDE OF STEEP SLOPES, WETLANDS, FLOODPLAINS OR
- 42. BMPs SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER MEASURABLE STORM EVENTS (I.E. AT LEAST 0.25 INCH).

DRAINAGE SWALES AND IMMEDIATELY STABILIZED OR PLACED IN SOIL STOCKPILES AND STABILIZED.

43. TEMPORARY STABILIZATION SHALL BE PROVIDED TO ALL BARE SOIL AREAS THAT WILL REMAIN UNDISTURBED FOR 4 - OR MORE DAYS IN THE FORM OF 3 TON/ACRE STRAW MULCH OR EQUIVALENT.

### CONSTRUCTION SEQUENCE

- AT LEAST 7 DAYS PRIOR TO START OF WORK OR EARTH DISTURBANCE ACTIVITIES, A PRE-CONSTRUCTION MEETING SHALL BE HELD INCLUDING THE OWNER, SITE CONTRACTOR(S), TOWNSHIP ENGINEER(S), COUNTY CONSERVATION DISTRICT, AND THE PLAN DESIGNER.
- THE LIMITS OF DISTURBANCE SHALL BE DELINEATED WITH SURVEY STAKES OR SIMILAR, PRIOR TO ANY DISTURBANCE AND SHALL NOT BE DISTURBED DURING SITE CONSTRUCTION EXCEPT FOR TEMPORARY IMPACTS FOR MITIGATION OR RESTORATION PER THE PLAN.
- 3. INSTALL THE ROCK CONSTRUCTION ENTRANCE PER THE PLAN.
- CONSTRUCTION FENCING SHALL BE INSTALLED ALONG THE SOUTH AND EASTERN PROPERTY LINES. PLACE COMPOST FILTER SOCK AND INLET PROTECTION AS DELINEATED ON THE PLAN AND DOWNSTREAM OF ALL EXCAVATED OR TOPSOIL STOCKPILING AREAS TO BE DISTURBED. INSTALL CONCRETE WASHOUT FACILITY PER THE PLAN.
- 5. DEMOLISH EXISTING BUILDING.
- 6. STRIP TOPSOIL WITHIN AREAS OF PROPOSED EARTHWORK AND STOCKPILE PER THE PLAN. CESSATION OF GRADING ACTIVITIES FOR FOUR (4) DAYS OR LONGER REQUIRES TEMPORARY SEEDING.
- 7. BEGIN CONSTRUCTION OF PROPOSED BUILDING FEATURES, DRIVEWAY AND PARKING AREAS, DUNKIN SIGNAGE, SIDEWALK AND ADA FEATURES AND ANY OTHER ASSOCIATED FEATURES.
- 8. ALL 3:1 SLOPES SHALL BE STABILIZED AND LINED WITH TEMPORARY EROSION CONTROL MATTING.
- 9. FINISH GRADE AND SPREAD TOPSOIL SEED AND MULCH EACH AREA OF DISTURBANCE IMMEDIATELY AFTER CONSTRUCTION IS COMPLETED. AS DISTURBED AREAS WITHIN A PROJECT APPROACH FINAL GRADE, PREPARATIONS SHOULD BE MADE FOR SEEDING AND MULCHING TO BEGIN (I.E. ANTICIPATE COMPLETION DATE AND SCHEDULE SEEDING). PRIOR TO PLACING TOPSOIL, SUBSOIL SHALL BE SCARIFIED. PLANT LANDSCAPING AS DEPICTED ON THE LANDSCAPE AND LIGHTING PLAN.
- 10. REMOVE TEMPORARY CONTROL MEASURES AFTER UNIFORM EROSION RESISTANT PERENNIAL VEGETATION HAS BEEN ESTABLISHED, MINIMUM OF UNIFORM COVERAGE OR A DENSITY OF 70% ACROSS THE DISTURBED AREA, TO THE POINT WHERE THE SURFACE SOIL IS CAPABLE OF RESISTING EROSION DURING RUNOFF EVENTS AND STABILIZATION OF THE SITE IS COMPLETE TO THE SATISFACTION OF THE COUNTY CONSERVATION DISTRICT. AREAS DISTURBED DURING THE REMOVAL OF THE CONTROLS MUST BE STABILIZED. THE COUNTY CONSERVATION DISTRICT AND TOWNSHIP SHALL BE NOTIFIED PRIOR TO REMOVAL OF ANY EROSION CONTROLS.

#### PROJECT CONSTRUCTION WASTES

1. TREES - CHIPPED AND REMOVED OFFSITE FOR MULCH.

BUILDING MATERIALS - SHALL BE REMOVED AND DISPOSED OF AT PROPER RECYCLING FACILITY AS PER NOTE #6 OF THE GENERAL EROSION & SEDIMENT CONTROL NOTES.

3. ROCK WASTE - ALL ROCKS THAT ARE SUITABLE FOR LANDSCAPING USES SHALL BE USED ON SITE OR AT ANOTHER FACILITY.

4. SOIL WASTE - ALL EXCAVATED SOIL SHALL BE USED ON SITE, AS FILL MATERIAL WHERE POSSIBLE. ANY EXCESS MATERIAL NOT USED ON SITE MAY ONLY BE DISPOSED AT A FACILITY WITH A VALID EARTH DISTURBANCE AND NPDES PERMIT APPROVED AND WERE ALL BMPS ARE PROPERLY INSTALLED. ALL SEDIMENT REMOVED DURING MAINTENANCE AND CLEANING OF EROSION AND SEDIMENTATION CONTROLS DEVICES WILL BE INCORPORATED INTO THE FINE GRADING UPSTREAM FROM A SEDIMENT CONTROL DEVICE. EXCESS MATERIAL THAT IS REMOVED FROM EROSION AND SEDIMENTATION CONTROL MEASURES AND CANNOT BE INCORPORATED INTO THE FINE GRADING STABILIZATION SHALL BE REMOVED FROM THE SITE AND MAY ONLY BE DISPOSED AT A FACILITY WITH A VALID EARTH DISTURBANCE AND NPDES PERMIT APPROVAL AND WERE ALL BMPS ARE PROPERLY INSTALLED.

5. ACCUMULATED SEDIMENT FROM COMPOST FILTER SOCKS TO BE RETURNED TO SITE UPSTREAM OF ALL PROPOSED BMP'S.

#### MAINTENANCE OF FACILITIES

1. SILT FENCE/FILTER SOCK SHOULD BE INSPECTED AND MAINTAINED ON A DAILY BASIS.

2. UNTIL THE SITE IS STABILIZED, ALL EROSION AND SEDIMENT CONTROL DEVICES MUST BE MAINTAINED PROPERLY. MAINTENANCE MUST INCLUDE INSPECTIONS OF ALL EROSION AND SEDIMENT CONTROLS AFTER EACH RUNOFF EVENT AND ON A WEEKLY BASIS. ALL PREVENTATIVE AND REMEDIAL MAINTENANCE WORK, INCLUDING CLEAN OUT, REPAIR, REPLACEMENT, REGRADING, RESEEDING, REMULCHING, AND RENETTING MUST BE PERFORMED IMMEDIATELY. IF E&S CONTROL BMPS FAIL TO PERFORM AS EXPECTED, REPLACEMENT BMPS OR MODIFICATIONS OF INSTALLED MEASURES WILL BE

3. CONTRACTOR SHALL MAINTAIN AND MAKE AVAILABLE TO THE COUNTY CONSERVATION DISTRICT COMPLETE, WRITTEN INSPECTION LOGS OF THE ABOVE NOTED INSPECTION AND MAINTENANCE.

- 4. SEEDING, MULCHING AND FERTILIZING SHALL BE IN ACCORDANCE WITH THE SEEDING AND MULCHING SCHEDULE.
- 5. THE CONTRACTOR SHALL HAVE AVAILABLE WATER TRUCKS OR OTHER MEANS OF CONTROLLING EXCESSIVE DUST AND AIRBORNE DEBRIS.
- 6. ALL AREAS OF CONCENTRATED SURFACE DRAINAGE SHALL BE SEEDED AND MULCHED, AND PROTECTED WITH TEMPORARY TURF REINFORCEMENT MAT: NORTH AMERICAN GREEN #D575 (OR EQUAL). IF AREAS ARE TO BE SODDED, TURF REINFORCEMENT IS NOT REQUIRED.

7. SEEDED AREAS THAT HAVE WASHED AWAY SHALL BE FILLED AND GRADED AS NECESSARY AND THEN RESEEDED. A BURLAP OR STRAW COVER WILL BE APPLIED TO RETAIN THE SEED UNTIL IT HAS A CHANCE TO ROOT PROPERLY.

8. THE ABOVE PROCEDURE SHALL BE REPEATED AFTER EACH SIZABLE STORM UNTIL NO MORE SIGNS OF EROSION ARE EVIDENT. AT MONTHLY INTERVALS THEREAFTER, INSPECTIONS AND NECESSARY CLEANING WILL BE DONE. TRASH THAT IS REMOVED FROM ANY OF THE CONTROL DEVICES SHALL BE DISPOSED OF AT AN APPROVED DISPOSAL AREA. SILT THAT HAS ACCUMULATED SHALL BE REMOVED AND ALLOWED TO DRY AND USED AS

IMMEDIATELY UPON DISCOVERING UNFORESEEN CIRCUMSTANCES POSING THE POTENTIAL FOR ACCELERATED EROSION AND/OR SEDIMENT POLLUTION, THE CONTRACTOR SHALL IMPLEMENT APPROPRIATE MEASURES TO ADDRESS SAID CIRCUMSTANCES.

IF THE SITE WILL NEED TO HAVE FILL IMPORTED FROM AN OFF SITE LOICATION, THE RESPONSIBILITY FOR PERFORMING ENVIRONMENTAL DUE DILIGENCE AND THE DETERMINATION OF CLEAN FILL WILL RESIDE WITH THE OPERATOR.

<u>CLEAN FILL</u>: IS DEFINED AS UNCONTAMINATED, NON-WATER SOLUBLE, ON-DECOMPOSABLE, INERT, SOLID MATERIAL. THE TERM INCLUDES SOIL, ROCK, STONE, DREDGED MATERIAL, USED ASPHALT, AND BRICK, BLOCK OR CONCRETE FROM CONSTRUCTION AND DEMOLITION ACTIVITIES HAT IS SEPARATE FROM OTHER WASTE AND IS RECOGNIZABLE AS SUCH. THE TERM DOES NOT INCLUDE MATERIALS PLACED IN OR ON THE WATERS OF THE COMMONWEALTH UNLESS OTHERWISE AUTHORIZED. (THE ERM "USED ASPHALT" DOES NOT INCLUDE MILLED ASPHALT OR ASPHALT THAT HAS BEEN PROCESSED FOR RE-USE.)

ENVIRONMENTAL DUE DILIGENCE: INVESTIGATIVE TECHNIQUES, INCLUDING, BUT NOT LIMITED TO, VISUAL PROPERTY INSPECTIONS, ELECTRONIC DATA BASE SEARCHES, REVIEW OF PROPERTY OWNERSHIP, REVIEW OF PROPERTY USE HISTORY, SANBORN MAPS, ENVIRONMENTAL QUESTIONNAIRES, TRANSACTION SCREENS, ANALYTICAL TESTING, ENVIRONMENTAL ASSESSMENTS OR AUDITS. ANALYTICAL TESTING IS NOT A REQUIRED PART OF DUE DILIGENCE UNLESS VISUAL INSPECTION AND/OR REVIEW OF THE PAST LAND USE OF THE PROPERTY INDICATES THAT THE FILL MAY HAVE BEEN SUBJECTED TO A SPILL OR RELEASE OF REGULATED SUBSTANCE, IF THE FILL MAY HAVE BEEN AFFECTED BY A SPILL OR RELEASE OF A REGULATED SUBSTANCE, IT MUST BE TESTED TO DETERMINE IF IT QUALIFIES AS CLEAN FILL. TESTING SHOULD BE PERFORMED IN ACCORDANCE WITH APPENDIX A OF THE DEPARTMENT'S POLICY "MANAGEMENT OF FILL" FILL MATERIAL THAT DOES NOT QUALIFY AS CLEAN FILL IS REGULATED FILL, REGULATED FILL IS WASTE AND MUST BE MANAGED IN ACCORDANCE WITH

THE DEPARTMENT'S MUNICIPAL OR RESIDUAL WASTE REGULATIONS BASED ON 25 PA. CODE CHAPTERS 287 RESIDUAL WASTE MANAGEMENT OR 271 MUNICIPAL WASTE MANAGEMENT, WHICHEVER IS APPLICABLE.

### E&S PLAN PLANNING & DESIGN NOTES

- 1. THE CONTRACTOR SHALL MINIMIZE THE EXTENT AND DURATION OF THE EARTH DISTURBANCE BY LIMITING THE CLEARING, GRUBBING, AND TOPSOIL STRIPPING TO AREAS IN EACH PHASE OF THE CONSTRUCTION SEQUENCE. GENERAL SITE CLEARING, GRUBBING AND TOPSOIL STRIPPING MAY NOT COMMENCE IN ANY STAGE OR PHASE OF THE PROJECT UNTIL THE E&S BMP'S SPECIFIED BY THE CONSTRUCTION SEQUENCE FOR THAT PHASE HAVE BEEN INSTALLED AND ARE FUNCTIONING AS DESCRIBED IN THE E&S PLAN. IMMEDIATELY AFTER EARTH DISTURBANCE ACTIVITIES CEASE, THE CONTRACTOR SHALL STABILIZE AND AREAS DISTURBED BY ANY ACTIVITIES.
- 2. TO MAXIMIZE THE PROTECTION OF EXISTING DRAINAGE FEATURES AND VEGETATION, E&S BMP'S MUST BE CONSTRUCTED, STABILIZED, AND FUNCTIONAL BEFORE SITE DISTURBANCE BEGINS WITHIN THE TRIBUTARY AREAS TO THOSE BMP'S. E&S BMP'S SHALL REMAIN FUNCTIONAL UNTIL ALL AREAS TRIBUTARY TO THEM ARE PERMANENTLY STABILIZED. AT NO TIME WILL CONSTRUCTION VEHICLES BE ALLOWED TO ENTER AREAS OUTSIDE THE LIMIT OF DISTURBANCE BOUNDARY. SEDIMENT TRACKED ONTO ANY PUBLIC ROADWAYS SHALL BE RETURNED TO THE CONSTRUCTION SITE BY THE END OF EACH WORK DAY. IN NO CASE SHALL THE SEDIMENT BE WASHED. SHOVELED, OR SWEPT INTO ANY ROADSIDE DITCH, STORM SEWER, OR SURFACE WATER. ALL GRADED AREAS SHALL BE PERMANENTLY STABILIZED IMMEDIATELY UPON REACHING FINISHED GRADE.
- TO MINIMIZE SOIL COMPACTION. NO LAND CLEARING AND/OR GRADING SHALL BE PERFORMED OUTSIDE OF LIMIT OF DISTURBANCE FENCING. PROPER CARE SHALL BE TAKEN WITHIN UNDERGROUND INFILTRATION TRENCH TO MINIMIZE SOIL COMPACTION.
- 4. THE FOLLOWING MEASURES OR CONTROLS UTILIZED TO MINIMIZE GENERATION OF INCREASED STORMWATER RUNOFF: A ROCK CONSTRUCTION ENTRANCE, COMPOST FILTER SOCKS AROUND THE DISTURBANCE, A CONCRETE WASHOUT, EROSION CONTROL BLANKET, AND TEMPORARY/PERMANENT STABILIZATION OF ALL LOT FEATURES.
- 5. THERE ARE NO PROPOSED INFILTRATION BMP'S OUTSIDE PROPOSED GRADING AREAS.
- 6. THERE ARE NO EXISTING/PROPOSED RIPARIAN FOREST BUFFERS WITHIN THE LIMITS OF DISTURBANCE THEREFORE NO ASSOCIATED REQUIREMENTS ARE SHOWN ON THE PLANS.
- 7. THIS PROJECT IS NOT LOCATED IN A SPECIAL PROTECTION (HQ/EV) WATERSHED, THEREFORE NO ANTIDEGRADATION ANALYSIS IS REQUIRED.
- 8. THERE ARE NO NATURALLY OCCURRING GEOLOGIC FORMATIONS (KARST SOILS) THAT MAY CAUSE POLLUTION WITHIN THE PROJECT LIMIT OF DISTURBANCE, THEREFORE, MEASURES TO AVOID/MINIMIZE/OR MITIGATE ARE NOT NECESSARY.

### GRASS SEEDING SPECIFICATIONS:

TEMPORARY:

ANCHOR MATERIAL

SEEDING SEASON DATES

ANNUAL RYEGRASS SPECIES % PURE LIVE SEED 87.0 LB/ACRE FERTILIZER TYPE COMMERCIAL 10-20-10 FERTILIZER APPL, RATE 1,100 LB/ACRE LIMING RATE MULCH RATE 3 T/ACRE PERMANENT: SPECIES KENTUCKY BLUEGRASS / SPREADING FESCUE / PERENNIAL RYEGRASS / CHEWING'S RED FESCUE % PURE LIVE SEED APPLICATION RATE 41 / 20 / 14 / 20 LB/ACRI FERTILIZER TYPE FERTILIZER APPL, RATE 1.100 LB/ACRE 2 T/ACRE HAY OR STRAW MULCH RATE 3 T/ACRE

RATE OF ANCHOR MATERIAL APPL. N/A APRIL 15 - MAY 30, AUGUST 15 - OCTOBER 15

EV DESCRIPTION

SERIAL NO. 20223183581



PLAN NOTATION ONLY THOSE PLANS WHICH CONTAIN A DIGIT. IMPRESSED, OR COLORIZED INK SEAL OF T RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERE VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARE FOR THE OWNER DESIGNATED HEREON. A MODIFICATION, REVISION, DUPLICATION OR U MITHOUT THE WRITTEN CONSENT OF VAN CLE



ENGINEERING ASSOCIATES IS PROHIBITE RELIANCE ON THIS PLAN FOR ANY PURPO

THER THAN THAT WHICH IS INTENDED SHALL

EAN DAVID McGRANAHAN

EER NUMBER 085090



**VAN CLEEF ENGINEERING ASSOCIATES, LL** 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM PHONE (215) 345-1876

**EROSION & SEDIMENTATION CONTROL NOTES** 

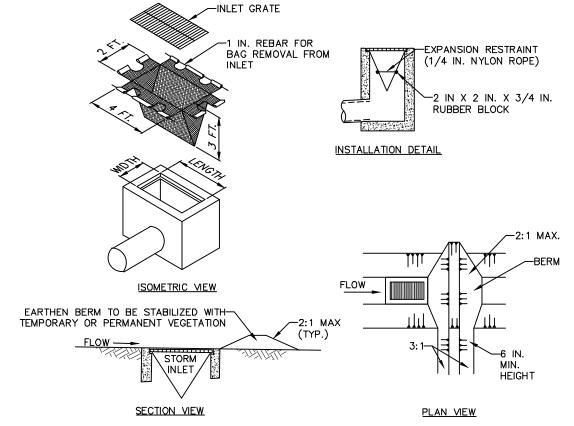
FOR

FLAGSHIP CARWASH

LDM, SD DESIGNED B RAWN BY HECKED BY B NUMBE

TMP'S 26-006-101-001

**NEW BRTIAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 



NOTES:

MAXIMUM DRAINAGE AREA = 1/2 ACRE.

STABILIZATION IS COMPLETED OR REMAIN PERMANENTLY.

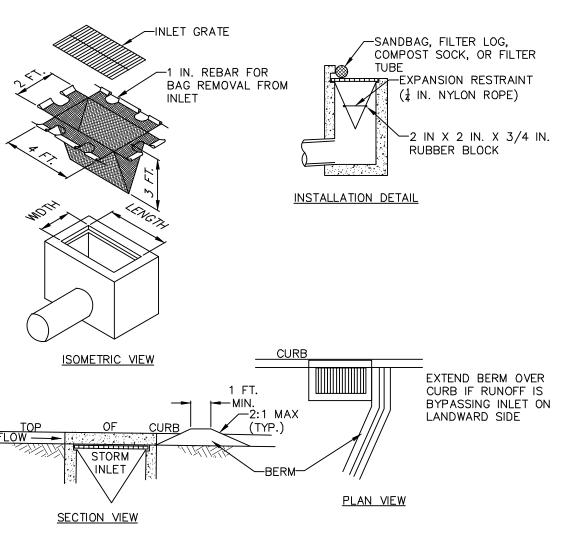
INLET PROTECTION SHALL NOT BE REQUIRED FOR INLET TRIBUTARY TO SEDIMENT BASIN OR TRAP. BERMS SHALL BE REQUIRED ROLLED EARTHEN BERM IN ROADWAY SHALL BE MAINTAINED UNTIL ROADWAY IS STONED. ROAD SUBBASE BERM ON ROADWAY SHALL BE MAINTAINED UNTIL ROADWAY IS PAVED. EARTHEN BERM IN CHANNEL SHALL BE MAINTAINED UNTIL PERMANENT

AT A MINIMUM, THE FABRIC SHALL HAVE A MINIMUM GRAB TENSILE STRENGTH OF 120 LBS., A MINIMUM BURST STRENGTH OF 200 PSI, AND A MINIMUM TRAPEZOIDAL TEAR STRENGTH OF 50 LBS. FILTER BAGS SHALL BE CAPABLE OF TRAPPING ALL PARTICLES NOT PASSING A NO. 40 SIEVE.

INLET FILTER BAGS SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EACH RUNOFF EVENT. BAGS SHALL BE EMPTIED AND RINSED OR REPLACED WHEN HALF FULL OR WHEN FLOW CAPACITY HAS BEEN REDUCED SO AS TO CAUSE FLOODING OR BYPASSING OF THE INLET. DAMAGED OR CLOGGED BAGS SHALL BE REPLACED. A SUPPLY SHALL BE MAINTAINED ON SITE FOR REPLACEMENT OF BAGS. ALL NEEDED REPAIRS SHALL BE INITIATED IMMEDIATELY AFTER THE INSPECTION. DISPOSE ACCUMULATED SEDIMENT AS WELL AS ALL USED BAGS ACCORDING TO THE PLAN NOTES.

> STANDARD CONSTRUCTION DETAIL #4-16 FILTER BAG INLET PROTECTION - TYPE M INLET (NOT TO SCALE)

DO NOT USE ON MAJOR PAVED ROADWAYS WHERE PONDING MAY CAUSE TRAFFIC HAZARDS.



NOTES:

MAXIMUM DRAINAGE AREA = 1/2 ACRE.

INLET PROTECTION SHALL NOT BE REQUIRED FOR INLET TRIBUTARY TO SEDIMENT BASIN OR TRAP. BERMS SHALL BE REQUIRED FOR ALL INSTALLATIONS.

ROLLED EARTHEN BERM SHALL BE MAINTAINED UNTIL ROADWAY IS STONED. ROAD SUBBASE BERM SHALL BE MAINTAINED UNTIL ROADWAY IS PAVED. SIX INCH MINIMUM HEIGHT ASPHALT BERM SHALL BE MAINTAINED UNTIL ROADWAY SURFACE RECEIVES FINAL COAT. AT A MINIMUM, THE FABRIC SHALL HAVE A MINIMUM GRAB TENSILE STRENGTH OF 120 LBS, A MINIMUM BURST STRENGTH OF 200 PSI, AND A MINIMUM TRAPEZOIDAL TEAR STRENGTH OF 50 LBS. FILTER BAGS SHALL BE CAPABLE OF TRAPPING ALL PARTICLES NOT PASSING A NO. 40

INLET FILTER BAGS SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EACH RUNOFF EVENT. BAGS SHALL BE EMPTIED AND RINSED OR REPLACED WHEN HALF FULL OR WHEN FLOW CAPACITY HAS BEEN REDUCED SO AS TO CAUSE FLOODING OR BYPASSING OF THE INLET. DAMAGED OR CLOGGED BAGS SHALL BE REPLACED. A SUPPLY SHALL BE MAINTAINED ON SITE FOR REPLACEMENT OF BAGS. ALL NEEDED REPAIRS SHALL BE INITIATED IMMEDIATELY AFTER THE INSPECTION. DISPOSE OF ACCUMULATED SEDIMENT AS WELL AS ALL USED BAGS ACCORDING TO THE PLAN NOTES.

DO NOT USE ON MAJOR PAVED ROADWAYS WHERE PONDING MAY CAUSE TRAFFIC HAZARDS. STANDARD CONSTRUCTION DETAIL #4-15 FILTER BAG INLET PROTECTION — TYPË C INLET

(NOT TO SCALE)

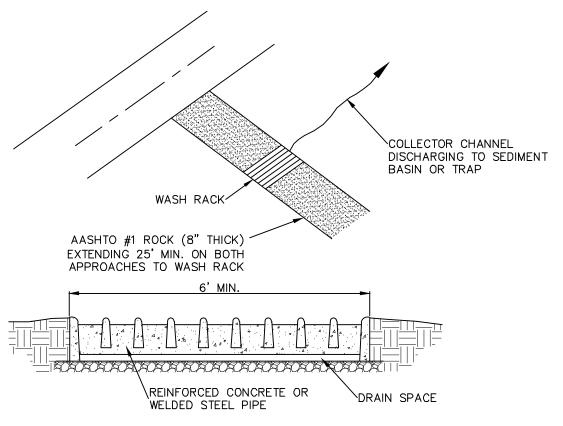
BLANKET EDGES INSTALL BEGINNING OF ROLL STAPLED AND IN 6 IN. x 6 IN. ANCHOR TRENCH, STAPLE, BACKFILL OVERI APPED (4 IN. MIN.) AND COMPACT SOIL STARTING AT TOP OF SLOPE, ROLL BLANKETS IN DIRECTION OF WATER FLOW REPARE SEED BED (INCLUDING APPLICATION OF LIME FERTILIZER AND SEED) PRIOR TO BLANKET INSTALLATION REFER TO MANUF. RECOMMENDED NOT BE STRETCHED; WITH THE UPSLOPE BLANKED STAPLING PATTERN FOR IT MUST MAINTAIN OVERLYING THE DOWNSLOPE BLANKET STEEPNESS AND LENGTH OF GOOD SOIL CONTACT (SHINGLE STYLE). STAPLE SECURELY. SLOPE BEING BLANKETED

SEED AND SOIL AMENDMENTS SHALL BE APPLIED ACCORDING TO THE RATES IN THE PLAN DRAWINGS PRIOR TO

PROVIDE ANCHOR TRENCH AT TOE OF SLOPE IN SIMILAR FASHION AS AT TOP OF SLOPE. SLOPE SURFACE SHALL BE FREE OF ROCKS, CLODS, STICKS, AND GRASS. BLANKET SHALL HAVE GOOD CONTINUOUS CONTACT WITH UNDERLYING SOIL THROUGHOUT ENTIRE LENGTH. LAY

BLANKET LOOSELY AND STAKE OR STAPLE TO MAINTAIN DIRECT CONTACT WITH SOIL. DO NOT STRETCH

THE BLANKET SHALL BE STAPLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. BLANKETED AREAS SHALL BE INSPECTED WEEKLY AND AFTER EACH RUNOFF EVENT UNTIL PERENNIAL VEGETATION IS ESTABLISHED TO A MINIMUM UNIFORM 70% COVERAGE THROUGHOUT THE BLANKETED AREA. DAMAGED OR DISPLACED BLANKETS SHALL BE RESTORED OR REPLACED WITHIN 4 CALENDAR DAYS. STANDARD CONSTRUCTION DETAIL #11-1 EROSION CONTROL BLANKET INSTALLATION



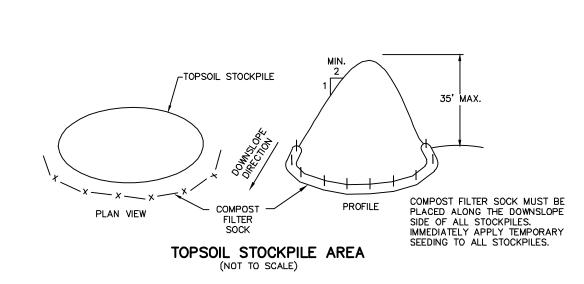
CONSTRUCTION VEHICULAR TRAFFIC.

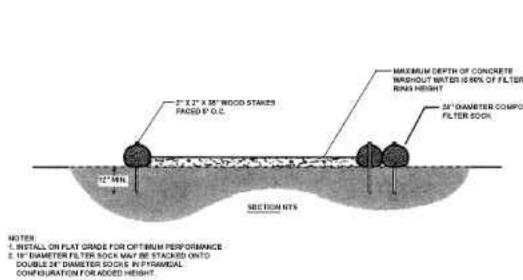
WASH RACK SHALL BE 20 FEET (MIN.) WIDE OR TOTAL WIDTH OF ACCESS. WASH RACK SHALL BE DESIGNED AND CONSTRUCTED TO ACCOMMODATE ANTICIPATED

A WATER SUPPLY SHALL BE MADE AVAILABLE TO WASH THE WHEELS OF ALL VEHICLES EXITING

MAINTENANCE: ROCK CONSTRUCTION ENTRANCE THICKNESS SHALL BE CONSTANTLY MAINTAINED TO THE SPECIFIED DIMENSIONS BY ADDING ROCK. A STOCKPILE OF ROCK MATERIAL SHALL BE MAINTAINED ON SITE FOR THIS PURPOSE. DRAIN SPACE UNDER WASH RACK SHALL BE KEPT OPEN AT ALL TIMES. DAMAGE TO THE WASH RACK SHALL BE REPAIRED PRIOR TO FURTHER USE OF THE RACK. ALL SEDIMENT DEPOSITED ON ROADWAYS SHALL BE REMOVED AND RETURNED TO THE CONSTRUCTION SITE IMMEDIATELY. WASHING THE ROADWAY OR SWEEPING THE DEPOSITS INTO ROADWAY DITCHES, SEWERS, CULVERTS, OR OTHER DRAINAGE COURSES IS

> ROCK CONSTRUCTION ACCESS WITH WASH RACK (NOT TO SCALE)





2" X 2" K 38" WO CORN STATES

COMPOST SOCK WASHOUT DETAIL (NOT TO SCALE)

WELL VEGETATED, GRASSY AREA -HEAVY DUTY LIFTING STRAPS PLAN VIEW -DISCHARGE HOSE BLOWN/PLACED CLAMP (TYP.) -FILTER MEDIA BLOWN/PLACED FILTER MEDIA-ELEVATION VIEW

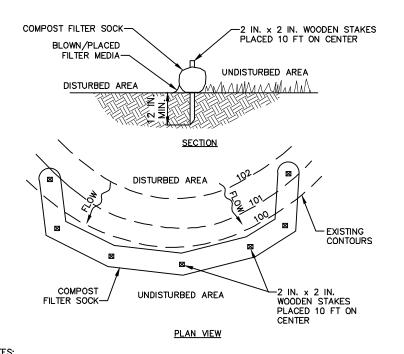
LOW VOLUME FILTER BAGS SHALL BE MADE FROM NON-WOVEN GEOTEXTILE MATERIAL SEWN WITH HIGH STRENGTH, DOUBLE STITCHED "J" TYPE SEAMS. THEY SHALL BE CAPABLE OF TRAPPING PARTICLES LARGER THAN 150 MICRONS. HIGH VOLUME FILTER BAGS SHALL BE MADE FROM WOVEN GEOTEXTILES THAT MEET THE

PROPERTY	TEST METHOD	MINIMUM STANDARD	
AVG. WIDE WIDTH STRENGTH	ASTM D-4884	60 LB/IN	1
GRAB TENSILE	ASTM D-4632	205 LB	
PUNCTURE	ASTM D-4833	110 LB	
MULLEN BURST	ASTM D-3786	350 PSI	]
UV RESISTANCE	ASTM D-4355	70%	1
AOS % RETAINED	ASTM D-4751	80 SIEVE	1

PROVIDED. FILTER BAGS SHALL BE REPLACED WHEN THEY BECOME 1/2 FULL OF SEDIMENT. SPARE BAGS SHALL BE KEPT AVAILABLE FOR REPLACEMENT OF THOSE THAT HAVE FAILED OR ARE FILLED. BAGS SHALL BE PLACED ON STRAPS TO FACILITATE REMOVAL UNLESS BAGS COME WITH LIFTING STRAPS ALREADY ATTACHED. BAGS SHALL BE LOCATED IN WELL-VEGETATED (GRASSY) AREA, AND DISCHARGE ONTO STABLE, EROSION RESISTANT AREAS. WHERE THIS IS NOT POSSIBLE, A GEOTEXTILE UNDERLAYMENT AND FLOW PATH SHALL BE PROVIDED. BAGS MAY BE PLACED ON FILTER STONE TO INCREASE DISCHARGE CAPACITY. BAGS SHALL NOT BE PLACED ON SLOPES GREATER THAN 5% FOR SLOPES EXCEEDING 5%, CLEAN ROCK OR OTHER NON-ERODIBLE AND NON-POLLUTING MATERIAL MAY BE PLACED UNDER THE BAG TO REDUCE SLOPE STEEPNESS.

NO DOWNSLOPE SEDIMENT BARRIER IS REQUIRED FOR MOST INSTALLATIONS. COMPOST BERM OR COMPOST FILTER SOCK SHALL BE INSTALLED BELOW BAGS LOCATED IN HQ OR EV WATERSHEDS, WITHIN 50 FEET OF ANY RECEIVING SURFACE WATER OR WHERE GRASSY AREA IS NOT AVAILABLE. THE PUMP DISCHARGE HOSE SHALL BE INSERTED INTO THE BAGS IN THE MANNER SPECIFIED BY THE MANUFACTURER AND SECURELY CLAMPED. A PIECE OF PVC PIPE IS RECOMMENDED FOR THIS PURPOSE. THE PUMPING RATE SHALL BE NO GREATER THAN 750 GPM OR 1/2 THE MAXIMUM SPECIFIED BY THE MANUFACTURER, WHICHEVER IS LESS. PUMP INTAKES SHALL BE FLOATING AND SCREENED. FILTER BAGS SHALL BE INSPECTED DAILY. IF ANY PROBLEM IS DETECTED, PUMPING SHALL CEASE IMMEDIATELY AND NOT RESUME UNTIL THE PROBLEM IS CORRECTED.

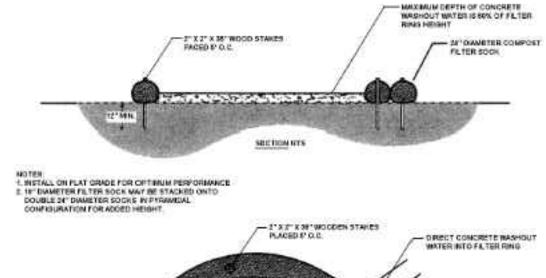
PUMPED WATER FILTER BAG (NOT TO SCALE)



SOCK FABRIC SHALL MEET STANDARDS OF TABLE 4.1 OF THE PA DEP EROSION CONTROL CONTROL MANUAL COMPOST FILTER SOCK SHALL BE PLACED AT EXISTING LEVEL GRADE. BOTH ENDS OF THE BARRIER SHALL BE EXTENDED AT LEAST 8 FEET UP SLOPE AT 45 DEGREES TO THE MAIN BARRIER ALIGNMENT. MAXIMUM SLOPE LENGTH ABOVE ANY BARRIER SHALL NOT EXCEED THAT SPECIFIED FOR THE SIZE OF THE SOCK AND THE SLOPE OF ITS TRIBUTARY AREA. TRAFFIC SHALL NOT BE PERMITTED TO CROSS COMPOST FILTER SOCKS. ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN IT REACHES 1/2 THE ABOVE GROUND HEIGHT OF THE BARRIER AND DISPOSED IN THE MANNER DESCRIBED ELSEWHERE IN THE PLAN. COMPOST FILTER SOCKS SHALL BE INSPECTED WEEKLY AND AFTER FACH RUNOFF EVENT. DAMAGED SOCKS SHALL BE INSPECTION WEEKLT AND AFTER EACH KUNOFF EVENT.

DAMAGED SOCKS SHALL BE REPAIRED ACCORDING TO MANUFACTURER'S SPECIFICATIONS OR REPLACED WITHIN 24 HOURS OF INSPECTION. BIODEGRADABLE COMPOST FILTER SOCKS SHALL BE REPLACED AFTER 6 MONTHS; PHOTODEGRADABLE SOCKS AFTER 1 YEAR. POLYPROPYLENE SOCKS SHALL BE REPLACED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS. UPON STABILIZATION OF THE AREA TRIBUTARY TO THE SOCK, STAKES SHALL BE REMOVED. THE SOCK MAY BE LEFT IN PLACE AND VEGETATED OR REMOVED. IN THE LATTER CASE, THE MESH SHALL BE CUT OPEN AND THE MULCH SPREAD AS A SOIL SUPPLEMENT.

> COMPOST FILTER SOCK (NOT TO SCALE)



**EROSION & SEDIMENTATION CONTROL DETAILS** 

**VAN CLEEF ENGINEERING ASSOCIATES, LLC** 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM

PHONE (215) 345-1876

REV DESCRIPTION

PLAN NOTATION

**SERIAL NO. 20223183581** 

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ONLY THOSE PLANS WHICH CONTAIN A DIGIT

IMPRESSED, OR COLORIZED INK SEAL OF THE RESPONSIBLE PROFESSIONAL SHALL BE CONSIDER!

ALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARE OR THE OWNER DESIGNATED HEREON. AN

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NGINEERING ASSOCIATES IS PROHIBITE RELIANCE ON THIS PLAN FOR ANY PURPO

OTHER THAN THAT WHICH IS INTENDED SHALL

AT THE SOLE DISCRETION AND LIABILITY OF APPLICABLE PARTY.

SEAN DAVID MCGRA

EAN DAVID McGRANAHAN

ENGINEER

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PENNSYLVAN

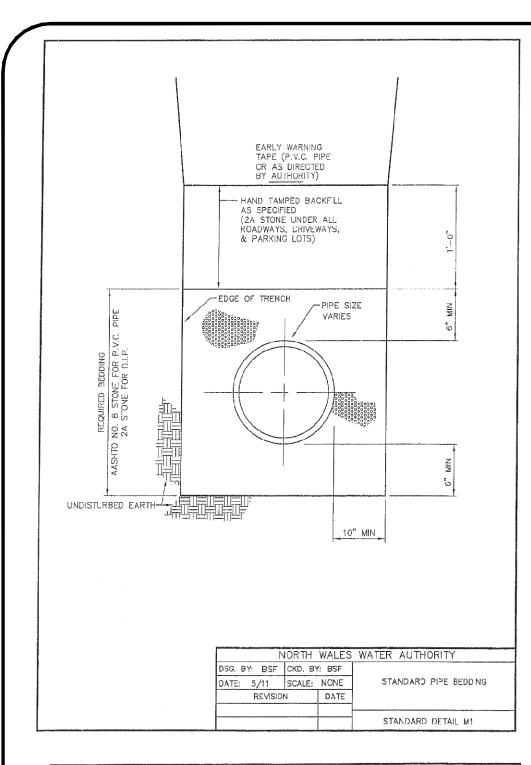
AUTH DATE

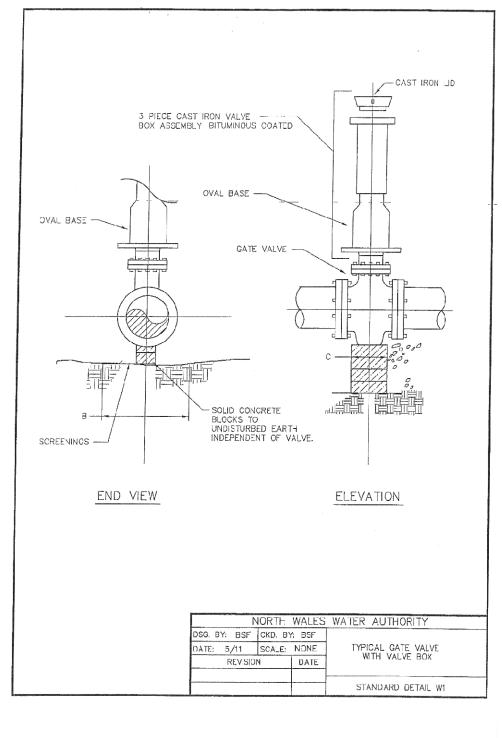
**FLAGSHIP CARWASH** 

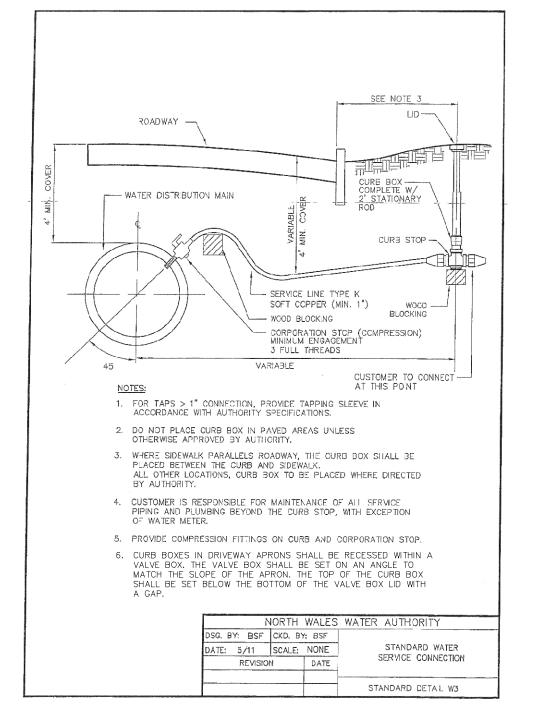
**DESIGNED BY** LDM, SDN DRAWN BY CHECKED BY OB NUMBER: 2402-NBR

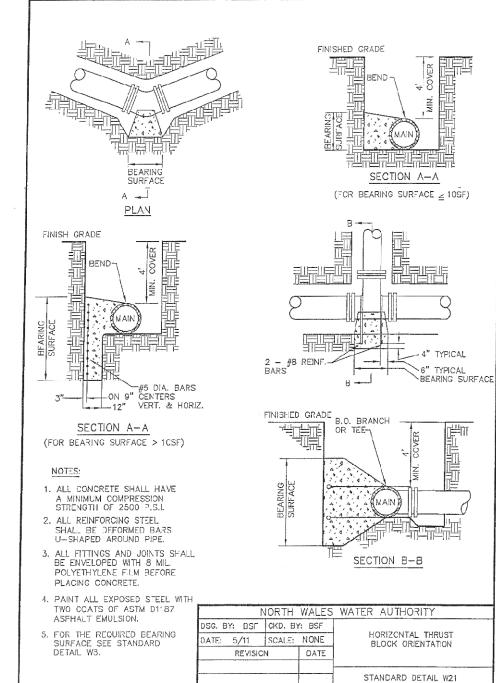
TMP'S 26-006-101-001

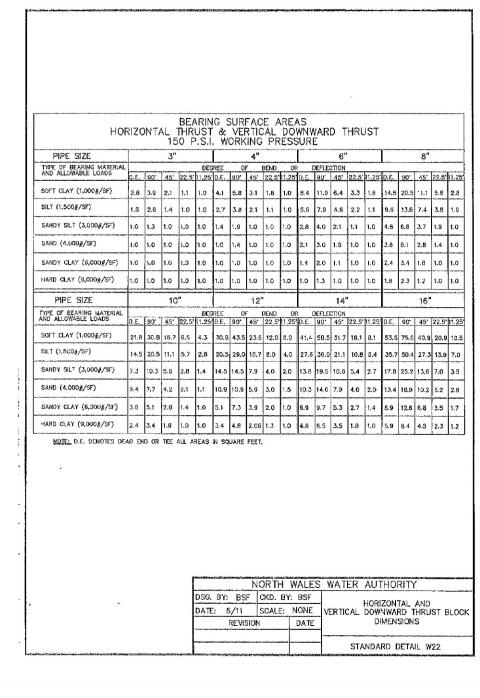
**NEW BRTIAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 

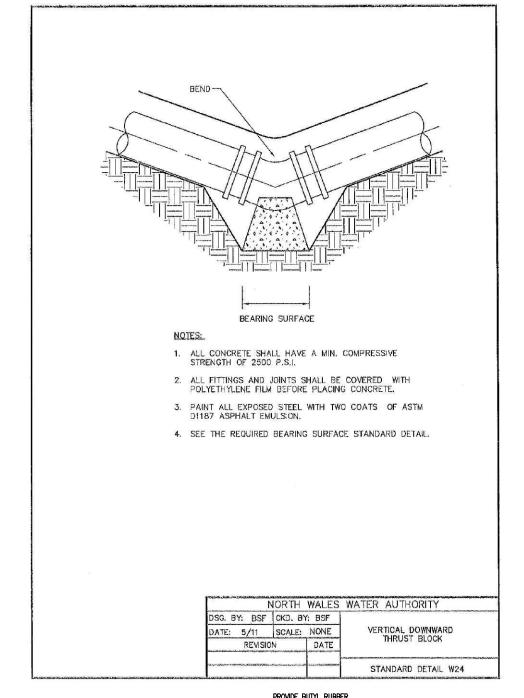


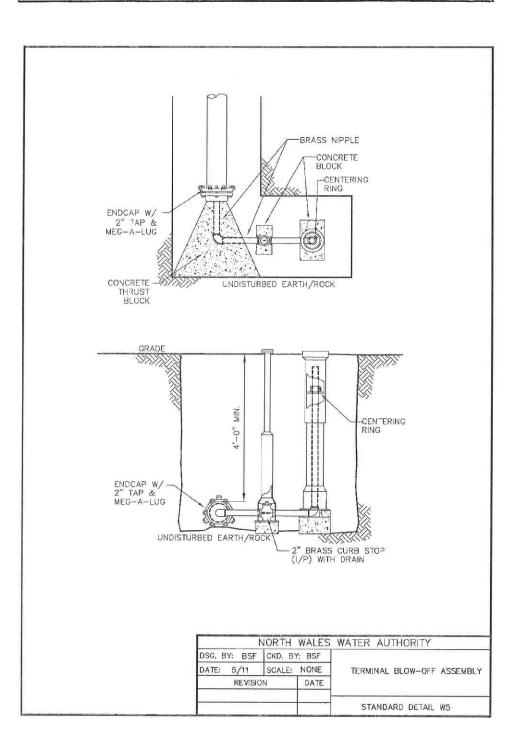


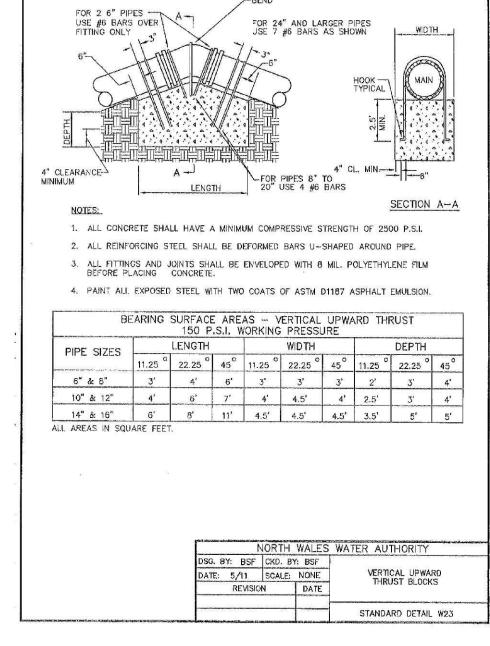


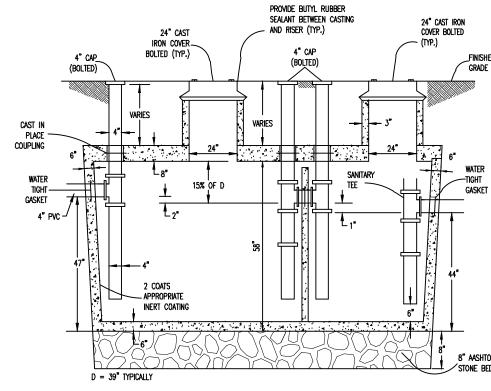












ALL INLET AND OUTLET PIPES SHALL BE INSTALLED NO MORE THAN 6" FROM THE BOTTOM OF THE GREASE TRAP.

TANK TAPERS TOP TO BOTTOM AND IS TRAPEZOIDAL IN CROSS SECTION.

TANK IS 5000 PSI CONCRETE-STEEL REINFORCED (@ 28 DAYS) CONCRETE CONFORMS TO ACI 318-16-4.5.1 AND 318-16-4.5.2, ASTM A615 AND A185

-DIMENSION: 151" INTERIOR/ 163" EXTERIOR LENGTH x 72" INTERIOR/ 84" EXTERIOR WIDTH

NOTES:

1) WHEN LOCATED IN DRIVEWAYS OR PAVED AREAS, GREASE TRAP TO BE DESIGNED FOR APPROPRIATE LOAD BEARING CONDITIONS. GREASE TRAP SHALL BE CAPABLE OF WITHSTANDING HS—20 LOADING.

2) ALL PIPE PENETRATIONS SHALL BE WATERTIGHT.

3) GREASE TRAP SHALL BE PROVIDED WITH GAS-TIGHT MANHOLE COVERS, IN ACCORDANCE WITH TOWNSHIP STANDARD SPECIFICATIONS.
 4) PRECAST CONCRETE SHALL HAVE A MINIMUM 28-DAY COMPRESSIVE STRENGTH 5000 PSI.
 5) EXTERIOR CONCRETE SURFACES BELOW GRADE SHALL HAVE 2 COATS OF COAL TAR EPOXY.
 6) SPECIFIC SEALANT DETAIL AT CONCRETE RISER TO CONCRETE VAULT INTERFACE SHALL BE WATERTIGHT. AT A MINIMUM, THE

JOINT SHALL BE SEALED WITH BUTYL RUBBER SEALANT (KENT SEAL #2 OR APPROVED EQUIVALENT) AND THE EXTERIOR OF

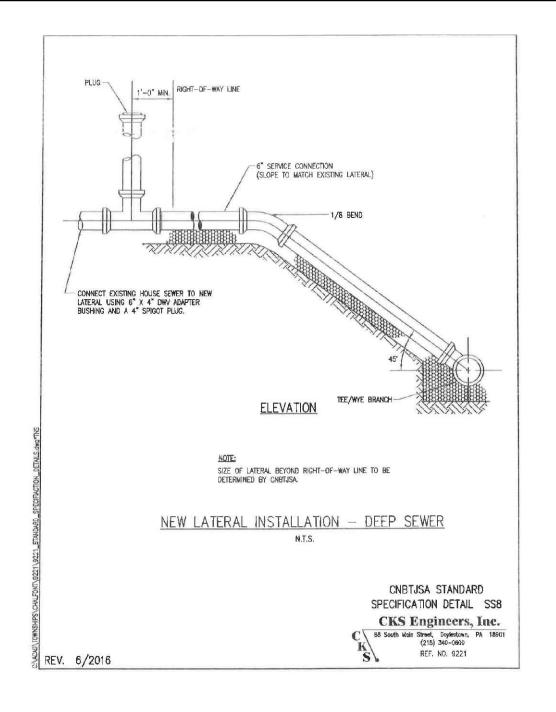
THE JOINT SHALL BE SEALED WITH NON-SHRINK GROUT IN CONFORMANCE WITH THE TOWNSHIP STANDARD GREASE TRAP DETAIL.

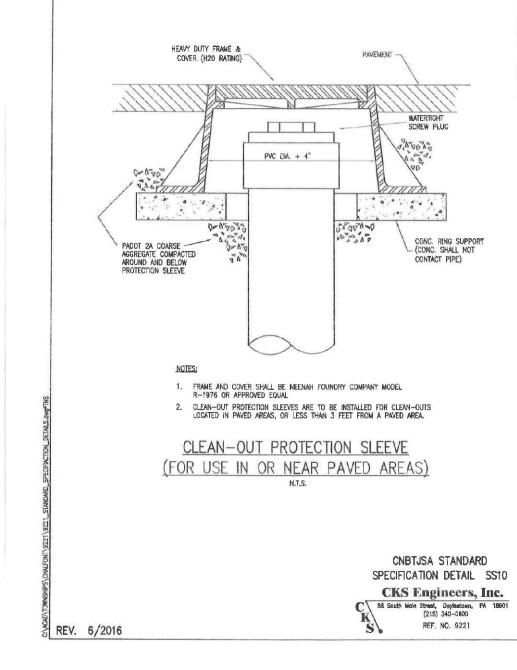
7) TANK SHALL BE TESTED FOR WATER TIGHTNESS BY FILLING FOR 24 HRS. TO SOAK, THEN TOPPED OFF, AND THEN WATCHED FOR 24 HRS. NO DROP IN WATER IS ALLOWED.

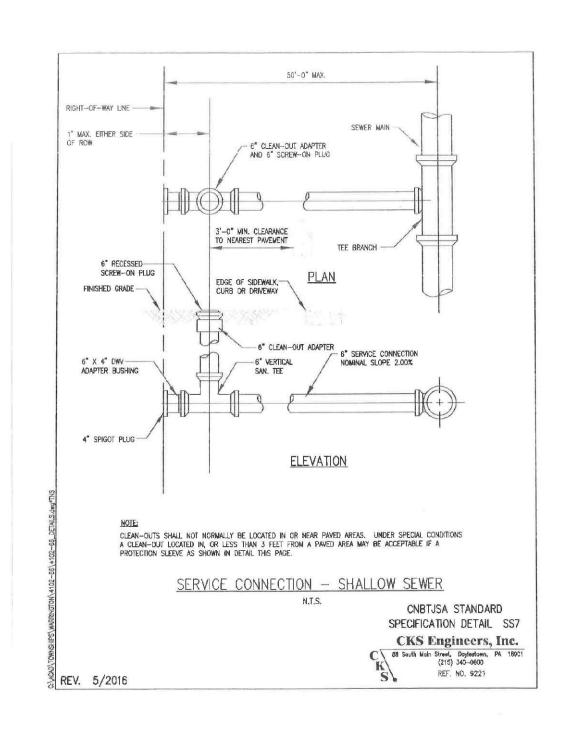
8) CAST IRON SHALL BE BOLTED TO CONCRETE WITH MASTIC TAPE (KENT SEAL OR APPROVED EQUIVALENT) SEALANT.
 9) MAXIMUM EARTH COVER=5.0', HS-20 LOADING.
 10) INLET AND OUTLET EQUIPPED WITH PIPE SEALS.

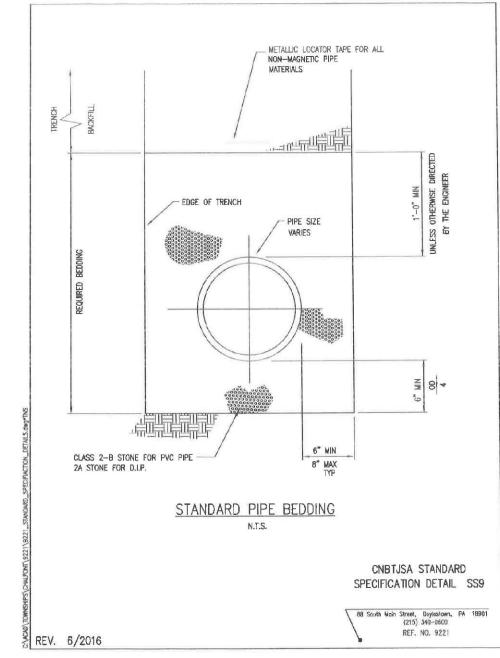
GREASE TRAP (1,500 GALLON - 2 COMPARTMENT)

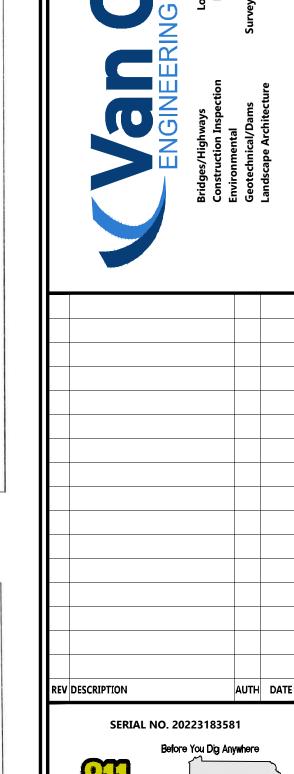
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PLAN NOTATION

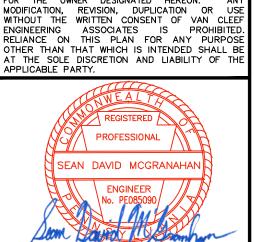
ONLY THOSE PLANS WHICH CONTAIN A DIGITAL,
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VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARED
FOR THE OWNER DESIGNATED HEREON. ANY

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**VAN CLEEF ENGINEERING ASSOCIATES, LLC** 

**UTILITY DETAILS** 

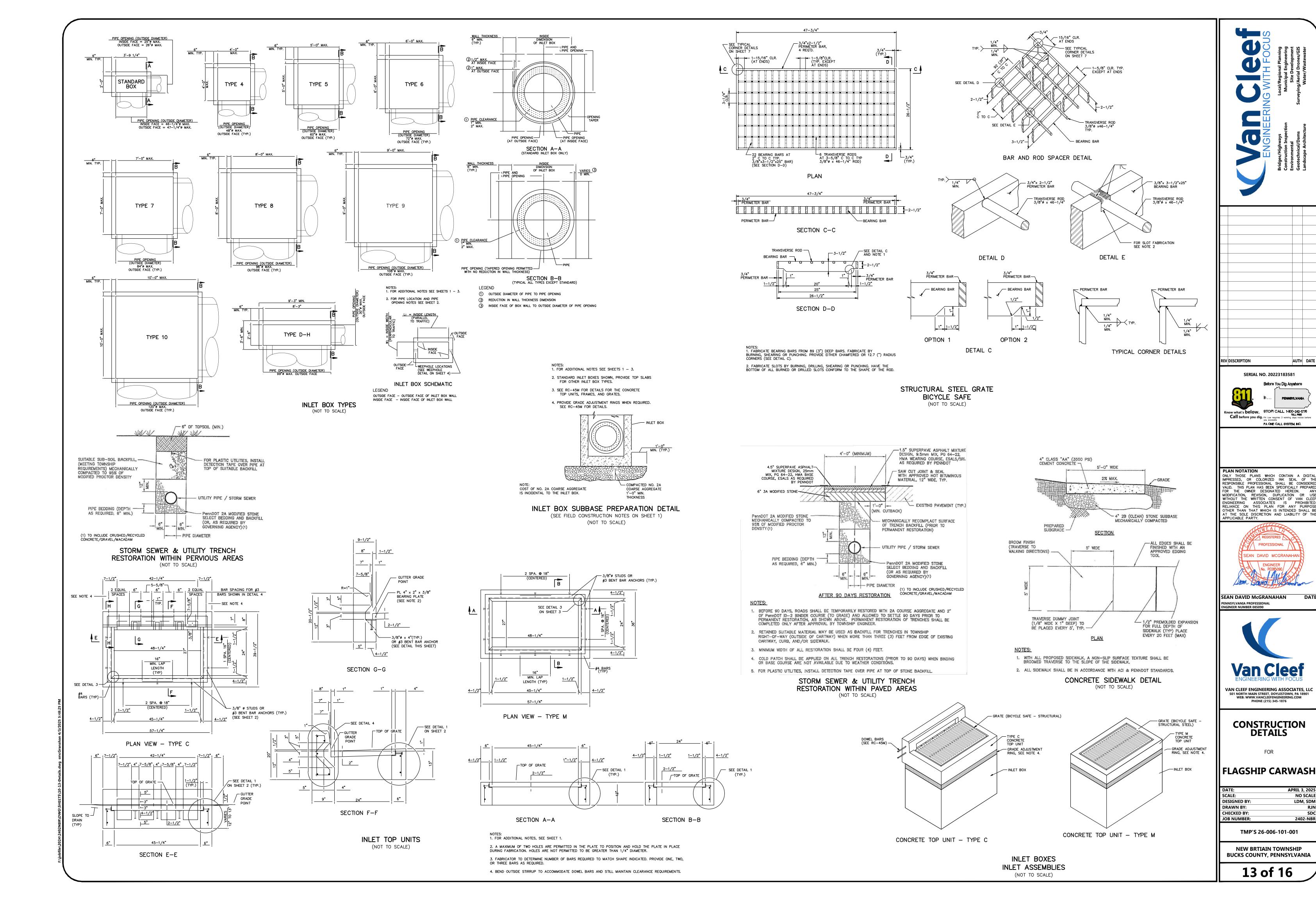
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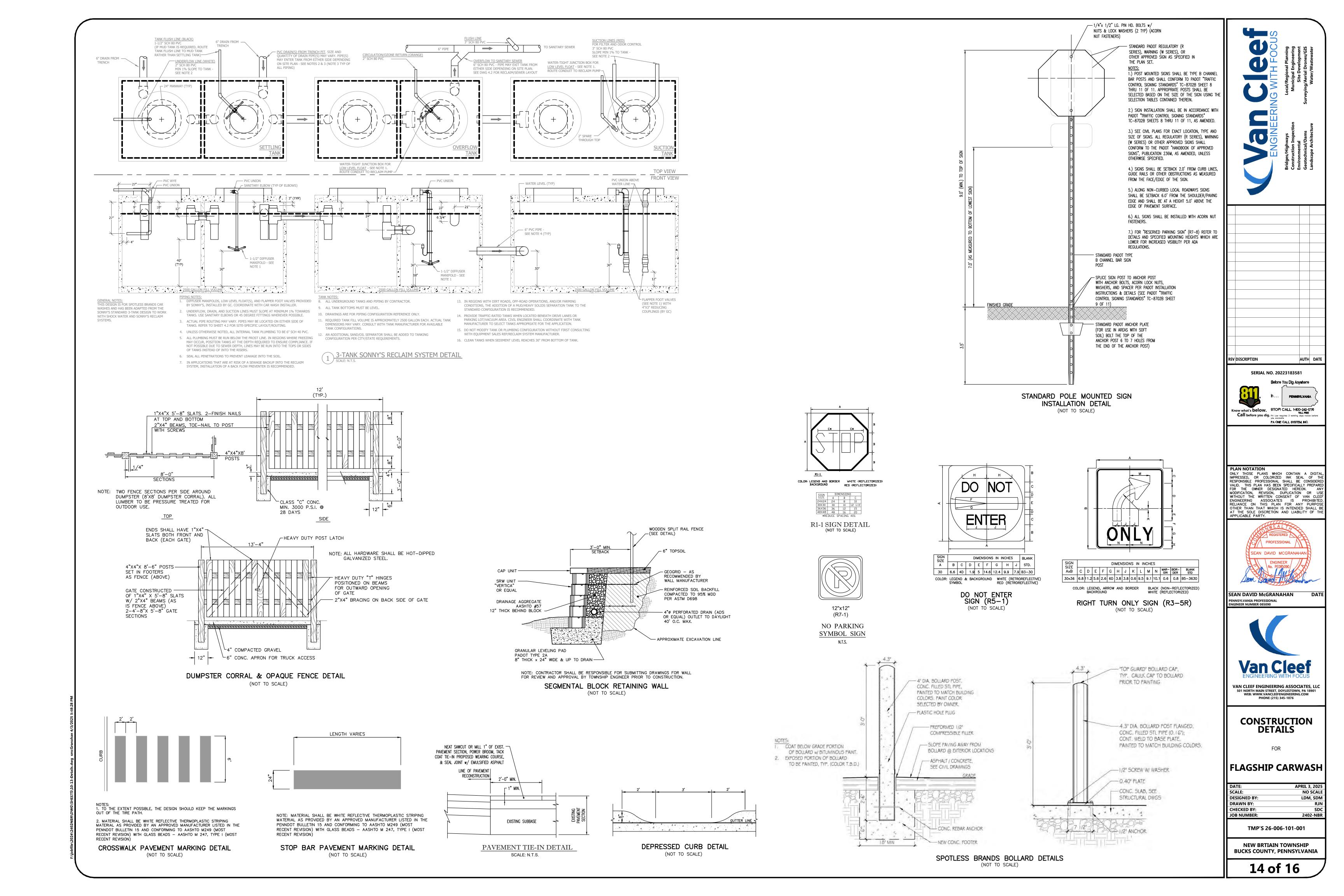
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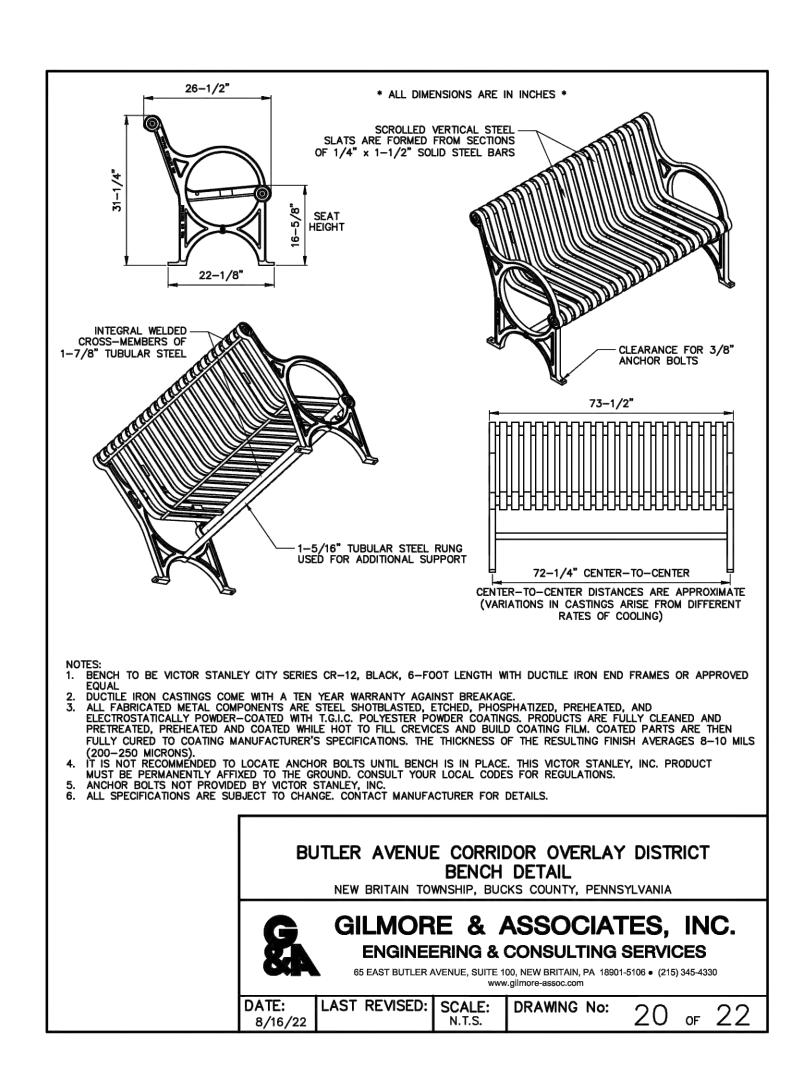
DATE: APRIL 3, 2025
SCALE: NO SCALE
DESIGNED BY: LDM, SDM
DRAWN BY: RJN
CHECKED BY: SDC
JOB NUMBER: 2402-NBR

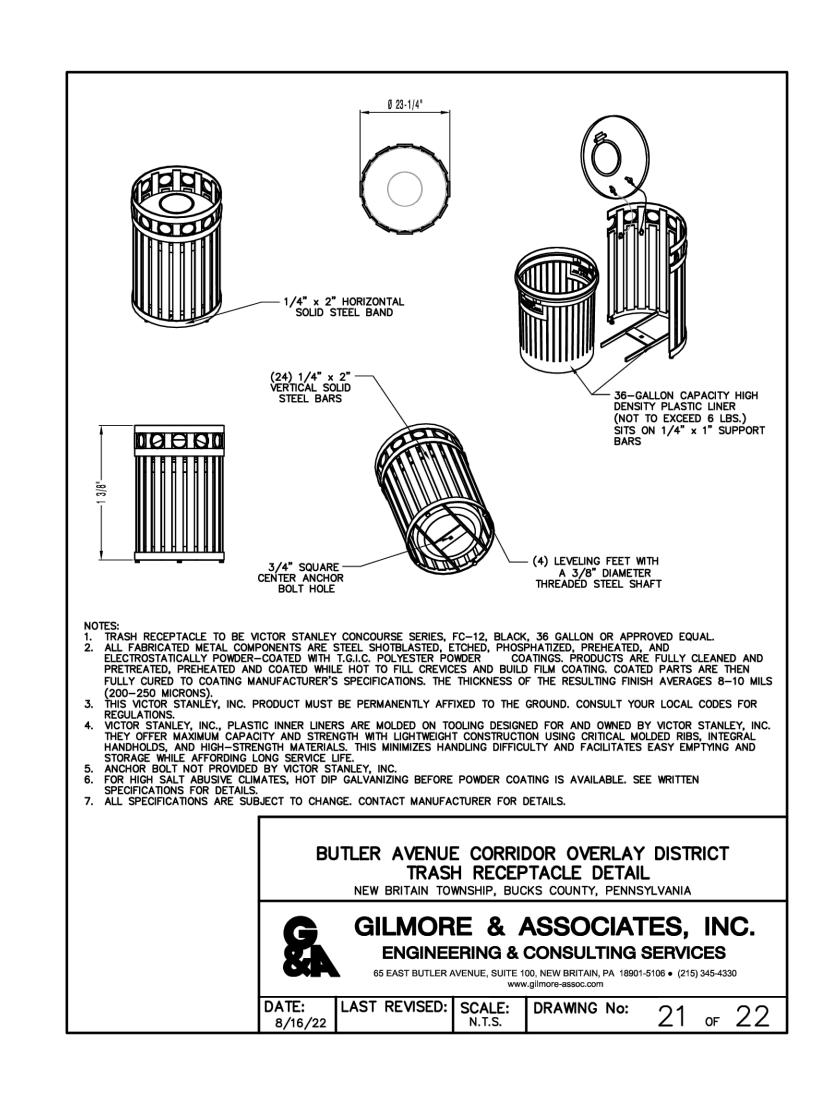
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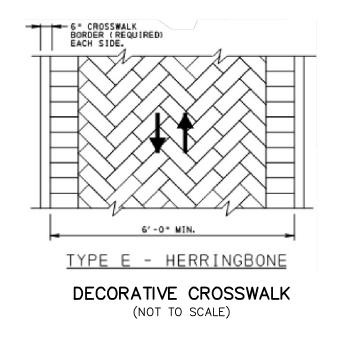
NEW BRTIAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA

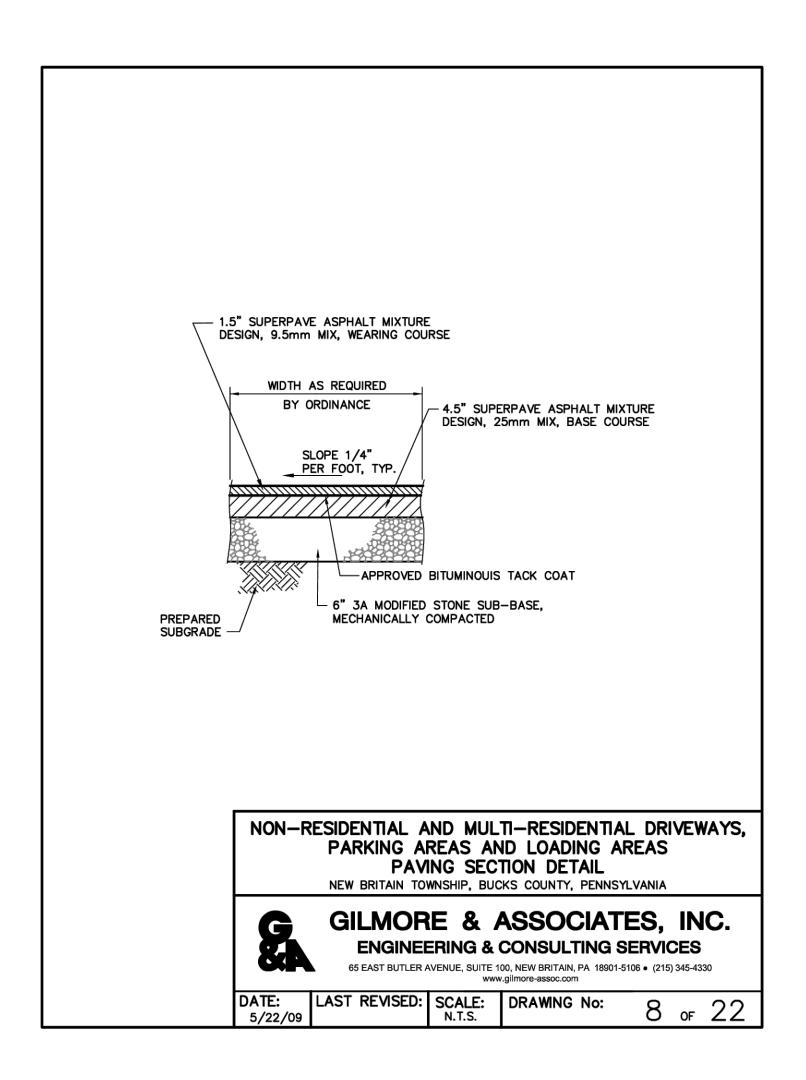


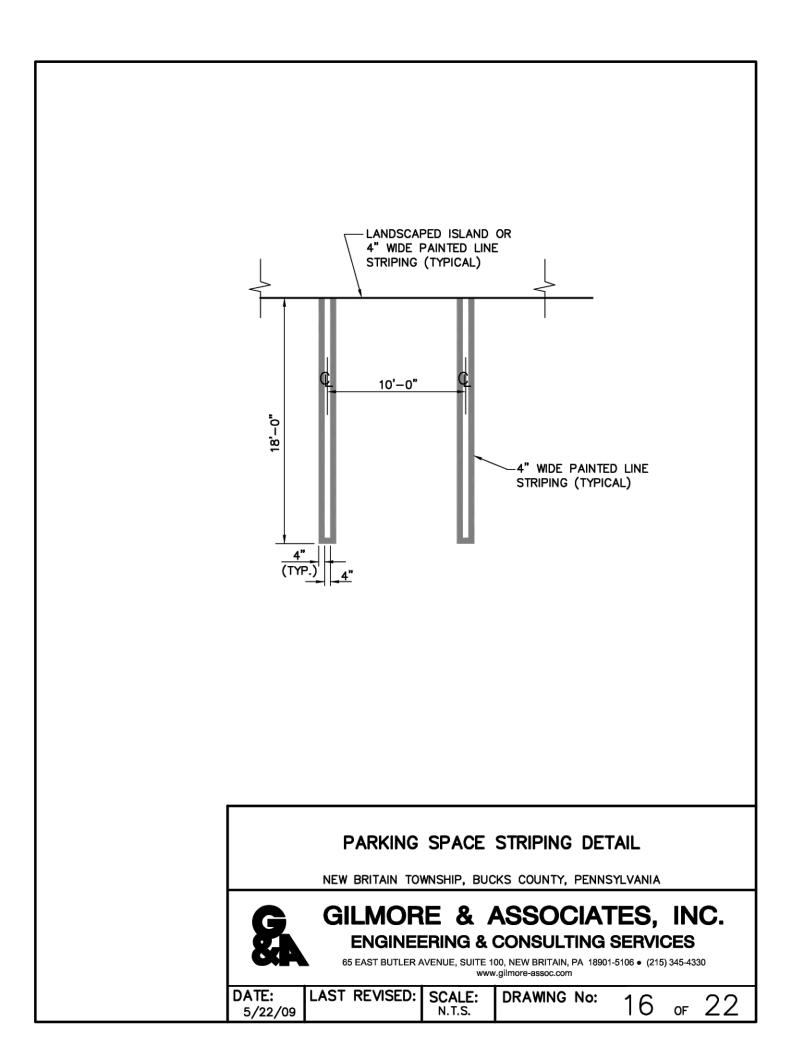


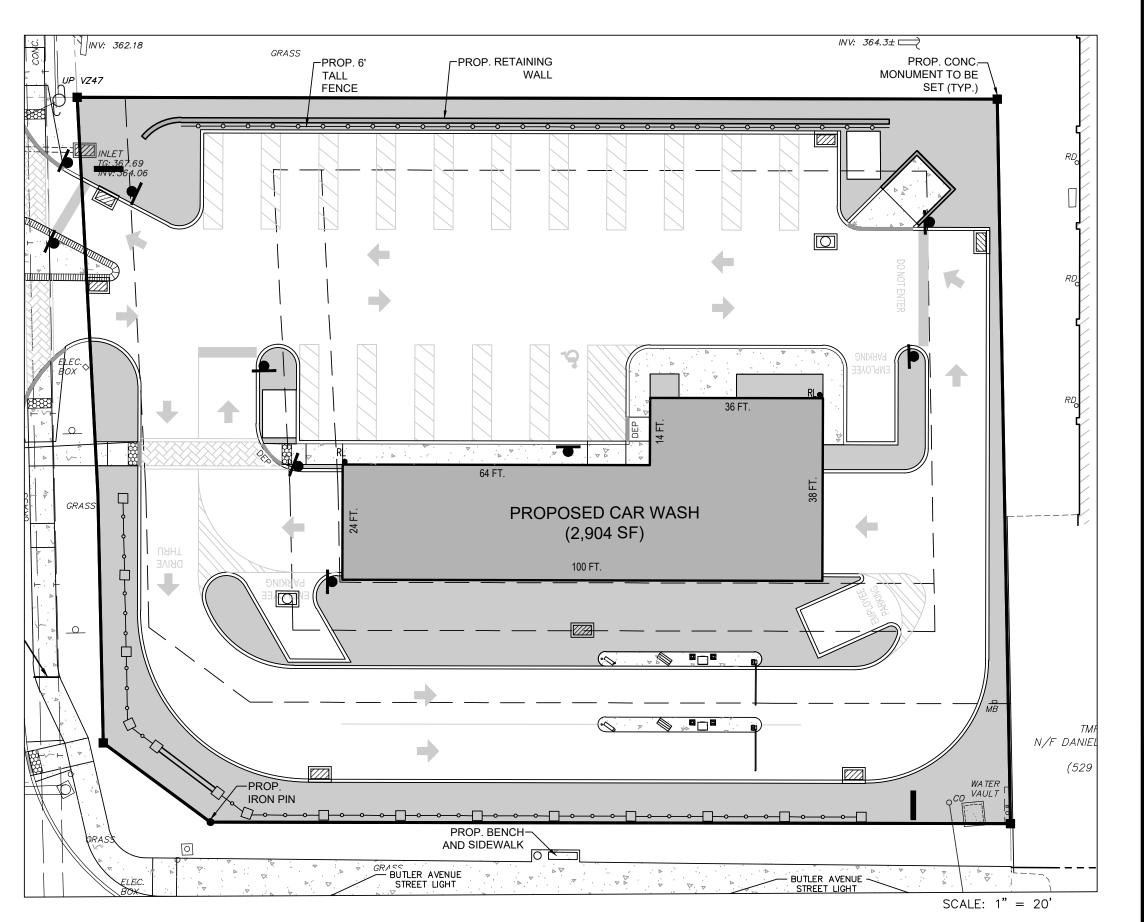






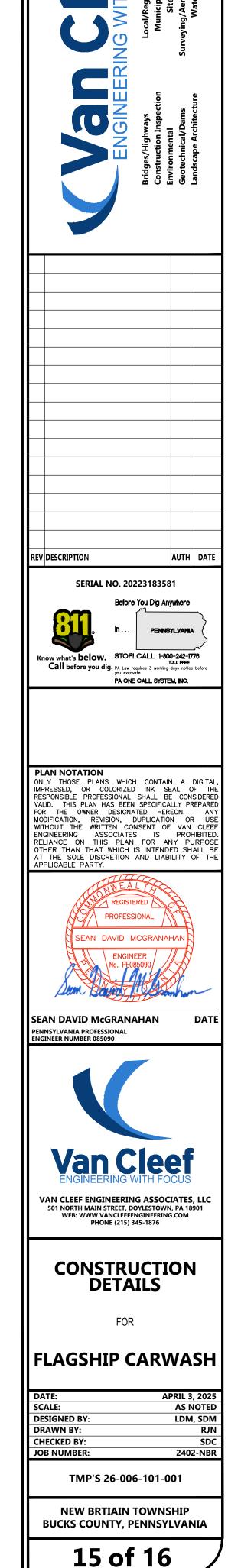


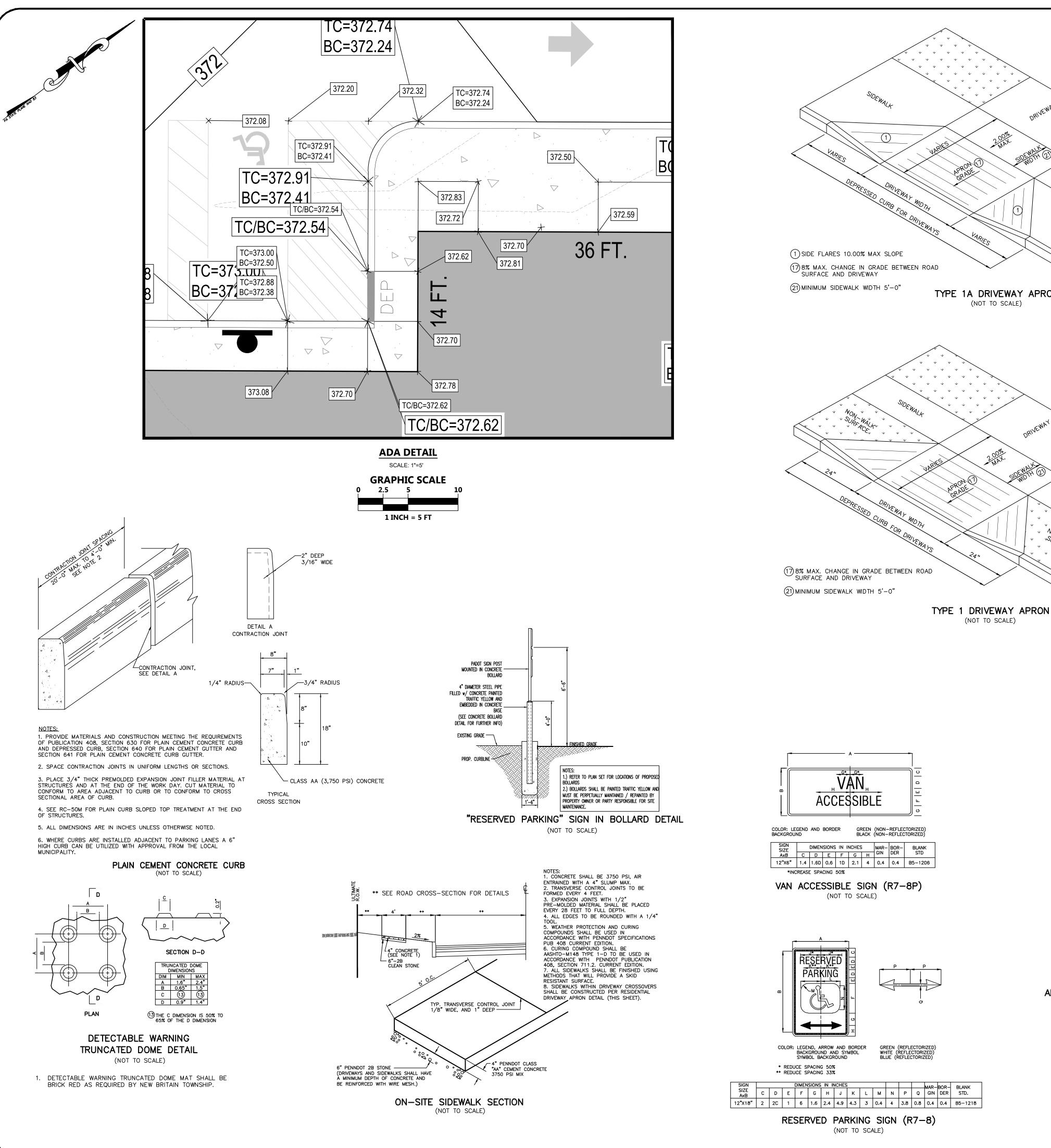


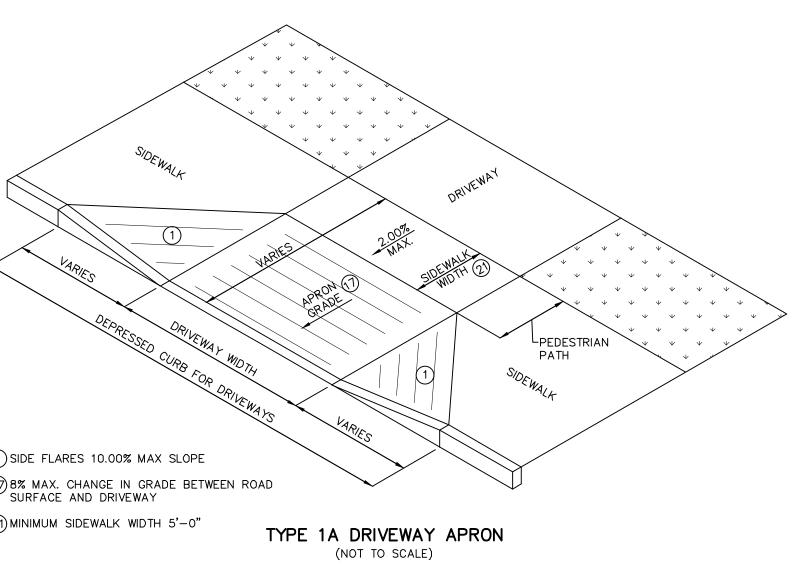


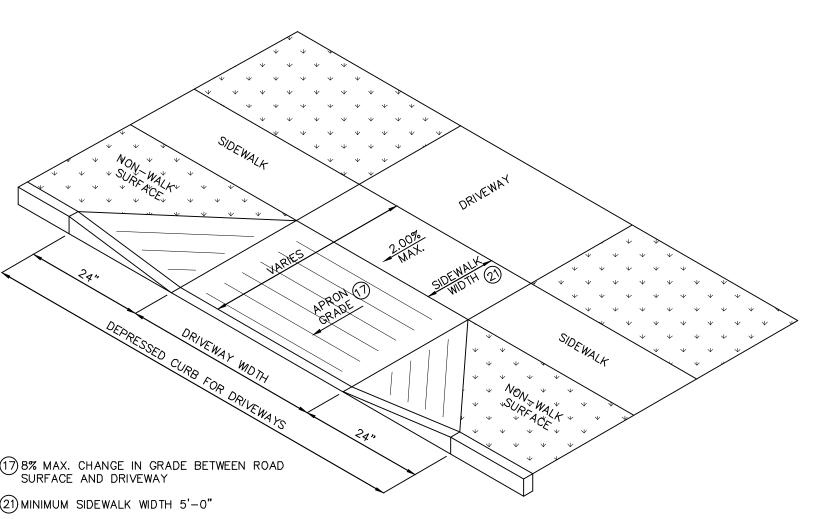


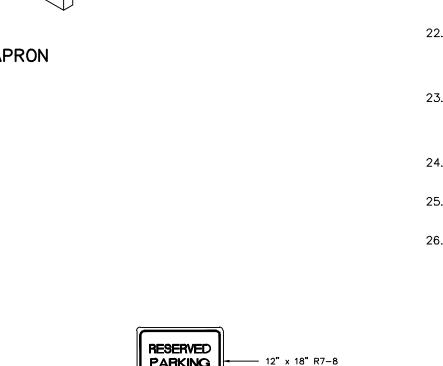
0.65 ACRES X 43,560 SF = 28,314 SF X 0.15 = 4,247 SF GREEN SPACE REQUIRED. GREEN SPACE AROUND BENCH, GATEWAY MONUMENT SIGN, AND TRASH RECEPTACLE OPEN GREEN AREA = 7,112 SF PROVIDED.

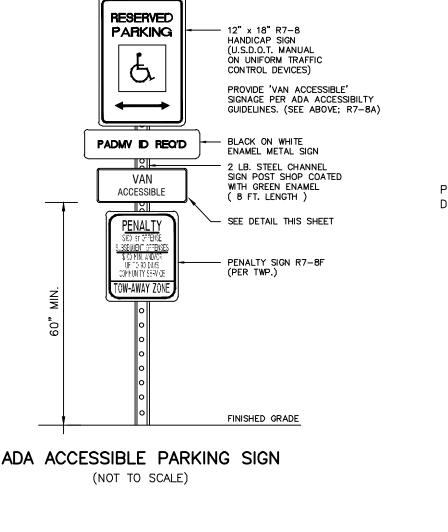


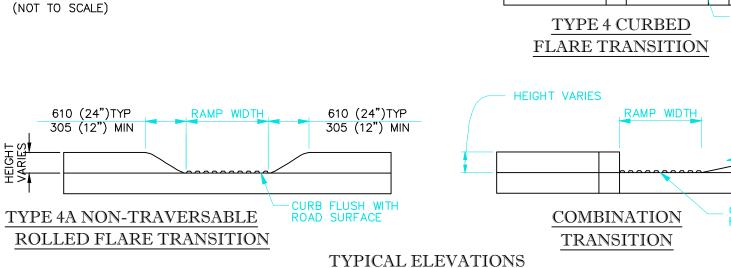












FOR DEPRESSED CURBS

**GENERAL NOTES:** 

- 1. PROVIDE MATERIALS AND CONSTRUCTION MEETING THE REQUIREMENTS OF PUBLICATION 408, SECTIONS 350, 409, 630, 676 AND 694.
- 2. PROVIDE EXPANSION JOINT MATERIAL 1/2" THICK WHERE CURB RAMP ADJOINS ANY RIGID PAVEMENT, SIDEWALK OR STRUCTURE WITH THE TOP OF JOINT FILLER FLUSH
- 3. CONSTRUCT DIAGONAL CURB RAMPS WITH A 4'-0" x 4'-0" CLEAR SPACE BEYOND THE CURB FACE, WITHIN THE WIDTH OF THE CROSSWALK AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE.
- 4. SEAL JOINTS WITH AN APPROVED SEALING MATERIAL.

WITH ADJACENT CONCRETE SURFACE.

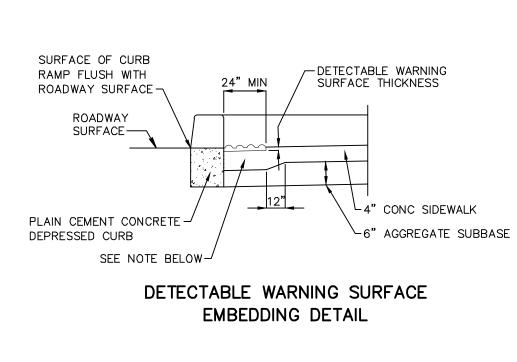
- 5. PROVIDE SLIP RESISTANT TEXTURE ON CURB RAMP BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP. EXTEND TEXTURE THE FULL WIDTH AND LENGTH OF THE CURB RAMP INCLUDING FLARED SIDE RAMPS.
- 6. MODIFY CONSTRUCTION DETAILS TO ADAPT DIMENSIONS TO EXISTING CURB ALTERATIONS WHERE THE CURB IS LESS THAN THE STANDARD 8" HEIGHT.
- 7. CURB RAMP AND SIDE FLARE LENGTHS ARE VARIABLE AND BASED ON CURB HEIGHT AND THE SIDEWALK PITCH.
- 8. TO AVOID CHASING GRADE INDEFINITELY WHEN TRAVERSING THE HEIGHT OF CURB, RAMP LENGTH NOT TO EXCEED 15'-0". ADJUST RAMP SLOPE AS NEEDED TO PROVIDE ACCESS TO THE MAXIMUM EXTENT FEASIBLE.
- 9. NON-WALK AREA IS AN OBSTRUCTED OR GRASS/NON-PAVED AREA ADJACENT TO THE PEDESTRIAN ACCESS ROUTE THAT IS NOT USED BY THE PEDESTRIAN FOR
- 10. THE DETAILS DEPICT PEDESTRIAN PUSHBUTTON POLES TO ILLUSTRATE THE RECOMMENDED PLACEMENT OF PEDESTRIAN PUSHBUTTONS. FOR ALTERATION PROJECTS, PROVIDE ACCESS TO EXISTING PEDESTRIAN PUSHBUTTONS TO THE MAXIMUM EXTENT FEASIBLE. INSTALL PEDESTRIAN PUSHBUTTON STUB POLES, WHERE APPLICABLE, SO AS NOT TO CREATE PEDESTRIAN OBSTRUCTIONS.
- 11. ALL DIMENSIONS ARE IN U.S. CUSTOMARY UNITS UNLESS OTHERWISE NOTED.
- 12. ALIGN DETECTABLE WARNING DOMES ON A SQUARE GRID IN THE PREDOMINANT DIRECTION OF THE RAMP AND PERPENDICULAR TO CURB.
- 13. PROVIDE DETECTABLE WARNING SURFACES (DWS) 24" MINIMUM (IN THE DIRECTION OF PEDESTRIAN TRAVEL) ACROSS FULL WIDTH OF RAMP AT THE GRADE BREAK NEAR STREET EDGE. PROVIDE DWS THAT CONTRAST VISUALLY WITH ADJACENT WALKWAY SURFACES, EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT FOR THE FULL
- 14. FOR NEW CONSTRUCTION, DO NOT EXCEED 2.00% CROSS SLOPE ON THE CURB RAMP OR ACCESSIBLE ROUTE.
- 15. FOR NEW CONSTRUCTION AND ALTERATIONS, CONSTRUCT CURB RAMP AND FLARE SLOPES WITH THE FLATTEST SLOPE POSSIBLE. THE SLOPES INDICATED IN THE DETAILS SHOW THE MAX SLOPE ALLOWABLE. SLOPES THAT EXCEED THOSE INDICATED IN THE DETAILS, OR CONTRACT DOCUMENTS AS APPLICABLE, WILL NOT BE ACCEPTED AND WILL BE RECONSTRUCTED.
- 16. CONSTRUCT SIDEWALKS AT A LONGITUDINAL SLOPE NOT TO EXCEED 5.0%. FOR ROADWAY PROFILE SLOPES THAT EXCEED 5.0%, CONSTRUCT PARALLEL SIDEWALKS ADJACENT TO ROADWAY AT A LONGITUDINAL SLOPE NOT TO EXCEED ROADWAY
- SURFACE IS NOT TO EXCEED AN ALGEBRAIC DIFFERENCE OF 11.0%. THE COUNTER SLOPE OF THE GUTTER OR ROAD AT THE FOOT OF A CURB RAMP, LANDING OR BLENDED TRANSITION IS NOT TO EXCEED 5.0%.

17. THE CHANGE IN GRADE AT THE BOTTOM OF THE CURB RAMP AND ADJOINING ROAD

CONSTRUCTION. ALL CONSTRUCTION MUST MEET THE STANDARDS CONTAINED HEREIN UNLESS OTHERWISE NOTED OR DIRECTED. 19. ALL SLOPES ARE MEASURED WITH RESPECT TO A LEVEL PLANE. THEREFORE, THE

18. THE CONSTRUCTION STANDARDS DEPICTED ARE MOST APPROPRIATE FOR NEW

- LENGTH OF RAMP IS NOT SOLELY DEPENDANT ON THE HEIGHT OF CURB. (FOR EXAMPLE, A 6" CURB DOES NOT NECESSARILY MEAN A RAMP LENGTH OF 6'-0" FOR A 1:12 SLOPE.
- 20. SIDEWALK WIDTH MAY BE REDUCED TO 4'-0", WHEN PASSING AREAS  $5'-0" \times 5'-0"$ ARE PROVIDED EVERY 200'.
- 21. THE TRAVEL LANE IS DEFINED BY THE OUTSIDE EDGE OF THE WHITE PAVEMENT MARKING LINE. IF A WHITE PAVEMENT MARKING LINE DOES NOT EXIST, THE TRAVEL LANE IS DEFINED BY THE CONTRACT DOCUMENTS.
- 22. CONSTRUCT DEPRESSED CURB FOR CURB RAMPS FLUSH TO ADJACENT ROADWAY. GRADE EDGE OF ROAD ELEVATIONS AT THE FLOW LINE TO ENSURE POSITIVE DRAINAGE AND PREVENT PONDING. FOR LEVEL LANDINGS BEHIND DEPRESSED CURB, ADJUST SLOPES TO PROVED POSITIVE DRAINAGE.
- 23. CHEEK WALLS ARE PERMITTED WHEN ADJACENT TO NON-WALK AREAS OR ELEVATION DIFFERENCES CANNOT BE ACCOMMODATED BY FLARES OR GRADING. GRADE GRASS AREAS OR OTHER NON-WALK AREAS AT 3:1 MAX. DO NOT INSTALL CHEEK WALLS THAT INTERSECT THE PEDESTRIAN ACCESS ROUTE.
- 24. CONSTRUCT TOP OF PLAIN CEMENT CONCRETE DEPRESSED CURB TO BE FLUSH WITH ADJACENT SURFACES (RAMPS, SIDEWALKS, FLARES)
- 25. FOR CURB RAMPS THAT LEAD TO A SINGLE CROSSWALK, THE RAMP (EXCLUDING FLARES) TO BE FULLY INSIDE OF MARKED CROSSWALK LINES.
- 26. A 4'-O" MAXIMUM DIGITAL DISPLAY LEVEL WILL BE USED TO VERIFY THE SLOPES OF CURB RAMPS AND SIDEWALKS.



501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM PHONE (215) 345-1876

**ADA DETAILS** 

FOR

VAN CLEEF ENGINEERING ASSOCIATES, LLC

REV DESCRIPTION

AUTH DATE

**SERIAL NO. 20223183581** 

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EAN DAVID MCGRA

EAN DAVID McGRANAHAN

NEER NUMBER 085090

ENGINEER

ALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARI OR THE OWNER DESIGNATED HEREON. AN

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Call before you dig. PA Law requires 3 working days no

Before You Dig Anywhere

PA ONE CALL SYSTEM, INC.

PENNSYLVANIA

FLAGSHIP CARWASH

**AS NOTE** DRAWN BY CHECKED BY **DB NUMBER** 

TMP'S 26-006-101-001

**NEW BRTIAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 



Traffic Engineering and Mobility Solutions

133 Rutgers Avenue Swarthmore, PA 19081

October 29, 2023

John Lombardo Sr. Development Manager – Entitlements and Permits Spotless Brands 950 Herndon Parkway, Suite 400 Herndon, VA 20170 via email only

**RE:** Traffic Impact Analysis - 545 West Butler Avenue (Flagship Car Wash)

Butler Avenue (SR 4202) and County Line Road (SR 2038)

New Britain Township, Bucks County

#### Mr. Lombardo:

#### **EXECUTIVE SUMMARY**

Spotless Brands proposes to redevelop the site of a vacant convenience store as a *Flagship Carwash*, with a single tunnel (100 feet) and sixteen vacuum positions. The site is within the northwest quadrant of the intersection of County Line Road (SR 2038) and Butler Avenue (SR 4202) in New Britain Township, Bucks County. The site currently has full movement vehicular access to both County Line Road and Butler Avenue. As part of the project the Butler Avenue driveway will be removed and exiting left turns will be prohibited at the County Line Road driveway. The location of the site is illustrated in **Figure 1**.

Car Wash

Figure 1 – Location

This document evaluates the traffic impacts of the proposed redevelopment relative to generally

accepted traffic engineering practice and §27-2501 of the New Britain Township Zoning Code. This evaluation relies in part on the Traffic Impact Analysis approved by PennDOT in April 2024 as part of previous application to redevelop the site as a *Dunkin'* and *Papa Johns* (EPS 336574) and the discussion at the September 24, 2024 Township Planning Commission meeting.

As discussed with the Township at the sketch plan meeting, the traffic associated with the proposed Car Wash will be significantly less than the previous convenience store use, and the access modifications will address previous issues with collisions. On-site circulation will be one-way counterclockwise, maximizing the available stacking.



The results of the Traffic Statement indicate that:

- Compared to the previous use, the proposed redevelopment is anticipated to generate significantly less traffic.
- The on-site layout is anticipated to provide adequate space for queued vehicles without impacting the adjacent public roads.
- The site accesses and study intersection will operate at acceptable Overall Levels of Service (LOS) and that mitigation or improvements are not warranted.

#### **EXISTING CONDITIONS**

#### **Property and Current Use**

The subject property parcel is  $\pm 0.65$ -acres (gross) within the C1 Commercial District of New Britain Township, Bucks County. The site is currently developed with a vacant  $\pm 3,300$  square foot convenience store. The site is accessed via two full movement driveways, one to County Line Road and one to Butler Avenue.

Based on the information provided in <u>Trip Generation Manual</u>, 11<sup>th</sup> Edition, published by the Institute of Transportation Engineers (ITE), it is estimated that the vacant convenience store use would generate 2,516 gross trips per day, including 206 during the morning peak hour and 162 during the evening peak hour.

#### **Existing Transportation Infrastructure**

County Line Road (SR 2038) is a two-way, State maintained Arterial roadway. For the purposes of this analysis County Line Road is assumed to run east-west. Adjacent to the site, County Line Road has a paved cartway of  $\pm 65$  feet providing for two lanes westbound (through and drop), three lanes eastbound (left, through and through/right), and shoulders. West of the site, County Line Road provides one lane in each direction. The posted speed limit for County Line Road is 45 miles per hour.

Butler Avenue (SR 4202) is a two-way, State maintained Arterial roadway. Adjacent to the site, Butler Avenue has a paved cartway of  $\pm 50$  feet providing for three lanes southbound (left, through and right) and one lane northbound. North of the site Butler Avenue provides one lane in each direction. The posted speed limit is 45 miles per hour.

The intersection of County Line Road and Butler Avenue is controlled by an actuated, coordinated traffic signal (Permit #64-0336, System #I-0121). Left turn lanes are provided on all approaches and the signal operates with protected-permitted left turn phasing on all approaches. Right turn lanes are provided on the northbound and southbound Butler Avenue approaches. Marked crossings, pedestrian indications, and pushbuttons are provided for all approaches.

There is no designated on-street parking within the vicinity of the site. The Link Belt Station (SEPTA Regional Rail - Lansdale/Doylestown) is  $\pm 1.25$  miles from the project site. The Route 132 Bus is  $\pm 1.5$  miles from the site at Doylestown Road and Cowpath Road. There are sidewalks along the site frontage, including upgraded curb ramps at the intersection of County Line Road and Butler Avenue.



#### **Traffic Volumes**

Existing Annual Average Daily Traffic (AADT) Volumes for the adjacent roadways are provided in **Table 1**.

Roadway	Existing AADT	Source
County Line Road	15,000	2023 AADT Traffic Volume Map, Bucks County
Butler Avenue	12,000	prepared by PennDOT

Table 1 - Existing Average Daily Traffic

As part of the previous application, morning and evening peak period traffic volume data for the intersection of County Line Road and Butler Avenue was collected on Tuesday, September 26, 2023. Supplemental Saturday peak period traffic volumes were collected on September 7, 2024. The peak hour vehicular turning movement volumes are illustrated in **Figure 2**.

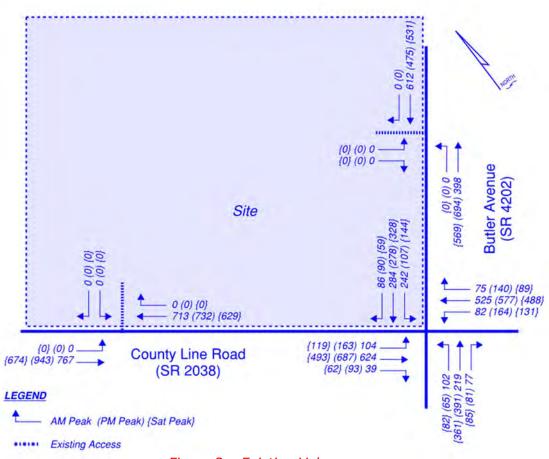


Figure 2 - Existing Volumes



#### Existing Level of Service

The existing morning peak hour operations of off-site study intersections were evaluated using the procedures outlined in the <u>Highway Capacity Manual 6<sup>th</sup> Edition</u> via *Trafficware Synchro version 11* software. The results of these analyses indicate that the intersection of County Line Road and Butler Avenue currently operates at an Overall (LOS) "D".

#### Sight Distance

The accesses were evaluated to determine compliance with PennDOT sight distance requirements. As previously noted, and presented in **Table 2**, there is adequate sight distance.

Access	Speed Limit	Movement	Desirable <sup>(1)</sup>	Measured
County Line Road	45 mph	Exiting Right Turn	635′	>700′(2)
		Entering Left Turn	470′	>400′(2)
Butler Avenue	utler Avenue 45 mph Exiting Right Tu		635′	>700′(2)

<sup>(1)</sup> PA 67, Chapter 441

Table 2 - Sight Distance

#### **Collision History**

The most current five years of publicly available, <u>state reportable</u> collision data was reviewed using the Pennsylvania Crash Information Tool (PCIT). The data indicated that from 2019 through 2023 there were nine reportable crashes along the Butler Avenue site frontage, seven along the County Line Road site frontage, and twenty-one at the signalized intersection.

PennDOT considers collision records confidential pursuant to 75 Pa. C.S. §3754 and 23 U.S.C. §409. Therefore, the referenced data is provided under separate cover.

#### FUTURE "PRE-DEVELOPMENT" TRAFFIC CONDITIONS Pre-Development Traffic Volumes

To assess the impact of the traffic generated by the proposed development an analysis of 2026 "pre-development" conditions without the proposed project was prepared. The applicant is unaware of other pending developments anticipated to impact the study intersections. Therefore, the evaluation of pre-development conditions relied on the application of the current 0.12% growth rate (PennDOT Bureau of Planning and Research – August 2024 to July 2025) to the counted traffic on the public roadways. **Figure 3** illustrates the pre-development traffic volumes for an assumed 2025 buildout.

#### **Pre-Development Operations**

The pre-development traffic volumes were evaluated using the same methodology as the existing conditions analysis. The results of these analyses indicate that under pre-development conditions the study intersection is projected to operate in a manner similar to existing conditions.

<sup>(2)</sup> Through the signalized intersection

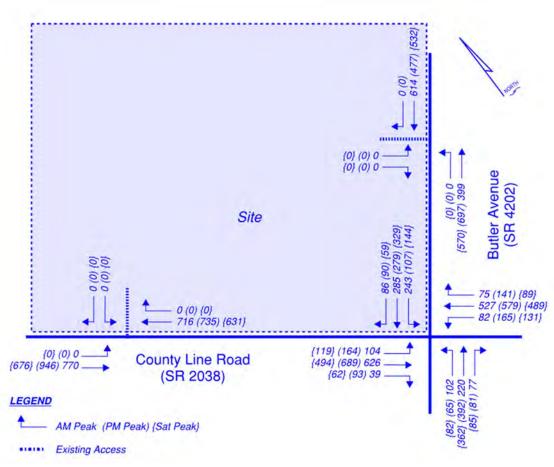


Figure 3 - 2026 Pre-development Volumes

#### **PROPOSED CONDITIONS**

#### **Project Overview**

It is proposed to redevelop the site of a vacant convenience store as a *Flagship Carwash*, with a single tunnel (100 feet) and sixteen vacuum positions. The site is within the northwest quadrant of the intersection of County Line Road (SR 2038) and Butler Avenue (SR 4202) in New Britain Township, Bucks County.

As illustrated in the plan prepared by Van Cleef Engineering Associates, LLC, the Butler Avenue driveway will be eliminated, and exiting left turns will be prohibited at the County Line Road driveway. Traffic will enter the site from County Line Road and circulate counterclockwise. The proposed layout will accommodate at least eighteen queued vehicles.



#### Traffic Generation

The traffic to be generated by the proposed Car Wash has been estimated using the information provided in <u>Trip Generation Manual</u>, 11<sup>th</sup> Edition, published by the Institute of Transportation Engineers (ITE). <u>Trip Generation</u> provides data and a methodology for estimating the traffic to be generated by a range of developments.

Traffic during the weekday evening and Saturday peaks was estimated based on the average rate for Land Uses 948 (Automated Car Wash) using the number of tunnels as the independent variable. <u>Trip Generation</u> does not provide information for the morning peak; therefore, the evening peak volumes were used to evaluate the morning peak conditions. No adjustments were made for potential pass-by traffic, transit usage, or pedestrian traffic. The results are presented in **Table 3**.

Hee	Morning		Evening			Saturday			
Use	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total
Automated Car Wash (LU 948 – 1 tunnel)	39	39	78	39	39	78	19	22	41

Table 3 - Proposed Redevelopment Peak Hour Trip Generation

A comparison of the driveway traffic generation for the proposed redevelopment and previous convenience store use is presented in **Table 4**. The proposed redevelopment is anticipated to generate significantly less traffic than the previous use.

Hee	Morning			Evening			Evening		
Use	Enter	Enter	Enter	Enter	Exit	Total	Enter	Exit	Total
Convenience Store <sup>1</sup>	103	103	206	83	79	162	131	130	261
Redevelopment	39	39	78	39	39	78	19	22	41
Change	(64)	(64)	(128)	(44)	(40)	(84)	(112)	(108)	(220)

<sup>1</sup>DLU 851, 3,300 sf

Table 4 - Driveway Trip Generation Comparison

#### **Future Traffic Volumes**

For analysis purposes it was assumed that the vehicular traffic associated with the proposed redevelopment will arrive and depart in a manner similar to existing traffic within the study area. The traffic volumes associated with background growth and the proposed redevelopment were added to the counted existing volumes to provide future traffic volumes. These traffic volumes are illustrated in **Figure 4**.

#### Post-Development Level of Service

Projected post-development traffic volumes were evaluated using the same methodology as the existing and pre-development conditions analyses. The results of these analyses indicate that under post-development conditions the redevelopment will have a de minimus impact on the intersection of County Line Road and Butler Avenue, and the site driveways are projected to operate at acceptable Overall LOS. The results of the capacity analyses are presented in **Table 5**.

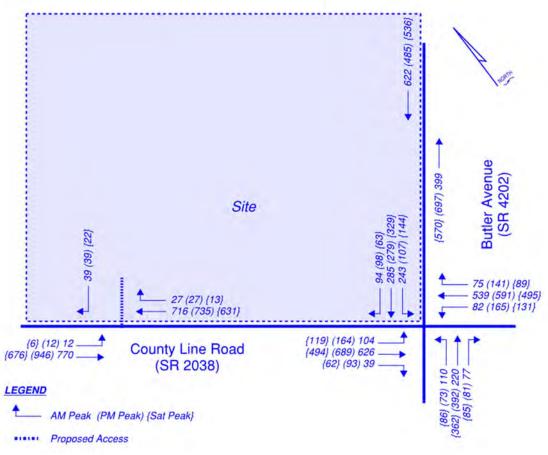


Figure 4 - Future 2026 Volumes

County Lir	ne Road and Butler Avenue	Eastbound	Westbound	Northbound	Southbound	Overall
	Existing	C (24.9)	C (25.7)	E (55.0)	D (46.0)	D (35.3)
AM	No Build	C (25.0)	C (25.9)	E (55.1)	D (46.0)	D (35.3)
	Build	C (25.0)	C (26.1)	D (54.9)	D (45.9)	D (35.4)
	Existing	C (33.9)	C (33.6)	E (59.1)	D (37.7)	D (39.2)
PM	No Build	C (34.1)	C (33.8)	E (59.2)	D (37.7)	D (39.3)
	Build	C (34.1)	C (34.1)	E (58.8)	D (38.1)	D (39.5)
	Existing	C (28.7)	C (29.7)	E (58.3)	D (39.3)	D (37.7)
Saturday	No Build	C (28.7)	C (29.8)	E (58.4)	D (39.4)	D (37.8)
	Build	C (28.8)	C (29.9)	E (58.2)	D (39.6)	D (37.8)

Table 5 – Post-development (2025) Levels of Service



County Line Road and Site Access		Eastbound Left	Southbound Right	Overall
AM	Build	b (10.8)	b (11.1)	a (0.4)
PM	Build	b (11.0)	b (11.2)	a (0.3)
Saturday	Build	b (10.3)	b (10.4)	a (0.2)

Table 5 – Post-development (2025) Levels of Service (continued)

#### Car Wash Operations

Vehicles using the car wash will start at one of the dual pay stations located on the east side of the building before proceeding through the tunnel. The tunnel is proposed to be 100 feet long and is anticipated to be able to accommodate up to 100 washes an hour.

The potential queuing of drive-thru traffic was evaluated for the weekday evening and Saturday peaks utilizing the <u>Highway Capacity Manual</u> equation 19-68. The results indicate that the proposed space for eighteen vehicles is adequate to accommodate queued vehicles on-site.

#### **SUMMARY AND CONCLUSION**

Spotless Brands proposes to redevelop the site of a vacant convenience store as a *Flagship Carwash*, with a single tunnel (100 feet) and sixteen vacuum positions. The site currently has full movement vehicular access to both County Line Road and Butler Avenue. As part of the project the Butler Avenue driveway will be removed and exiting left turns will be prohibited at the County Line Road driveway.

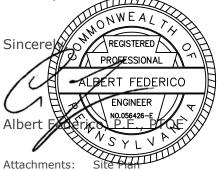
The car wash is proposed to include two pay stations and stacking is provided for over eighteen vehicles. On-site circulation will be one-way counterclockwise, maximizing the available stacking.

The results of the study indicate that:

- The proposed redevelopment is anticipated to generate less traffic than the previous convenience store use.
- The site layout can accommodate anticipated queues.
- The site driveways will operate at an acceptable LOS during the morning, evening, and Saturday peak periods.
- The traffic associated with the proposed redevelopment will not impact the adjacent signalized intersection and that mitigation is not warranted.



Please do not hesitate to contact me at 610.608.4336 or <u>albert@federico-consulting.com</u> should you have any guestions or require additional information.



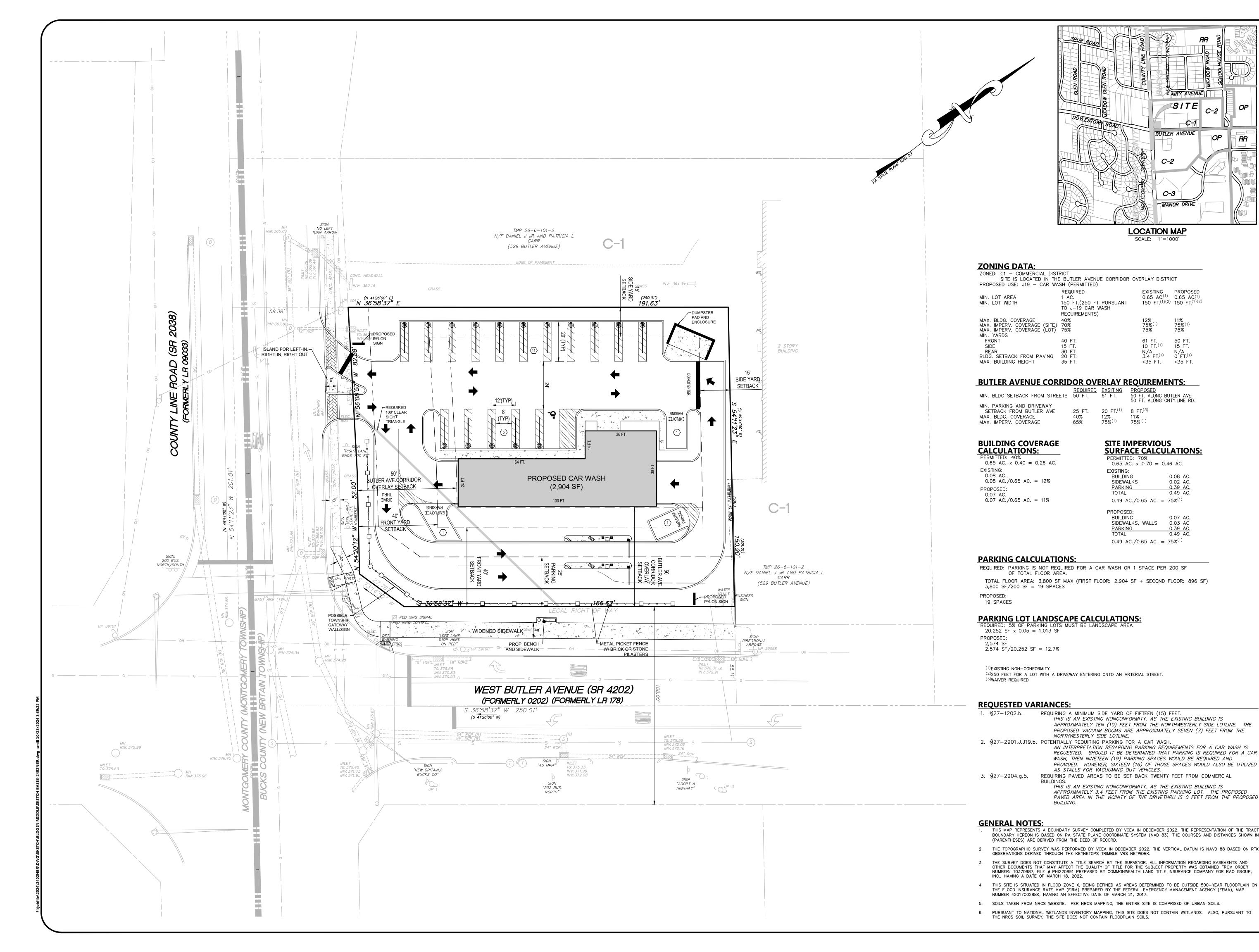
Review Correspondence

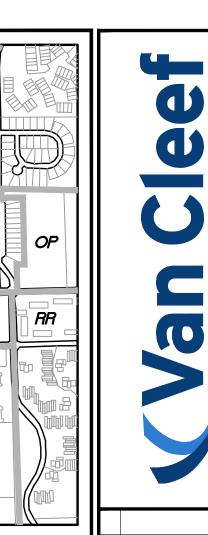
Traffic Counts

Volume Development Traffic Signal Permit Plan

Capacity Analysis

Drive-thru Queuing Evaluation





C-2

**LOCATION MAP** 

12% 75%<sup>(1)</sup> 75%

11% 75%<sup>(1)</sup> 75%

10 FT.<sup>(1)</sup> 15 FT.

N/A 3.4 FT.<sup>(1)</sup> N/A 0 FT.<sup>(1)</sup>

PROPOSED
50 FT. ALONG BUTLER AVE.

50 FT. ALONG CNTY.LINE RD.

0.02 AC.

<35 FT. <35 FT.

	SERIAL NO. 2022318358	:1	
REV	DESCRIPTION	AUTH	DATE
1	LIST OF REQUESTED VARIANCES ADDED.	SDC	10/23/24

Before You Dig Anywhere



**GRAPHIC SCALE** 0 10 20

1 INCH = 20 FT

PLAN NOTATION ONLY THOSE PLANS WHICH CONTAIN A DIGIT IMPRESSED, OR COLORIZED INK SEAL OF THE RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERED TO THE PROFESSIONAL SHALL BE CONSIDERED TO THE PROFESSIONAL SHALL BE CONSIDERED TO THE PR VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARE FOR THE OWNER DESIGNATED HEREON. AN MODIFICATION, REVISION, DUPLICATION OR US WITHOUT THE WRITTEN CONSENT OF VAN CLEE ENGINEERING ASSOCIATES IS PROHIBITEI RELIANCE ON THIS PLAN FOR ANY PURPOS OTHER THAN THAT WHICH IS INTENDED SHALL E AT THE SOLE DISCRETION AND LIABILITY OF APPLICABLE PARTY.

**SKETCH PLAN** 

**VAN CLEEF ENGINEERING ASSOCIATES, LLC** 

501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM PHONE (215) 345-1876

FOR

**SPOTLESS BRANDS** 

ال		
IN	DATE:	OCTOBER 22, 2024
	SCALE:	1"=20'
TK	<b>DESIGNED BY:</b>	LSM
	DRAWN BY:	LSM
	CHECKED BY:	SDC
	JOB NUMBER:	24-02-NBR

TMP: 26-006-101-001

**NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 



## TRANSPORTATION IMPACT STUDY (TIS) DETERMINATION & SCOPING MEETING APPLICATION

Scoping Meeting Application:	<u>Received</u>
Scoping Meeting Date:	Scoping Number: S0620220107
Tax Parcel Number:	
Project/Development Name: 545 West Butler Avenue R	redevelopment - Dunkin/Papa John's
Applicant Information:	
Business Partner ID:	
Applicant Name: RAO Group Inc	
Phone: <u>2154903936</u>	Email 1: mike@raogroupinc.com
Primary Contact: Mike Bahm	Email 2: dms@shafkowitzlaw.com
Additional Engineering Firm Information:	
Business Partner ID:	
Engineering Firm:	
Phone:	. Email 1:
Primary Contact:	Email 2:
Creator Information:	
Business Partner ID: 013601 Firm Name:	Albert Federico Consulting, LLC.
Phone: <u>610-608-4336</u>	Email 1: albert@federico-consulting.com
(1) LOCATION OF PROPOSED DEVELOPMENT:	
PennDOT Engineering District: 06	Email:
County: Bucks	Email:
Municipality: NEW BRITAIN	Email: mwalsh@newbritaintownship.org

NO.	SR	Segment Offset Average Daily Trips			Driveway Classification	Local Road
1	2038	0051	0171	425	Low Volume	No
2	4202	0010	0215	425	Low Volume	No

Are there any vehicle weight or size restrictions along the SR in accordance with 75 PA C.S. ss 4902? : No

#### (2) DESCRIPTION OF PROPOSED DEVELOPMENT (Attach site plan if available):

Proposed site access:

SR 2038 (County Line Road) - Unsignalized SR 4202 (Butler Avenue) - Unsignalized

Proposed land uses:

Previous - Convenience Store Proposed - Dunkin/Papa John's

Community linkages (access to neighboring properties, cross easements, pedestrian and transit accommodations):

Sidewalk along site frontages

#### (3) DEVELOPMENT SCHEDULE AND STAGING:

Anticipated Opening Date: 04-01-2024

Full Buildout Date: 04-01-2024

Describe Proposed Development Schedule/Staging:

Single phase redevelopment

#### (4) TRIP GENERATION:

Land Use & Size	Land Use Code	Were ITE results used?	Daily Trips	AM P Hou		PM P		Saturday	/ Peak Hour
				Enter	Exit	Enter	Exit	Enter	Exit
Dunkin/Papa Johns	938/935	Yes	849	18	18	30	30		
		TOTAL:	849	18	18	30	30	0	0

#### (5) TRANSPORTATION IMPACT STUDY REQUIRED?

Transportation Impact Study Required? No

If Yes, based on:

Other considerations as described below:

#### (6) TRANSPORTATION IMPACT ASSESSMENT REQUIRED?

Transportation Impact Assessment Required? Yes

#### (7) STUDY AREA:

Roadway and Study Intersections:

SR 2038 (County Line Road) and Unsignalized driveway SR 4202 (Butler Avenue) and Unsignalized driveway SR 2038 (County Line Road) and SR 4202 (Butler Avenue)

Land use context (Refer to PennDOT Design Manual, Part 1X, Appendix B): Suburban corrdidor

**Known Congestion Areas:** 

SR 2038 (County Line Road) and SR 4202 (Butler Avenue)

Known Safety Concerns:

TBD

**Known Environmental Constraints:** 

TBD

Pedestrian/Bike Review (Community Centers, Parks, Schools, etc.):

Sidewalk along site frontages

Transit Review (Current routes/stops):

SEPTA Regional Rail (Lansdale/Doylestown, Link Belt Station) +/- 1.25 mi SEPTA Bus Route 132 +/- 1.5 mi at Doylestown Road and Cowpath Road

#### (8) STUDY AREA TYPE:

Study Area Type: Urban

#### (9) TIS ANALYSIS PERIODS AND TIMES:

Analysis period and times notes:

Opening year (2024) Weekday Morning and Evening peak periods

#### (10) TRAFFIC ADJUSTMENT FACTORS:

(a) Seasonal Adjustment (Identify counts requiring adjustment and methodology): None

Bureau of Planning & Research -

- (b) Annual Base Traffic Growth: 0.29 %/yr. Source: Aug '21 to Jul '22
- (c) Pass-By Trips (Attach justification where required):

NO.	Land Use	%	Source

(d) Captured Trips for Multi-Use Sites:

None

(e) Modal Split Reductions:

None

(f) Other Reductions:

None

#### (11) OTHER PROJECTS WITHIN STUDY AREA TO BE ADDED TO BASE TRAFFIC:

Notes:

TBD

#### (12) TRIP DISTRIBUTION AND ASSIGNMENT:

Trip Distribution Notes:

Analogy method based on intersection counts

#### (13) APPROVAL OF DATA COLLECTION ELEMENTS AND METHODOLOGIES:

NO.	Location	Period	Туре
1	County Line Road and Butler Avenue	Weekday AM and PM Peaks	TMC

#### (14) CAPACITY/LOS ANALYSIS:

NC	. Location	Period	Туре
1	County Line Road and Butler Avenue	Weekday AM and PM peaks	HCM 6 from Synchro
2	County Line Road and Unsignalized Driveway	Weekday AM and PM peaks	HCM 6 from Synchro

#### (15) ROADWAY IMPROVEMENTS/MODIFICATIONS BY OTHERS TO BE INCLUDED:

Roadway Improvements:

**TBD** 

3

#### (16) OTHER NEEDED ANALYSES:

(a) Sight Distance Analysis:

Sight accesses

(b) Signal Warrant Analysis (Identify locations):

Nο

(c) Required Signal Phasing/Timing Modifications (Determine for all signalized intersections; specify methodology):

If warranted

(d) Traffic Signal Corridor/Network Analysis (Identify locations/methodology):

No

(e) Analysis of the Need for Turning Lanes (Identify locations/methodology):

Νo

(f) Turning Lane Lengths (Identify methodology to be used):

No

(g) Left Turn Signal Phasing Analysis (Identify locations/methodology):

No

(h) Queuing Analysis (Identify locations/methodology):

If warranted

(i) Gap Studies (Identify locations/methodology):

No

(j) Crash Analysis (Identify locations):

Site Accesses

(k) Weaving Analysis (Identify locations):

No

(I) Other Required Studies (Specify locations/methodology):

None

### (17) ADDITIONAL COMMENTS OR RECOMMENDATIONS RELATIVE TO THE SCOPE OF THE TIS:

#### Additional Comments:

The proposed Dunkin/Papa John's will generate less traffic than the previous convenience store use. Previous - Convenience Store (LU 851) - AM 206, PM 162 Proposed - Dunkin Donuts (LU 938)/Papa John's (LU 935) - AM 40, PM 75

#### PennDOT Review Comments: (Current Cycle Comments)

After review of the scoping meeting application, the Department will contact the applicant regarding the need for a scoping meeting prior to applying for a highway occupancy permit.

This Electronic Copy Created on: 2022-11-15 16:11:05

#### **Albert Federico**

From: ePermitting Help <penndotepermittinghelp@pa.gov>

Sent: Wednesday, December 14, 2022 10:25 AM

To: mike@raogroupinc.com; dms@shafkowitzlaw.com; Albert Federico; fhanney@pa.gov;

jotten@pa.gov; slapenta@pa.gov; c-smccutch@pa.gov; scburton@pa.gov;

ajparker@mccormicktaylor.com; mwalsh@newbritaintownship.org

Cc: RA-PDEPSPROD@pa.gov

Subject: :: Scoping Meeting Application - Returned - Application Number is : S0620220107

(Sent on: 12/14/2022 10:24:52 AM)

PennDOT has completed its review of the TIS Determination and Scoping Meeting Application.

Please address the following comments below, and resubmit the application to PennDOT for review.

#### **PennDOT Review Comments:**

- 1. This is a preliminary review. The Department reserves the right to make additional comments when the application includes detailed plans and a Transportation Impact Assessment (TIA). If you have any questions about the technical aspects of this review, please contact the Department's representative, Erik Schmidt, PE, PTOE at ECSchmidt@mccormicktaylor.com.
- 2. The Department agrees that a TIA is appropriate for this development.
- 3. Due to the vastly increased popularity of drive-thru restaurants and the safety considerations of vehicle queues extending beyond the available site storage, the Department requires an analysis and queue storage tabulation for the peak drive-thru to ensure this queue can be stored entirely on-site and does not impact the roadway or function of the site accesses. Drive-thru analysis must be based on the peak hour of the generator. The Department is concerned that the site only provides queuing space for about four vehicles at the menu boards before the site circulation and, subsequently, operations at the SR 4202 (Butler Avenue) driveway are impacted.
- 4. The sketch site plan shows ?possible striping for right-in, right-out? only restrictions on both driveways. Any prohibited movements must be physically restricted with raised channelizing islands. The formal TIA must clearly establish the movements that will be permitted or restricted. However, please note that PennDOT will not approve lefts-in from SR 4202 (Butler Avenue). Additionally, lefts-out to SR 2038 (County Line Road) are a known operational issue during peak hours. Any lefts-in or lefts-out to/from this site must be justified with gap studies in addition to the capacity analyses. The analyses must also consider the impacts that queues from the traffic signal will have on the operation of the proposed driveways.
- 5. Accesses with movement and/or turning restrictions must be geometrically designed to physically restrict the prohibited movements to the maximum extent feasible. Provide appropriate signage and pavement markings to supplement access geometry that physically restricts prohibited movements. Should the provided turn restrictions be found ineffective, the permittee shall be responsible for installing a raised mountable median island or continuous delineator curb to further physically enforce the turn restrictions. If this median must be installed, it will become the Permittee's responsibility to maintain it in perpetuity. This will become a condition of the permit.
- 6. The Department is concerned that the design does not include a drive-thru bypass lane and that the employee parking stalls back out directly into a drive-thru lane with no additional buffer/maneuvering space. Clarify how employees would exit the site if there were a queue at the drive-thru. Since these are primarily internal site concerns, we will defer final approval of these elements to the municipality.
- 7. Collect Saturday turning movement counts and provide Saturday peak intersection and drive-thru analyses. Turning movement counts should overlap with the typical Dunkin weekend peak hours. Coordinate with the developer as needed to determine the expected Dunkin weekend peak hours.
- 8. In addition to the opening year analysis, provide an analysis of existing conditions at SR 2038 (County Line Road) & SR 4202 (Butler Avenue) for comparison.
- 9. It appears that the trip generation for Papa John?s was estimated with LU 935 (fast-food restaurant with drive-thru window and no indoor seating). Please confirm that the drive-thru window is only intended for the Dunkin. The Department acknowledges that ITE currently lacks a more appropriate land use code for Papa John?s. Considering the scale and context of this application, the Department will accept the use of LU 935 for Papa John?s for this site only.
- 10. The trip generation values shown on the transmittal letter appear to be inconsistent with the values entered in EPS. In the formal TIA, please ensure that the trip generation methodology is consistent and clearly explained. Clarify if each

result is based on the ITE average rate or fitted curve. Please note, for future scoping submissions in EPS, each land use should be listed individually in the trip generation table, rather than combined in a single row.

- 11. The scoping application does not show any pass-by trips. In the TIA, please clarify pass-by trip assumptions as needed.
- 12. For background traffic growth, use the BPR growth factors that are in effect at the time of the traffic counts. The factors listed in the application are outdated.
- 13. Confirm with the municipality if any other nearby developments should be accounted for in the opening year scenarios.
- 14. Ensure that an accessible pedestrian path is provided between the frontage sidewalk and the proposed building. ADA compliance within the limits of work must be evaluated in the final design.
- 15. Verify the segment/offset of the existing driveway on SR 2038 (County Line Road).
- 16. Truck turning templates must be provided for the largest vehicle anticipated to access the site (all movements) to ensure that the driveways can safely accommodate the design vehicle. Additionally, the templates must show how these vehicles will circulate within the site. For each site access, if the largest permitted vehicle type to utilize that access is smaller than a WB-67, then a note must be included on the plans, turning templates for that vehicle size provided, appropriate signage provided, and that vehicle size will become a condition of the permit and be recorded. 17. The Department requires written evidence, prior to the issuance of a permit, that New Britain Township has had the opportunity to comment. Provide written evidence (e.g., municipal engineering review, council or planning commission meeting minutes, executed TE-160, etc.), which is less than one year old, to satisfy this requirement. Also, provide County Planning Commission Review Letters if available. PennDOT will consider any comments or recommendations from the municipal and county reviews prior to approving the access permit. In addition, please provide the email address(es) for the appropriate individual(s) at the municipality (i.e., Township Manager, Borough Manager, Mayor, etc.) and at the County Planning Commission on the Application Information screen so that a copy of the Department's review letter can be forwarded. (PA Code, Title 67, Chapter 441.3(j)).
- 18. As per Title 67, Chapter 441.8(h)(2)(iv), the Safe Stopping Sight Distance is the absolute minimum acceptable sight distance for driveways. It is the designer?s responsibility to ensure that this minimum requirement is satisfied.
- 19. The connection to, or the installation of, a new closed drainage system within the Department Right-of-Way requires a separate permit application in the name of the municipality for the maintenance of this system. Please submit the application as Permit Type: Miscellaneous, Permit Subtype: Curb, sidewalk, drainage or other application, Permit Use: Drainage for maintenance of the drainage system.
- 20. The Department is now requiring a cost estimate be entered into EPS prior to HOP issuance. This cost estimate must include all work proposed within Department Right-of-Way including signal equipment, roadway work, pavement markings, and signage plus a 15% contingency to be entered into the Fee information & Estimate Construction Cost section in the Application setup box. Further, this section requires the cost estimate to be broken down into site frontage costs and off-site improvements costs. Please enter this information in the manner required. This cost estimate will be the basis for the value of the Letter of Credit the Department will require as security prior to the start of actual construction.
- 21. Department records indicate that PennDOT Fiber Optic facilities are present within the proposed limits of work and may be impacted by the anticipated improvements. If fiber optic facilities are affected, the applicant is responsible for the relocation of the fiber, testing of the fiber communications both prior to relocation and after the relocation of the fiber indicated on the approved plans, and the associated costs. Any relocation must be performed by a communications contractor that is acceptable to the District Traffic Unit, and the Highway Occupancy Permit will not be issued until the fiber optic impact has been reviewed and the relocation scheme approved by the Department.
- 22. Consistent with current Department Policy, applicants for Highway Occupancy Permits must apply for an EPS Business Partner ID (BPID). The EPS BPID is used in the establishment of a billing account for the invoicing of inspection costs. After an EPS BPID is obtained and activated by the applicant's system administrator, a user ID will then need to be created to ensure that the EPS BPID is integrated into EPS and searchable through the "looking glass" feature. Once this has been established, please provide the following information in the applicant contact information tab under "Applicant Team": a. BPID b. Contact information (name/title/phone/email) for a general contact person (person that typically deals with the Highway Occupancy Permit application process) c. Contact information (name/title/phone/email) for a billing contact person (person that typically deals with the Highway Occupancy Permit invoicing process) For information on obtaining an EPS BPID, you may visit:

https://www.epermittingsyst.penndot.gov/EPS/home/manageBPRegistration.jsp (follow the instructions that are in the PINK area) or contact the ECMS Help Desk. Please be aware that having an ECMS BPID does not guarantee the establishment of an EPS BPID as they are not reciprocal to one another.

Click here to access the Scoping Application



Traffic Engineering and Mobility Solutions

MEMORANDUM October 5, 2023

To: File

**From:** Albert Federico, P.E., PTOE

**Subject:** Interim Response to PennDOT Scoping Comments (S0620220107)

545 West Butler Avenue - Dunkin' and Papa John's

New Britain Township, Bucks County

On December 14, 2022, the PennDOT issued a Scoping review (EPS S0620220107) for the Dunkin' and Papa John's (545 West Butler Avenue) redevelopment. The comments and interim responses are below:

1. This is a preliminary review. The Department reserves the right to make additional comments when the application includes detailed plans and a Transportation Impact Assessment (TIA). If you have any questions about the technical aspects of this review, please contact the Department's representative, Erik Schmidt, PE, PTOE at ECSchmidt@mccormicktaylor.com.

Response: Noted.

2. The Department agrees that a TIA is appropriate for this development.

Response: Noted.

3. Due to the vastly increased popularity of drive-thru restaurants and the safety considerations of vehicle queues extending beyond the available site storage, the Department requires an analysis and queue storage tabulation for the peak drive-thru to ensure this queue can be stored entirely on-site and does not impact the roadway or function of the site accesses. Drive-thru analysis must be based on the peak hour of the generator. The Department is concerned that the site only provides queuing space for about four vehicles at the menu boards before the site circulation and, subsequently, operations at the SR 4202 (Butler Avenue) driveway are impacted.

Response: A queuing analysis is included in the Traffic Analysis. It is noted that site provides queuing for ten vehicles and that the Butler Avenue driveway is proposed to be right turn exit only, eliminating the potential impacts.

4. The sketch site plan shows possible striping for right-in, right-out only restrictions on both driveways. Any prohibited movements must be physically restricted with raised channelizing islands. The formal TIA must clearly establish the movements that will be permitted or restricted. However, please note that PennDOT will not approve lefts in from SR 4202 (Butler Avenue). Additionally, lefts-out to SR 2038 (County Line Road) are a known operational issue during peak hours. Any lefts-in or lefts-out to/from this site must be justified with gap studies in addition to the capacity analyses. The analyses must also consider the impacts that queues from the traffic signal will have on the operation of the proposed driveways.

Response: The Butler Avenue driveway will be modified to only allow exiting right turns, and exiting left turns will be prohibited at the County Line Road driveway.



5. Accesses with movement and/or turning restrictions must be geometrically designed to physically restrict the prohibited movements to the maximum extent feasible. Provide appropriate signage and pavement markings to supplement access geometry that physically restricts prohibited movements. Should the provided turn restrictions be found ineffective, the permittee shall be responsible for installing a raised mountable median island or continuous delineator curb to further physically enforce the turn restrictions. If this median must be installed, it will become the Permittee's responsibility to maintain it in perpetuity. This will become a condition of the permit.

Response: Prohibited movements are proposed to be physically restricted.

6. The Department is concerned that the design does not include a drive-thru bypass lane and that the employee parking stalls back out directly into a drive-thru lane with no additional buffer/maneuvering space. Clarify how employees would exit the site if there were a queue at the drive-thru. Since these are primarily internal site concerns, we will defer final approval of these elements to the municipality.

Response: The current plan includes a by-pass lane.

7. Collect Saturday turning movement counts and provide Saturday peak intersection and drive-thru analyses. Turning movement counts should overlap with the typical Dunkin weekend peak hours. Coordinate with the developer as needed to determine the expected Dunkin weekend peak hours.

Response: ITE does not provide Saturday Rates for LU 938. Based on the available rates for LU 935 the gross trip generation of the proposed redevelopment (131) is anticipated to be half of the previous convenience store use (261). In consideration of the reduction, the de minimus impact during the weekday peaks and the proposed modifications to the site access an evaluation of Saturday conditions is considered unwarranted.

8. In addition to the opening year analysis, provide an analysis of existing conditions at SR 2038 (County Line Road) & SR 4202 (Butler Avenue) for comparison.

Response: The Traffic Analysis includes an assessment of existing conditions.

9. It appears that the trip generation for Papa John's was estimated with LU 935 (fast-food restaurant with drive-thru window and no indoor seating). Please confirm that the drive-thru window is only intended for the Dunkin. The Department acknowledges that ITE currently lacks a more appropriate land use code for Papa John's. Considering the scale and context of this application, the Department will accept the use of LU 935 for Papa John's for this site only.

Response: Both Dunkin' and Papa John's will operate as drive-thruonly with no seating.

10. The trip generation values shown on the transmittal letter appear to be inconsistent with the values entered in EPS. In the formal TIA, please ensure that the trip generation methodology is consistent and clearly explained. Clarify if each result is based on the ITE average rate or fitted curve. Please note, for future scoping submissions in EPS, each land use should be listed individually in the trip generation table, rather than combined in a single row.

Response: Per the Scoping Application, the morning trip generation assumed the Dunkin' (LU 938) as the primary use and the evening assumes the Papa John's (LU 935) as the primary.



11. The scoping application does not show any pass-by trips. In the TIA, please clarify pass-by trip assumptions as needed.

Response: Pass-by was applied based on ITE LU 938 (90% morning) and LU 935 (31% evening).

12. For background traffic growth, use the BPR growth factors that are in effect at the time of the traffic counts. The factors listed in the application are outdated.

Response: The Bureau of Planning and Research – September 2023 to July 2024 growth rate was used in the traffic analysis.

13. Confirm with the municipality if any other nearby developments should be accounted for in the opening year scenarios.

Response: The Municipality has not specified any other developments to include in the analysis.

14. Ensure that an accessible pedestrian path is provided between the frontage sidewalk and the proposed building. ADA compliance within the limits of work must be evaluated in the final design.

Response: An accessible route is provided to Butler Avenue.

15. Verify the segment/offset of the existing driveway on SR 2038 (County Line Road).

Response: The SR 2038 access is proposed to be located at Segment 0042, Offset 0420.

16. Truck turning templates must be provided for the largest vehicle anticipated to access the site (all movements) to ensure that the driveways can safely accommodate the design vehicle. Additionally, the templates must show how these vehicles will circulate within the site. For each site access, if the largest permitted vehicle type to utilize that access is smaller than a WB-67, then a note must be included on the plans, turning templates for that vehicle size provided, appropriate signage provided, and that vehicle size will become a condition of the permit and be recorded.

### Response: Turning Templates will be provided with the formal design submission.

17. The Department requires written evidence, prior to the issuance of a permit, that New Britain Township has had the opportunity to comment. Provide written evidence (e.g., municipal engineering review, council or planning commission meeting minutes, executed TE-160, etc.), which is less than one year old, to satisfy this requirement. Also, provide County Planning Commission Review Letters if available. PennDOT will consider any comments or recommendations from the municipal and county reviews prior to approving the access permit. In addition, please provide the email address(es) for the appropriate individual(s) at the municipality (i.e., Township Manager, Borough Manager, Mayor, etc.) and at the County Planning Commission on the Application Information screen so that a copy of the Department's review letter can be forwarded. (PA Code, Title 67, Chapter 441.3(j)).

### Response: Municipal documentation will be provided with the formal design submission.

18. As per Title 67, Chapter 441.8(h)(2)(iv), the Safe Stopping Sight Distance is the absolute minimum acceptable sight distance for driveways. It is the designer's responsibility to ensure that this minimum requirement is satisfied.

### Response: There is adequate sight distance for the proposed driveway movements.

19. The connection to, or the installation of, a new closed drainage system within the Department Right-of-Way requires a separate permit application in the name of the



municipality for the maintenance of this system. Please submit the application as Permit Type: Miscellaneous, Permit Subtype: Curb, sidewalk, drainage or other application, Permit Use: Drainage for maintenance of the drainage system.

#### Response: Noted.

20. The Department is now requiring a cost estimate be entered into EPS prior to HOP issuance. This cost estimate must include all work proposed within Department Right-of-Way including signal equipment, roadway work, pavement markings, and signage plus a 15% contingency to be entered into the Fee information & Estimate Construction Cost section in the Application setup box. Further, this section requires the cost estimate to be broken down into site frontage costs and off-site improvements costs. Please enter this information in the manner required. This cost estimate will be the basis for the value of the Letter of Credit the Department will require as security prior to the start of actual construction.

## Response: A cost estimate will be provided with the formal design submission.

21. Department records indicate that PennDOT Fiber Optic facilities are present within the proposed limits of work and may be impacted by the anticipated improvements. If fiber optic facilities are affected, the applicant is responsible for the relocation of the fiber, testing of the fiber communications both prior to relocation and after the relocation of the fiber indicated on the approved plans, and the associated costs. Any relocation must be performed by a communications contractor that is acceptable to the District Traffic Unit, and the Highway Occupancy Permit will not be issued until the fiber optic impact has been reviewed and the relocation scheme approved by the Department.

#### Response: Noted.

22. Consistent with current Department Policy, applicants for Highway Occupancy Permits must apply for an EPS Business Partner ID (BPID). The EPS BPID is used in the establishment of a billing account for the invoicing of inspection costs. After an EPS BPID is obtained and activated by the applicant's system administrator, a user ID will then need to be created to ensure that the EPS BPID is integrated into EPS and searchable through the "looking glass" feature. Once this has been established, please provide the following information in the applicant contact information tab under "Applicant Team": a. BPID b. Contact information (name/title/phone/email) for a general contact person (person that typically deals with the Highway Occupancy Permit application process) c. Contact information (name/title/phone/email) for a billing contact person (person that typically deals with the Highway Occupancy Permit invoicing process) For information on obtaining an EPS BPID, you may visit: https://www.epermittingsyst.penndot.gov/EPS/home/manageBPRegistration.jsp (follow the instructions that are in the PINK area) or contact the ECMS Help Desk. Please be aware that having an ECMS BPID does not guarantee the establishment of an EPS BPID as they are not reciprocal to one another.

Response: Noted.



Traffic Engineering and Mobility Solutions

133 Rutgers Avenue Swarthmore, PA 19081

March 7, 2024

Pennsylvania Department of Transportation Engineering District 6-0 7000 Geerdes Boulevard King of Prussia, PA 19406-1525

ATTN: John Gallagher, P.E.

RE: Dunkin' Redevelopment (545 W. Butler Avenue) - TIA Submission EPS 336574 (Scoping S0620220107)
New Britain Township, Bucks County

Dear Mr. Gallagher:

Albert Federico Consulting is submitting a Transportation Impact Analysis (TIA) on behalf of the *RAO Group* to develop a Dunkin' with Drive-thru in New Britain Township, Bucks County.

The property (545 W. Butler Avenue) is developed with a recently closed convenience store (~3,300 sf) with full movement driveways to both Butler Avenue (SR 4202) and County Line Road (SR 2038). It is proposed to replace the existing building with a Dunkin' with Drivethru and modify the accesses to restrict the Butler Avenue driveway to right-out only, and to prohibit exiting left turns from the County Line Road driveway.

Scoping materials (S0620220107) were submitted to PennDOT and New Britain Township in November 2022. At the time of submission, the redevelopment included a Dunkin' and Papa John's. Review comments were issued by PennDOT December 14, 2022. As part of the municipal entitlement process the TIA was completed and submitted to the Township in October 2023. However, due to further discussions between the Applicant and the Township the study has not yet been reviewed by the municipal engineer. The project has been refined based on Township feedback to remove the Papa John's and proceed as a Dunkin' only.

As coordinated with Department and Consultant review staff, the Department has consented to review the October 2023 TIA which assumes both uses, understanding that the TIA overestimates the anticipated site traffic and that the proposed redevelopment will generate substantially less traffic than the previous convenience store use.

The materials submitted via EPS as Application <u>336574</u> include the following attachments:

- Submission Transmittal Letter, dated March 7, 2024
- Traffic Impact Analysis 545 West Butler Avenue (Dunkin' and Papa John's), prepared by Albert Federico Consulting, LLC., dated October 5, 2023
- Synchro file for "RAO New Britain", prepared by Albert Federico Consulting, LLC., dated October 5, 2023
- Zoning Hearing Exhibit Plan of Dunkin Donuts for RAO Group, prepared by Van Cleef Engineering Associates, LLC, dated October 10, 2023, last revised January 30, 2024
- New Britain Township Zoning Decision, issued March 6, 2024



- Supplemental coordination email, March 4, 2023
- o PennDOT Form M-950AA

The following responses are offered to the Department December 2022 Scoping comments:

1. This is a preliminary review. The Department reserves the right to make additional comments when the application includes detailed plans and a Transportation Impact Assessment (TIA). If you have any questions about the technical aspects of this review, please contact the Department's representative, Erik Schmidt, PE, PTOE at ECSchmidt@mccormicktaylor.com.

#### Noted.

2. The Department agrees that a TIA is appropriate for this development.

#### Noted.

3. Due to the vastly increased popularity of drive-thru restaurants and the safety considerations of vehicle queues extending beyond the available site storage, the Department requires an analysis and queue storage tabulation for the peak drive-thru to ensure this queue can be stored entirely on-site and does not impact the roadway or function of the site accesses. Drive-thru analysis must be based on the peak hour of the generator. The Department is concerned that the site only provides queuing space for about four vehicles at the menu boards before the site circulation and, subsequently, operations at the SR 4202 (Butler Avenue) driveway are impacted.

# A queuing analysis is included in the Traffic Analysis. It is noted that site provides queuing for ten vehicles and that the Butler Avenue driveway is proposed to be right turn exit only, eliminating the potential impacts.

4. The sketch site plan shows possible striping for right-in, right-out only restrictions on both driveways. Any prohibited movements must be physically restricted with raised channelizing islands. The formal TIA must clearly establish the movements that will be permitted or restricted. However, please note that PennDOT will not approve lefts in from SR 4202 (Butler Avenue). Additionally, lefts-out to SR 2038 (County Line Road) are a known operational issue during peak hours. Any lefts-in or lefts-out to/from this site must be justified with gap studies in addition to the capacity analyses. The analyses must also consider the impacts that queues from the traffic signal will have on the operation of the proposed driveways.

## The Butler Avenue driveway will be modified to only allow exiting right turns, and exiting left turns will be prohibited at the County Line Road driveway.

5. Accesses with movement and/or turning restrictions must be geometrically designed to physically restrict the prohibited movements to the maximum extent feasible. Provide appropriate signage and pavement markings to supplement access geometry that physically restricts prohibited movements. Should the provided turn restrictions be found ineffective, the permittee shall be responsible for installing a raised mountable median island or continuous delineator curb to further physically enforce the turn restrictions. If this median must be installed, it will become the Permittee's responsibility to maintain it in perpetuity. This will become a condition of the permit.

#### Prohibited movements are proposed to be physically restricted.

6. The Department is concerned that the design does not include a drive-thru bypass lane and that the employee parking stalls back out directly into a drive-thru lane with no additional buffer/maneuvering space. Clarify how employees would exit the site if there were a queue at the drive-thru. Since these are primarily internal site concerns, we will defer final approval of these elements to the municipality.

The current plan includes a by-pass lane.



7. Collect Saturday turning movement counts and provide Saturday peak intersection and drive-thru analyses. Turning movement counts should overlap with the typical Dunkin weekend peak hours. Coordinate with the developer as needed to determine the expected Dunkin weekend peak hours.

ITE does not provide Saturday Rates for LU 938. Based on the available rates for LU 935 the gross trip generation of the proposed redevelopment (131) is anticipated to be half of the previous convenience store use (261). In consideration of the reduction, the de minimus impact during the weekday peaks and the proposed modifications to the site access, an evaluation of Saturday conditions is considered unwarranted.

8. In addition to the opening year analysis, provide an analysis of existing conditions at SR 2038 (County Line Road) & SR 4202 (Butler Avenue) for comparison.

The Traffic Analysis includes an assessment of existing conditions.

9. It appears that the trip generation for Papa John's was estimated with LU 935 (fast-food restaurant with drive-thru window and no indoor seating). Please confirm that the drive-thru window is only intended for the Dunkin. The Department acknowledges that ITE currently lacks a more appropriate land use code for Papa John's. Considering the scale and context of this application, the Department will accept the use of LU 935 for Papa John's for this site only.

As previously noted, the project will proceed as a Dunkin' only. The Dunkin' will operate as drive-thru only with no seating.

10. The trip generation values shown on the transmittal letter appear to be inconsistent with the values entered in EPS. In the formal TIA, please ensure that the trip generation methodology is consistent and clearly explained. Clarify if each result is based on the ITE average rate or fitted curve. Please note, for future scoping submissions in EPS, each land use should be listed individually in the trip generation table, rather than combined in a single row.

Per the Scoping Application, the morning trip generation assumed the Dunkin' (LU 938) as the primary use and the evening assumes the Papa John's (LU 935) as the primary. The results for both uses are based on the ITE average rate. As previously noted, the project will proceed as a Dunkin' only, which will generate fewer evening trips (15) than the Papa John's (60) assumed in the TIA.

11. The scoping application does not show any pass-by trips. In the TIA, please clarify pass-by trip assumptions as needed.

Pass-by was applied based on ITE LU 938 (90% morning) and LU 935 (31% evening).

12. For background traffic growth, use the BPR growth factors that are in effect at the time of the traffic counts. The factors listed in the application are outdated.

The Bureau of Planning and Research – September 2023 to July 2024 growth rate was used in the traffic analysis.

13. Confirm with the municipality if any other nearby developments should be accounted for in the opening year scenarios.

The Municipality has not specified any other developments to include in the analysis.



14. Ensure that an accessible pedestrian path is provided between the frontage sidewalk and the proposed building. ADA compliance within the limits of work must be evaluated in the final design.

#### An accessible route is provided to Butler Avenue.

15. Verify the segment/offset of the existing driveway on SR 2038 (County Line Road).

#### The SR 2038 access is proposed to be located at Segment 0043, Offset 0420.

16. Truck turning templates must be provided for the largest vehicle anticipated to access the site (all movements) to ensure that the driveways can safely accommodate the design vehicle. Additionally, the templates must show how these vehicles will circulate within the site. For each site access, if the largest permitted vehicle type to utilize that access is smaller than a WB-67, then a note must be included on the plans, turning templates for that vehicle size provided, appropriate signage provided, and that vehicle size will become a condition of the permit and be recorded.

#### Turning Templates will be provided with the formal design submission.

17. The Department requires written evidence, prior to the issuance of a permit, that New Britain Township has had the opportunity to comment. Provide written evidence (e.g., municipal engineering review, council or planning commission meeting minutes, executed TE-160, etc.), which is less than one year old, to satisfy this requirement. Also, provide County Planning Commission Review Letters if available. PennDOT will consider any comments or recommendations from the municipal and county reviews prior to approving the access permit. In addition, please provide the email address(es) for the appropriate individual(s) at the municipality (i.e., Township Manager, Borough Manager, Mayor, etc.) and at the County Planning Commission on the Application Information screen so that a copy of the Department's review letter can be forwarded. (PA Code, Title 67, Chapter 441.3(j)).

## The Zoning Hearing Board Decision is included with the current submission. Documentation of municipal reviews will be provided with the formal design submission.

18. As per Title 67, Chapter 441.8(h)(2)(iv), the Safe Stopping Sight Distance is the absolute minimum acceptable sight distance for driveways. It is the designer's responsibility to ensure that this minimum requirement is satisfied.

#### There is adequate sight distance for the proposed driveway movements.

19. The connection to, or the installation of, a new closed drainage system within the Department Right-of-Way requires a separate permit application in the name of the municipality for the maintenance of this system. Please submit the application as Permit Type: Miscellaneous, Permit Subtype: Curb, sidewalk, drainage or other application, Permit Use: Drainage for maintenance of the drainage system.

#### Noted.

20. The Department is now requiring a cost estimate be entered into EPS prior to HOP issuance. This cost estimate must include all work proposed within Department Right-of-Way including signal equipment, roadway work, pavement markings, and signage plus a 15% contingency to be entered into the Fee information & Estimate Construction Cost section in the Application setup box. Further, this section requires the cost estimate to be broken down into site frontage costs and off-site improvements costs. Please enter this information in the manner required. This cost estimate will be the basis for the value of the Letter of Credit the Department will require as security prior to the start of actual construction.

A cost estimate will be provided with the formal design submission.



21. Department records indicate that PennDOT Fiber Optic facilities are present within the proposed limits of work and may be impacted by the anticipated improvements. If fiber optic facilities are affected, the applicant is responsible for the relocation of the fiber, testing of the fiber communications both prior to relocation and after the relocation of the fiber indicated on the approved plans, and the associated costs. Any relocation must be performed by a communications contractor that is acceptable to the District Traffic Unit, and the Highway Occupancy Permit will not be issued until the fiber optic impact has been reviewed and the relocation scheme approved by the Department.

#### Noted.

22. Consistent with current Department Policy, applicants for Highway Occupancy Permits must apply for an EPS Business Partner ID (BPID). The EPS BPID is used in the establishment of a billing account for the invoicing of inspection costs. After an EPS BPID is obtained and activated by the applicant's system administrator, a user ID will then need to be created to ensure that the EPS BPID is integrated into EPS and searchable through the "looking glass" feature. Once this has been established, please provide the following information in the applicant contact information tab under "Applicant Team": a. BPID b. Contact information (name/title/phone/email) for a general contact person (person that typically deals with the Highway Occupancy Permit application process) c. Contact information (name/title/phone/email) for a billing contact person (person that typically deals with the Highway Occupancy Permit invoicing process) For information on obtaining **EPS** BPID, vou visit: https://www.epermittingsyst.penndot.gov/EPS/home/manageBPRegistration.jsp (follow the instructions that are in the PINK area) or contact the ECMS Help Desk. Please be aware that having an ECMS BPID does not guarantee the establishment of an EPS BPID as they are not reciprocal to one another.

Noted.

Please do not hesitate to contact me at 610.608.4336 or albert@federico-consulting.com should you have any questions or require additional information.

Sincerely,

Albert Federico, P.E., PTOE



**Date:** 04/03/2024

**Subject:** Highway Occupancy Permit Application No. 336574, Cycle No.1 - Returned For

Revisions

**To:** RAO Group Inc.

2200 Michener St, Ste-4 Philadelphia, PA 19115

**From:** PennDOT Engineering District 6-0

7000 Geerdes Boulevard King of Prussia, PA 19406

#### Dear Applicant,

PennDOT has reviewed your application for completeness, consistency and compliance with applicable Department Regulations. This review has identified issues that must be addressed in order for our review to continue.

The Department's review comments are attached.

Once the comments have been addressed, please resubmit the application and associated material for further review.

Upon resubmission, the applicant's engineer should put together a letter that describes how each comment has been addressed and where each can be found. This will help expedite the review. For guidance on HOP applications refer to 67 PA Code, Chapter 441, Chapter 459 and PennDOT Publication 282, "Highway Occupancy Permit Guidelines". Additional comments may follow upon review of the resubmitted application.

If you have any questions regarding this matter, you may contact Manasa Kondreddi, at mlkondreddi@mccormicktaylor.com.



**Response Comments Date:** 04/03/2024

**Application Number:** 336574, Cycle No.1

#### **Form Letter Notes**

- (1) \* Upon resubmission, the applicant's engineer should put together a letter that describes how each comment has been addressed and where each can be found in the plan set. A copy of these comments and any previously submitted plans should also be provided.
  - \* Additional comments may follow upon review of the resubmitted application. If you have any questions pertaining to the technical aspects of this review, please contact the Department's representative, Manasa Kondreddi at mlkondreddi@mccormicktaylor.com.
  - \* For guidance on Highway Occupancy Permit applications refer to PA Code Title 67, Chapter 441, Chapter 459 and PennDOT Publication 282. This will help expedite the review."

#### General

- (1) Provide HOP Plans for review by the Department.
- (2) The concrete median island shown on the site plan appears to not effectively restrict the lefts out movement at the access along County Line Road (S.R. 2038). It must be redesigned with effectively restrictive geometry for the associated movement (i.e. to prohibit lefts out of the driveway). This appears possible by providing more of a 45 degree angle for the egress lane. The modifications would also allow for increasing the small size of the currently proposed islands and would allow for a larger refuge area for pedestrians crossing the access.
- (3) Per Strike-Off Letter 494-23-04, in the next submission include completed TIS/TIA Review Checklist and HOP Application Project Checklists, Part 1 and Part 2, found in Publication 282. The checklists are also found in EPS under the Application Setup/Reference Material and Forms/Checklists section.

#### **Application**

(1) Please be advised: the Department is now requiring a cost estimate be entered into EPS prior to HOP issuance. This cost estimate must include all work proposed within the Departments Right-of-Way including signal equipment, all roadway work, pavement markings and signage plus a 15% contingency to be entered into the Fee information & Estimate Construction Cost section in the Application setup box. Further, this section requires the cost estimate to be broken down into site

frontage costs and off site improvements costs. Please enter this information in the manner required. This cost estimate will be the basis for the value of the Letter of Credit the Department will require as security prior to the start of actual construction.

#### **Transportation Impact Study/Transportation Impact Assessment**

(1) The Traffic Services Division has reviewed the submitted Traffic Impact Analysis (TIA) prepared by Albert Federico Consulting, LLC, last revised Month 7, 2024, and has no further comments. This is based on the presented land use, trip generation, design horizon year, and proposed mitigation. Any modifications to these parameters will require a revision of the TIS and additional review by the Department. This will be a condition of the permit.

Development: Car Wash

**Location:** New Britain Township, Bucks County

Period: Morning Peak

Pass-by - - - 0%
External 39 39 78 Assigned to Driveways and Intersection

			utler Avenue Northbound	2		nty Line Roa Westbound	ad		<b>utler Avenue</b> Southbound	•	Cou	Inty Line Roa Eastbound	ad
		<u>Left</u>	<u>Through</u>	<u>Right</u>	<u>Left</u>	<u>Through</u>	<u>Right</u>	<u>Left</u>	<u>Through</u>	<u>Right</u>	<u>Left</u>	<u>Through</u>	<u>Right</u>
2023	Counted	102	219	77	82	525	75	242	284	86	104	624	39
0.12%	Background	0	1	0	0	2	0	1	1	0	0	2	0
2026	Pre-development	102	220	77	82	527	75	243	285	86	104	626	39
Pass-by	Distribution	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
•	Assignment	0	0	0	0	0	0	0	0	0	0	0	0
External	Distribution	20%	0%	0%	0%	30%	0%	0%	0%	20%	0%	0%	0%
	Assignment	8	0	0	0	12	0	0	0	8	0	0	0
2026	Post-development	110	220	77	82	539	75	243	285	94	104	626	39

						inty Line Ro	ad		Site Access		Cou	inty Line Ro	ad
			Northbound			Westbound			Southbound			Eastbound	
		<u>Left</u>	Through	Right	<u>Left</u>	Through	Right	<u>Left</u>	Through	Right	<u>Left</u>	Through	Right
2023	Counted	0	0	0	0	713	0	0	0	0	0	767	0
0.12%	Background		_			3			_	_	_	_ 3	
2026	Pre-development	0	0	0	0	716	0	0	0	0	0	770	0
Pass-by	Distribution	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
	Assignment	0	0	0	0	0	0	0	0	0	0	0	0
External	Distribution	0%	0%	0%	0%	0%	70%	0%	0%	-100%	30%	0%	0%
	Assignment	0	0	0	0	0	27	0	0	39	12	0	0
2026	Post-development	0	0	0	0	716	27	0	0	39	12	770	0



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Count Name: 1. Butler Avenue & County Line Road Site Code: 1 Start Date: 09/26/2023 Page No: 1

### **Turning Movement Data**

a =			•	ine Road					•	ine Road					,	wn Road bound						Avenue bound			
Start Time	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	Int. Total
7:00 AM	0	18	133	5	0	156	0	14	95	7	0	116	0	10	24	17	0	51	0	47	64	22	0	133	456
7:15 AM	0	14	146	10	0	170	0	17	118	9	0	144	0	18	31	13	0	62	0	57	82	11	0	150	526
7:30 AM	0	30	146	10	0	186	0	17	132	13	0	162	0	22	46	17	0	85	0	75	76	18	0	169	602
7:45 AM	0	27	163	11	0	201	0	24	129	20	0	173	0	20	60	25	0	105	0	57	80	26	0	163	642
Hourly Total	0	89	588	36	0	713	0	72	474	49	0	595	0	70	161	72	0	303	0	236	302	77	0	615	2226
8:00 AM	0	29	174	7	0	210	0	18	115	21	0	154	0	31	60	24	0	115	0	48	53	21	0	122	601
8:15 AM	0	18	141	11	0	170	0	23	149	21	0	193	0	29	53	11	0	93	0	62	75	21	0	158	614
8:30 AM	0	27	133	13	0	173	0	17	106	22	0	145	0	16	52	10	0	78	0	43	81	18	0	142	538
8:45 AM	0	25	133	10	0	168	0	19	111	23	0	153	0	8	53	23	0	84	0	30	72	19	0	121	526
Hourly Total	0	99	581	41	0	721	0	77	481	87	0	645	0	84	218	68	0	370	0	183	281	79	0	543	2279
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
4:00 PM	0	29	134	17	0	180	0	37	128	26	0	191	0	20	88	20	0	128	0	26	78	22	0	126	625
4:15 PM	0	43	157	16	0	216	0	44	149	32	0	225	0	19	96	27	0	142	0	29	87	24	0	140	723
4:30 PM	0	32	179	25	0	236	0	39	159	35	0	233	0	20	111	18	0	149	0	27	55	26	0	108	726
4:45 PM	0	40	185	24	0	249	0	43	139	36	0	218	0	10	85	17	0	112	0	18	79	21	0	118	697
Hourly Total	0	144	655	82	0	881	0	163	575	129	0	867	0	69	380	82	0	531	0	100	299	93	0	492	2771
5:00 PM	0	34	160	28	0	222	0	38	158	36	1	232	0	17	87	9	0	113	0	31	61	13	0	105	672
5:15 PM	0	57	163	16	0	236	0	44	121	33	0	198	0	18	108	37	0	163	0	31	83	30	0	144	741
5:30 PM	0	34	125	16	0	175	0	22	141	27	0	190	0	17	81	32	0	130	0	25	84	29	0	138	633
5:45 PM	0	42	117	21	0	180	0	7	106	24	0	137	0	26	59	29	0	114	0	28	72	27	0	127	558
Hourly Total	0	167	565	81	0	813	0	111	526	120	1	757	0	78	335	107	0	520	0	115	300	99	0	514	2604
Grand Total	0	499	2389	240	0	3128	0	423	2056	385	1	2864	0	301	1094	329	0	1724	0	634	1182	348	0	2164	9880
Approach %	0.0	16.0	76.4	7.7	-	-	0.0	14.8	71.8	13.4	-	-	0.0	17.5	63.5	19.1	-	-	0.0	29.3	54.6	16.1	-	-	-
Total %	0.0	5.1	24.2	2.4	-	31.7	0.0	4.3	20.8	3.9	-	29.0	0.0	3.0	11.1	3.3	-	17.4	0.0	6.4	12.0	3.5	-	21.9	-
Lights	0	483	2312	222	-	3017	0	418	1923	376	-	2717	0	277	1074	326	-	1677	0	609	1160	320	-	2089	9500
% Lights	-	96.8	96.8	92.5	-	96.5	-	98.8	93.5	97.7	-	94.9	-	92.0	98.2	99.1	-	97.3	-	96.1	98.1	92.0	-	96.5	96.2
Mediums	0	12	63	11	-	86	0	5	120	8	-	133	0	20	18	3	-	41	0	23	21	25	-	69	329
% Mediums	-	2.4	2.6	4.6	-	2.7	-	1.2	5.8	2.1	-	4.6	-	6.6	1.6	0.9	-	2.4	-	3.6	1.8	7.2	-	3.2	3.3
Articulated Trucks	0	4	14	7	-	25	0	0	13	1	-	14	0	4	2	0	-	6	0	2	1	3	-	6	51
% Articulated Trucks	-	0.8	0.6	2.9	-	0.8	-	0.0	0.6	0.3	-	0.5	-	1.3	0.2	0.0	-	0.3	-	0.3	0.1	0.9	-	0.3	0.5
Bicycles on Crosswalk	-	-			0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-			-	-		-	-		0.0	-	-	-			-		-	-			-		-
Pedestrians	-	-		_	0	_	-	-	_	-	1	-	-	-		_	0	-	-	-	_	-	0		-



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Count Name: 1. Butler Avenue & County Line Road Site Code: 1 Start Date: 09/26/2023 Page No: 4

#### Turning Movement Peak Hour Data (7:30 AM)

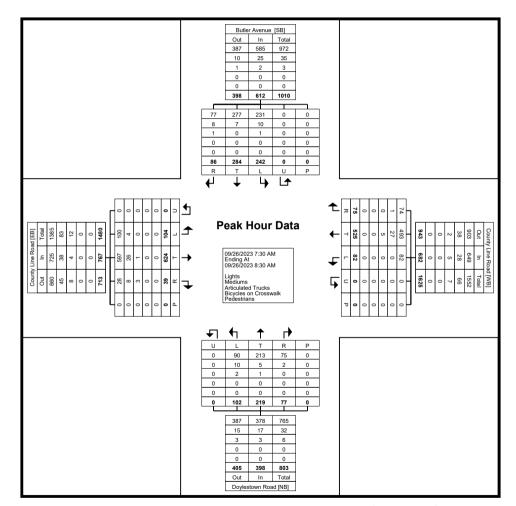
							Tun	iii ig iv	/IOV <del>C</del> II	IGHT I	-can i	noui	Dala	(7.30	Aivi)									
		County L	ine Road					County I	ine Road					Doylesto	own Road			[		Butler /	Avenue			
		Eastl	oound					West	bound					North	bound					South	bound			
U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	Int. Total
0	30	146	10	0	186	0	17	132	13	0	162	0	22	46	17	0	85	0	75	76	18	0	169	602
0	27	163	11	0	201	0	24	129	20	0	173	0	20	60	25	0	105	0	57	80	26	0	163	642
0	29	174	7	0	210	0	18	115	21	0	154	0	31	60	24	0	115	0	48	53	21	0	122	601
0	18	141	11	0	170	0	23	149	21	0	193	0	29	53	11	0	93	0	62	75	21	0	158	614
0	104	624	39	0	767	0	82	525	75	0	682	0	102	219	77	0	398	0	242	284	86	0	612	2459
0.0	13.6	81.4	5.1	-	-	0.0	12.0	77.0	11.0	-	-	0.0	25.6	55.0	19.3	-	-	0.0	39.5	46.4	14.1	-	-	-
0.0	4.2	25.4	1.6	-	31.2	0.0	3.3	21.4	3.1	-	27.7	0.0	4.1	8.9	3.1	-	16.2	0.0	9.8	11.5	3.5	-	24.9	-
0.000	0.867	0.897	0.886	-	0.913	0.000	0.854	0.881	0.893	-	0.883	0.000	0.823	0.913	0.770	-	0.865	0.000	0.807	0.888	0.827	-	0.905	0.958
0	100	597	28	-	725	0	82	493	74	-	649	0	90	213	75	-	378	0	231	277	77	-	585	2337
-	96.2	95.7	71.8	-	94.5	-	100.0	93.9	98.7	-	95.2	-	88.2	97.3	97.4	-	95.0	-	95.5	97.5	89.5	-	95.6	95.0
0	4	26	8	-	38	0	0	27	1	-	28	0	10	5	2	-	17	0	10	7	8	-	25	108
-	3.8	4.2	20.5	-	5.0	-	0.0	5.1	1.3	-	4.1	-	9.8	2.3	2.6	-	4.3	-	4.1	2.5	9.3	-	4.1	4.4
0	0	1	3	-	4	0	0	5	0	-	5	0	2	1	0	-	3	0	1	0	1	-	2	14
-	0.0	0.2	7.7	-	0.5	-	0.0	1.0	0.0	-	0.7	-	2.0	0.5	0.0	-	0.8	-	0.4	0.0	1.2	-	0.3	0.6
-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	
-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
-	-		_	0	_	-	_		-	0	-	-	-	_	-	0	-	-	-		-	0	_	-
-	-			-		-				_	-	-		-			-		-		-	-		-
	0 0 0 0 0 0 0.0 0.0 0.00 0.000 0	0 30 0 27 0 29 0 18 0 104 0.0 13.6 0.0 4.2 0.000 0.867 0 100 - 96.2 0 4 - 3.8 0 0	U-Turn         Left         Thru           0         30         146           0         27         163           0         29         174           0         18         141           0         104         624           0.0         13.6         81.4           0.0         4.2         25.4           0.000         0.867         0.897           0         100         597           -         96.2         95.7           0         4         26           -         3.8         4.2           0         0         1           -         0.0         0.2	0         30         146         10           0         27         163         11           0         29         174         7           0         18         141         11           0         104         624         39           0.0         13.6         81.4         5.1           0.0         4.2         25.4         1.6           0.000         0.867         0.897         0.886           0         100         597         28           -         96.2         95.7         71.8           0         4         26         8           -         3.8         4.2         20.5           0         0         1         3           -         0.0         0.2         7.7	Eastbound           U-Turn         Left         Thru         Right         Peds           0         30         146         10         0           0         27         163         11         0           0         29         174         7         0           0         18         141         11         0           0         104         624         39         0           0.0         13.6         81.4         5.1         -           0.0         4.2         25.4         1.6         -           0.000         0.867         0.897         0.886         -           0         100         597         28         -           -         96.2         95.7         71.8         -           0         4         26         8         -           -         3.8         4.2         20.5         -           0         0         1         3         -           -         0.0         0.2         7.7         -	Eastbound           U-Turn         Left         Thru         Right         Peds         App. Total           0         30         146         10         0         186           0         27         163         11         0         201           0         29         174         7         0         210           0         18         141         11         0         170           0         104         624         39         0         767           0.0         13.6         81.4         5.1         -         -           0.0         4.2         25.4         1.6         -         31.2           0.000         0.867         0.897         0.886         -         0.913           0         100         597         28         -         725           -         96.2         95.7         71.8         -         94.5           0         4         26         8         -         38           -         3.8         4.2         20.5         -         5.0           0         0         1         3         -         4	Eastbound           U-Turn         Left         Thru         Right         Peds         App. Total         U-Turn           0         30         146         10         0         186         0           0         27         163         11         0         201         0           0         29         174         7         0         210         0           0         18         141         11         0         170         0           0         104         624         39         0         767         0           0.0         13.6         81.4         5.1         -         -         0.0           0.0         4.2         25.4         1.6         -         31.2         0.0           0.00         0.867         0.897         0.886         -         0.913         0.000           0         100         597         28         -         725         0           -         96.2         95.7         71.8         -         94.5         -           0         4         26         8         -         38         0           -	County Line Road Eastbound           U-Turn         Left         Thru         Right         Peds         App. Total Total         U-Turn         Left           0         30         146         10         0         186         0         17           0         27         163         11         0         201         0         24           0         29         174         7         0         210         0         18           0         18         141         11         0         170         0         23           0         104         624         39         0         767         0         82           0.0         13.6         81.4         5.1         -         -         0.0         12.0           0.0         4.2         25.4         1.6         -         31.2         0.0         3.3           0.000         0.867         0.897         0.886         -         0.913         0.000         0.854           0         100         597         28         -         725         0         82           -         96.2         95.7         71.8         -	County Line Road Eastbound         County Line Road Eastbound         County Line Road Eastbound         County Line Road West           U-Turn         Left         Thru         Right         Peds         App. Total App. Total         U-Turn         Left         Thru           0         30         146         10         0         186         0         17         132           0         27         163         11         0         201         0         24         129           0         29         174         7         0         210         0         18         115           0         18         141         11         0         170         0         23         149           0         104         624         39         0         767         0         82         525           0.0         13.6         81.4         5.1         -         -         0.0         12.0         77.0           0.0         4.2         25.4         1.6         -         31.2         0.0         3.3         21.4           0.00         0.867         0.897         0.886         -         0.913	County Line Road   Eastbound   Eastbound   Eastbound   Eastbound   Eastbound   County Line Road   Westbound   W	County Line Road Eastbound         County Line Road Westbound           U-Turn         Left         Thru         Right         Peds         App. Total Total         U-Turn         Left         Thru         Right         Peds           0         30         146         10         0         186         0         17         132         13         0           0         27         163         11         0         201         0         24         129         20         0           0         29         174         7         0         210         0         18         115         21         0           0         18         141         11         0         170         0         23         149         21         0           0         104         624         39         0         767         0         82         525         75         0           0.0         13.6         81.4         5.1         -         -         0.0         12.0         77.0         11.0         -           0.0         4.2         25.4         1.6         -         31.2         0.0         3.3         21.4<	U-Turn	County Line Road Eastbound         County Line Road Westbound           U-Turn         Left         Thru         Right         Peds         App. Total Total Total Total         U-Turn         Left         Thru         Right Right Road Protal         U-Turn           0         30         146         10         0         186         0         17         132         13         0         162         0           0         27         163         111         0         201         0         24         129         20         0         173         0           0         29         174         7         0         210         0         18         115         21         0         154         0           0         18         141         11         0         170         0         23         149         21         0         193         0           0         136         81.4         5.1         -         -         0.0         12.0         77.0         11.0         -         -         0.0           0.0         4.2         25.4         1.6         -         31.2         0.0         3.3         21.4	U-Turn   Left   Thru   Right   Peds   App.   Total   U-Turn   Left   Thru   Right   Peds   App.   U-Turn   Left   U-Turn   U-U-Turn   U-Turn   U	County Line Road   Eastborn   County Line Road   Westborn   Wes	U-Turn   Left   Thru   Right   Peds   App.   U-Turn   Left   Thru   Right   Peds   App.   Total   U-Turn   Left   Thru   Right   Peds   App.   U-Turn   Left   Thru   Right   Peds   App.   U-Turn   Left   Thru   Right   Right   Peds   App.   U-Turn   Left   Thru   Right   Right   Peds   App.   U-Turn   Left   Thru   Right   Right	U-Turn	County   Frank   Fr	U-Turn	U-Turn   Left   Thru   Right   Peds   App.   U-Turn   Left   U-Turn   Left   Thru   Right   Peds   App.   U-Turn   Left   U-Turn   U-Tur	County   Line   Road   Eastborned   Eastb	U-Turn   Left   Through   Peds   App.   Peds   App.   Peds   App.   Peds   App.   Peds   Ped	U-Turn Left Thru Night Peds App. U-Turn Left Thru Night Peds U-Turn Left U-Turn Night Peds U-Turn U	U-Tum   Left   Thru   Right   Peds   App.   U-Tum   Left   Thru   Right   Peds   Thru   Right   Peds   Thru



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Count Name: 1. Butler Avenue & County Line Road

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Turning Movement Peak Hour Data Plot (7:30 AM)



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Count Name: 1. Butler Avenue & County Line Road Site Code: 1 Start Date: 09/26/2023 Page No: 6

#### Turning Movement Peak Hour Data (4:30 PM)

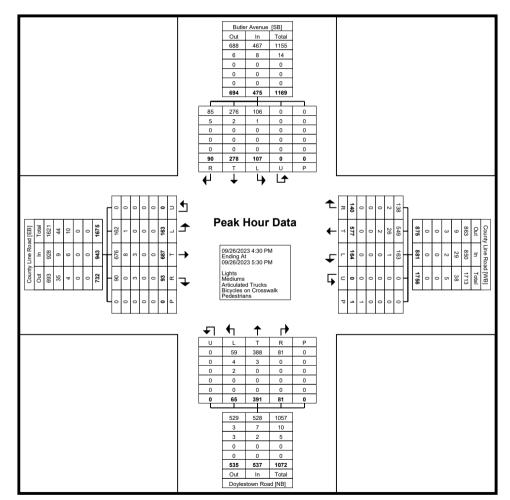
|        |  |   |   |   |   |  
   
  | Tun  
         | iii ig iv   | ioveii  | IEIIL F  | can   | noui   | Dala   | (4.30   | L IAI)   |   
  |  |  |   |   |  
   |   |                             |   |
|--------|--|---|---|---|---
--
--
---|--|---
---|--|---|--------|--|---|--|--
--	--	---	---
		County L	ine Road
   
  |  
         | County L  | ine Road  |  |   |        |  | Doylesto  | own Road   |   
  |  |  |   | Butler A  | Avenue   
   |   |                             |   |
|        |  | East  | bound   |   |   |  
   
  |  
         | West  | bound   |  |   |        |  | North   | bound  |   
  |  |  |   | South   | bound  
   |   |                             |   |
| U-Turn | Left   | Thru  | Right   | Peds  | App.<br>Total   | U-Turn   
   
  | Left   
         | Thru  | Right   | Peds   | App.<br>Total   | U-Turn | Left   | Thru  | Right  | Peds  
  | App.<br>Total  | U-Turn   | Left  | Thru  | Right  
   | Peds  | App.<br>Total               | Int. Total  |
| 0      | 32   | 179   | 25  | 0   | 236   | 0  
   
  | 39   
         | 159   | 35  | 0  | 233   | 0      | 20   | 111   | 18   | 0   
  | 149  | 0  | 27  | 55  | 26   
   | 0   | 108                         | 726   |
| 0      | 40   | 185   | 24  | 0   | 249   | 0  
   
  | 43   
         | 139   | 36  | 0  | 218   | 0      | 10   | 85  | 17   | 0   
  | 112  | 0  | 18  | 79  | 21   
   | 0   | 118                         | 697   |
| 0      | 34   | 160   | 28  | 0   | 222   | 0  
   
  | 38   
         | 158   | 36  | 1  | 232   | 0      | 17   | 87  | 9  | 0   
  | 113  | 0  | 31  | 61  | 13   
   | 0   | 105                         | 672   |
| 0      | 57   | 163   | 16  | 0   | 236   | 0  
   
  | 44   
         | 121   | 33  | 0  | 198   | 0      | 18   | 108   | 37   | 0   
  | 163  | 0  | 31  | 83  | 30   
   | 0   | 144                         | 741   |
| 0      | 163  | 687   | 93  | 0   | 943   | 0  
   
  | 164  
         | 577   | 140   | 1  | 881   | 0      | 65   | 391   | 81   | 0   
  | 537  | 0  | 107   | 278   | 90   
   | 0   | 475                         | 2836  |
| 0.0    | 17.3   | 72.9  | 9.9   | -   | -   | 0.0  
   
  | 18.6   
         | 65.5  | 15.9  | -  | -   | 0.0    | 12.1   | 72.8  | 15.1   | -   
  | -  | 0.0  | 22.5  | 58.5  | 18.9   
   | -   | -                           | -   |
| 0.0    | 5.7  | 24.2  | 3.3   | -   | 33.3  | 0.0  
   
  | 5.8  
         | 20.3  | 4.9   | -  | 31.1  | 0.0    | 2.3  | 13.8  | 2.9  | -   
  | 18.9   | 0.0  | 3.8   | 9.8   | 3.2  
   | -   | 16.7                        | -   |
| 0.000  | 0.715  | 0.928   | 0.830   | -   | 0.947   | 0.000  
   
  | 0.932  
         | 0.907   | 0.972   | -  | 0.945   | 0.000  | 0.813  | 0.881   | 0.547  | -   
  | 0.824  | 0.000  | 0.863   | 0.837   | 0.750  
   | -   | 0.825                       | 0.957   |
| 0      | 162  | 676   | 90  | -   | 928   | 0  
   
  | 163  
         | 549   | 138   | -  | 850   | 0      | 59   | 388   | 81   | -   
  | 528  | 0  | 106   | 276   | 85   
   | -   | 467                         | 2773  |
| -      | 99.4   | 98.4  | 96.8  | -   | 98.4  | -  
   
  | 99.4   
         | 95.1  | 98.6  | -  | 96.5  | -      | 90.8   | 99.2  | 100.0  | -   
  | 98.3   | -  | 99.1  | 99.3  | 94.4   
   | -   | 98.3                        | 97.8  |
| 0      | 1  | 8   | 0   | -   | 9   | 0  
   
  | 1  
         | 26  | 2   | -  | 29  | 0      | 4  | 3   | 0  | -   
  | 7  | 0  | 1   | 2   | 5  
   | -   | 8                           | 53  |
| -      | 0.6  | 1.2   | 0.0   | -   | 1.0   | -  
   
  | 0.6  
         | 4.5   | 1.4   | -  | 3.3   | -      | 6.2  | 0.8   | 0.0  | -   
  | 1.3  | -  | 0.9   | 0.7   | 5.6  
   | -   | 1.7                         | 1.9   |
| 0      | 0  | 3   | 3   | -   | 6   | 0  
   
  | 0  
         | 2   | 0   | -  | 2   | 0      | 2  | 0   | 0  | -   
  | 2  | 0  | 0   | 0   | 0  
   | -   | 0                           | 10  |
| -      | 0.0  | 0.4   | 3.2   | -   | 0.6   | -  
   
  | 0.0  
         | 0.3   | 0.0   | -  | 0.2   | -      | 3.1  | 0.0   | 0.0  | -   
  | 0.4  | -  | 0.0   | 0.0   | 0.0  
   | -   | 0.0                         | 0.4   |
| -      | -  | -   | -   | 0   | -   | -  
   
  | -  
         | -   | -   | 0  | -   | -      | -  | -   | -  | 0   
  | -  | -  | -   | -   | -  
   | 0   | -                           |   |
| -      | -  | -   | -   | -   | -   | -  
   
  | -  
         | -   | -   | 0.0  | -   | -      | -  | -   | -  | -   
  | -  | -  | -   | -   | -  
   | -   | -                           | -   |
| -      | -  |   | _   | 0   | -   | -  
   
  | _  
         |   | _   | 1  | -   | -      | -  | _   | -  | 0   
  | -  | -  |   | -   | -  
   | 0   | _                           | -   |
| -      | -  | -   |   | -   |   | -  
   
  |  
         |   |   | 100.0  | -   | -      | -  |   |  | -   
  | -  | -  |   | -   | -  
   | -   | -                           | -   |
|        | 0<br>0<br>0<br>0<br>0<br>0<br>0.0<br>0.0<br>0.00<br>0.000<br>0 | 0 32 0 40 0 34 0 57 0 163 0.0 17.3 0.0 5.7 0.000 0.715 0 162 - 99.4 0 1 - 0.6 0 0 | U-Turn Left Thru  0 32 179  0 40 185  0 34 160  0 57 163  0 163 687  0.0 17.3 72.9  0.0 5.7 24.2  0.000 0.715 0.928  0 162 676  - 99.4 98.4  0 1 8  - 0.6 1.2  0 0 3  - 0.0 0.4 | 0         32         179         25           0         40         185         24           0         34         160         28           0         57         163         16           0         163         687         93           0.0         17.3         72.9         9.9           0.0         5.7         24.2         3.3           0.000         0.715         0.928         0.830           0         162         676         90           -         99.4         98.4         96.8           0         1         8         0           -         0.6         1.2         0.0           0         3         3           -         0.0         0.4         3.2 | Eastbound           U-Turn         Left         Thru         Right         Peds           0         32         179         25         0           0         40         185         24         0           0         34         160         28         0           0         57         163         16         0           0         163         687         93         0           0.0         17.3         72.9         9.9         -           0.0         5.7         24.2         3.3         -           0.000         0.715         0.928         0.830         -           0         162         676         90         -           -         99.4         98.4         96.8         -           0         1         8         0         -           -         0.6         1.2         0.0         -           0         0         3         3         -           -         0.0         0         3         3         - | Eastbound           U-Turn         Left         Thru         Right         Peds         App. Total           0         32         179         25         0         236           0         40         185         24         0         249           0         34         160         28         0         222           0         57         163         16         0         236           0         163         687         93         0         943           0.0         17.3         72.9         9.9         -         -           0.0         5.7         24.2         3.3         -         33.3           0.000         0.715         0.928         0.830         -         0.947           0         162         676         90         -         928           -         99.4         98.4         96.8         -         98.4           0         1         8         0         -         9           -         0.6         1.2         0.0         -         1.0           0         0         3         3         -         6 </td <td>Eastbound           U-Turn         Left         Thru         Right         Peds         App. Total         U-Turn           0         32         179         25         0         236         0           0         40         185         24         0         249         0           0         34         160         28         0         222         0           0         57         163         16         0         236         0           0         163         687         93         0         943         0           0.0         17.3         72.9         9.9         -         -         0.0           0.0         5.7         24.2         3.3         -         33.3         0.0           0.00         5.7         24.2         3.3         -         33.3         0.0           0.000         0.715         0.928         0.830         -         0.947         0.000           0         162         676         90         -         928         0           -         99.4         98.4         96.8         -         98.4         -</td> <td>County Line Road Eastbound           U-Turn         Left         Thru         Right         Peds         App. Total Total         U-Turn         Left           0         32         179         25         0         236         0         39           0         40         185         24         0         249         0         43           0         34         160         28         0         222         0         38           0         57         163         16         0         236         0         44           0         163         687         93         0         943         0         164           0.0         17.3         72.9         9.9         -         -         0.0         18.6           0.0         5.7         24.2         3.3         -         33.3         0.0         5.8           0.000         0.715         0.928         0.830         -         0.947         0.000         0.932           0         162         676         90         -         928         0         163           -         99.4         98.4         96.8         -</td> <td>County Line Road Eastbound         County Line Road Eastbound         County Line Road Eastbound         County Line Road Eastbound         County Line West           U-Turn         Left         Thru         Right         Peds         App. Total App. Total         U-Turn         Left         Thru           0         32         179         25         0         236         0         39         159           0         40         185         24         0         249         0         43         139           0         34         160         28         0         222         0         38         158           0         57         163         16         0         236         0         44         121           0         163         687         93         0         943         0         164         577           0.0         17.3         72.9         9.9         -         -         0.0         18.6         65.5           0.0         5.7         24.2         3.3         -         33.3         0.0         5.8         20.3           0.000         0.715         0.928         0.830</td> <td>  County Line Road   Eastburd   Eastburd   Eastburd   Eastburd   Eastburd   Eastburd   U-Turn   Left   Thru   Right   Peds   App. Total   U-Turn   Left   Thru   Right   Right   O   32   179   25   0   236   0   39   159   35   O   40   185   24   0   249   0   43   139   36   O   34   160   28   0   222   0   38   158   36   O   57   163   16   0   236   0   44   121   33   O   163   687   93   0   943   0   164   577   140   O   17.3   72.9   9.9   -   -   0.0   18.6   65.5   15.9   O   0   5.7   24.2   3.3   -   33.3   0.0   5.8   20.3   4.9   O   0   0   5.7   24.2   3.3   -   33.3   0.0   5.8   20.3   4.9   O   0   0   162   676   90   -   928   0   163   549   138   O   162   676   90   -   928   0   163   549   138   O   162   676   90   -   98.4   -   99.4   95.1   98.6   O   1   8   0   -   9   0   1   26   2   O   O   O   0   0   0   0   0   0   0</td> <td>County Line Road Eastbound         County Line Road Westbound           U-Turn         Left         Thru         Right         Peds         App. Total Total         U-Turn         Left         Thru         Right         Peds           0         32         179         25         0         236         0         39         159         35         0           0         40         185         24         0         249         0         43         139         36         0           0         34         160         28         0         222         0         38         158         36         1           0         57         163         16         0         236         0         44         121         33         0           0         163         687         93         0         943         0         164         577         140         1           0.0         17.3         72.9         9.9         -         -         0.0         18.6         65.5         15.9         -           0.0         17.3         72.9         9.9         -         -         0.0         18.6         65.</td> <td>  U-Turn</td> <td>County Line Road Eastbound         County Line Road Westbound           U-Turn         Left         Thru         Right         Peds         App. Total Total Total Protal         U-Turn         Left         Thru         Right Peds         App. Total Protal Protal         U-Turn         Left         Thru         Right Right         Peds         App. Total Protal         U-Turn         Left         Thru         Right Right         Peds         App. Total Protal         U-Turn         Left         Thru         Right         Peds         App. Total Protal         U-Turn         Left         Thru         Right         Peds         App. Total         U-Turn         Left         Thru         Right         App. 23         0         233         0         233         0         18         0         198         0         &lt;</td> <td>County Line Road Eastborn         County Line Road Westborn           U-Turn         Left         Thru         Right         Peds         App. Total Total         U-Turn         Left         Thru         Right         Peds         App. Total Total         U-Turn         Left         Thru         Right         Peds         App. Total Potal         U-Turn         Left           0         32         179         25         0         236         0         39         159         35         0         233         0         20           0         40         185         24         0         249         0         43         139         36         0         218         0         10           0         34         160         28         0         222         0         38         158         36         1         232         0         17           0         57         163         16         0         236         0         44         121         33         0         198         0         18           0         163         687         93         0         943         0         166         65.5         15.9</td> <td>  County Line Road   Fastbound   County Line Road   Westbound   We</td> <td>  U-Turn   Left   Thru   Right   Peds   App   U-Turn   Left   Thru   Right   Peds   Thru   Right   Peds   App   U-Turn   Right   Right   Peds   App   U-Turn   Right   Right   Peds   App   U-Turn   Right   Right  </td> <td>  County   C</td> <td>  County   Frank   Frank   County   Frank   Fr</td> <td>  U-Turn   Left   Thru   Right   Peds   App.   U-Turn   Left   Thru   Right   Right   Right   Right   Peds   App.   U-Turn   Left   Thru   Right   Rig</td> <td>  U-Turn   Left   Thru   Right   Peds   App.   U-Turn   Left   Thru   Right   Reft   Thru   Right   Peds   App.   U-Turn   Left   Thru   Right   Peds   App.   U-Turn   Left   Thru   Right   Reft   Thru   Right   Peds   Thru   Thru   Right   Reft   Thru   Right   Peds   Thru   Thru   Thru   Thru   Right   Thru   Thru  </td> <td>  U-Turn   U-Turn  </td> <td>U-Turn Left Flat   Fla</td> <td>U-Turn Left Thru Right Peds</td> <td>  L-Time   Left   Thru   Refer   Left   Thru   Refer   Left   Thru   Refer   Left   Thru   Refer   Refer   Left   Thru   Refer   Refer   Left   Thru   Refer   Refer   Left   Thru   Refer   Refer   Refer   Left   Thru   Refer   Refer   Refer   Left   Thru   Refer   Refe</td> | Eastbound           U-Turn         Left         Thru         Right         Peds         App. Total         U-Turn           0         32         179         25         0         236         0           0         40         185         24         0         249         0           0         34         160         28         0         222         0           0         57         163         16         0         236         0           0         163         687         93         0         943         0           0.0         17.3         72.9         9.9         -         -         0.0           0.0         5.7         24.2         3.3         -         33.3         0.0           0.00         5.7         24.2         3.3         -         33.3         0.0           0.000         0.715         0.928         0.830         -         0.947         0.000           0         162         676         90         -         928         0           -         99.4         98.4         96.8         -         98.4         - | County Line Road Eastbound           U-Turn         Left         Thru         Right         Peds         App. Total Total         U-Turn         Left           0         32         179         25         0         236         0         39           0         40         185         24         0         249         0         43           0         34         160         28         0         222         0         38           0         57         163         16         0         236         0         44           0         163         687         93         0         943         0         164           0.0         17.3         72.9         9.9         -         -         0.0         18.6           0.0         5.7         24.2         3.3         -         33.3         0.0         5.8           0.000         0.715         0.928         0.830         -         0.947         0.000         0.932           0         162         676         90         -         928         0         163           -         99.4         98.4         96.8         - | County Line Road Eastbound         County Line Road Eastbound         County Line Road Eastbound         County Line Road Eastbound         County Line West           U-Turn         Left         Thru         Right         Peds         App. Total App. Total         U-Turn         Left         Thru           0         32         179         25         0         236         0         39         159           0         40         185         24         0         249         0         43         139           0         34         160         28         0         222         0         38         158           0         57         163         16         0         236         0         44         121           0         163         687         93         0         943         0         164         577           0.0         17.3         72.9         9.9         -         -         0.0         18.6         65.5           0.0         5.7         24.2         3.3         -         33.3         0.0         5.8         20.3           0.000         0.715         0.928         0.830 | County Line Road   Eastburd   Eastburd   Eastburd   Eastburd   Eastburd   Eastburd   U-Turn   Left   Thru   Right   Peds   App. Total   U-Turn   Left   Thru   Right   Right   O   32   179   25   0   236   0   39   159   35   O   40   185   24   0   249   0   43   139   36   O   34   160   28   0   222   0   38   158   36   O   57   163   16   0   236   0   44   121   33   O   163   687   93   0   943   0   164   577   140   O   17.3   72.9   9.9   -   -   0.0   18.6   65.5   15.9   O   0   5.7   24.2   3.3   -   33.3   0.0   5.8   20.3   4.9   O   0   0   5.7   24.2   3.3   -   33.3   0.0   5.8   20.3   4.9   O   0   0   162   676   90   -   928   0   163   549   138   O   162   676   90   -   928   0   163   549   138   O   162   676   90   -   98.4   -   99.4   95.1   98.6   O   1   8   0   -   9   0   1   26   2   O   O   O   0   0   0   0   0   0   0 | County Line Road Eastbound         County Line Road Westbound           U-Turn         Left         Thru         Right         Peds         App. Total Total         U-Turn         Left         Thru         Right         Peds           0         32         179         25         0         236         0         39         159         35         0           0         40         185         24         0         249         0         43         139         36         0           0         34         160         28         0         222         0         38         158         36         1           0         57         163         16         0         236         0         44         121         33         0           0         163         687         93         0         943         0         164         577         140         1           0.0         17.3         72.9         9.9         -         -         0.0         18.6         65.5         15.9         -           0.0         17.3         72.9         9.9         -         -         0.0         18.6         65. | U-Turn | County Line Road Eastbound         County Line Road Westbound           U-Turn         Left         Thru         Right         Peds         App. Total Total Total Protal         U-Turn         Left         Thru         Right Peds         App. Total Protal Protal         U-Turn         Left         Thru         Right Right         Peds         App. Total Protal         U-Turn         Left         Thru         Right Right         Peds         App. Total Protal         U-Turn         Left         Thru         Right         Peds         App. Total Protal         U-Turn         Left         Thru         Right         Peds         App. Total         U-Turn         Left         Thru         Right         App. 23         0         233         0         233         0         18         0         198         0         < | County Line Road Eastborn         County Line Road Westborn           U-Turn         Left         Thru         Right         Peds         App. Total Total         U-Turn         Left         Thru         Right         Peds         App. Total Total         U-Turn         Left         Thru         Right         Peds         App. Total Potal         U-Turn         Left           0         32         179         25         0         236         0         39         159         35         0         233         0         20           0         40         185         24         0         249         0         43         139         36         0         218         0         10           0         34         160         28         0         222         0         38         158         36         1         232         0         17           0         57         163         16         0         236         0         44         121         33         0         198         0         18           0         163         687         93         0         943         0         166         65.5         15.9 | County Line Road   Fastbound   County Line Road   Westbound   We | U-Turn   Left   Thru   Right   Peds   App   U-Turn   Left   Thru   Right   Peds   Thru   Right   Peds   App   U-Turn   Right   Right   Peds   App   U-Turn   Right   Right   Peds   App   U-Turn   Right   Right | County   C | County   Frank   Frank   County   Frank   Fr | U-Turn   Left   Thru   Right   Peds   App.   U-Turn   Left   Thru   Right   Right   Right   Right   Peds   App.   U-Turn   Left   Thru   Right   Rig | U-Turn   Left   Thru   Right   Peds   App.   U-Turn   Left   Thru   Right   Reft   Thru   Right   Peds   App.   U-Turn   Left   Thru   Right   Peds   App.   U-Turn   Left   Thru   Right   Reft   Thru   Right   Peds   Thru   Thru   Right   Reft   Thru   Right   Peds   Thru   Thru   Thru   Thru   Right   Thru   Thru | U-Turn   U-Turn | U-Turn Left Flat   Fla | U-Turn Left Thru Right Peds | L-Time   Left   Thru   Refer   Left   Thru   Refer   Left   Thru   Refer   Left   Thru   Refer   Refer   Left   Thru   Refer   Refer   Left   Thru   Refer   Refer   Left   Thru   Refer   Refer   Refer   Left   Thru   Refer   Refer   Refer   Left   Thru   Refer   Refe |



Imperial Traffic & Data Collection www.imperialtdc.com PO BOX 4637 Cherry Hill, New Jersey, United States 08034 609-706-6100 hfurey@imperialtdc.com

Count Name: 1. Butler Avenue & County Line Road

Site Code: 1 Start Date: 09/26/2023 Page No: 7



Turning Movement Peak Hour Data Plot (4:30 PM)



Project: County Line & Butler Municipality: New Providence, Bucks County, PA Setup: CB Location: 40.265838, -75.22843

Imperial Traffic & Data Collection www.imperialtdc.com 1804 Haddonfield-Berlin Road Cherry Hill, New Jersey, United States 08034 609-706-6100 hfurey@imperialtdc.com

Count Name: 1. County Line Road and Butler Avenue Site Code: 1 Start Date: 09/07/2024 Page No: 1

### **Turning Movement Data**

	ı						I.				mig i	710 7 01	i i Ci i C	Julu					ı						I.
			,	ine Road			ļ		•	ine Road					,	wn Road						r Avenue			
			Eastl	oound			ļ		West	bound					North	bound					South	bound			
Start Time	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	Int. Total
11:00 AM	0	28	127	22	1	177	0	23	92	20	0	135	0	18	81	17	0	116	0	31	89	14	0	134	562
11:15 AM	0	30	123	17	0	170	0	41	104	22	0	167	0	21	86	19	0	126	0	35	78	15	0	128	591
11:30 AM	0	31	111	13	0	155	0	36	126	19	0	181	0	23	78	25	0	126	0	39	75	6	0	120	582
11:45 AM	0	32	110	18	0	160	0	29	119	22	0	170	0	15	106	17	0	138	0	41	82	16	0	139	607
Hourly Total	0	121	471	70	1	662	0	129	441	83	0	653	0	77	351	78	0	506	0	146	324	51	0	521	2342
12:00 PM	0	26	149	14	1	189	0	25	139	26	0	190	0	23	91	24	0	138	0	29	93	22	0	144	661
12:15 PM	0	23	107	15	0	145	0	27	81	19	0	127	0	17	89	12	0	118	0	30	83	17	1	130	520
12:30 PM	0	37	94	21	0	152	0	36	120	25	0	181	0	16	75	13	0	104	0	21	82	16	0	119	556
12:45 PM	0	40	119	18	1	177	0	31	129	22	0	182	0	26	67	29	0	122	0	19	72	12	0	103	584
Hourly Total	0	126	469	68	2	663	0	119	469	92	0	680	0	82	322	78	0	482	0	99	330	67	1	496	2321
1:00 PM	0	19	122	17	0	158	0	38	104	17	0	159	0	24	74	12	0	110	0	28	107	30	0	165	592
1:15 PM	0	34	107	19	0	160	0	21	137	26	0	184	0	23	89	17	0	129	0	25	83	17	0	125	598
1:30 PM	0	19	109	16	0	144	0	28	111	27	0	166	0	22	82	19	0	123	0	29	73	22	0	124	557
1:45 PM	0	31	97	15	0	143	0	36	131	21	0	188	0	24	98	21	0	143	0	16	83	17	0	116	590
Hourly Total	0	103	435	67	0	605	0	123	483	91	0	697	0	93	343	69	0	505	0	98	346	86	0	530	2337
Grand Total	0	350	1375	205	3	1930	0	371	1393	266	0	2030	0	252	1016	225	0	1493	0	343	1000	204	1	1547	7000
Approach %	0.0	18.1	71.2	10.6	_	-	0.0	18.3	68.6	13.1	-	_	0.0	16.9	68.1	15.1	-	-	0.0	22.2	64.6	13.2	_	_	-
Total %	0.0	5.0	19.6	2.9	-	27.6	0.0	5.3	19.9	3.8	-	29.0	0.0	3.6	14.5	3.2	-	21.3	0.0	4.9	14.3	2.9	-	22.1	-
Lights	0	345	1351	202	-	1898	0	370	1358	264	-	1992	0	247	1010	224	-	1481	0	341	991	199		1531	6902
% Lights	-	98.6	98.3	98.5	_	98.3	-	99.7	97.5	99.2	-	98.1	-	98.0	99.4	99.6	-	99.2	-	99.4	99.1	97.5	_	99.0	98.6
Mediums	0	4	17	1	-	22	0	1	30	2	-	33	0	3	6	1	-	10	0	2	7	4	-	13	78
% Mediums	-	1.1	1.2	0.5	-	1.1	-	0.3	2.2	0.8	-	1.6	-	1.2	0.6	0.4	-	0.7		0.6	0.7	2.0	-	0.8	1.1
Articulated Trucks	0	1	. 7	2	_	10	0	0	5	0	-	5	0	2	0	0	-	2	0	0	2	1	_	3	20
% Articulated Trucks	-	0.3	0.5	1.0	-	0.5	-	0.0	0.4	0.0	-	0.2	-	0.8	0.0	0.0	-	0.1	-	0.0	0.2	0.5	-	0.2	0.3
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	_	-	-	-	-	-	_	0.0	-	-
Pedestrians	-	-	-		3	-	-	-		-	0	-	-	-			0	-	-	-	-	_	1	-	-
% Pedestrians	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-



Project: County Line & Butler Municipality: New Providence, Bucks County, PA Setup: CB Location: 40.265838, -75.22843

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Count Name: 1. County Line Road and Butler

Avenue
Site Code: 1
Start Date: 09/07/2024
Page No: 3

### Turning Movement Peak Hour Data (11:15 AM)

								ı um	ii ig ivi	OVCII	CIICI	can	ioui L	rata (	, , , , , ,										1
			County L	ine Road					County L	ine Road					Doylesto	own Road					W Butler	Avenue			
			Eastl	bound					West	bound					North	nbound					South	bound			
Start Time	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	Int. Total
11:15 AM	0	30	123	17	0	170	0	41	104	22	0	167	0	21	86	19	0	126	0	35	78	15	0	128	591
11:30 AM	0	31	111	13	0	155	0	36	126	19	0	181	0	23	78	25	0	126	0	39	75	6	0	120	582
11:45 AM	0	32	110	18	0	160	0	29	119	22	0	170	0	15	106	17	0	138	0	41	82	16	0	139	607
12:00 PM	0	26	149	14	1	189	0	25	139	26	0	190	0	23	91	24	0	138	0	29	93	22	0	144	661
Total	0	119	493	62	1	674	0	131	488	89	0	708	0	82	361	85	0	528	0	144	328	59	0	531	2441
Approach %	0.0	17.7	73.1	9.2	-	-	0.0	18.5	68.9	12.6	-	-	0.0	15.5	68.4	16.1	-	-	0.0	27.1	61.8	11.1	-	-	-
Total %	0.0	4.9	20.2	2.5	-	27.6	0.0	5.4	20.0	3.6	-	29.0	0.0	3.4	14.8	3.5	-	21.6	0.0	5.9	13.4	2.4	-	21.8	-
PHF	0.000	0.930	0.827	0.861	-	0.892	0.000	0.799	0.878	0.856	-	0.932	0.000	0.891	0.851	0.850	-	0.957	0.000	0.878	0.882	0.670	-	0.922	0.923
Lights	0	117	484	61	-	662	0	131	473	88	-	692	0	80	358	84	-	522	0	143	325	58	-	526	2402
% Lights	-	98.3	98.2	98.4	-	98.2	-	100.0	96.9	98.9	-	97.7	-	97.6	99.2	98.8	-	98.9	-	99.3	99.1	98.3	-	99.1	98.4
Mediums	0	1	7	0	-	8	0	0	14	1	-	15	0	2	3	1	-	6	0	1	3	0	-	4	33
% Mediums	-	0.8	1.4	0.0	-	1.2	-	0.0	2.9	1.1	-	2.1	-	2.4	0.8	1.2	-	1.1	-	0.7	0.9	0.0	-	0.8	1.4
Articulated Trucks	0	1	2	1	-	4	0	0	1	0	-	1	0	0	0	0	-	0	0	0	0	1	-	1	6
% Articulated Trucks	-	0.8	0.4	1.6	-	0.6	-	0.0	0.2	0.0	-	0.1	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	1.7	-	0.2	0.2
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	0.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Pedestrians	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	0	-	-	_	-	-	0	-	-
% Pedestrians	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

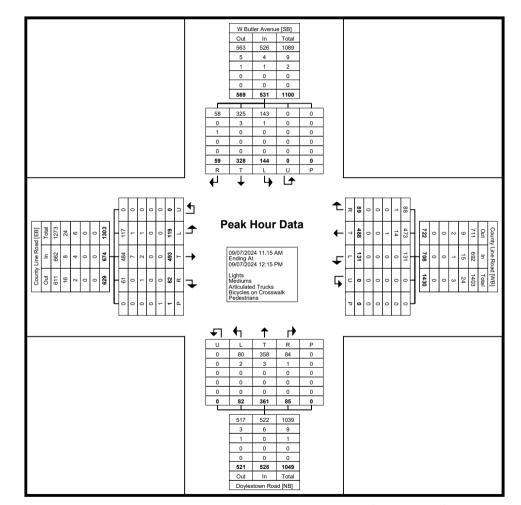


Project: County Line & Butler Municipality: New Providence, Bucks County, PA Setup: CB Location: 40.265838, -75.22843

Imperial Traffic & Data Collection www.imperialtdc.com 1804 Haddonfield-Berlin Road Cherry Hill, New Jersey, United States 08034 609-706-6100 hfurey@imperialtdc.com

Count Name: 1. County Line Road and Butler

Avenue
Site Code: 1
Start Date: 09/07/2024
Page No: 4



Turning Movement Peak Hour Data Plot (11:15 AM)

Development: Car Wash

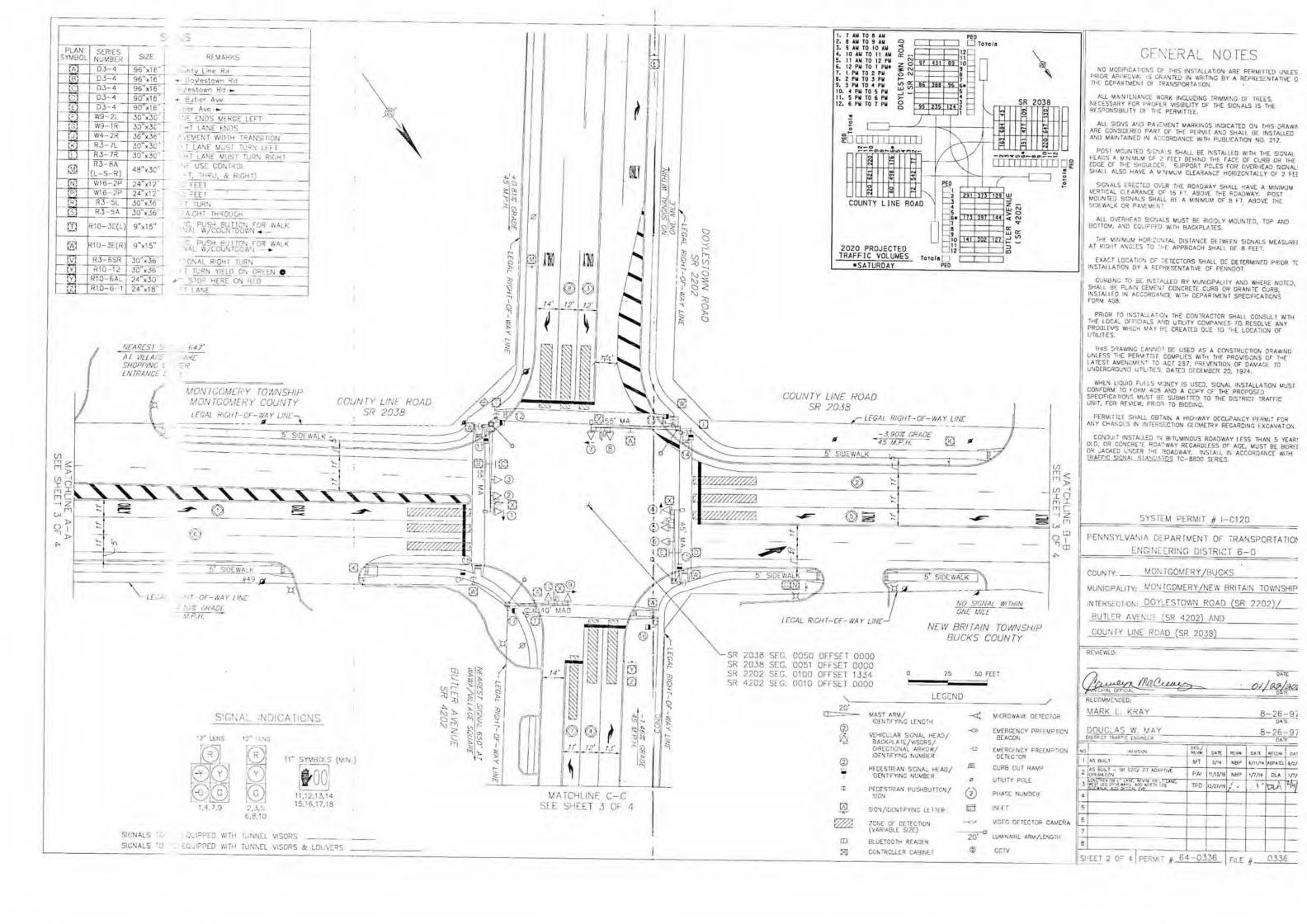
**Location:** New Britain Township, Bucks County

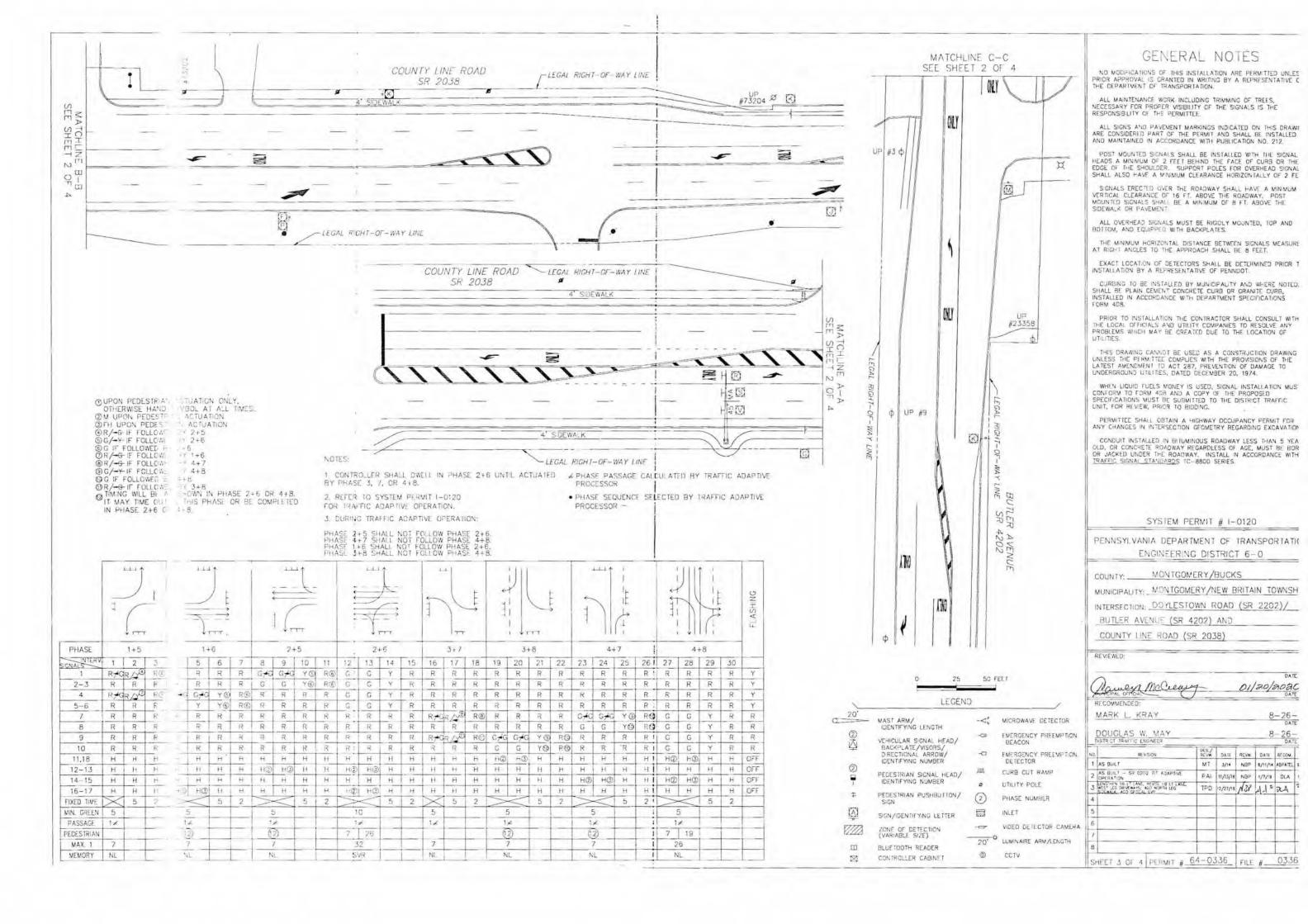
Period: Evening Peak

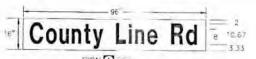
	<u>Enter</u>	<u>Exit</u>	<u>Total</u>		
Pass-by	-	-	-	0%	
External	39	39	78		Assigned to Driveways and Intersection

			<b>itler Avenue</b> Northbound			nty Line Roa Westbound	ad		utler Avenue Southbound	1	Cou	nty Line Roa Eastbound	ad
		<u>Left</u>	<b>Through</b>	Right	<u>Left</u>	<b>Through</b>	Right	<u>Left</u>	<b>Through</b>	Right	<u>Left</u>	<b>Through</b>	<u>Right</u>
2023	Counted	65	391	81	164	577	140	107	278	90	163	687	93
0.12%	Background	0	1	0	1	2	1	0	1	0	1	2	0
2026	Pre-development	65	392	81	165	579	141	107	279	90	164	689	93
Pass-by	Distribution	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
	Assignment	0	0	0	0	0	0	0	0	0	0	0	0
External	Distribution	20%	0%	0%	0%	30%	0%	0%	0%	20%	0%	0%	0%
	Assignment	8	0	0	0	12	0	0	0	8	0	0	0
2026	Post-development	73	392	81	165	591	141	107	279	98	164	689	93

			Northbound			<b>Inty Line Ro</b> Westbound	ad		Site Access Southbound		Cou	Inty Line Ro	ad
		<u>Left</u>	Through	Right	<u>Left</u>	Through	<u>Right</u>	<u>Left</u>	Through	Right	<u>Left</u>	Through	Right
2023	Counted	0	0	0	0	732	0	0	0	0	0	943	0
0.12% 2026	Background Pre-development	0	0	0	0	3 735	0	0	0	0	0	3 946	0
Pass-by	Distribution Assignment	0% 0	0% 0	0% 0	0% 0	0% 0	70% 0	0% 0	0% 0	0% 0	0% 0	0% 0	0% 0
External	Distribution Assignment	0% 0	0% 0	0% 0	0% 0	0% 0	70% 27	0% 0	0% 0	-100% 39	30% 12	0% 0	0% 0
2026	Post-development	0	0	0	0	735	27	0	0	39	12	946	0







SIGN A TE CLEARVIEW SER 9% REDUC 10.6" UC/2 WHITE REFLECTORIZED | " ON CREEN REFLECTORIZED B

Dovlestown Rd

SIGN BL CLEARVIEW SE

> BACKGRO 0.8" ROE

2.5" CORNEL ASI

8" UC/6" WHITE REFLECTORIZE

HORDER ON GREEN

SIGNALS

2-3

4

5-6

8

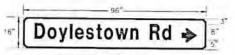
10

11,18

12,13

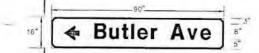
14,15

16,17

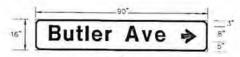


SIGN C DETAIL

CLEARVIEW SERIES 1-W : 8" UC/6" LC WHITE REFLECTORIZED LEGEND AND BORDER ON GREEN REFLECTORIZED BACKGROUND 0.8" BORDER 2.5" CORNER RADII



SIGN DETAIL CLEARVIEW SERIES 3-W 8" UC/6" LC WHITE REFLECTORIZED LEGEND AND BORDER ON GREEN REFLECTORIZED BACKGROUND O.B" BORDER 2.5" CORNER RADII



SIGN E DETAIL

CLEARVIEW SERIES 3-W 8" UC/6" LC WHITE REFLECTORIZED LEGEND AND BORDER ON GREEN REFLECTORIZED BACKGROUND O.B" BORDER 2.5" CORNER RADII

# EMERGENC REEMPTION TIMING DIAGRAM 111 PHASE

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MENT HAS ENCODING CAPABILITIES FOR IT IS RECOVMENDED TO HAVE THE ZERO NOTE: IF PRE-EMPTION "OD" FEATURE CN. THE UNCODED EMITTERS THE ABILITY TO ACTIVATE THE EY CY PREEMPTION.

\*\* FOR DURATION - ME-EMPTION EMERGENCY PRE-EMPTION NOTES:

 CONTROLLER TO BE EQUIPPED WITH EMERGENCY PRE-EMPTION FOR THE EASTBOUND & WESTBOUND APPROACHES OF COUNTY LINE ROAD AND THE NORTHBOUND AND SOUTHBOUND APPROACHES OF DOYLESTOWN ROAD/BUTLER AVENUE WITH A FLASHING FAIL SAFE DEVICE FOR EACH DIRECTION OF OPERATION.—THIS EMERGENCY BEACON SHALL CONSIST OF A FLASHING WHITE FLOOD LIGHT, AND SHALL FLASH WHEN THE EMERGENCY VEHICLE HAS CONTROL OF THE INTERSECTION FOR THE APPROPRIATE APPROACH

APPROACH.

THE SIGNALS, WHEN ACTIVATED BY EMERGENCY VEHICLE, SHALL TERMINATE ALL GREEN INDICATIONS IMMEDIATELY, FOLLOWED BY THE COMPLETE YELLOW AND RED CLEARANCE INTERVALS, ACCORDINGLY, THEN THE CREEN INTERVAL FOR THE PRE-EMPTION PHASE SHALL FOLLOW, ONLY THOSE PHASES NOT POSING A YELLOW TRAP CONDITION MAY REMAIN GREEN (1+6,2+5,3+8, OR 4+7) WHEN GOVERNED BY APPROACHING EMERGENCY VEHICLE.

 THE SIGNALS, WHEN ACTIVATED BY EMERGENCY VEHICLE, SHALL TIME OUT ALL YELLOW AND RED INDICATIONS, FOLLOWED BY THE GREEN INTERVAL OF THE PRE-EMPTION PHASE GOVERNED BY THE APPROACHING EMERGENCY. VEHICLE.

• IF THE SIGNAL HAS BEEN ACTUATED BY A PEDESTRIAN PUSH BUTTON AND THE SIGNAL IS PRE-EMPTED DURING THE "MAN" INTERVAL, THE MAN INTERVAL SHALL TERMINATE IMMEDIATELY FOLLOWED BY THE "FLASHING HAND" INDICATION IN ITS ENTIRETY, FOLLOWED BY THE APPROPRIATE SELECTIVE CLEARANCES BEFORE PROCEEDING TO THE PRE-EMPTION PHASE.

• IF THE SIGNALS, WHEN ACTIVATED BY AN EMERGENCY VEHICLE, ARE FLASHING, ALL SIGNALS SHALL REMAIN FLASHING.

• IF ADDITIONAL PRE-EMPTION PHASES ARE ACTIVATED WHILE IN PRE-EMPTION, THE ORIGINAL PRE-EMPTION PHASE SHALL TIME OUT BEFORE PROCEEDING TO THE NEXT PRE-EMPTION PHASE.

RETURNING TO NORMAL OPERATION, PHASE 216 INTERVAL 12 SHALL

• IN EMERGENCY PRE-EMPTION, NO PRIORITY SHALL BE ESTABLISHED, PRE-EMPTION SHALL BE A "FIRST COME, FIRST SERVE" OPERATION.

· LOCATION OF EMERGENCY VEHICLE DETECTORS ARE TO BE FIELD ADJUSTED

· UPON COMPLETION OF PRE-EMPTION PHASE 2, 4, 6 OR 8, IN

TO ACHIEVE MAXIMUM OPERATION

REVIEWED: 25 50 FEET LEGEND MICROWAVE DETECTOR IDENTIFYING LENGTH EMERGENCY PREEMETION VEHICULAR SIGNAL HEAD/ BEACON BACKPLATE/VISORS/ DIRECTIONAL ARROW/ EMERGENCY PREEMPTION IDENTIFYING NUMBER DETECTOR AS BUILT PEDESTRIAN SIGNAL HEAD/ CURB CUT RAVE LITILITY POLE PEDESTRIAN PUSHBUTTON/ (2) PHASE NUMBER 500 MLET SIGN/IDENTIFYING LETTER VIDEO DETECTOR CAMERA LUMINAIRE ARM/LENGTH 20 BLUETOOTH READER 3  $\boxtimes$ CONTROLLER CABINET SHEET 4 OF 4 PERMIT # 64-0336 FILE # 0336

### GENERAL NOTES

NO MODIFICATIONS OF THIS INSTALLATION ARE PERMITTED UNLESS PRIOR APPROVAL IS CRANTED IN WRITING BY A REPRESENTATIVE OF THE DEPARTMENT OF TRANSPORTATION.

ALL MAINTENANCE WORK INCLUDING TRIMMING OF TREES, NECESSARY FOR PROPER VISIBILITY OF THE SIGNALS IS THE RESPONSIBILITY OF THE PERMITTEE.

ALL SIGNS AND PAVEMENT MARKINGS INDICATED ON THIS DRAWING ARE CONSIDERED PART OF THE PERMIT AND SHALL BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH PUBLICATION NO. 212.

POST MOUNTED SIGNALS SHALL BE INSTALLED WITH THE SIGNAL HEADS A MINIMUM OF 2 FEET BEHIND THE FACE OF CURB OR THE EDGE OF THE SHOULDER. SUPPORT POLES FOR OVERHEAD SIGNALS SHALL ALSO HAVE A MINIMUM CLEARANCE HORIZONTALLY OF 2 FEET

SIGNALS ERECTED OVER THE ROADWAY SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 16 FT. ABOVE THE ROADWAY. POST MOUNTED SIGNALS SHALL BE A MINIMUM OF B FT. ABOVE THE SIDEWALK OR PAVEMENT.

ALL OVERHEAD SIGNALS MUST BE RIGIDLY MOUNTED, TOP AND BOTTOM, AND EQUIPPED WITH BACKPLATES.

THE MINIMUM HORIZONTAL DISTANCE BETWEEN SIGNALS MEASURED AT RICHT ANGLES TO THE APPROACH SHALL BE 8-FEET.

EXACT LOCATION OF DETECTORS SHALL BE DETERMINED PRIOR TO INSTALLATION BY A REPRESENTATIVE OF PENNDOT.

CURBING TO BE INSTALLED BY MUNICIPALITY AND WHERE NOTED, SHALL BE PLAIN CEMENT CONGRETE CURB OR GRANITE CURB, INSTALLED IN ACCORDANCE WITH DEPARTMENT SPECIFICATIONS FORM 40B.

PRIOR TO INSTALLATION THE CONTRACTOR SHALL CONSULT WITH THE LOCAL OFFICIALS AND UTILITY COMPANIES TO RESOLVE ANY PROBLEMS WHICH MAY BE CREATED DUE TO THE LOCATION OF UTILITIES.

THIS DRAWING CANNOT HE USED AS A CONSTRUCTION DRAWING UNLESS THE PERMITTEE COMPLES WITH THE PROVISIONS OF THE LATEST AMENDMENT TO ACT 287, PREVENTION OF DAMAGE TO UNDERGROUND UTILITIES, DATED DECEMBER 20, 1974.

WHEN LIQUID FUELS MONEY IS USED, SIGNAL INSTALLATION MUST CONFORM TO FORM 408 AND A COPY OF THE PROPOSED SPECIFICATIONS MUST RE SUBMITTED TO THE DISTRICT TRAFFIC UNIT, FOR REVIEW, PRICE TO BIDDING.

PERMITTEE SHALL OBTAIN A HIGHWAY OCCUPANCY PERMIT FOR ANY CHANGES IN INTERSECTION GEOMETRY REGARDING EXCAVATION.

CONDUIT INSTALLED IN BITUMINOUS ROADWAY LESS THAN 5 YEARS OLD, OR CONCRETE ROADWAY REGARDLESS OF AGE, MUST BE BOREI OR JACKED UNDER THE ROADWAY, INSTALL IN ACCORDANCE WITH IRAFFIC SIGNAL STANDARDS TC-8800 SERIES.

SYSTEM PERMIT # 1-0120

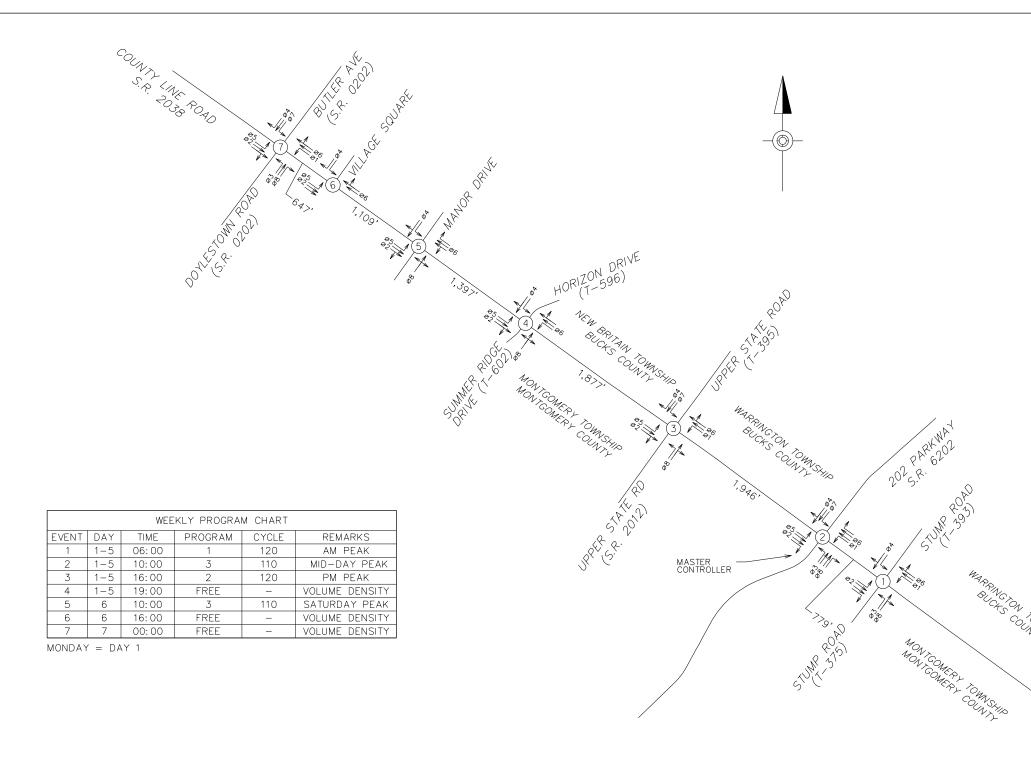
PENNSYLVANIA DEPARTMENT OF TRANSPORTATION ENGINEERING DISTRICT 6-0

MONTGOMERY/BUCKS COUNTY: \_\_ MUNICIPALITY: MONTGOMERY/NEW BRITAIN TOWNSHIP INTERSECTION: DOYLESTOWN ROAD (SR 2202)/

BUTLER AVENUE (SR 4202) AND COUNTY LINE ROAD (SR 2038)

Danly McCremy 01/20/202 RECOMMENDE MARK L. KRAY

BEST DATE REVW DATE RECOM DATE MT 3/14 NHP 6/11/14 ASPATEL 9/2/14 AS BUILT - SR G202 TIT ACAPTIVE OPERATION PAI 11/13/18 NBP 1/7/19 DLA 1/7/15 TPD 17/19/19/19 ASIN DLA 7/4/2



	SYSTEM		PROG	RAM CYCLE L	ENGTH
	INTERSECTION	FILE #	1	2	3
1	COUNTY LINE ROAD (SR 2038) & STUMP ROAD (T-393/375)	64-2225	120	120	110
2	COUNTY LINE ROAD (SR 2038) & 202 PARKWAY (SR 6202)	3706	120	120	110
3	COUNTY LINE ROAD (SR 2038) & UPPER STATE ROAD (T-395/SR 2012)	1966	120	120	110
4	COUNTY LINE ROAD (SR 2038) & HORIZON DRIVE (T-596)/SUMMER RIDGE DRIVE (T-602)	3707	120	120	110
5	COUNTY LINE ROAD (SR 2038) & MANOR DRIVE	61-3395	120	120	110
6	COUNTY LINE ROAD (SR 2038) & VILLAGE SQUARE ENTRANCE DRIVE	61-2320	120	120	110
7	COUNTY LINE ROAD (SR 2038) & BUTLER AVENUE/DOYLESTOWN ROAD (SR 0202)	61-0336	120	120	110

SYSTEM PLAN Traffic Signal Plan

DISTRICT	COUNTY	ROUTE	SECTION	SH	EET
6-0	MONTGOMERY/BUCKS	2038	WD1	1 (	)F 3
MONTGO	MERY, NEW BRITA	IN & WARR	INGTON TO	OWNSH I	IPS .
REVISION NUMBER	REV	ISIONS		DATE	BY

### GENERAL NOTES

NO MODIFICATIONS OF THIS INSTALLATION ARE
PERMITTED UNLESS PRIOR APPROVAL IS GRANTED IN WRITING
BY A REPRESENTATIVE OF THE DEPARTMENT OF
TRANSPORTATION.

REFER TO TRAFFIC SIGNAL PERMIT DRAWING FOR INDIVIDUAL INTERSECTION OPERATION, GEOMETRY, PHASING AND CRITICAL TIMES.

FOR CONSTRUCTION AND INSPECTION THE SYSTEM PERMIT SHOULD ALWAYS BE ACCOMPANIED WITH TRAFFIC SIGNAL PERMIT DRAWING.

TEST THE SYSTEM AT LOCAL INTERSECTION LEVEL, SUBSYSTEM LEVEL, MASTER CONTROLLER LEVEL AND PERSONAL COMPUTER REMOTE DIAL UP LEVEL.

GATHER THE SYSTEM FAILURE CRITICAL ALARMS REPORT AND ARCHIVE THEM WHERE APPLICABLE.

SET UP PENNDOT DISTRICT 6-0 COMPUTER WITH THE SYSTEM DATABASE AND GRAPHICS. MODIFY THE DATABASE AND GRAPHICS FOR SYSTEMS REVISIONS.

ASSIGN LOOP DETECTORS AND PROGRAM THE CONTROLLERS TO GATHER TRAFFIC VOLUMES IN 15 MINUTE INTERVALS, WHERE APPLICABLE.

PRIOR TO INSTALLATION BY A REPRESENTATIVE OF PENNDOT

OBTAIN POLE ATTACHMENT PERMIT FOR AERIAL FIBER

EXACT LOCATION OF DETECTORS SHALL BE DETERMINED

OPTIC INSTALLATION.

MAINTAIN MASTER CONTROLLER COMMUNICATION SUCH

PRIOR TO INSTALLATION THE CONTRACTOR SHALL CONSULT WITH THE LOCAL OFFICIALS AND UTILITY COMPANIES TO RESOLVE ANY PROBLEMS WHICH MAY BE

CREATED DUE TO THE LOCATION OF THE UTILITIES.

THE DRAWING CANNOT BE USED AS A CONSTRUCTION DRAWING UNLESS THE PERMITTEE COMPLIES WITH THE PROVISIONS OF ACT 181, PREVENTION OF DAMAGE TO UNDERGROUND UTILITIES EFFECTIVE DATE MARCH 29, 2007

WHEN LIQUID FUELS MONEY IS USED, SIGNAL INSTALLATION MUST CONFORM TO FORM 408 AND A COPY OF THE PROPOSED SPECIFICATIONS MUST BE SUBMITTED TO THE DISTRICT TRAFFIC UNIT FOR REVIEW PRIOR TO BIDDING.

PERMITTEE SHALL OBTAIN A HIGHWAY OCCUPANCY PERMIT FOR ANY CHANGES IN INTERSECTION GEOMETRY REGARDING EXCAVATION.

CONDUIT INSTALLED IN BITUMINOUS ROADWAY LESS THAN 5 YEARS OLD, OR CONCRETE ROADWAY REGARDLESS OF AGE.

MUST BE BORED OR JACKED UNDER THE ROADWAY. INSTALL IN ACCORDANCE WITH TRAFFIC SIGNAL STANDARDS TC-8800 SERIES.

SYSTEM FILE # I-0121

PENNSYLVANIA DEPARTMENT OF TRANSPORTATION ENGINEERING DISTRICT 6-0

COUNTY: MONTGOMERY/BUCKS

MUNICIPALITY: MONTGOMERY, NEW BRITAIN &

WARRINGTON TOWNSHIPS

INTERSECTION: COUNTY LINE ROAD (SR 2038)

TRAFFIC SIGNAL SYSTEM

D:

MONTGOMERY TOWNSHIP OFFICIAL

NEW BRITAIN TOWNSHIP OFFICIAL

RECOMMENDED:

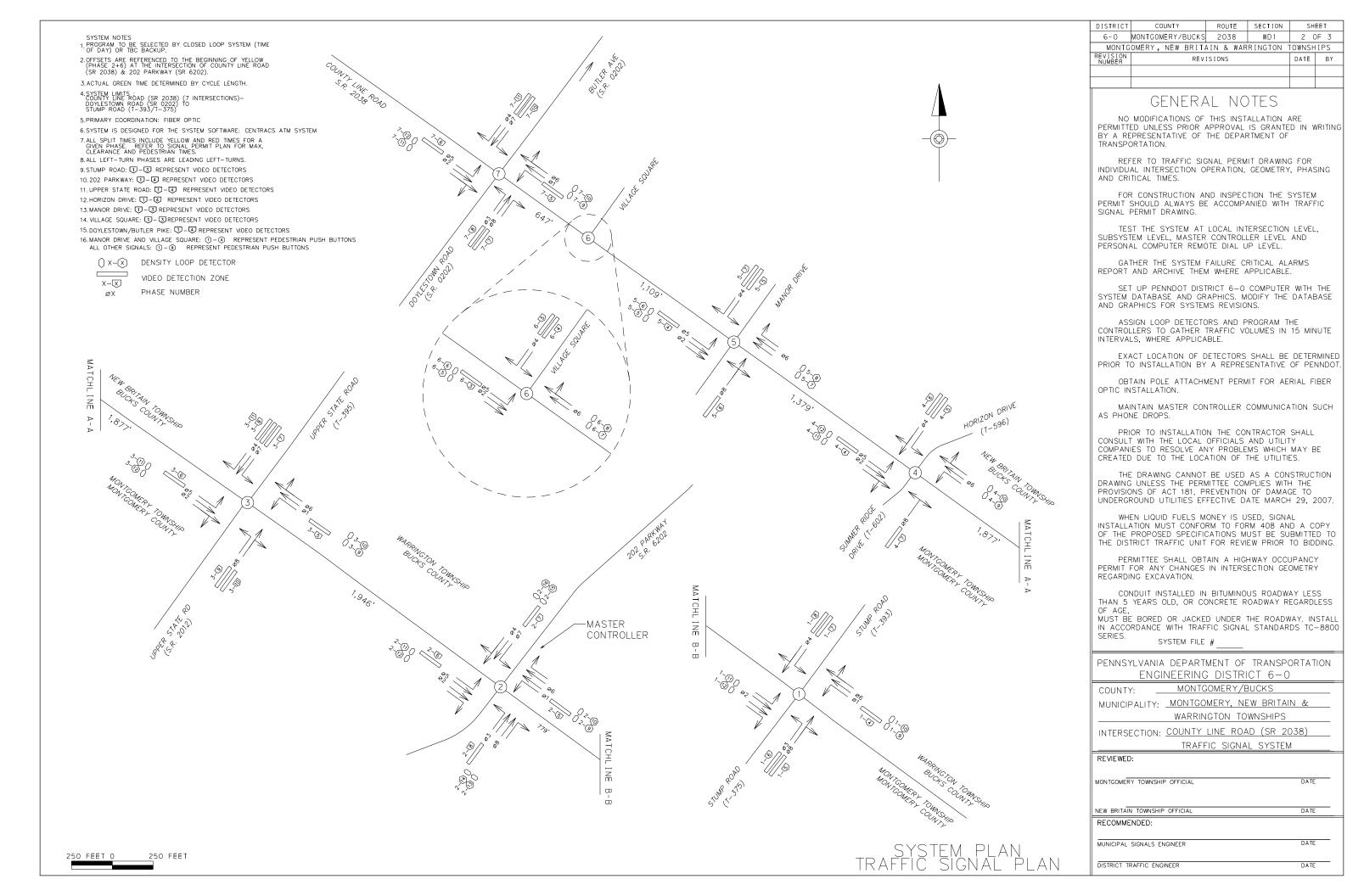
MUNICIPAL SIGNALS ENGINEER

DISTRICT TRAFFIC ENGINEER

DATE

DATE

5<u>00 FEET 0 5</u>00 FEET



PROGRAM 1						PHASE					CYCLE	OFFSET	REFERENCE
INTERSECTION	FILE #	1	2	3	4	5	6	7	8	9			111121117712
1 COUNTY LINE ROAD (SR 2038) & STUMP ROAD (T-393/375)	64-2225	14(LEAD)	45	14(LEAD)	42		59		61		120	11	7
2 COUNTY LINE ROAD (SR 2038) & 202 PARKWAY (SR 6202)	3706	20(LEAD)	43	14(LEAD)	43	14(LEAD)	49	20(LEAD)	37		120	0	14
3 COUNTY LINE ROAD (SR 2038) & UPPER STATE ROAD (T-395/SR 2012)	1966	14(LEAD)	45		61	14(LEAD)	45	18(LEAD)	43		120	41	14
4 COUNTY LINE ROAD (SR 2038) & HORIZON DRIVE (T-596)/SUMMER RIDGE DRIVE (T-602)	3707		83		37	28(LEAD)	55		37		120	89	7
5 COUNTY LINE ROAD (SR 2038) & MANOR DRIVE	61-3395		79		41	23(LEAD)	56		41		120	8	6
6 COUNTY LINE ROAD (SR 2038) & VILLAGE SQUARE ENTRANCE DRIVE	61-2320		79		41	24(LEAD)	55				120	13	6
7 COUNTY LINE ROAD (SR 2038) & BUTLER AVENUE/DOYLESTOWN ROAD (SR 0202)	61-0336	22(LEAD)	40	15(LEAD)	43	14(LEAD)	48	25(LEAD)	33		120	103	14
PROGRAM 2						PHASE					CYCLE	OFFSET	REFERENCE INTERVAL
INTERSECTION	FILE #	1	2	3	4	5	6	7	8	9			
1 COUNTY LINE ROAD (SR 2038) & STUMP ROAD (T-393/375)	64-2225	14(LEAD)	49	23(LEAD)	34		63		57		120	8	7
2 COUNTY LINE ROAD (SR 2038) & 202 PARKWAY (SR 6202)	3706	20(LEAD)	43	19(LEAD)	38	15(LEAD)	48	20(LEAD)	37		120	0	14
3 COUNTY LINE ROAD (SR 2038) & UPPER STATE ROAD (T-395/SR 2012)	1966	16(LEAD)	45		59	14(LEAD)	47	18(LEAD)	41		120	48	14
4 COUNTY LINE ROAD (SR 2038) & HORIZON DRIVE (T-596)/SUMMER RIDGE DRIVE (T-602)	3707		86		34	19(LEAD)	67		34		120	83	7
5 COUNTY LINE ROAD (SR 2038) & MANOR DRIVE	61-3395		85		35	18(LEAD)	67		35		120	8	6
6 COUNTY LINE ROAD (SR 2038) & VILLAGE SQUARE ENTRANCE DRIVE	61-2320		88		32	19(LEAD)	69				120	1	6
7 COUNTY LINE ROAD (SR 2038) & BUTLER AVENUE/DOYLESTOWN ROAD (SR 0202)	61-0336	23(LEAD)	40	14(LEAD)	43	16(LEAD)	47	17(LEAD)	40		120	112	14
PROGRAM 3						PHASE					CYCLE	OFFSET	REFERENCE
INTERSECTION	FILE #	1 1	2	3	4	5	6	7	8	9			INTERVAL
1 COUNTY LINE ROAD (SR 2038) & STUMP ROAD (T-393/375)	64-2225	14(LEAD)	41	15(LEAD)	40		55		55		110	9	7
2 COUNTY LINE ROAD (SR 2038) & 202 PARKWAY (SR 6202)	3706	16(LEAD)	39	16(LEAD)	39	14(LEAD)	41	18(LEAD)	37		110	0	14
3 COUNTY LINE ROAD (SR 2038) & UPPER STATE ROAD (T-395/SR 2012)	1966	15(LEAD)	39	1.0(22.10)	56	14(LEAD)	40	18(LEAD)	38		110	43	14
4 COUNTY LINE ROAD (SR 2038) & HORIZON DRIVE (T-596)/SUMMER RIDGE DRIVE (T-602)	3707		75		35	23(LEAD)	52	-()	35		110	79	7
5 COUNTY LINE ROAD (SR 2038) & MANOR DRIVE	61-3395		72		38	20(LEAD)	52		35		110	15	6
6 COUNTY LINE ROAD (SR 2038) & VILLAGE SQUARE ENTRANCE DRIVE	61-2320		73		37	20(LEAD)	53				110	28	6
7 COUNTY LINE ROAD (SR 2038) & BUTLER AVENUE/DOYLESTOWN ROAD (SR 0202)	61-0336	15(LEAD)	43	15(LEAD)	37	18(LEAD)	40	19(LEAD)	33		110	0	14
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GENERAL NOTES  NO MONTGOMERY, NEW BRITAIN & WARRINGTON TOWNSH  REVISION  CENERAL NOTES  NO MODIFICATIONS OF THIS INSTALLATION ARE PERMITTED UNLESS PRIOR APPROVAL IS GRANTED IN W BY A REPRESENTATIVE OF THE DEPARTMENT OF TRANSPORTATION.  REFER TO TRAFFIC SIGNAL PERMIT DRAWING FOR INDIVIDUAL INTERSECTION OPERATION, GEOMETRY, PHAS AND CRITICAL TIMES.  FOR CONSTRUCTION AND INSPECTION THE SYSTEM PERMIT SHOULD ALWAYS BE ACCOMPANIED WITH TRAFF SIGNAL PERMIT DRAWING.  TEST THE SYSTEM AT LOCAL INTERSECTION LEVEL SUBSYSTEM LEVEL, MASTER CONTROLLER LEVEL AND PERSONAL COMPUTER REMOTE DIAL UP LEVEL.  GATHER THE SYSTEM FAILURE CRITICAL ALARMS REPORT AND ARCHIVE THEM WHERE APPLICABLE.  SET UP PENNDOT DISTRICT 6-0 COMPUTER WITH SYSTEM DATABASE AND GRAPHICS. MODIFY THE DATAB AND GRAPHICS FOR SYSTEMS REVISIONS.  ASSIGN LOOP DETECTORS AND PROGRAM THE CONTROLLERS TO GATHER TRAFFIC VOLUMES IN 15 MIN INTERVALS, WHERE APPLICABLE.  EXACT LOCATION OF DETECTORS SHALL BE DETER PRIOR TO INSTALLATION BY A REPRESENTATIVE OF PEN  OBTAIN POLE ATTACHMENT PERMIT FOR AERIAL FI OPTIC INSTALLATION.  MAINTAIN MASTER CONTROLLER COMMUNICATION S AS PHONE DROPS.  PRIOR TO INSTALLATION THE CONTRACTOR SHALL CONSULT WITH THE LOCAL OFFICIALS AND UTILITY COMPANIES TO RESOLVE ANY PROBLEMS WHICH MAY B CREATED DUE TO THE LOCATION OF THE UTILITIES.  THE DRAWING CANNOT BE USED AS A CONSTRUCT DRAWING UNLESS THE PERMITTEE COMPLIES WITH THE PROVISIONS OF ACT 181, PREVENTION OF DAMAGE TO UNDERGROUND UTILITIES FFFECTIVE DATE MARGE TO THE PROPOSED SPECIFICATIONS MUST BE SUBMITTET THE DISTRICT TRAFFIC UNIT FOR REVIEW PRIOR TO BID  PERMITTE SHALL DELIN A HIGHWAY OCCUPANCY PERMIT FOR ANY CHANGES IN INTERSECTION GEOMETRY REGARDING EXCAVATION.  CONDUIT INSTALLED IN BITUMINOUS ROADWAY LES THAN 5 YEARS OLD, OR CONCRETE ROADWAY REGARDL OF ALE,  MUST BE BORED OR JACKED UNDER THE ROADWAY RES  INTERSECTION: COUNTY LINE ROAD (SR 2038).  TRAFFIC SIGNAL SYSTEM  MON	STRICT	COUNTY	ROUTE	SECTION	SHE	
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WARRINGTON TOWNSHIPS  INTERSECTION: COUNTY LINE ROAD (SR 2038)  TRAFFIC SIGNAL SYSTEM  REVIEWED:  MONTGOMERY TOWNSHIP OFFICIAL  DATE  NEW BRITAIN TOWNSHIP OFFICIAL  DATE						
TRAFFIC SIGNAL SYSTEM  REVIEWED:  MONTGOMERY TOWNSHIP OFFICIAL  DATE  NEW BRITAIN TOWNSHIP OFFICIAL  DATE						_
TRAFFIC SIGNAL SYSTEM  REVIEWED:  MONTGOMERY TOWNSHIP OFFICIAL  NEW BRITAIN TOWNSHIP OFFICIAL  DATE	 NTERSF	CTION: COUNTY	LINE ROA	D (SR 20	038)	
MONTGOMERY TOWNSHIP OFFICIAL DATE  NEW BRITAIN TOWNSHIP OFFICIAL DATE						_
NEW BRITAIN TOWNSHIP OFFICIAL DATE	VIEWED:					
	NTGOMERY	TOWNSHIP OFFICIAL			DATE	
	W BRITAIN	TOWNSHIP OFFICIAL			DATE	
MUNICIPAL SIGNALS ENGINEER DATE	JNICIPAL S	IGNALS ENGINEER			DATE	

DISTRICT TRAFFIC ENGINEER



o. Batter / Worldo & C												<u> </u>
	•	<b>→</b>	*	1	•	*	1	†	1	-	Ţ	4
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	7	<b>1</b>		7	<b>1</b>		*	<b>^</b>	7	7	<b>†</b>	7
Traffic Volume (veh/h)	104	624	39	82	525	75	102	219	77	242	284	86
Future Volume (veh/h)	104	624	39	82	525	75	102	219	77	242	284	86
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1864	1864	1864	1683	1683	1683	1724	1724	1793	1780	1780	1852
Adj Flow Rate, veh/h	108	650	41	85	547	78	106	228	80	252	296	90
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	6	6	6	2	2	2	5	5	5	4	4	4
Cap, veh/h	377	1462	92	332	1200	171	233	261	230	318	398	351
Arrive On Green	0.05	0.43	0.43	0.04	0.43	0.43	0.07	0.15	0.15	0.14	0.22	0.22
Sat Flow, veh/h	1775	3383	213	1603	2810	399	1642	1724	1520	1696	1780	1569
Grp Volume(v), veh/h	108	340	351	85	310	315	106	228	80	252	296	90
Grp Sat Flow(s),veh/h/ln	1775	1771	1825	1603	1599	1611	1642	1724	1520	1696	1780	1569
Q Serve(g_s), s	4.1	16.2	16.2	3.6	16.6	16.7	6.5	15.5	5.7	14.5	18.6	5.7
Cycle Q Clear(g_c), s	4.1	16.2	16.2	3.6	16.6	16.7	6.5	15.5	5.7	14.5	18.6	5.7
Prop In Lane	1.00		0.12	1.00		0.25	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	377	765	789	332	683	688	233	261	230	318	398	351
V/C Ratio(X)	0.29	0.44	0.45	0.26	0.45	0.46	0.46	0.87	0.35	0.79	0.74	0.26
Avail Cap(c_a), veh/h	393	765	789	461	683	688	233	374	329	337	534	471
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	18.8	23.9	24.0	18.9	24.4	24.5	39.7	49.8	45.6	35.4	43.4	38.4
Incr Delay (d2), s/veh	0.4	1.9	1.8	0.4	2.2	2.2	1.4	14.7	0.9	11.7	3.8	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	2.9	11.1	11.4	2.3	10.5	10.7	4.7	12.1	3.9	11.1	13.1	3.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	19.3	25.8	25.8	19.3	26.6	26.6	41.1	64.5	46.5	47.1	47.2	38.8
LnGrp LOS	В	С	С	В	С	С	D	Е	D	D	D	<u>D</u>
Approach Vol, veh/h		799			710			414			638	
Approach Delay, s/veh		24.9			25.7			55.0			46.0	
Approach LOS		С			С			Е			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	12.3	58.9	15.0	33.8	12.9	58.3	23.6	25.2				
Change Period (Y+Rc), s	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0				
Max Green Setting (Gmax), s	15.0	33.0	8.0	36.0	7.0	41.0	18.0	26.0				
Max Q Clear Time (g_c+l1), s	5.6	18.2	8.5	20.6	6.1	18.7	16.5	17.5				
Green Ext Time (p_c), s	0.1	2.1	0.0	1.1	0.0	2.1	0.1	0.6				
Intersection Summary												
HCM 6th Ctrl Delay			35.3									
HCM 6th LOS			D									
			_									

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Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	1	<b>1</b>		7	<b>1</b>		7	<b>↑</b>	7	7	<b>↑</b>	7
Traffic Volume (veh/h)	163	687	93	164	577	140	65	391	81	107	278	90
Future Volume (veh/h)	163	687	93	164	577	140	65	391	81	107	278	90
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1864	1864	1864	1683	1683	1683	1724	1724	1793	1780	1780	1852
Adj Flow Rate, veh/h	170	716	97	171	601	146	68	407	84	111	290	94
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	6	6	6	2	2	2	5	5	5	4	4	4
Cap, veh/h	323	1147	155	302	957	232	252	436	385	187	488	430
Arrive On Green	0.08	0.37	0.37	0.08	0.38	0.38	0.04	0.25	0.25	0.06	0.27	0.27
Sat Flow, veh/h	1775	3134	424	1603	2551	618	1642	1724	1520	1696	1780	1569
Grp Volume(v), veh/h	170	404	409	171	376	371	68	407	84	111	290	94
Grp Sat Flow(s),veh/h/ln	1775	1771	1787	1603	1599	1571	1642	1724	1520	1696	1780	1569
Q Serve(g_s), s	7.1	22.5	22.5	7.9	23.1	23.2	3.7	27.7	5.2	5.7	16.9	5.6
Cycle Q Clear(g_c), s	7.1	22.5	22.5	7.9	23.1	23.2	3.7	27.7	5.2	5.7	16.9	5.6
Prop In Lane	1.00		0.24	1.00		0.39	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	323	648	654	302	600	590	252	436	385	187	488	430
V/C Ratio(X)	0.53	0.62	0.62	0.57	0.63	0.63	0.27	0.93	0.22	0.59	0.59	0.22
Avail Cap(c_a), veh/h	323	648	654	381	600	590	278	474	418	220	534	471
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	23.4	31.3	31.3	23.3	30.6	30.7	32.0	43.8	35.4	33.6	37.8	33.6
Incr Delay (d2), s/veh	1.6	4.5	4.5	1.7	4.9	5.0	0.6	24.6	0.3	3.1	1.5	0.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	5.5	15.5	15.6	5.5	14.6	14.5	2.7	21.1	3.6	4.6	12.2	3.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	25.0	35.7	35.7	24.9	35.5	35.7	32.6	68.4	35.7	36.7	39.3	33.9
LnGrp LOS	С	D	D	С	D	D	С	E	D	D	D	<u>C</u>
Approach Vol, veh/h		983			918			559			495	
Approach Delay, s/veh		33.9			33.6			59.1			37.7	
Approach LOS		С			С			Е			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.1	50.9	12.1	39.9	16.0	52.0	14.6	37.4				
Change Period (Y+Rc), s	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0				
Max Green Setting (Gmax), s	16.0	33.0	7.0	36.0	9.0	40.0	10.0	33.0				
Max Q Clear Time (g_c+l1), s	9.9	24.5	5.7	18.9	9.1	25.2	7.7	29.7				
Green Ext Time (p_c), s	0.3	2.1	0.0	1.3	0.0	2.5	0.1	0.7				
Intersection Summary												
HCM 6th Ctrl Delay			39.2									
HCM 6th LOS			D									

### Existing Saturday 3: Butler Avenue & County Line Road

	٠	<b>→</b>	*	•	•	•	4	<b>†</b>	~	/	Ţ	4
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	1	<b>1</b>		7	<b>1</b>		7	<b>^</b>	7	7	<b>↑</b>	7
Traffic Volume (veh/h)	119	493	62	131	488	89	82	361	85	144	328	59
Future Volume (veh/h)	119	493	62	131	488	89	82	361	85	144	328	59
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1921	1921	1921	1683	1683	1683	1780	1780	1852	1823	1823	1896
Adj Flow Rate, veh/h	129	536	67	142	530	97	89	392	92	157	357	64
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	1	1	1	1	1	1
Cap, veh/h	348	1149	143	347	978	178	238	418	368	231	486	429
Arrive On Green	0.06	0.35	0.35	0.07	0.36	0.36	0.05	0.23	0.23	0.09	0.27	0.27
Sat Flow, veh/h	1829	3265	407	1603	2700	492	1696	1780	1569	1736	1823	1607
Grp Volume(v), veh/h	129	299	304	142	313	314	89	392	92	157	357	64
Grp Sat Flow(s),veh/h/ln	1829	1825	1847	1603	1599	1594	1696	1780	1569	1736	1823	1607
Q Serve(g_s), s	4.9	14.0	14.1	6.2	17.1	17.2	4.3	23.8	5.2	7.4	19.6	3.3
Cycle Q Clear(g_c), s	4.9	14.0	14.1	6.2	17.1	17.2	4.3	23.8	5.2	7.4	19.6	3.3
Prop In Lane	1.00		0.22	1.00		0.31	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	348	642	650	347	579	577	238	418	368	231	486	429
V/C Ratio(X)	0.37	0.47	0.47	0.41	0.54	0.54	0.37	0.94	0.25	0.68	0.73	0.15
Avail Cap(c_a), veh/h	417	642	650	347	579	577	270	421	371	271	497	438
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	21.7	27.6	27.7	21.1	27.8	27.9	30.7	41.3	34.2	30.8	36.8	30.8
Incr Delay (d2), s/veh	0.7	2.4	2.4	0.8	3.6	3.7	1.0	28.7	0.4	5.4	5.5	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	3.8	10.5	10.7	4.2	11.3	11.3	3.3	19.8	3.7	6.2	14.6	2.4
Unsig. Movement Delay, s/veh		10.0	10.1	1.2	11.0	11.0	0.0	10.0	0.1	0.2	11.0	
LnGrp Delay(d),s/veh	22.3	30.0	30.1	21.9	31.4	31.5	31.6	70.0	34.6	36.2	42.2	31.0
LnGrp LOS	C	C	C	C	С	C	C	E	C	D	D	С
Approach Vol, veh/h		732			769			573			578	
Approach Delay, s/veh		28.7			29.7			58.3			39.3	
Approach LOS		20.7 C			C C			50.5 E			D D	
											U	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	15.0	45.7	12.9	36.3	13.9	46.8	16.5	32.8				
Change Period (Y+Rc), s	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0				
Max Green Setting (Gmax), s	8.0	36.0	8.0	30.0	11.0	33.0	12.0	26.0				
Max Q Clear Time (g_c+I1), s	8.2	16.1	6.3	21.6	6.9	19.2	9.4	25.8				
Green Ext Time (p_c), s	0.0	2.0	0.0	1.1	0.1	2.0	0.1	0.1				
Intersection Summary												
HCM 6th Ctrl Delay			37.7									
HCM 6th LOS			D									
Notes												

User approved pedestrian interval to be less than phase max green.

### Pre-development AM 3: Butler Avenue & County Line Road

e. Batter / Worldo & C												<u> </u>
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Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	1	<b>*</b> 1>		7	<b>1</b>		*	<b>^</b>	7	7	<b>^</b>	7
Traffic Volume (veh/h)	104	626	39	82	527	75	102	220	77	243	285	86
Future Volume (veh/h)	104	626	39	82	527	75	102	220	77	243	285	86
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1864	1864	1864	1683	1683	1683	1724	1724	1793	1780	1780	1852
Adj Flow Rate, veh/h	108	652	41	85	549	78	106	229	80	253	297	90
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	6	6	6	2	2	2	5	5	5	4	4	4
Cap, veh/h	376	1459	92	331	1198	170	233	262	231	318	399	352
Arrive On Green	0.05	0.43	0.43	0.04	0.43	0.43	0.07	0.15	0.15	0.14	0.22	0.22
Sat Flow, veh/h	1775	3383	213	1603	2811	398	1642	1724	1520	1696	1780	1569
Grp Volume(v), veh/h	108	341	352	85	311	316	106	229	80	253	297	90
Grp Sat Flow(s),veh/h/ln	1775	1771	1825	1603	1599	1611	1642	1724	1520	1696	1780	1569
Q Serve(g_s), s	4.1	16.3	16.3	3.6	16.7	16.8	6.5	15.6	5.7	14.6	18.6	5.7
Cycle Q Clear(g_c), s	4.1	16.3	16.3	3.6	16.7	16.8	6.5	15.6	5.7	14.6	18.6	5.7
Prop In Lane	1.00		0.12	1.00		0.25	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	376	763	787	331	681	687	233	262	231	318	399	352
V/C Ratio(X)	0.29	0.45	0.45	0.26	0.46	0.46	0.46	0.87	0.35	0.79	0.74	0.26
Avail Cap(c_a), veh/h	391	763	787	460	681	687	233	374	329	337	534	471
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	18.9	24.0	24.1	18.9	24.5	24.6	39.6	49.8	45.5	35.4	43.3	38.3
Incr Delay (d2), s/veh	0.4	1.9	1.8	0.4	2.2	2.2	1.4	14.8	0.9	11.8	3.8	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	3.0	11.2	11.5	2.3	10.6	10.7	4.7	12.1	3.9	11.1	13.1	3.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	19.3	25.9	25.9	19.3	26.7	26.8	41.0	64.6	46.4	47.2	47.2	38.7
LnGrp LOS	В	С	С	В	С	С	D	Е	D	D	D	<u>D</u>
Approach Vol, veh/h		801			712			415			640	
Approach Delay, s/veh		25.0			25.9			55.1			46.0	
Approach LOS		С			С			Е			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	12.3	58.7	15.0	33.9	12.9	58.1	23.7	25.2				
Change Period (Y+Rc), s	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0				
Max Green Setting (Gmax), s	15.0	33.0	8.0	36.0	7.0	41.0	18.0	26.0				
Max Q Clear Time (g_c+l1), s	5.6	18.3	8.5	20.6	6.1	18.8	16.6	17.6				
Green Ext Time (p_c), s	0.1	2.1	0.0	1.1	0.0	2.1	0.1	0.6				
Intersection Summary												
HCM 6th Ctrl Delay			35.3									
HCM 6th LOS			D									

### Pre-development PM 3: Butler Avenue & County Line Road

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Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	7	<b>↑</b> ↑		7	<b>1</b>		1	<b>^</b>	7	×	<b>†</b>	7
Traffic Volume (veh/h)	164	689	93	165	579	141	65	392	81	107	279	90
Future Volume (veh/h)	164	689	93	165	579	141	65	392	81	107	279	90
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1864	1864	1864	1683	1683	1683	1724	1724	1793	1780	1780	1852
Adj Flow Rate, veh/h	171	718	97	172	603	147	68	408	84	111	291	94
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	6	6	6	2	2	2	5	5	5	4	4	4
Cap, veh/h	321	1144	155	302	955	232	252	437	385	187	489	431
Arrive On Green	0.08	0.37	0.37	0.08	0.37	0.37	0.04	0.25	0.25	0.06	0.27	0.27
Sat Flow, veh/h	1775	3135	423	1603	2549	620	1642	1724	1520	1696	1780	1569
Grp Volume(v), veh/h	171	405	410	172	378	372	68	408	84	111	291	94
Grp Sat Flow(s),veh/h/ln	1775	1771	1788	1603	1599	1571	1642	1724	1520	1696	1780	1569
Q Serve(g_s), s	7.2	22.6	22.7	7.9	23.2	23.3	3.7	27.8	5.2	5.7	17.0	5.5
Cycle Q Clear(g_c), s	7.2	22.6	22.7	7.9	23.2	23.3	3.7	27.8	5.2	5.7	17.0	5.5
Prop In Lane	1.00		0.24	1.00		0.39	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	321	646	653	302	599	589	252	437	385	187	489	431
V/C Ratio(X)	0.53	0.63	0.63	0.57	0.63	0.63	0.27	0.93	0.22	0.59	0.60	0.22
Avail Cap(c_a), veh/h	321	646	653	380	599	589	278	474	418	220	534	471
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	23.5	31.4	31.4	23.3	30.7	30.7	32.0	43.8	35.4	33.6	37.7	33.6
Incr Delay (d2), s/veh	1.7	4.6	4.5	1.7	5.0	5.1	0.6	24.7	0.3	3.1	1.5	0.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	5.5	15.6	15.7	5.5	14.7	14.5	2.7	21.2	3.6	4.6	12.2	3.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	25.2	35.9	35.9	25.0	35.7	35.9	32.5	68.5	35.7	36.7	39.3	33.8
LnGrp LOS	С	D	D	С	D	D	С	E	D	D	D	<u>C</u>
Approach Vol, veh/h		986			922			560			496	
Approach Delay, s/veh		34.1			33.8			59.2			37.7	
Approach LOS		С			С			Е			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.2	50.8	12.1	40.0	16.0	52.0	14.6	37.4				
Change Period (Y+Rc), s	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0				
Max Green Setting (Gmax), s	16.0	33.0	7.0	36.0	9.0	40.0	10.0	33.0				
Max Q Clear Time (g_c+l1), s	9.9	24.7	5.7	19.0	9.2	25.3	7.7	29.8				
Green Ext Time (p_c), s	0.3	2.1	0.0	1.3	0.0	2.5	0.1	0.7				
Intersection Summary												
HCM 6th Ctrl Delay			39.3									
HCM 6th LOS			D									

### Pre-development Saturday 3: Butler Avenue & County Line Road

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Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	1	<b>1</b>		7	<b>1</b>		7	<b>^</b>	7	7	<b>↑</b>	7
Traffic Volume (veh/h)	119	494	62	131	489	89	82	362	85	144	329	59
Future Volume (veh/h)	119	494	62	131	489	89	82	362	85	144	329	59
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1921	1921	1921	1683	1683	1683	1780	1780	1852	1823	1823	1896
Adj Flow Rate, veh/h	129	537	67	142	532	97	89	393	92	157	358	64
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	1	1	1	1	1	1
Cap, veh/h	347	1148	143	346	978	178	238	419	369	231	487	429
Arrive On Green	0.06	0.35	0.35	0.07	0.36	0.36	0.05	0.24	0.24	0.09	0.27	0.27
Sat Flow, veh/h	1829	3266	406	1603	2702	491	1696	1780	1569	1736	1823	1607
Grp Volume(v), veh/h	129	299	305	142	314	315	89	393	92	157	358	64
Grp Sat Flow(s),veh/h/ln	1829	1825	1847	1603	1599	1594	1696	1780	1569	1736	1823	1607
Q Serve(g_s), s	4.9	14.0	14.1	6.2	17.2	17.3	4.3	23.8	5.2	7.4	19.7	3.3
Cycle Q Clear(g_c), s	4.9	14.0	14.1	6.2	17.2	17.3	4.3	23.8	5.2	7.4	19.7	3.3
Prop In Lane	1.00		0.22	1.00		0.31	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	347	641	649	346	578	577	238	419	369	231	487	429
V/C Ratio(X)	0.37	0.47	0.47	0.41	0.54	0.55	0.37	0.94	0.25	0.68	0.74	0.15
Avail Cap(c_a), veh/h	416	641	649	346	578	577	270	421	371	271	497	438
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	21.7	27.7	27.7	21.1	27.9	27.9	30.6	41.3	34.2	30.8	36.8	30.8
Incr Delay (d2), s/veh	0.7	2.4	2.4	0.8	3.6	3.7	1.0	28.8	0.3	5.4	5.5	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	3.8	10.6	10.7	4.2	11.3	11.4	3.3	19.9	3.7	6.2	14.6	2.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	22.4	30.1	30.1	21.9	31.5	31.6	31.6	70.1	34.5	36.2	42.3	30.9
LnGrp LOS	С	С	С	C	С	С	С	Е	С	D	D	С
Approach Vol, veh/h		733			771			574			579	
Approach Delay, s/veh		28.7			29.8			58.4			39.4	
Approach LOS		C			C			E			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	15.0	45.7	12.9	36.4	13.9	46.8	16.5	32.9				
Change Period (Y+Rc), s	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0				
Max Green Setting (Gmax), s	8.0	36.0	8.0	30.0	11.0	33.0	12.0	26.0				
Max Q Clear Time (g_c+I1), s	8.2	16.1	6.3	21.7	6.9	19.3	9.4	25.8				
Green Ext Time (p_c), s	0.0	2.1	0.0	1.0	0.1	2.0	0.1	0.0				
Intersection Summary												
HCM 6th Ctrl Delay			37.8									
HCM 6th LOS			D									
Notes												

User approved pedestrian interval to be less than phase max green.

### Post-development AM 3: Butler Avenue & County Line Road

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Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	7	<b>1</b>		1	<b>1</b>		1	<b>^</b>	7	Y	<b>†</b>	7
Traffic Volume (veh/h)	104	626	39	82	539	75	110	220	77	243	285	94
Future Volume (veh/h)	104	626	39	82	539	75	110	220	77	243	285	94
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1864	1864	1864	1683	1683	1683	1724	1724	1793	1780	1780	1852
Adj Flow Rate, veh/h	108	652	41	85	561	78	115	229	80	253	297	98
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	6	6	6	2	2	2	5	5	5	4	4	4
Cap, veh/h	371	1459	92	331	1202	167	232	262	231	318	399	352
Arrive On Green	0.05	0.43	0.43	0.04	0.43	0.43	0.07	0.15	0.15	0.14	0.22	0.22
Sat Flow, veh/h	1775	3383	213	1603	2820	391	1642	1724	1520	1696	1780	1569
Grp Volume(v), veh/h	108	341	352	85	317	322	115	229	80	253	297	98
Grp Sat Flow(s),veh/h/ln	1775	1771	1825	1603	1599	1612	1642	1724	1520	1696	1780	1569
Q Serve(g_s), s	4.1	16.3	16.3	3.6	17.1	17.2	7.1	15.6	5.7	14.6	18.6	6.2
Cycle Q Clear(g_c), s	4.1	16.3	16.3	3.6	17.1	17.2	7.1	15.6	5.7	14.6	18.6	6.2
Prop In Lane	1.00		0.12	1.00		0.24	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	371	763	787	331	681	687	232	262	231	318	399	352
V/C Ratio(X)	0.29	0.45	0.45	0.26	0.47	0.47	0.49	0.87	0.35	0.79	0.74	0.28
Avail Cap(c_a), veh/h	386	763	787	460	681	687	232	374	329	337	534	471
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	19.0	24.0	24.1	18.9	24.6	24.7	39.9	49.8	45.5	35.4	43.3	38.5
Incr Delay (d2), s/veh	0.4	1.9	1.8	0.4	2.3	2.3	1.6	14.8	0.9	11.8	3.8	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	3.0	11.2	11.5	2.3	10.8	10.9	5.2	12.1	3.9	11.1	13.1	4.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	19.4	25.9	25.9	19.3	26.9	27.0	41.5	64.6	46.4	47.2	47.2	38.9
LnGrp LOS	В	С	С	В	С	С	D	E	D	D	D	<u>D</u>
Approach Vol, veh/h		801			724			424			648	
Approach Delay, s/veh		25.0			26.1			54.9			45.9	
Approach LOS		С			С			D			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	12.3	58.7	15.0	33.9	12.9	58.1	23.7	25.2				
Change Period (Y+Rc), s	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0				
Max Green Setting (Gmax), s	15.0	33.0	8.0	36.0	7.0	41.0	18.0	26.0				
Max Q Clear Time (g_c+l1), s	5.6	18.3	9.1	20.6	6.1	19.2	16.6	17.6				
Green Ext Time (p_c), s	0.1	2.1	0.0	1.2	0.0	2.1	0.1	0.6				
Intersection Summary												
HCM 6th Ctrl Delay			35.4									
HCM 6th LOS			D									

Intersection						
Int Delay, s/veh	0.4					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		<b>^</b>	<b>1</b>			7
Traffic Vol, veh/h	12	770	716	27	0	39
Future Vol, veh/h	12	770	716	27	0	39
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	0
Veh in Median Storage,	# -	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	4	4	2	2	0	0
Mvmt Flow	13	837	778	29	0	42
	lajor1		Major2		/linor2	
Conflicting Flow All	807	0	-	0	-	404
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	4.3	-	-	-	-	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	3	-	_	-	_	3.1
Pot Cap-1 Maneuver	629	-	_	-	0	634
Stage 1	-	_	_	_	0	-
Stage 2	_	_	-	_	0	-
Platoon blocked, %		_	_	<u>-</u>		
Mov Cap-1 Maneuver	629			_	_	634
Mov Cap-1 Maneuver	029			-		- 004
		-	-	-	-	
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Approach	EB		WB		SB	
HCM Control Delay, s	0.2		0		11.1	
HCM LOS	0.2		- 0		В	
110W EOO					J	
Minor Lane/Major Mvmt		EBL	EBT	WBT	WBR :	SBLn1
Capacity (veh/h)		629	-	-	-	634
HCM Lane V/C Ratio		0.021	-	-	-	0.067
HCM Control Delay (s)		10.8	-	-		11.1
HCM Lane LOS		В	-	-	-	В
HCM 95th %tile Q(veh)		0.1	_	-	_	0.2
Julio Q(Voil)		V. I				V.2

### Post-development PM 3: Butler Avenue & County Line Road

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Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	7	<b>1</b>		1	<b>1</b>		1	<b>^</b>	7	Y	<b>†</b>	7
Traffic Volume (veh/h)	164	689	93	165	591	141	73	392	81	107	279	94
Future Volume (veh/h)	164	689	93	165	591	141	73	392	81	107	279	94
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1864	1864	1864	1683	1683	1683	1724	1724	1793	1780	1780	1852
Adj Flow Rate, veh/h	171	718	97	172	616	147	76	408	84	111	291	98
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	6	6	6	2	2	2	5	5	5	4	4	4
Cap, veh/h	317	1144	155	302	960	229	254	437	385	187	481	424
Arrive On Green	0.08	0.37	0.37	0.08	0.37	0.37	0.05	0.25	0.25	0.06	0.27	0.27
Sat Flow, veh/h	1775	3135	423	1603	2561	610	1642	1724	1520	1696	1780	1569
Grp Volume(v), veh/h	171	405	410	172	384	379	76	408	84	111	291	98
Grp Sat Flow(s),veh/h/ln	1775	1771	1788	1603	1599	1573	1642	1724	1520	1696	1780	1569
Q Serve(g_s), s	7.2	22.6	22.7	7.9	23.7	23.8	4.1	27.8	5.2	5.7	17.1	5.8
Cycle Q Clear(g_c), s	7.2	22.6	22.7	7.9	23.7	23.8	4.1	27.8	5.2	5.7	17.1	5.8
Prop In Lane	1.00		0.24	1.00		0.39	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	317	646	653	302	599	589	254	437	385	187	481	424
V/C Ratio(X)	0.54	0.63	0.63	0.57	0.64	0.64	0.30	0.93	0.22	0.59	0.60	0.23
Avail Cap(c_a), veh/h	317	646	653	380	599	589	273	474	418	220	534	471
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	23.6	31.4	31.4	23.3	30.9	30.9	31.8	43.8	35.4	33.6	38.2	34.1
Incr Delay (d2), s/veh	1.8	4.6	4.5	1.7	5.2	5.3	0.7	24.7	0.3	3.1	1.6	0.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	5.6	15.6	15.7	5.5	15.0	14.8	3.0	21.2	3.6	4.6	12.3	4.1
Unsig. Movement Delay, s/veh	l											
LnGrp Delay(d),s/veh	25.5	35.9	35.9	25.0	36.1	36.2	32.5	68.5	35.7	36.7	39.8	34.4
LnGrp LOS	С	D	D	С	D	D	С	E	D	D	D	<u>C</u>
Approach Vol, veh/h		986			935			568			500	
Approach Delay, s/veh		34.1			34.1			58.8			38.1	
Approach LOS		С			С			Е			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.2	50.8	12.6	39.4	16.0	52.0	14.6	37.4				
Change Period (Y+Rc), s	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0				
Max Green Setting (Gmax), s	16.0	33.0	7.0	36.0	9.0	40.0	10.0	33.0				
Max Q Clear Time (g_c+l1), s	9.9	24.7	6.1	19.1	9.2	25.8	7.7	29.8				
Green Ext Time (p_c), s	0.3	2.1	0.0	1.3	0.0	2.5	0.1	0.7				
Intersection Summary												
HCM 6th Ctrl Delay			39.5									
HCM 6th LOS			D									

Lane Configurations Traffic Vol, veh/h Traffic Vol,	WBR SBL  27 0 27 0 0 0 Free Stop None	39 0
Movement EBL EBT WBT  Lane Configurations Traffic Vol, veh/h 12 946 735  Future Vol, veh/h 12 946 735  Conflicting Peds, #/hr Sign Control Free Free Free  RT Channelized Storage Length  PBL EBT WBT  WBT  FF 946 735  Free Free Free Free Free Free Free Fre	27 0 27 0 0 0 Free Stop	39 39 39 0
Lane Configurations Traffic Vol, veh/h Traffic Vol,	27 0 27 0 0 0 Free Stop	39 39 39 0
Traffic Vol, veh/h 12 946 735 Future Vol, veh/h 12 946 735 Conflicting Peds, #/hr 0 0 0 Sign Control Free Free Free RT Channelized - None - Storage Length 0 -	27 0 0 0 Free Stop	39 39 0
Future Vol, veh/h Conflicting Peds, #/hr Sign Control RT Channelized Storage Length  12 946 735 0 0 0 0 Free Free Free Free Free Free - None	27 0 0 0 Free Stop	39 0
Conflicting Peds, #/hr 0 0 0 Sign Control Free Free Free RT Channelized - None - Storage Length 0 -	0 0 Free Stop	0
Sign Control Free Free Free RT Channelized - None - Storage Length 0	Free Stop	
RT Channelized - None - Storage Length 0		Cton
Storage Length 0	None -	
3 3		
Vah in Madian Ctarage # 0 0		0
	- 0	
Grade, % - 0 0	- 0	
Peak Hour Factor 92 92 92	92 92	92
Heavy Vehicles, % 2 2 4	4 0	0
Mvmt Flow 13 1028 799	29 0	42
Major/Minor Major4 Major0	Minaro	
Major/Minor Major1 Major2	Minor2	
Conflicting Flow All 828 0 -	0 -	
Stage 1		-
Stage 2		-
Critical Hdwy 4.3		6.9
Critical Hdwy Stg 1		-
Critical Hdwy Stg 2		
Follow-up Hdwy 3		• • • •
Pot Cap-1 Maneuver 618	- 0	624
Stage 1	- 0	-
Stage 2	- 0	-
Platoon blocked, %	-	
Mov Cap-1 Maneuver 618		624
Mov Cap-2 Maneuver		
Stage 1		_
Stage 2		_
Jiago L		
Approach EB WB	SB	
HCM Control Delay, s 0.1 0	11.2	
HCM Control Delay, s 0.1 0	11.2	
HCM Control Delay, s 0.1 0 HCM LOS	11.2 B	
HCM Control Delay, s 0.1 0 HCM LOS  Minor Lane/Major Mvmt EBL EBT	11.2 B	SBLn1
HCM Control Delay, s 0.1 0 HCM LOS  Minor Lane/Major Mvmt EBL EBT Capacity (veh/h) 618 -	11.2 B WBT WBR	SBLn1 624
HCM Control Delay, s 0.1 0 HCM LOS  Minor Lane/Major Mvmt EBL EBT Capacity (veh/h) 618 - HCM Lane V/C Ratio 0.021 -	11.2 B WBT WBR 	SBLn1 624 0.068
HCM Control Delay, s 0.1 0 HCM LOS  Minor Lane/Major Mvmt EBL EBT Capacity (veh/h) 618 - HCM Lane V/C Ratio 0.021 - HCM Control Delay (s) 11 -	11.2 B WBT WBR  	SBLn1 624 0.068 11.2
HCM Control Delay, s	11.2 B WBT WBR 	SBLn1 624 0.068 11.2 B

### Post-development Saturday 3: Butler Avenue & County Line Road

	٠	<b>→</b>	*	•	•	•	4	<b>†</b>	~	/	Ţ	4
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	1	<b>1</b>		7	<b>1</b>		7	<b>^</b>	7	7	<b>↑</b>	7
Traffic Volume (veh/h)	119	494	62	131	495	89	86	362	85	144	329	63
Future Volume (veh/h)	119	494	62	131	495	89	86	362	85	144	329	63
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1921	1921	1921	1683	1683	1683	1780	1780	1852	1823	1823	1896
Adj Flow Rate, veh/h	129	537	67	142	538	97	93	393	92	157	358	68
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	1	1	1	1	1	1
Cap, veh/h	345	1148	143	346	979	176	239	419	369	231	483	426
Arrive On Green	0.06	0.35	0.35	0.07	0.36	0.36	0.06	0.24	0.24	0.09	0.27	0.27
Sat Flow, veh/h	1829	3266	406	1603	2707	486	1696	1780	1569	1736	1823	1607
Grp Volume(v), veh/h	129	299	305	142	317	318	93	393	92	157	358	68
Grp Sat Flow(s),veh/h/ln	1829	1825	1847	1603	1599	1595	1696	1780	1569	1736	1823	1607
Q Serve(g_s), s	4.9	14.0	14.1	6.2	17.4	17.5	4.5	23.8	5.2	7.4	19.8	3.6
Cycle Q Clear(g_c), s	4.9	14.0	14.1	6.2	17.4	17.5	4.5	23.8	5.2	7.4	19.8	3.6
Prop In Lane	1.00		0.22	1.00		0.30	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	345	641	649	346	578	577	239	419	369	231	483	426
V/C Ratio(X)	0.37	0.47	0.47	0.41	0.55	0.55	0.39	0.94	0.25	0.68	0.74	0.16
Avail Cap(c_a), veh/h	413	641	649	346	578	577	267	421	371	271	497	438
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	21.8	27.7	27.7	21.1	27.9	28.0	30.6	41.3	34.2	30.8	37.0	31.0
Incr Delay (d2), s/veh	0.7	2.4	2.4	0.8	3.7	3.8	1.0	28.8	0.3	5.4	5.7	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	3.8	10.6	10.7	4.2	11.4	11.5	3.5	19.9	3.7	6.2	14.7	2.6
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	22.4	30.1	30.1	21.9	31.6	31.7	31.6	70.1	34.5	36.2	42.7	31.2
LnGrp LOS	С	С	С	C	С	С	С	Е	С	D	D	С
Approach Vol, veh/h		733			777			578			583	
Approach Delay, s/veh		28.8			29.9			58.2			39.6	
Approach LOS		C			C			E			D D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	15.0	45.7	13.2	36.2	13.9	46.8	16.5	32.9				
Change Period (Y+Rc), s	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0				
Max Green Setting (Gmax), s	8.0	36.0	8.0	30.0	11.0	33.0	12.0	26.0				
Max Q Clear Time (g_c+I1), s	8.2	16.1	6.5	21.8	6.9	19.5	9.4	25.8				
Green Ext Time (p_c), s	0.0	2.1	0.0	1.1	0.1	2.0	0.1	0.0				
Intersection Summary												
HCM 6th Ctrl Delay			37.8									
HCM 6th LOS			D									
Notes												

User approved pedestrian interval to be less than phase max green.

Intersection						
Int Delay, s/veh	0.2					
		<b>FDT</b>	WDT	WDD	CDI	CDD
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	ዃ	<b>^</b>	<b>1</b>	40	_	7
Traffic Vol, veh/h	6	676	631	13	0	22
Future Vol, veh/h	6	676	631	13	0	22
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	
Storage Length	0	-	-	-	-	0
Veh in Median Storage	e, # -	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	4	4	0	0
Mvmt Flow	7	735	686	14	0	24
	1	, 00	500	17		<b></b>
Major/Minor I	Major1	N	Major2	١	/linor2	
Conflicting Flow All	700	0	-	0	-	350
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	_	-	-
Critical Hdwy	4.3	_	_	_	_	6.9
Critical Hdwy Stg 1	-	-	-	_	_	-
Critical Hdwy Stg 2	_			_	_	_
Follow-up Hdwy	3			_	_	3.1
Pot Cap-1 Maneuver	686	_	<u>-</u>	_	0	688
	- 000	_		-	0	- 000
Stage 1	-	_	-			
Stage 2	-	-	-	-	0	-
Platoon blocked, %	000	-	-	-		000
Mov Cap-1 Maneuver	686	-	-	-	-	688
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	_	-	-	-	-	-
Annanah	ED		MD		O.D.	
Approach	EB		WB		SB	
HCM Control Delay, s	0.1		0		10.4	
HCM LOS					В	
Minor Lane/Major Mvm	t	EBL	EBT	WBT	WBR	QRI n1
			LDI	וטייי		
Capacity (veh/h)		686	-	-	-	688
HCM Caretral Dalay (a)		0.01	-	-		0.035
HCM Control Delay (s)		10.3	-	-	-	
HCM Lane LOS		В	-	-	-	В
HCM 95th %tile Q(veh)		0	-	-	-	0.1

Project: Spotless Brands 10/7/2024

**Development:** Car Wash

**Location:** New Britain Township, Bucks County

### **Car Wash Queuing**

### Highway Capacity Manual, equation 19-68: 95th Percetile Queue Lengths

 $Q_{95} = 900T[v_x/c_{m,x}-1+((v_x/c_{m,x}-1)^2+(3600/c_{m,x})(v_x/c_{m,x})/150T)^{0.5}](c_{m,x}/3600)$ 

= 95th percetile queue (veh)

		<u>Input</u>
$\mathbf{V}_{\mathbf{x}}$	= flow rate of movement x (ve/h)	39
	per ITE LU 948, PM Peak of Adjacaent Street	
C <sub>m,x</sub>	= capacity of movement x (ve/h)	100
Т	= analysis time period (h)	0.25

Q<sub>95</sub> **1.6 veh** 



### **NEW BRITAIN TOWNSHIP**

207 Park Avenue • Chalfont, PA 18914 • Telephone: (215) 822-1391

### SUBDIVISION & LAND DEVELOPMENT APPLICATION

1.	Date of Application:	April 11, 2025		TOWNSHIP U	SE ONLY
2.	Date of Plan or Revision:	March 21,	2025	Date Recieved:	
3.	Application for:	Land Deve	elopment	Payment: Check #:	
4.	Name of Subdivision or Land Developmen	Spotless C	arwash	Receipt #: Escrow Acc. #:	
5.	Location:	545 West	Butler Ave	LSCIOW ACC. #.	
3.	Tax Map Parcel #: 26-006-101-001	Total Acreage:	Gross	Base Site Area	a
7.	Net Buildable Site Area (from Section 240	0.625 AC			
3.	Zoning Requirements:  Zoning District M	inimum Lot Size	1AC	Maximum Density	N/A
	Front Yard 40 ft Si	de Yard	15 ft	Rear Yard	30 ft
€.	Number of Lots or Dwelling Units:	1		_	
10.	Equitable Owner of Record of Land:	Flagship Penr	nsylvania Propco L	LC	
Address:		2 Mid Ame	rica Plz, Ste 4	50	
		Oakbrook 7	errace, IL 6018	 81	
	Phone: 610-952-5100		spotlessbrands.co	om	
1.	Applicant:		s Owner		
	Address:			-	
	Phone:	E-mail:			
2.	Registered Engineer or Surveyor:	Van Cleef Eng	gineering Associat	es	
	Address:	501 Nort	h Main Stre	et	
		Doylesto	wn, PA 1890	)1	
	Phone: (215) 660-0268	-mail: smcGranahan@	vancleefengineering.c	om	
3.	Type of Water & Sewer:  Public Wa		te Water te Sewer		
4.	Proposed Use:	Car Wash	)		
⊺his is hat th	s to certify that I have read Article V of the Ne se accompanying plan meets the requirement by how by Signature of Applicant	w Britain Township s s of that Article to th	e best of my kno	Land Development Ordin wledge. egistered Engineer or Sur	

		SUBMISSION CI	EVELOPMENT HECKLIST			
Date o	f Application:	April 11, 2025				
	ision/Land Development Name:	Spotless Carwash				
	s of Property:	545 West Butler Ave				
	(s) Name:	Flagship Pennsylvania	a Propco LLC			
	ent(s) Name:	Flagship Pennsylvania	·			
	ap Parcel Number:	26-006-101-001				
	Sets for New Britain Township - Fol	ded to 8 1/2 v 11: /0 [	Full Sizo 5 11v17)			
<u> </u>						
_	NBT Board of Supervisors (Full Size) - 5 Cop		NBT Planning Commission (11x17) - 2 Copies			
<b>∀</b>	Township Engineer (Full Size) - 1 Copy	☑ □	NBT File (Full Size) - 2 Copies			
V	Fire Marshal (Full Size) - 1 Copy	<b>✓</b>	Digital Submission - Email or File Sharing Service			
	tion will be considered administratively incopies of all applicable forms/reports.  New Britain Township Subdivision and Land					
	New Britain Township Subdivision and Land					
	Filing Fee according to the most current Fee Escrow Fee according to the most current Fe					
	Contract for Professional Services Agreemen		Solution			
	Proof of Submission to Bucks County Planni					
	Proof of Submission to Bucks County Conse					
	Proof of Submission to Water Authority (Nort		), if applicable			
✓	Proof of Submission to Chalfont New Britain	Joint Sewer Authority OR (	Buck County Department of Health			
Ø	Stornwater Management Report (2 Copies) (	Digital Acceptable)				
	PADEP Sewage Facilities Planning Module	Application/Mailer approved	d by Sewer Authority or SEO (Original & 2 Copies			
<b>V</b>	Traffic Impact Study, if applicable (2 Copies)	(Digital Acceptable)				
	PADOT Highway Occupancy Permit Plan/Ap		opies)			
	Community Impact Assessment Report, if ap					
			Application (§ 22-401.8 General Procedures)			
	Township Road Opening Permit, if applicable		LIA)			
	waiver Request Letter with Justilication					

\*All fees or contributions in lieu of shall be payable to New Britain Township. All plan sets, applications and forms shall be submitted directly to New Britain Township.

\*\*The attached checklist is provided for the applicant as a guideline to assist in the submission process. All applicants must include these items or the application will be considered administratively incomplete and returned to the applicant.\*\*



## NEW BRITAIN TOWNSHIP SUBDIVISION & LAND DEVELOPMENT PROCEDURES INFORMATION FOR APPLICANTS

#### **Applications:**

All formal applications must be submitted to the Township Zoning Officer during regular office hours.

All required plans, fees and accompanying documentation must be submitted to the Township in accordance with the New Britain Township Plan Submission Checklist (attached). No application shall be considered complete without this information.

If the application is found to be incomplete, the Zoning Officer will advise the applicant, in writing, of the deficiencies within five (5) business days.

If the application is found to be complete, the application will be date-stamped and a letter will be forwarded to the applicant with the anticipated application processing dates and scheduled Planning Commission and Board of Supervisor's meeting dates. The application will then be distributed for review to the Township professional staff, boards, commissions and professional consultants.

If for any reason the Planning Commission does not meet within 30 days of the initial application filing approval date of the Township Zoning Officer, the Municipalities Planning Code (MPC) timetable for plan review processing will then automatically begin, unless waived by the applicant.

#### **Review Process:**

Upon receipt of the Township Engineer's review letter, it is recommended that the applicant conference with Township staff and/or the Township Engineer prior to the scheduled Planning Commission meeting when the Planning Commission would consider the application for a recommendation. During the conference, if it is determined that the application will require substantial revisions for compliance with Township ordinances, the Township staff and/or engineer will advise the applicant of an appropriate course of action for revising the application. An extension/waiver to the 90-day limit (attached to SALDO Application), as set forth by the MPC, may be necessary to allow sufficient time for the applicant to make necessary plan revisions and for Township staff and its engineer to review the revised application.

All revised applications must be resubmitted directly to the Township Zoning Officer. It shall be the responsibility of the applicant to submit the same number of revised plans to the Township as with the original application.

#### **Recommendations and Plan Decisions:**

The applicant is responsible for posting the property, notifying property owners within 1000ft (or adjoining owners for minor plans), and submitting an affidavit of mailing & the mailings content to the Township five days prior to the plans appearing before the Township Planning Commission (§ 22-401.8 General Procedures).

The Planning Commission in its review shall consider the comments, if submitted, of the Bucks County Planning Commission, Fire Marshal, Bucks County Conservation District and Township professional staff and consultants. At their anticipated monthly meeting, the Planning Commission may do any of the following:

- · Recommend that the applicant address plan deficiencies or unresolved planning issues with Township staff.
- Recommend to the Board of Supervisors that the plan be approved with conditions and specifically list such conditions.
- Recommend to the Board of Supervisors that the plan be denied and specifically list the reasons for such a recommendation.

The Board of Supervisors shall take official action on all applications, after receiving the Township engineer's report and the recommendation by the Planning Commission and within the 90-day time limit, unless the applicant grants a time extension/waiver. The action of the Board of Supervisors shall be in writing and communicated to the applicant by mail within five (5) days following the date of the official action.



#### **Record Plans:**

Final record plans to be recorded must be submitted *directly to the Township Engineer* for review and execution prior to approval by the Board of Supervisors. The plans must include four (4) paper copies of the record plan(s). All record plans must bear the original seal and signature and title of the professional engineer or land surveyor responsible for preparation of the plans and bear the notarized signature of the land owner(s). At the time of record plan submission, five (5) complete paper sets of construction drawings should be submitted to the Township Engineer to be stamped approved for construction.

No final record plans shall be recorded for any project unless all of the following have been satisfied:

- All conditions of final approval have been met (including water and sewer approvals/agreement) and approved by Township Solicitor and Engineer.
- All required outside agency approvals/permits have been obtained.
- All Township legal, engineering, planning and administrative costs relative to the plan submission have been paid in full.
- The required financial security and developer's agreements have been executed and the construction escrow has been posted with the Township.

The Township Solicitor will record all final record plans at the Bucks County Courthouse. The Township will notify the applicant when the copies of the recorded plans are available to be picked up at the Township Zoning Office.

### Contract for Professional Services Agreement and Professional Escrows:

The applicant shall be responsible for maintaining the original escrow balance relative to the escrow account established with the Township for the payment of Township incurred costs and fees on behalf of the applicant per the Contract for Professional Services Agreement. The Township shall forward an invoice to the applicant setting forth the amount deducted from said escrow account for payment of costs and fees. Within fourteen (14) days of the date of the invoice, the applicant shall remit the amount of the invoice to the Township, thereby bringing the balance of the escrow back to the original amount. In the event the applicant's escrow account balance falls below the original amount, the Township may direct its professionals and consultants to cease work on the applicant's submission until such time as the escrow account balance is restored to its original amount.

Any applicant who desires to meet with the Township's professionals or consultants prior to submitting a plan application shall execute a Contract for Professional Services Agreement with the Township and post an escrow amount to be determined therein to cover any costs incurred by the Township associated with the aforementioned consultations.

The applicant should also be advised that the Township has enacted a "Disclosure Ordinance No. 2001-09-01" which mandates that the seller of any lot divulge certain information to the purchaser of the lot prior to settlement.

\*These guidelines may be amended from time to time, as needed.



### NEW BRITAIN TOWNSHIP 90-DAY REVIEW PERIOD WAIVER

I/We, the applicant, understand that the time necessary for adequate review of this application and plans for compliance with current Zoning and Subdivision & Land Development Ordinances, including reviews of any revised plans, may exceed the 90-day review period stipulated under the Municipalities Planning Code.

In recognition of the above, I/we hereby waive the 90-day review period, with the understanding that I/we may revoke this waiver at any time in the future, upon 30-day written notice to the New Britain Township Zoning Officer.

Name of Subdivision/Land Development:	Spotless Carwash				
Tax Map Parcel Number(s):	26-006-101-001				
Signature of Applicant:	Date: 4/11/2025				
Signature of Applicant:	Date: 4/11/2025				



### Project #A25-0105

### **Details**

Project #: A25-0105 Status: Portal Submitted Parcel #: 26-006-101-001

Location: 545 W BUTLER AVE

**Project Type:** Subdivisions and Land Development

Project Use: Nonresidential - Major (land developments and 3-or-more-lot subdivisions)

**Project Date:** 

File Date: 2025-04-04

**Start Date:** 

**Expiration Date:** Renew Date:

**Expected Completion Date:** 

**Parent Project Number:** 

**Related Projects:** 

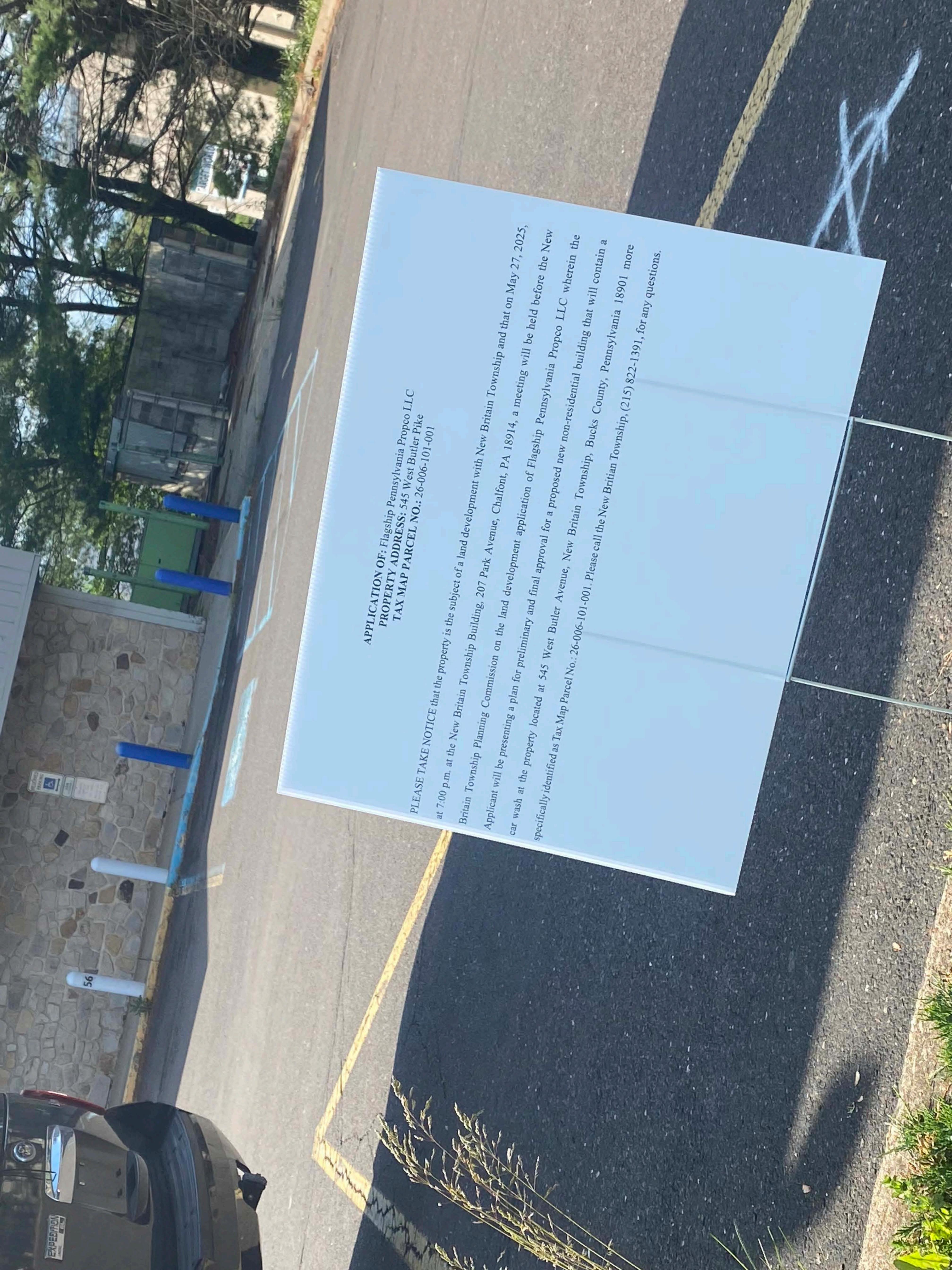
Project Number: Project Type: Project Use: File date: Status:

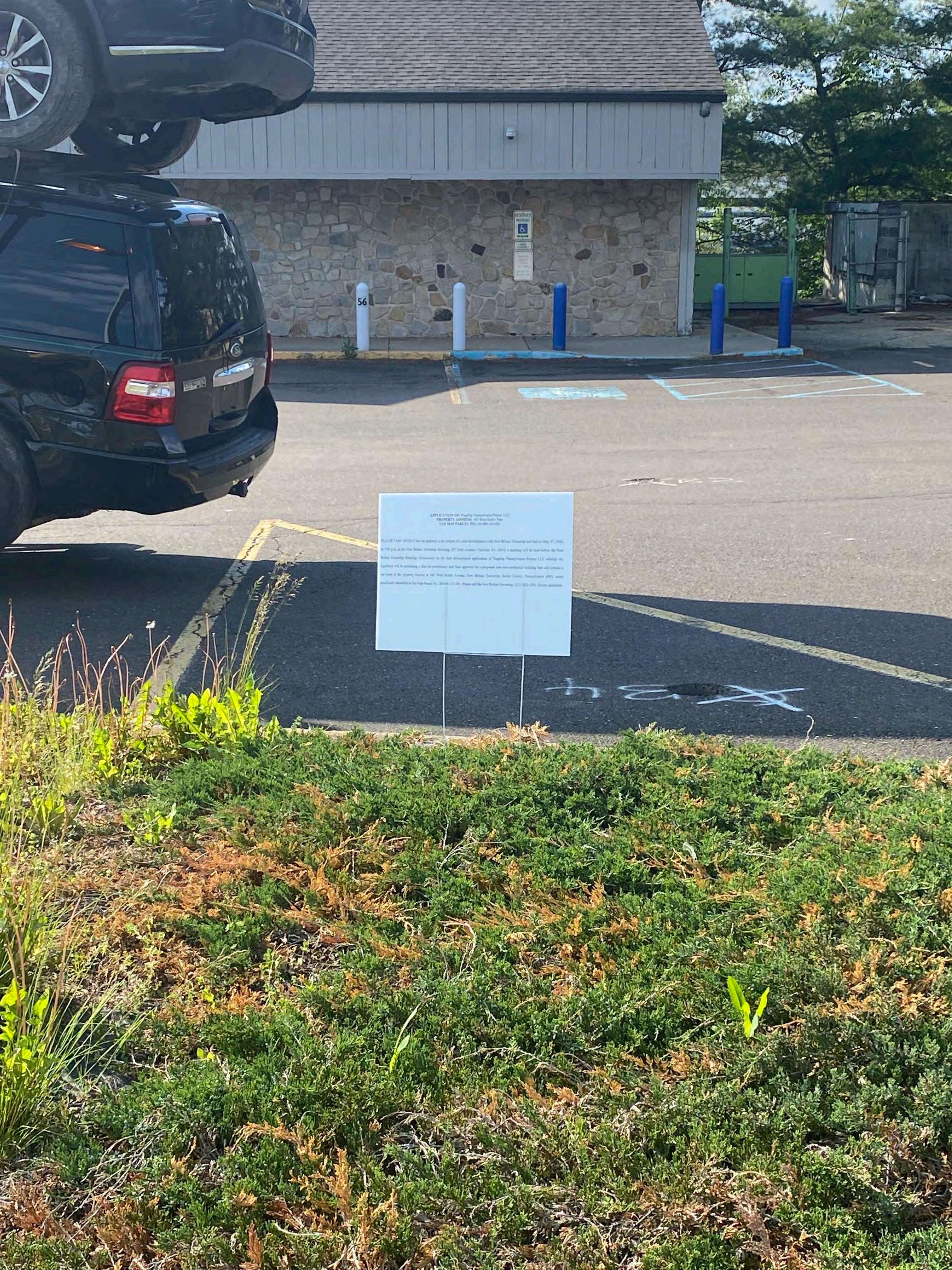
**Related Meetings:** 

Meeting: Type: Location: Status: Date/Time:

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## POST CONSTRUCTION STORMWATER CONVEYANCE CALCULATIONS

for

FLAGSHIP CARWASH VCEA No. 24-02-NBR

Situated in:

New Britain Township Bucks County, Pennsylvania

April 03, 2025

Prepared by:

Van Cleef Engineering Associates 501 North Main Street Doylestown, PA 18901

Sean David McGranahan, P.E. P.E. No. 085090



## **Post Construction Stormwater Management Flagship Carwash (VCEA No. 24-02-NBR)**

## **Applicant:**

Spotless Brands 545 West Butler Avenue Chalfont, PA 18914

## **General Site Description:**

Spotless Brands proposes to redevelop the existing parcel to include a car wash. The parcel is located at 545 West Butler Avenue in New Britain Township, Bucks County. The project will include the construction of a commercial building with, parking facilities, stormwater conveyance facilities, and other associated utilities. Pertinent data characterizing the existing and future site conditions are shown on the accompanying Land Development Plans.

## Pre-Development

The pre-development condition drains to an existing inlet located at the site entrance that directs traffic onto County Line Road. The site currently consists of a commercial building (no longer in use) parking lot and other amenities. The site has been in this condition for the past thirty years as indicated in aerial photographs available via the Penn Pilot website (www.pennpilot.psu.edu) and Google images.

## Post-Development

The post-development condition of the site maintains the same point of interest and relative drainage patterns. There is a decrease in the total impervious surface coverage with a subsequent decrease in runoff rates and volume. Therefore, no stormwater management storage facility has been designed.

## **Analysis of Impact:**

The pre-development condition of the development site consists of one (1) point of interest that flows offsite and ultimately converges downstream into the West Branch of the Neshaminy Creek. Stormwater flows from the southeast corner of the site to the northwest corner of the site to an existing inlet. Stormwater is then conveyed to the West Branch of the Neshaminy Creek (WWF, MF).

## Volume Control (Section 26-123)

The proposed condition for this site reduces the impervious coverage on site, therefore, these requirements are satisfied.

## Stormwater Peak Rate Control and Management Districts (Section 26-124)

The proposed condition for this site reduces the impervious coverage on site, therefore, these requirements are satisfied.

## **Storm Drainage System:**

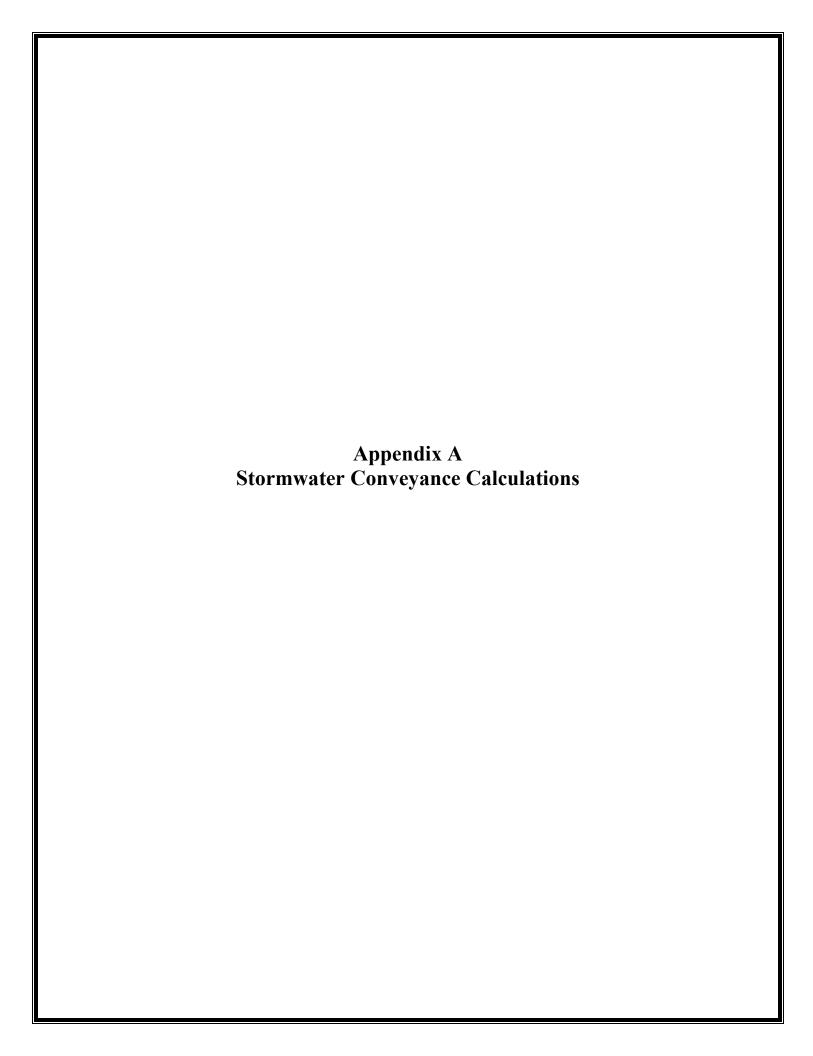
The storm drainage system has been designed to intercept runoff and convey stormwater to the existing inlet located along County Line Road. Hydraflow Storm Sewers Extension, version 10.4, has been utilized for designing the storm conveyance system. Conveyance design precipitation amounts are based on NOAA Altlas 14 precipitation data for the 100 year storm event. The Rational method was used in sizing all proposed storm sewers. The calculations and drainage areas have been provided on Drainage Area Plans included with this Report.

## **Soils Information:**

Soils data for the site was obtained from a Soil Survey of Bucks County, Pennsylvania issued September 26, 2023. Soils types found on the property are as follows:

UfuB – Urban Land, 0 to 8 percent slopes

There are no naturally occurring geologic formations (Karst Soils) that may cause pollution within the project limits of disturbance. The potential soil conditions that may cause pollution are listed in the soil limitations and resolutions in this report. The instructions for proper handling and/or disposal of all materials have been addressed within the Soils Resolution notes. Although there is no potential for stream pollution, inlet filter bags and silt socks have been provided downstream of earth disturbance to capture sediment runoff.





Project:	Spotless Carwash	By: SDM	Date:	3/14/2025
Location:	New Britain Township	Checked:	Date:	

## **RUNOFF C CALCULATIONS**

		NUNUE	F C CALC	<u>JLATIONS</u>	_	
			AREAS (SF)			
	LAND USE	Impervious	Pervious			
	HSG	С	С		TOTAL AREA	WEIGHTED
INLET#	RUNOFF C	0.99	0.25		(SF)	RUNOFF C
EX-I	INL-1	0	254		254	0.25
EX-II	NL-1B	6,463	1,310		7,773	0.87
EX-II	NL-1C	4,557	525		5,082	0.91
EX-II	VL-1D	3,615	0		3,615	0.99
IN	L-1	5,924	355		6,280	0.95
IN	L-2	2,569	449		3,018	0.88
IN	L-4	2,686	2,235		4,921	0.65
IN	L-5	2,549	2,135		4,684	0.65
IN	L-6	3,004	1,779		4,783	0.71
IN	L-7	1,656	119		1,775	0.94
IN	L-9	3,687	552		4,239	0.89



Project:	Spotless Carwash	By: SDM	<i>Date:</i> 3/:	14/2025
Location:	New Britain Township	Checked:	Date:	

## **RUNOFF C CALCULATIONS**

·		NONOI	r C CALC	JEA HONS	•	
			AREAS (AC)			
	LAND USE	Impervious	Pervious			
	HSG	С	С		TOTAL AREA	WEIGHTED
INLET#	RUNOFF C	0.99	0.25		(AC)	RUNOFF C
EX-I	INL-1	0.000	0.006		0.006	0.25
EX-II	NL-1B	0.148	0.030		0.178	0.87
EX-II	NL-1C	0.105	0.012		0.117	0.91
EX-II	NL-1D	0.083	0.000		0.083	0.99
IN	IL-1	0.136	0.008		0.144	0.95
IN	IL-2	0.059	0.010		0.069	0.88
IN	IL-4	0.062	0.051		0.113	0.65
IN	IL-5	0.059	0.049		0.108	0.65
IN	IL-6	0.069	0.041		0.110	0.71
IN	IL-7	0.038	0.003		0.041	0.94
IN	IL-9	0.085	0.013		0.097	0.89

## Date: 4/3/2025 Hydraflow Storm Sewers Extension for Autodesk® Civil 3D® Plan Number of lines: 11 Project File: SPOTLESS STORM PIPE.stm

# Storm Sewer Inventory Report

Cir 0.013 1.36 367.69
Cir 0.013 0.83
12 Cir
365.27 12 366.21 12
6.00 364.42 1.00 365.27 1.00 366.21
363.79 364.06 365.11
5.0 5.0 5.0
0.95
0.07
0.00
Grate Curb Curb MH Curb
49.831 G 62.280 C 30.039 C -36.261 M
27.000
27.000 6.000 16.000 74.000 35.000
E C C C C C C C C C C C C C C C C C C C
- 7 % 4 b 0

## Structure Report

5												
Struct	Structure ID	Junction	Rim Plev		Structure			Line Out			Line In	
5		246	(ft)	Shape	Length (ft)	Width (ft)	Size (in)	Shape	Invert (ft)	Size (in)	Shape	Invert (ft)
<del>-</del>	EX INL 1	Grate	367.69	Rect	4.00	2.00	12	Ö	364.06	12 12	نَ نَ	364.06 364.06
2	INL 1	Curb-Horiz	368.02	Rect	4.00	2.00	12	Öir	364.42	12	Öi	365.11
ო	INL 2	Curb-Horiz	368.75	Rect	4.00	2.00	12	Ċir	365.27	12	Ö	365.47
4	мнз	Manhole	371.20	Rect	4.00	2.00	12	ö	366.21	27	ວັ່ ວັ່	366.41 366.41
ς.	INL 4	Curb-Horiz	371.25	Rect	4.00	2.00	12	Öir	366.76	12	Öi	366.96
9	INL 5	Curb-Horiz	371.25	Rect	4.00	2.00	12	Ċir	368.04			
7	New	None	371.20	n/a	n/a	n/a	12	Ċir	366.84	12	Ö	366.84
∞	INL 6	Grate	371.20	Rect	4.00	2.00	12	Ċir	366.90			
თ	INL 7	Curb-Horiz	370.87	Rect	4.00	2.00	12	Çir	366.45	12	Ö	366.65
0	MH 8	Manhole	371.25	Rect	4.00	2.00	12	Öir	366.85	12	Ö	367.05
1	6 JNI	Curb-Horiz	370.56	Rect	4.00	2.00	12	ö	367.35			
Project	Project File: SPOTLESS STORM PIPE.stm	E.stm					Nu.	Number of Structures: 11	es: 11	Run	Run Date: 4/3/2025	

# Storm Sewer Summary Report

			5											
Line No.	Line ID	Flow rate (cfs)	Line Size (in)	Line shape	Line length (ft)	Invert EL Dn (ft)	Invert EL Up (ft)	Line Slope (%)	HGL Down (ft)	HGL (#)	Minor loss (ft)	HGL Junct (ft)	Dns Line No.	Junction Type
<b>-</b>	P - EX INL 1 TO EX MH	3.73	12	Cir	27.000	363.79	364.06	1.000	364.61	364.94	0.54	365.49	End	Grate
7	P - INL 1 TO EX INL 1	2.85	12	ö	000.9	364.06	364.42	000.9	365.49*	365.53*	0.17	365.70	<del>-</del>	Curb-Horiz
ო	P - INL 2 TO INL 1	1.95	12	ö	16.000	365.11	365.27	1.000	365.70	365.86	0.24	365.86	2	Curb-Horiz
4	P - MH 3 TO INL 2	1.55	12	ö	74.000	365.47	366.21	1.000	365.93	366.74	0.19	366.74	က	Manhole
5	P - INL 4 TO MH 3	1.02	12	Ċi	35.000	366.41	366.76	1.000	366.78	367.18	0.24	367.18	4	Curb-Horiz
9	P - INL 5 TO INL 4	0.56	12	Ċi	108.000	366.96	368.04	1.000	367.23	368.35	n/a	368.35	2	Curb-Horiz
7	P - INL 6 TO MH 3	0.62	12	Ċi	54.000	366.41	366.84	0.796	366.74	367.17	60.0	367.17	4	None
ω	P - INL 6 TO MH 3 (2)	0.62	12	Ċ	8.000	366.84	366.90	0.750	367.17	367.23	n/a	367.23	7	Grate
თ	P - INL 7 TO EX INL 1	96.0	12	Ċi	151.000	364.06	366.45	1.583	365.49	366.86	n/a	366.86 j	<del>-</del>	Curb-Horiz
10	P - MH 8 TO INL 7	0.67	12	Ċi	20.000	366.65	366.85	1.000	366.94	367.19	0.13	367.19	6	Manhole
7	P - INL 9 TO MH 8	69.0	12	Ö	30.000	367.05	367.35	1.000	367.35	367.70	0.13	367.70	10	Curb-Horiz
Project File:	File: SPOTLESS STORM PIPE.stm	ے							Number of lines: 11	lines: 11		Run D	Run Date: 4/3/2025	25
OTEN	. Potruga mintol - 100 Vrs	Owde IDI) posteder	300	ori I - i · 〈a	mii pyd saietaeo e							-		

NOTES: Return period = 100 Yrs.; \*Surcharged (HGL above crown).; j - Line contains hyd. jump.

## **Storm Sewer Tabulation**

													F									
Station		Len	Drng Area	rea	Rnoff	Area x C	ပ	T <sub>C</sub>	<u> </u>	Rain T	Total C	Cap	Nel	Pipe		Invert Elev	>	HGL Elev	<b>X</b>	Grnd / Rim Elev	im Elev	Line ID
Line	O L		Incr	Total		Incr	Total	Inlet	Syst				<u> </u>	Size	Slope	Du	ηD	Du	ď	ď	ď	
-		(ft)	(ac)	(ac)	(c)			(min)	(min)	(in/hr)	(cfs)	(cfs) (1	(ft/s) (i	(in)	(%)	(ft)	(ft)	(ft)	(ft)	(ft)	(ft)	
,		0		0				(	(	(		(				1				1	1	L ()
-	End End	27.000	٥.01	69.0	0.25	0.00	0.54	0.6	9.6	ກ ກ	3.73	3.56	5.24	77	9.	363.79	364.06	364.61	364.94	367.82	367.69	P-EXINCTIOE
7	_	000.9	0.14	0.54	0.95	0.13	0.41	5.0	9.6	6.9	2.85	8.72	3.63	12	00.9	364.06	364.42	365.49	365.53	367.69	368.02	P - INL 1 TO EX I
ო	7	16.000	0.07	0.40	0.88	90.0	0.28	5.0	8.5	6.9	1.95	3.56	4.03	12	1.00	365.11	365.27	365.70	365.86	368.02	368.75	P - INL 2 TO INL
4	ო	74.000	00.00	0.33	0.00	00.00	0.22	0.0	6.7	7.1	1.55	3.56	4.03	12	1.00	365.47	366.21	365.93	366.74	368.75	371.20	P - MH 3 TO INL
Ŋ	4	35.000	0.11	0.22	0.65	0.07	0.14	5.0	7.5	7.2	1.02	3.56	3.56	12	1.00	366.41	366.76	366.78	367.18	371.20	371.25	P - INL 4 TO MH
9	5	108.000	0.11	0.11	0.65	0.07	0.07	5.0	5.0	8.0	0.56	3.56	3.00	12	1.00	366.96	368.04	367.23	368.35	371.25	371.25	P - INL 5 TO INL
7	4	54.000	00.00	0.11	0.00	00.00	0.08	0.0	5.2	0 6.7	0.62	3.18	2.77	12	0.80	366.41	366.84	366.74	367.17	371.20	371.20	P - INL 6 TO MH
ω	7	8.000	0.11	0.11	0.71	0.08	0.08	5.0	5.0	8.0	0.62	3.08	2.79	12	0.75	366.84	366.90	367.17	367.23	371.20	371.20	P - INL 6 TO MH
თ		151.000 0.04	0.04	0.14	0.94	0.04	0.12	5.0	0.9	7.7	96.0	4.48	2.19	12	1.58	364.06	366.45	365.49	366.86	367.69	370.87	P - INL 7 TO EX I
0	<u>о</u>	20.000	00.00	0.10	0.00	00.00	60.0	0.0	5.6	7.8	0.67	3.56	3.16	12	1.00	366.65	366.85	366.94	367.19	370.87	371.25	P - MH 8 TO INL
7	6	30.000	0.10	0.10	0.89	60.0	60.0	5.0	5.0	8.0	69.0	3.56	3.18	12	1.00	367.05	367.35	367.35	367.70	371.25	370.56	P - INL 9 TO MH
Projec	Project File: \$	SPOTLESS STORM PIPE.stm	ESS ST	ORM PI	PE.stm											Number	Number of lines: 11	_		Run Da	Run Date: 4/3/2025	.5

NOTES:Intensity = 46.21 / (Inlet time + 9.30) ^ 0.66; Return period = Yrs. 100; c = cir e = ellip b = box

## Inlet Report

Byp	Depr No (in)	0.0 Off	0.0 Off	0.0 Off	0.0	0.0 Off	0.0	0.0	0.0	0.0	6 0.0	0.0 10	
Inlet	th Spread (ft)	2 0.35	6   11.60	5 10.52	0.00	3 5.03	3 4.99	00:00	3.88	3 3.38	0.00	8 5.79	
	d Depth (ft)	0.02	0.06	0.05	00.00	0.13	0.13	0.00	0.14	0.13	0.00	0.18	
	Spread (ft)	0.35	12.00	11.28	0.00	5.03	4.99	0.00	3.88	3.38	0.00	5.79	
	Depth (ft)	0.02	0.06	0.06	00.00	0.13	0.13	00.00	0.14	0.13	00:00	0.18	
	٩	0.013	0.013	0.013	0.013	0.013	0.013	0.013	0.013	0.013	0.013	0.013	
Gutter	Sx (ft/ft)	0.020	0.005	0.005	0.020	0.040	0.040	0.020	0.020	0.020	0.020	0.020	
	Sw (ft/ft)	0.050	0.005	0.005	0.050	0.005	0.005	0.050	0.050	0.050	0.050	0.050	
	<b>≯</b> €	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	
	So (ft/ft)	Sag	0.050	0.015	Sag	Sag	Sag	Sag	Sag	Sag	Sag	Sag	
	≥ £	2.00	0.00	0.00	0.00	0.00	0.00	0.00	2.00	0.00	0.00	0.00	
Grate Inlet	J (£)	4.00	0.00	0.00	0.00	0.00	0.00	0.00	4.00	0.00	0.00	0.00	
้อ	Area (sqft)	8.00	0.00	0.00	0.00	0.00	0.00	0.00	8.00	0.00	0.00	0.00	1
Inlet	] (#)	0.00	4.00	4.00	0.00	4.00	4.00	0.00	0.00	4.00	0.00	4.00	
Curb Inlet	Ħ (i)	0.0	4.0	4.0	0.0	4.0	4.0	0.0	0.0	4.0	0.0	4.0	
Junc	Туре	Grate	Curb	Curb	Ξ	Curb	Curb	None	Grate	Curb	Ξ	Curb	1
a	Byp (cfs)	0.00	0.97	0.41	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
a		0.02	0.09	0.08	0.00	0.57	0.56	0.00	0.62	0.31	0.00	69.0	
a	carry (cfs)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
a a	CIA (cfs)	0.02	1.06	0.49	0.00	0.57	0.56	0.00	0.62	0.31	0.00	0.69	
Inlet ID		EX INL 1	INL 1	INL 2	MH 3	INL 4	INL 5	New	INL 6	INL 7	MH 8	6 JNI	
Line	°Z	-	7	ო	4	2	ဖ	7	œ	ი	10	<del>-</del>	

NOTES: Inlet N-Values = 0.016; Intensity = 46.21 / (Inlet time + 9.30) ^ 0.66; Return period = 100 Yrs.; \* Indicates Known Q added. All curb inlets are Horiz throat.

## FL-DOT Report

									$\mid$												
Line	O i o i	Type	n - Orley	Len	Drainage Area	e Area	(	Time		Inten	Total	Add	Inlet	Elev	Elev of HGL		Rise	HGL	ADD		Date: 4/3/2025
2			v a ue		00	C1 = 0.2 C2 = 0.5		2	MOH :			T_	<u>D</u>	Elev	Elev of Crown		Span	Pipe	Full Flow	MC .	Frequency: 100 yrs
						I II		_	sect		<u> </u>	Flow		Elev	of Invert						Proj: SPOTLESS STORM P
						Sub-	Sum					a		dn	Down	Fall	Size	Slope	Nel	Cap	
				(£)	(ac)			(min)	(min)	(in/hr)		(cfs)	(ft)	(tt)	(ft)	(ft)	(in)	(%)	(tt/s)	(cfs)	Line description
<del>-</del>	End	Grate	0.013	27.000	0.00	0.00	00.00	8.63	60.0	88.9	0.54	3.73	367.69	364.94 365.06 364.06	364.61 364.79 363.79	0.33	12 12 Cir	1.23	5.24 4.53	3.73	P - EX INL 1 TO EX MH
7	_	Curb	0.013	6.000	00:00	0.00	00:00	8.61	0.03	68.9	0.41	0.00	368.02	365.53 365.42 364.42	365.49 365.06 364.06	0.04	12 Cir	0.64	3.63	2.85 8.72	P - INL 1 TO EX INL 1
ო	0	Curb	0.013	16.000	00.00	0.00	00:0	8.51	0.10	6.91	0.28	0.00	368.75	365.86 366.27 365.27	365.70 366.11 365.11	0.17	12 12 Cir	1.04	4.03	1.95 3.56	P - INL 2 TO INL 1
4	ო	Η	0.013	74.000	00.00	0.00	00:0	7.94	0.57	90.7	0.22	0.00	371.20	366.74 367.21 366.21	365.93 366.47 365.47	0.81	12 Cir	1.09	4.53	1.55 3.56	P - MH 3 TO INL 2
Ŋ	4	Curb	0.013	35.000	0.00	0.00	0.00	7.52	0.42	7.18	0.14	0.00	371.25	367.18 367.76 366.76	366.78 367.41 366.41	0.41	12 Cir	1.16	3.56	1.02 3.56	P - INL 4 TO MH 3
ဖ	r	Curb	0.013	108.000	00:00	0.00	00.0	5.00	2.52	66.7	0.07	0.00	371.25	368.35 369.04 368.04	367.23 367.96 366.96	1.12	12 Cir	1.00	3.00	0.56 3.56	P - INL 5 TO INL 4
^	4	None	0.013	54.000	0.00	0.00	00.00	5.17	41.1	7.93	0.08	0.00	371.20	367.17 367.84 366.84	366.74 367.41 366.41	0.43	12 Cir	0.79	4.05	3.18	P - INL 6 TO MH 3
ω	<b>^</b>	Grate	0.013	8.000	00.00	0.00	00:00	5.00	0.17	66.7	0.08	0.00	371.20	367.23 367.90 366.90	367.17 367.84 366.84	90.0	12 Cir	0.76	3.93	3.08	P - INL 6 TO MH 3 (2)
თ	~	Curb	0.013	151.000 0.00 0.00 0.00	00.00	0.00	0.00	5.95	2.01	7.66	0.12	0.00	370.87	366.86 367.45 366.45	365.49 365.06 364.06	1.37	12 Cir	0.91	2.19	0.96	P - INL 7 TO EX INL 1
10	თ	I Z	0.013	20.000	0.00	0.00	0.00	5.57	0.38	7.79	60.0	0.00	371.25	367.19 367.85 366.85	366.94 367.65 366.65	0.25	Cir 2 2	1.24	3.16	3.56	P - MH 8 TO INL 7
Į Š		NOTES: Intensity = 46.21 / Inlet time + 9.30) ^ 0.66 / in/hr)	)4 / (lpl		0 30) 4 (	//ui/ 32 (		Time of flow is wolf to a will	1												mb BIBE of the state of the sta

NOTES: Intensity = 46.21 / (Inlet time + 9.30) ^ 0.66 (in/lnr); Time of flow in section is based on full flow.

Project File: SPOTLESS STORM PIPE.stm

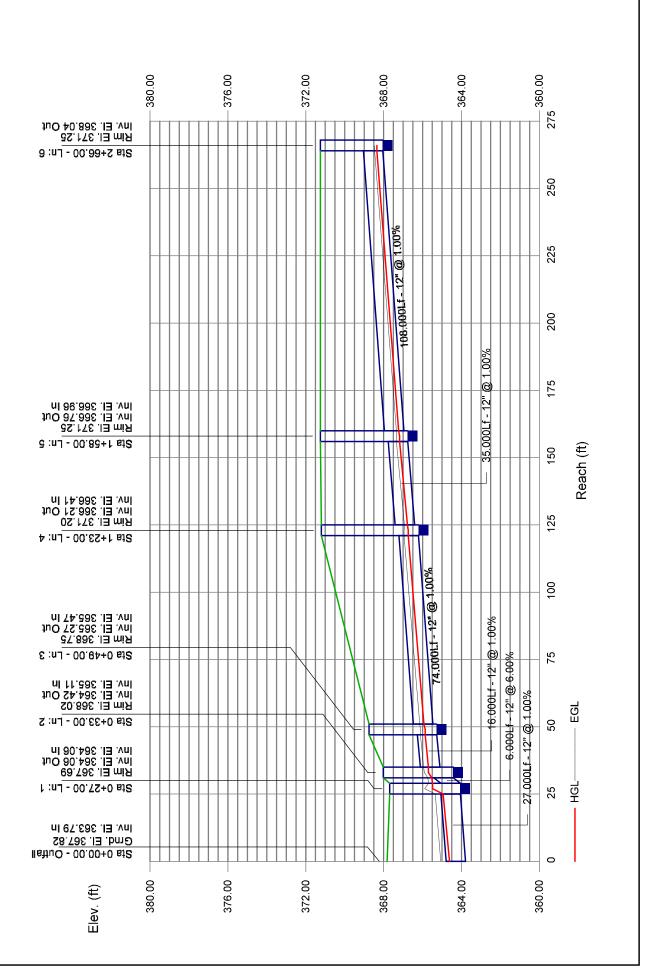
## FL-DOT Report

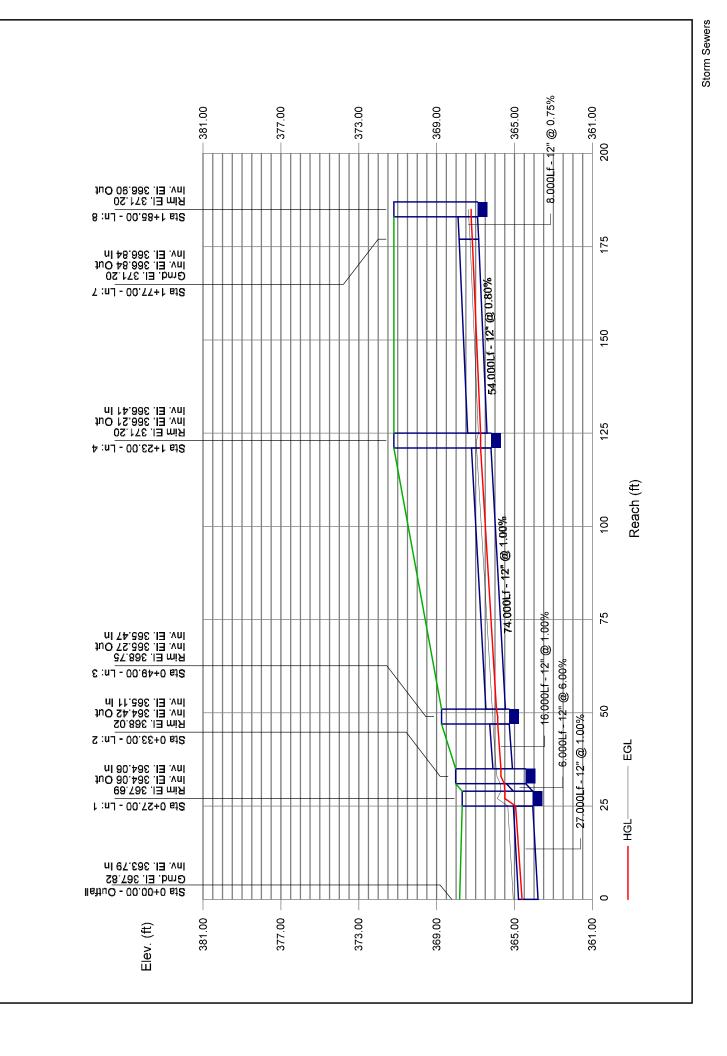
			•																		
Line	O L	Type	n - Valuo	Len	Drainage Area	e Area		Time		Inten	Total	Add	Inlet	Elev	Elev of HGL		Rise	HGL	ADD		Date: 4/3/2025
2			A dine		00	C1 = 0.2			NOE :				> 2 2 3	Elev	Elev of Crown		Span	Pipe	Full Flow	ow	Frequency: 100 yrs
					, O	3 = 0.9			sect		-	Flow		Elev	Elev of Invert						Proj: SPOTLESS STORM P
					Incre-	Sub-	Sum					σ		ηD	Down	Fall	Size	Slope	Vel	Сар	
				(ft)				(min)	(min)	(in/hr)		(cfs)	(#)	(ft)	(ft)	(#)	(in)	(%)	(£t/s)	(cfs)	Line description
<del>-</del>	6	Ourb	0.013	30.000	0000	0000	0000	9.00	0.57	66.2	60.0	0.00 0.00 0.00	370.56	367.70 367.35 367.35	367.35 367.05 367.05	0.35	Ç. 17. 2. 2. 2. 2. 2. 2. 2. 2. 2. 2. 2. 2. 2.	1.16	წ. 4. 8. წ.	3.56 8.56 9.56	P - INL 9 TO MH 8
NOTE	S: Inten	sity = 46.	.21 / (Inl	et time +	NOTES: Intensity = 46.21 / (Inlet time + 9.30) ^ 0.66 (in/hr); Time of flow in section is based on full flow.	).66 (in/	hr); Tir	ne of flo	w in sect	ion is ba	sed on fi	ull flow.							Project F	ile: SPC	Project File: SPOTLESS STORM PIPE.stm

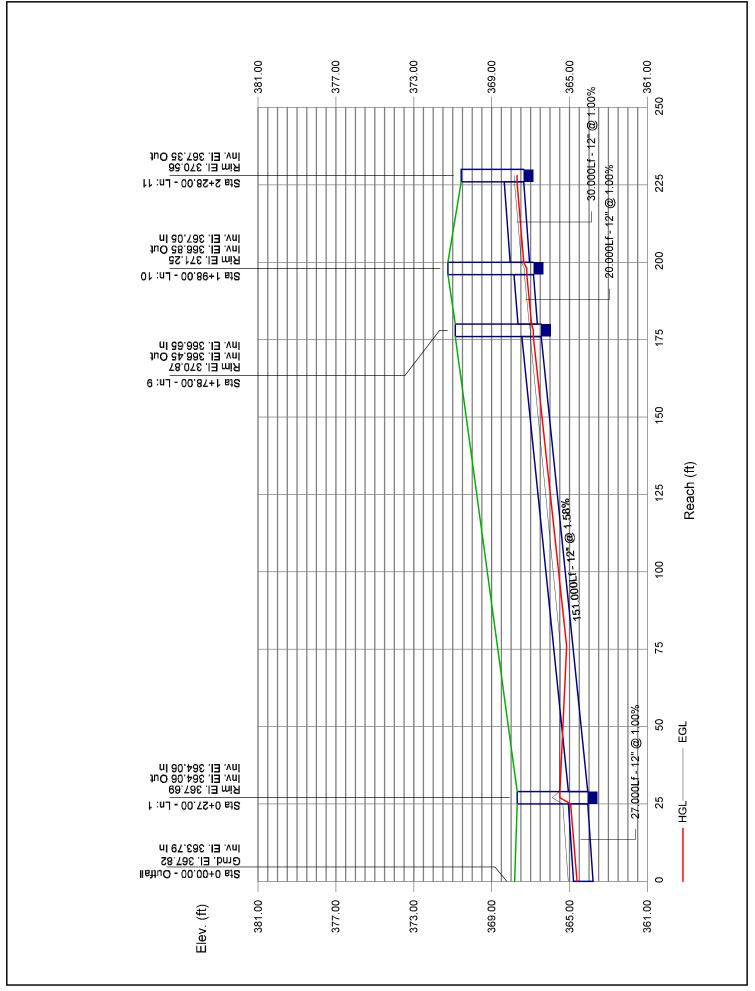
# Storm Sewer Inlet Time Tabulation

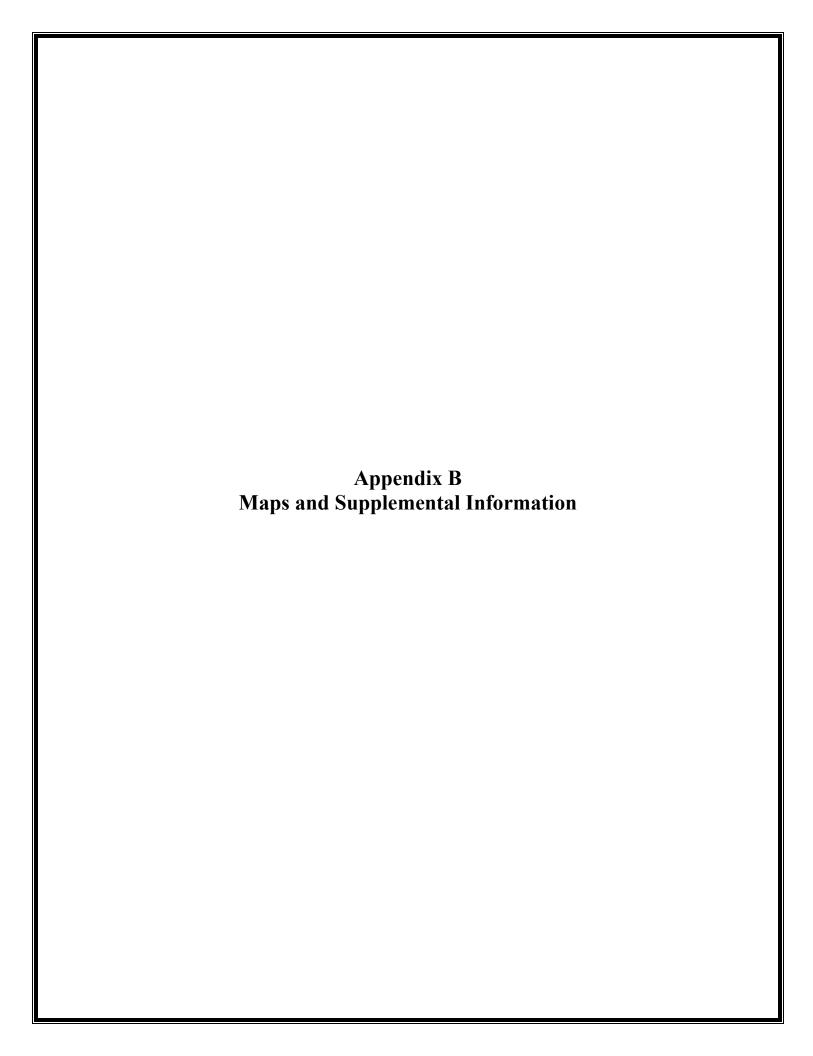
, [			,																	
Line	Line ID	Tc		She	Sheet Flow			Sha	llow Cor	Shallow Concentrated Flow	d Flow				Cha	Channel Flow				Total
No.		Method	n- Value	flow Length (ft)	2-yr 24h P (in)	Slope (%)	Travel Time (min)	flow Length (ft)	Water Slope (%)	Surf Descr	Ave Vel (ft/s)	Travel Time (min)	X-sec Area (sqft)	Wetted Perim (ft)	Chan Slope (%)	n- Value	Vel	flow Length (ft)	Travel Time (min)	Travel Time (min)
-	P - EX INL 1 TO E	User																		5.00
7	P - INL 1 TO EX I	User																		5.00
ო	P - INL 2 TO INL	User																		5.00
4	P - MH 3 TO INL	User																		0.00
S.	P - INL 4 TO MH	User																		5.00
ဖ	P - INL 5 TO INL	User																		5.00
7	P - INL 6 TO MH	User																		0.00
ω	P - INL 6 TO MH	User																		5.00
თ	P - INL 7 TO EX I	User																		5.00
10	P - MH 8 TO INL	User																		0.00
1	P - INL 9 TO MH	User																		5.00
Projec	Project File: SPOTLESS STORM PIPE.stm	STORM PIF	⊅E.stm		Ξ	in. Tc use	ed for inte	Min. Tc used for intensity calculations = 5 min	lations =	5 min		ž	Number of lines: 11	ines: 11			Date: 4/3/2025	3/2025		

Storm Sewers v2025











## NOAA Atlas 14, Volume 2, Version 3 Location name: Chalfont, Pennsylvania, USA\* Latitude: 40.2661°, Longitude: -75.2283° Elevation: 375 ft\*\*

\* source: ESRI Maps \*\* source: USGS



## POINT PRECIPITATION FREQUENCY ESTIMATES

G.M. Bonnin, D. Martin, B. Lin, T. Parzybok, M.Yekta, and D. Riley NOAA, National Weather Service, Silver Spring, Maryland

PF tabular | PF graphical | Maps & aerials

## PF tabular

PDS-b	ased poir	nt precipit	ation freq	uency est	timates w	ith 90% co	onfidence	intervals	(in inches	s/hour) <sup>1</sup>
	-					ce interval (y			•	<u>,                                      </u>
Duration	1	2	5	10	25	50	100	200	500	1000
5-min	<b>4.06</b> (3.71-4.43)	<b>4.82</b> (4.42-5.28)	<b>5.68</b> (5.17-6.19)	<b>6.28</b> (5.72-6.86)	<b>7.01</b> (6.35-7.64)	<b>7.51</b> (6.77-8.21)	<b>7.99</b> (7.19-8.75)	<b>8.42</b> (7.54-9.24)	<b>8.94</b> (7.92-9.83)	<b>9.31</b> (8.20-10.3)
10-min	<b>3.24</b> (2.96-3.54)	<b>3.86</b> (3.53-4.22)	<b>4.54</b> (4.15-4.96)	<b>5.02</b> (4.58-5.48)	<b>5.59</b> (5.06-6.10)	<b>5.98</b> (5.39-6.53)	<b>6.35</b> (5.71-6.95)	<b>6.68</b> (5.97-7.32)	<b>7.07</b> (6.26-7.77)	<b>7.33</b> (6.46-8.08)
15-min	<b>2.70</b> (2.47-2.95)	<b>3.23</b> (2.96-3.54)	<b>3.83</b> (3.50-4.18)	<b>4.23</b> (3.86-4.62)	<b>4.72</b> (4.28-5.15)	<b>5.05</b> (4.55-5.52)	<b>5.36</b> (4.81-5.86)	<b>5.62</b> (5.02-6.16)	<b>5.93</b> (5.26-6.52)	<b>6.13</b> (5.40-6.76)
30-min	<b>1.85</b> (1.69-2.02)	<b>2.23</b> (2.04-2.44)	<b>2.72</b> (2.48-2.97)	<b>3.07</b> (2.80-3.35)	<b>3.50</b> (3.17-3.81)	<b>3.80</b> (3.43-4.15)	<b>4.10</b> (3.69-4.48)	<b>4.37</b> (3.91-4.79)	<b>4.72</b> (4.18-5.19)	<b>4.96</b> (4.37-5.48)
60-min	<b>1.15</b> (1.06-1.26)	<b>1.40</b> (1.28-1.53)	<b>1.74</b> (1.59-1.90)	<b>2.00</b> (1.82-2.18)	<b>2.33</b> (2.11-2.54)	<b>2.58</b> (2.32-2.81)	<b>2.82</b> (2.54-3.09)	<b>3.07</b> (2.74-3.36)	<b>3.38</b> (3.00-3.72)	<b>3.62</b> (3.19-4.00)
2-hr	<b>0.686</b> (0.625-0.753)	<b>0.833</b> (0.760-0.914)	<b>1.04</b> (0.948-1.14)	<b>1.20</b> (1.09-1.32)	<b>1.42</b> (1.28-1.55)	<b>1.59</b> (1.42-1.74)	<b>1.76</b> (1.57-1.93)	<b>1.94</b> (1.71-2.12)	<b>2.17</b> (1.90-2.39)	<b>2.35</b> (2.04-2.59)
3-hr	<b>0.500</b> (0.453-0.552)	<b>0.606</b> (0.550-0.669)	<b>0.760</b> (0.689-0.838)	<b>0.879</b> (0.794-0.968)	<b>1.04</b> (0.933-1.14)	<b>1.16</b> (1.04-1.28)	<b>1.30</b> (1.15-1.42)	<b>1.43</b> (1.25-1.57)	<b>1.60</b> (1.39-1.77)	<b>1.74</b> (1.50-1.93)
6-hr	<b>0.313</b> (0.286-0.347)	<b>0.379</b> (0.345-0.419)	<b>0.473</b> (0.430-0.523)	<b>0.550</b> (0.498-0.606)	<b>0.658</b> (0.591-0.725)	<b>0.746</b> (0.665-0.820)	<b>0.839</b> (0.741-0.922)	<b>0.937</b> (0.819-1.03)	<b>1.07</b> (0.924-1.18)	<b>1.18</b> (1.00-1.31)
12-hr	<b>0.189</b> (0.173-0.210)	<b>0.229</b> (0.209-0.254)	<b>0.288</b> (0.262-0.319)	<b>0.337</b> (0.305-0.373)	<b>0.409</b> (0.366-0.450)	<b>0.469</b> (0.417-0.517)	<b>0.535</b> (0.470-0.589)	<b>0.606</b> (0.525-0.668)	<b>0.710</b> (0.603-0.786)	<b>0.797</b> (0.666-0.883)
24-hr	<b>0.112</b> (0.103-0.122)	<b>0.135</b> (0.124-0.148)	<b>0.170</b> (0.156-0.185)	<b>0.199</b> (0.182-0.216)	<b>0.241</b> (0.219-0.261)	<b>0.276</b> (0.250-0.299)	<b>0.314</b> (0.283-0.340)	<b>0.356</b> (0.317-0.385)	<b>0.416</b> (0.367-0.450)	<b>0.466</b> (0.406-0.505)
2-day	<b>0.065</b> (0.059-0.071)	<b>0.078</b> (0.071-0.086)	<b>0.098</b> (0.089-0.108)	<b>0.115</b> (0.104-0.126)	<b>0.138</b> (0.125-0.152)	<b>0.158</b> (0.142-0.173)	<b>0.178</b> (0.159-0.195)	<b>0.200</b> (0.178-0.220)	<b>0.232</b> (0.204-0.254)	<b>0.258</b> (0.224-0.283)
3-day	<b>0.045</b> (0.041-0.050)	<b>0.055</b> (0.050-0.060)	<b>0.069</b> (0.063-0.075)	<b>0.080</b> (0.073-0.088)	<b>0.096</b> (0.087-0.105)	<b>0.109</b> (0.099-0.119)	<b>0.123</b> (0.111-0.135)	<b>0.138</b> (0.124-0.151)	<b>0.160</b> (0.141-0.174)	<b>0.177</b> (0.155-0.194)
4-day	<b>0.036</b> (0.033-0.039)	<b>0.043</b> (0.040-0.047)	<b>0.054</b> (0.049-0.059)	<b>0.063</b> (0.057-0.068)	<b>0.075</b> (0.068-0.082)	<b>0.085</b> (0.077-0.093)	<b>0.096</b> (0.087-0.104)	<b>0.107</b> (0.097-0.117)	<b>0.123</b> (0.110-0.134)	<b>0.136</b> (0.121-0.149)
7-day	<b>0.024</b> (0.022-0.026)	<b>0.028</b> (0.026-0.031)	<b>0.035</b> (0.033-0.038)	<b>0.041</b> (0.038-0.044)	<b>0.049</b> (0.045-0.053)	<b>0.055</b> (0.051-0.060)	<b>0.062</b> (0.057-0.067)	<b>0.069</b> (0.063-0.075)	<b>0.080</b> (0.072-0.087)	<b>0.088</b> (0.079-0.096)
10-day	<b>0.019</b> (0.017-0.020)	<b>0.022</b> (0.021-0.024)	<b>0.027</b> (0.025-0.030)	<b>0.031</b> (0.029-0.034)	<b>0.037</b> (0.034-0.040)	<b>0.041</b> (0.038-0.045)	<b>0.046</b> (0.042-0.050)	<b>0.051</b> (0.046-0.055)	<b>0.057</b> (0.052-0.062)	<b>0.063</b> (0.057-0.068)
20-day	<b>0.012</b> (0.012-0.013)	<b>0.015</b> (0.014-0.016)	<b>0.018</b> (0.017-0.019)	<b>0.020</b> (0.019-0.022)	<b>0.023</b> (0.022-0.025)	<b>0.026</b> (0.024-0.028)	<b>0.028</b> (0.026-0.030)	<b>0.031</b> (0.028-0.033)	<b>0.034</b> (0.031-0.036)	<b>0.036</b> (0.033-0.039)
30-day	<b>0.010</b> (0.010-0.011)	<b>0.012</b> (0.011-0.013)	<b>0.014</b> (0.013-0.015)	<b>0.016</b> (0.015-0.017)	<b>0.018</b> (0.017-0.019)	<b>0.020</b> (0.018-0.021)	<b>0.021</b> (0.020-0.022)	<b>0.023</b> (0.021-0.024)	<b>0.025</b> (0.023-0.026)	<b>0.026</b> (0.024-0.028)
45-day	<b>0.009</b> (0.008-0.009)	<b>0.010</b> (0.010-0.011)	<b>0.012</b> (0.011-0.012)	<b>0.013</b> (0.012-0.014)	<b>0.014</b> (0.014-0.015)	<b>0.016</b> (0.015-0.016)	<b>0.017</b> (0.016-0.018)	<b>0.018</b> (0.017-0.019)	<b>0.019</b> (0.018-0.020)	<b>0.020</b> (0.018-0.021)
60-day	<b>0.008</b> (0.007-0.008)	<b>0.009</b> (0.009-0.010)	<b>0.010</b> (0.010-0.011)	<b>0.011</b> (0.011-0.012)	<b>0.013</b> (0.012-0.013)	<b>0.014</b> (0.013-0.014)	<b>0.014</b> (0.014-0.015)	<b>0.015</b> (0.014-0.016)	<b>0.016</b> (0.015-0.017)	<b>0.017</b> (0.016-0.018)

<sup>&</sup>lt;sup>1</sup> Precipitation frequency (PF) estimates in this table are based on frequency analysis of partial duration series (PDS).

Numbers in parenthesis are PF estimates at lower and upper bounds of the 90% confidence interval. The probability that precipitation frequency estimates (for a given duration and average recurrence interval) will be greater than the upper bound (or less than the lower bound) is 5%. Estimates at upper bounds are not checked against probable maximum precipitation (PMP) estimates and may be higher than currently valid PMP values.

Please refer to NOAA Atlas 14 document for more information.

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## PF graphical



NRCS Natural

Natural Resources Conservation Service A product of the National Cooperative Soil Survey, a joint effort of the United States Department of Agriculture and other Federal agencies, State agencies including the Agricultural Experiment Stations, and local participants

## Custom Soil Resource Report for Bucks County, Pennsylvania



## **Preface**

Soil surveys contain information that affects land use planning in survey areas. They highlight soil limitations that affect various land uses and provide information about the properties of the soils in the survey areas. Soil surveys are designed for many different users, including farmers, ranchers, foresters, agronomists, urban planners, community officials, engineers, developers, builders, and home buyers. Also, conservationists, teachers, students, and specialists in recreation, waste disposal, and pollution control can use the surveys to help them understand, protect, or enhance the environment.

Various land use regulations of Federal, State, and local governments may impose special restrictions on land use or land treatment. Soil surveys identify soil properties that are used in making various land use or land treatment decisions. The information is intended to help the land users identify and reduce the effects of soil limitations on various land uses. The landowner or user is responsible for identifying and complying with existing laws and regulations.

Although soil survey information can be used for general farm, local, and wider area planning, onsite investigation is needed to supplement this information in some cases. Examples include soil quality assessments (http://www.nrcs.usda.gov/wps/portal/nrcs/main/soils/health/) and certain conservation and engineering applications. For more detailed information, contact your local USDA Service Center (https://offices.sc.egov.usda.gov/locator/app?agency=nrcs) or your NRCS State Soil Scientist (http://www.nrcs.usda.gov/wps/portal/nrcs/detail/soils/contactus/?cid=nrcs142p2 053951).

Great differences in soil properties can occur within short distances. Some soils are seasonally wet or subject to flooding. Some are too unstable to be used as a foundation for buildings or roads. Clayey or wet soils are poorly suited to use as septic tank absorption fields. A high water table makes a soil poorly suited to basements or underground installations.

The National Cooperative Soil Survey is a joint effort of the United States Department of Agriculture and other Federal agencies, State agencies including the Agricultural Experiment Stations, and local agencies. The Natural Resources Conservation Service (NRCS) has leadership for the Federal part of the National Cooperative Soil Survey.

Information about soils is updated periodically. Updated information is available through the NRCS Web Soil Survey, the site for official soil survey information.

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# **How Soil Surveys Are Made**

Soil surveys are made to provide information about the soils and miscellaneous areas in a specific area. They include a description of the soils and miscellaneous areas and their location on the landscape and tables that show soil properties and limitations affecting various uses. Soil scientists observed the steepness, length, and shape of the slopes; the general pattern of drainage; the kinds of crops and native plants; and the kinds of bedrock. They observed and described many soil profiles. A soil profile is the sequence of natural layers, or horizons, in a soil. The profile extends from the surface down into the unconsolidated material in which the soil formed or from the surface down to bedrock. The unconsolidated material is devoid of roots and other living organisms and has not been changed by other biological activity.

Currently, soils are mapped according to the boundaries of major land resource areas (MLRAs). MLRAs are geographically associated land resource units that share common characteristics related to physiography, geology, climate, water resources, soils, biological resources, and land uses (USDA, 2006). Soil survey areas typically consist of parts of one or more MLRA.

The soils and miscellaneous areas in a survey area occur in an orderly pattern that is related to the geology, landforms, relief, climate, and natural vegetation of the area. Each kind of soil and miscellaneous area is associated with a particular kind of landform or with a segment of the landform. By observing the soils and miscellaneous areas in the survey area and relating their position to specific segments of the landform, a soil scientist develops a concept, or model, of how they were formed. Thus, during mapping, this model enables the soil scientist to predict with a considerable degree of accuracy the kind of soil or miscellaneous area at a specific location on the landscape.

Commonly, individual soils on the landscape merge into one another as their characteristics gradually change. To construct an accurate soil map, however, soil scientists must determine the boundaries between the soils. They can observe only a limited number of soil profiles. Nevertheless, these observations, supplemented by an understanding of the soil-vegetation-landscape relationship, are sufficient to verify predictions of the kinds of soil in an area and to determine the boundaries.

Soil scientists recorded the characteristics of the soil profiles that they studied. They noted soil color, texture, size and shape of soil aggregates, kind and amount of rock fragments, distribution of plant roots, reaction, and other features that enable them to identify soils. After describing the soils in the survey area and determining their properties, the soil scientists assigned the soils to taxonomic classes (units). Taxonomic classes are concepts. Each taxonomic class has a set of soil characteristics with precisely defined limits. The classes are used as a basis for comparison to classify soils systematically. Soil taxonomy, the system of taxonomic classification used in the United States, is based mainly on the kind and character of soil properties and the arrangement of horizons within the profile. After the soil

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scientists classified and named the soils in the survey area, they compared the individual soils with similar soils in the same taxonomic class in other areas so that they could confirm data and assemble additional data based on experience and research.

The objective of soil mapping is not to delineate pure map unit components; the objective is to separate the landscape into landforms or landform segments that have similar use and management requirements. Each map unit is defined by a unique combination of soil components and/or miscellaneous areas in predictable proportions. Some components may be highly contrasting to the other components of the map unit. The presence of minor components in a map unit in no way diminishes the usefulness or accuracy of the data. The delineation of such landforms and landform segments on the map provides sufficient information for the development of resource plans. If intensive use of small areas is planned, onsite investigation is needed to define and locate the soils and miscellaneous areas.

Soil scientists make many field observations in the process of producing a soil map. The frequency of observation is dependent upon several factors, including scale of mapping, intensity of mapping, design of map units, complexity of the landscape, and experience of the soil scientist. Observations are made to test and refine the soil-landscape model and predictions and to verify the classification of the soils at specific locations. Once the soil-landscape model is refined, a significantly smaller number of measurements of individual soil properties are made and recorded. These measurements may include field measurements, such as those for color, depth to bedrock, and texture, and laboratory measurements, such as those for content of sand, silt, clay, salt, and other components. Properties of each soil typically vary from one point to another across the landscape.

Observations for map unit components are aggregated to develop ranges of characteristics for the components. The aggregated values are presented. Direct measurements do not exist for every property presented for every map unit component. Values for some properties are estimated from combinations of other properties.

While a soil survey is in progress, samples of some of the soils in the area generally are collected for laboratory analyses and for engineering tests. Soil scientists interpret the data from these analyses and tests as well as the field-observed characteristics and the soil properties to determine the expected behavior of the soils under different uses. Interpretations for all of the soils are field tested through observation of the soils in different uses and under different levels of management. Some interpretations are modified to fit local conditions, and some new interpretations are developed to meet local needs. Data are assembled from other sources, such as research information, production records, and field experience of specialists. For example, data on crop yields under defined levels of management are assembled from farm records and from field or plot experiments on the same kinds of soil.

Predictions about soil behavior are based not only on soil properties but also on such variables as climate and biological activity. Soil conditions are predictable over long periods of time, but they are not predictable from year to year. For example, soil scientists can predict with a fairly high degree of accuracy that a given soil will have a high water table within certain depths in most years, but they cannot predict that a high water table will always be at a specific level in the soil on a specific date.

After soil scientists located and identified the significant natural bodies of soil in the survey area, they drew the boundaries of these bodies on aerial photographs and

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identified each as a specific map unit. Aerial photographs show trees, buildings, fields, roads, and rivers, all of which help in locating boundaries accurately.

# Soil Map

The soil map section includes the soil map for the defined area of interest, a list of soil map units on the map and extent of each map unit, and cartographic symbols displayed on the map. Also presented are various metadata about data used to produce the map, and a description of each soil map unit.



#### MAP LEGEND

#### Area of Interest (AOI)

Area of Interest (AOI)

#### Soils

Soil Map Unit Polygons



Soil Map Unit Lines



Soil Map Unit Points

#### Special Point Features

Blowout



Clay Spot

Closed Depression

Gravel Pit

Gravelly Spot

Landfill
A Lava Flow

■ Marsh or swamp

Mine or Quarry

Miscellaneous Water

Perennial Water

Rock Outcrop

+ Saline Spot

Sandy Spot

Severely Eroded Spot

Sinkhole

Slide or Slip

Sodic Spot

#### \_\_\_\_\_

۵

Spoil Area Stony Spot



Very Stony Spot



Wet Spot Other



Special Line Features

#### Water Features

~

Streams and Canals

#### Transportation

+++ Rails

Interstate Highways

.

US Routes
Major Roads

~

Local Roads

#### Background

1

Aerial Photography

#### MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:24.000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service

Web Soil Survey URL:

Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Bucks County, Pennsylvania Survey Area Data: Version 20, Sep 4, 2023

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Jun 3, 2022—Jul 20, 2022

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

# Map Unit Legend

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
UfuB	Urban land, 0 to 8 percent slopes	1.3	100.0%
Totals for Area of Interest		1.3	100.0%

# **Map Unit Descriptions**

The map units delineated on the detailed soil maps in a soil survey represent the soils or miscellaneous areas in the survey area. The map unit descriptions, along with the maps, can be used to determine the composition and properties of a unit.

A map unit delineation on a soil map represents an area dominated by one or more major kinds of soil or miscellaneous areas. A map unit is identified and named according to the taxonomic classification of the dominant soils. Within a taxonomic class there are precisely defined limits for the properties of the soils. On the landscape, however, the soils are natural phenomena, and they have the characteristic variability of all natural phenomena. Thus, the range of some observed properties may extend beyond the limits defined for a taxonomic class. Areas of soils of a single taxonomic class rarely, if ever, can be mapped without including areas of other taxonomic classes. Consequently, every map unit is made up of the soils or miscellaneous areas for which it is named and some minor components that belong to taxonomic classes other than those of the major soils.

Most minor soils have properties similar to those of the dominant soil or soils in the map unit, and thus they do not affect use and management. These are called noncontrasting, or similar, components. They may or may not be mentioned in a particular map unit description. Other minor components, however, have properties and behavioral characteristics divergent enough to affect use or to require different management. These are called contrasting, or dissimilar, components. They generally are in small areas and could not be mapped separately because of the scale used. Some small areas of strongly contrasting soils or miscellaneous areas are identified by a special symbol on the maps. If included in the database for a given area, the contrasting minor components are identified in the map unit descriptions along with some characteristics of each. A few areas of minor components may not have been observed, and consequently they are not mentioned in the descriptions, especially where the pattern was so complex that it was impractical to make enough observations to identify all the soils and miscellaneous areas on the landscape.

The presence of minor components in a map unit in no way diminishes the usefulness or accuracy of the data. The objective of mapping is not to delineate pure taxonomic classes but rather to separate the landscape into landforms or landform segments that have similar use and management requirements. The delineation of such segments on the map provides sufficient information for the development of resource plans. If intensive use of small areas is planned, however, onsite investigation is needed to define and locate the soils and miscellaneous areas.

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An identifying symbol precedes the map unit name in the map unit descriptions. Each description includes general facts about the unit and gives important soil properties and qualities.

Soils that have profiles that are almost alike make up a *soil series*. Except for differences in texture of the surface layer, all the soils of a series have major horizons that are similar in composition, thickness, and arrangement.

Soils of one series can differ in texture of the surface layer, slope, stoniness, salinity, degree of erosion, and other characteristics that affect their use. On the basis of such differences, a soil series is divided into *soil phases*. Most of the areas shown on the detailed soil maps are phases of soil series. The name of a soil phase commonly indicates a feature that affects use or management. For example, Alpha silt loam, 0 to 2 percent slopes, is a phase of the Alpha series.

Some map units are made up of two or more major soils or miscellaneous areas. These map units are complexes, associations, or undifferentiated groups.

A *complex* consists of two or more soils or miscellaneous areas in such an intricate pattern or in such small areas that they cannot be shown separately on the maps. The pattern and proportion of the soils or miscellaneous areas are somewhat similar in all areas. Alpha-Beta complex, 0 to 6 percent slopes, is an example.

An *association* is made up of two or more geographically associated soils or miscellaneous areas that are shown as one unit on the maps. Because of present or anticipated uses of the map units in the survey area, it was not considered practical or necessary to map the soils or miscellaneous areas separately. The pattern and relative proportion of the soils or miscellaneous areas are somewhat similar. Alpha-Beta association, 0 to 2 percent slopes, is an example.

An *undifferentiated group* is made up of two or more soils or miscellaneous areas that could be mapped individually but are mapped as one unit because similar interpretations can be made for use and management. The pattern and proportion of the soils or miscellaneous areas in a mapped area are not uniform. An area can be made up of only one of the major soils or miscellaneous areas, or it can be made up of all of them. Alpha and Beta soils, 0 to 2 percent slopes, is an example.

Some surveys include *miscellaneous areas*. Such areas have little or no soil material and support little or no vegetation. Rock outcrop is an example.

### **Bucks County, Pennsylvania**

#### UfuB—Urban land, 0 to 8 percent slopes

#### **Map Unit Setting**

National map unit symbol: 17sq Elevation: 800 to 1,500 feet

Mean annual precipitation: 36 to 46 inches Mean annual air temperature: 41 to 62 degrees F

Frost-free period: 130 to 170 days

Farmland classification: Not prime farmland

#### **Map Unit Composition**

Urban land: 90 percent

Minor components: 10 percent

Estimates are based on observations, descriptions, and transects of the mapunit.

#### **Description of Urban Land**

#### Setting

Parent material: Pavement, buildings and other artifically covered areas human transported material

#### Interpretive groups

Land capability classification (irrigated): None specified

Land capability classification (nonirrigated): 8s

Hydric soil rating: No

#### **Minor Components**

#### Udorthents, unstable fill

Percent of map unit: 10 percent Down-slope shape: Linear Across-slope shape: Linear Hydric soil rating: No

# References

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United States Department of Agriculture, Natural Resources Conservation Service. National range and pasture handbook. http://www.nrcs.usda.gov/wps/portal/nrcs/detail/national/landuse/rangepasture/?cid=stelprdb1043084

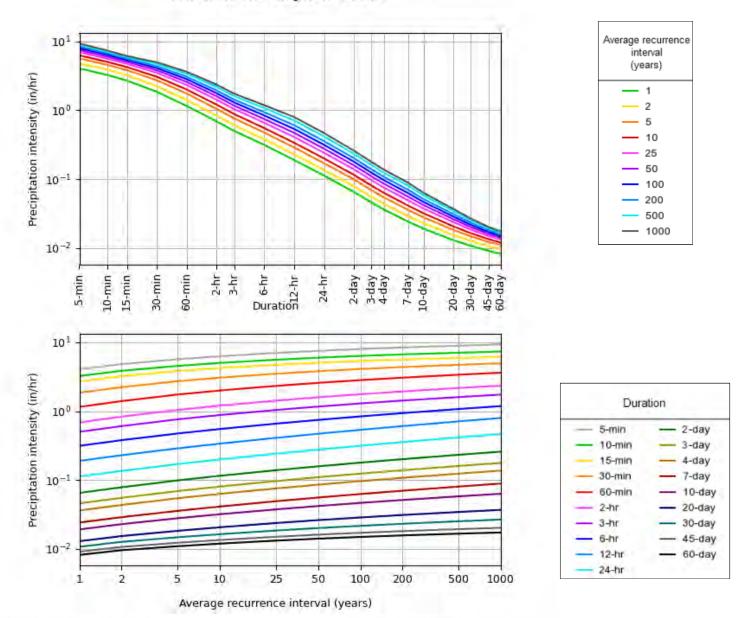
#### Custom Soil Resource Report

United States Department of Agriculture, Natural Resources Conservation Service. National soil survey handbook, title 430-VI. http://www.nrcs.usda.gov/wps/portal/nrcs/detail/soils/scientists/?cid=nrcs142p2\_054242

United States Department of Agriculture, Natural Resources Conservation Service. 2006. Land resource regions and major land resource areas of the United States, the Caribbean, and the Pacific Basin. U.S. Department of Agriculture Handbook 296. http://www.nrcs.usda.gov/wps/portal/nrcs/detail/national/soils/?cid=nrcs142p2\_053624

United States Department of Agriculture, Soil Conservation Service. 1961. Land capability classification. U.S. Department of Agriculture Handbook 210. http://www.nrcs.usda.gov/Internet/FSE\_DOCUMENTS/nrcs142p2\_052290.pdf

#### PDS-based intensity-duration-frequency (IDF) curves Latitude: 40.2661°, Longitude: -75.2283°



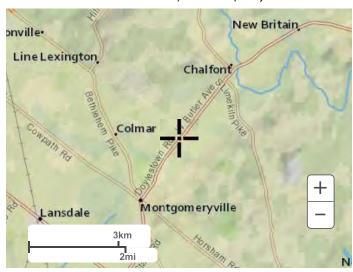
NOAA Atlas 14, Volume 2, Version 3

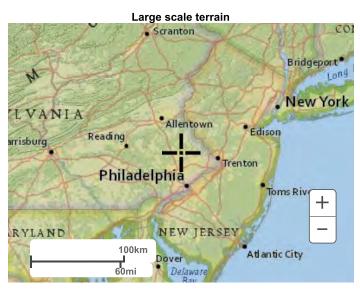
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#### Maps & aerials

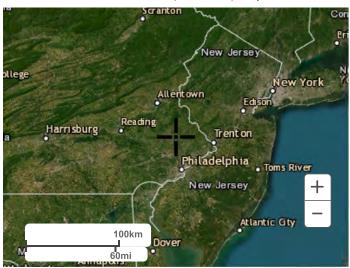
Small scale terrain







Large scale aerial



Back to Top

US Department of Commerce
National Oceanic and Atmospheric Administration
National Weather Service
National Water Center
1325 East West Highway
Silver Spring, MD 20910
Questions?: HDSC.Questions@noaa.gov

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March 14, 2025

#### Sent via E-mail

Mr. John Larson Chalfont-New Britain Twp Joint Sewage Authority 1645 Upper State Road Doylestown, PA 18901

RE: 545 West Butler Avenue

New Britain Township, Bucks County, PA

VCEA No. 24-02-NBR

Dear Mr. Larson:

Enclosed for your use are the following:

- 1. Sewage Facilities Planning Module Application mailer.
- 2. Project Narrative
- 3. Quad sheet with site location.
- 4. Sewage flow calculations
- 5. DEP Capacity Certification Form
- 6. PNDI
- 7. One (1) copy of the Utility Plan

A Planning Module Exemption from DEP is intended. Please review the attached materials and provide us with the 5-year "will-serve" letter as needed to further process the DEP application. Attached for your use is the DEP capacity certification form.

Public water and sewer are proposed. Sewer will tie into the existing sewer main within West Butler Avenue via gravity line.

If you have any questions, please feel free to contact me.

Sincerely,

Sean D. McGranahan, PE

Please Reply To:

DOYLESTOWN PA OFFICE • 501 North Main Street • Doylestown, PA 18901 • 215.345.1876 • Fax: 215.345.1730

# SEWAGE FACILITIES PLANNING MODULE APPLICATION MAILER



For more information, visit DEP's website at <a href="https://www.depweb.state.pa.us">www.depweb.state.pa.us</a>, keyword: Act 537.

#### INSTRUCTIONS FOR COMPLETING SEWAGE FACILITIES PLANNING MODULE APPLICATION MAILER

(remove before sending mailer to the approving agency (the Department of Environmental Protection (DEP) or delegated local agency))

Please read the following instructions carefully before completing the application mailer. This information will be used to determine if sewage facilities planning is necessary for your project, and, if it is, which forms are appropriate.

Act 537 Sewage Facilities' planning is the duty of local municipalities. The responsibility of meeting the sewage disposal needs of the municipality rests with the municipality. Each municipality is required to have an Act 537 Official Sewage Facilities Plan to adequately address these needs. Adoption by resolution of a planning module is the vehicle for legally amending the municipality's Official Plan. It is imperative that the municipality receives all of the information required in order to make informed decisions.

Upon completion, submit this Mailer / Application form to the approving agency (DEP or delegated local agency). Additionally, provide a copy of the completed form to the County Health Department having jurisdiction over the area in which the proposed project is located.

Unless your project qualifies for one of the planning exemptions provided in Act 537, a package of sewage facilities planning forms appropriate for the project will be sent to the return address indicated on the mailer, or instructions for obtaining the appropriate forms from DEP's website (www.depweb.state.pa.us) will be issued. Each form includes detailed instructions that explain the use of the form and how to complete it. The package of completed forms and its supporting documentation is called a **sewage facilities planning module**, or "planning module." Once the planning module is complete, it must be submitted to the municipality in which the project is located for review and, if acceptable, adoption.

After adoption by the municipality, complete planning modules are submitted to either DEP or, if appropriate, to the delegated agency for review and final action. Some municipalities (or groups of municipalities working together) have requested and received DEP approval to review and take final action on planning modules. These are known as **delegated agencies**. To find out if your project is located in an area served by a delegated agency, contact the municipality or DEP's regional office serving your area.

Please note that both DEP and delegated agencies are required by law to charge fees for the review of planning modules. The fees DEP must charge are set by law in Act 537, while delegated agencies may set fees which can be the same or different from those in Act 537. For more information on review fees for your particular project, see the planning module documents or contact the approving agency (DEP or delegated agency) serving the area of your project.

NOTE: DEP will provide all planning module forms, however, if your project is a **minor subdivision** (defined as a subdivision of 10 lots or less, intended for single family residential homes served by individual onlot sewage disposal systems) you may also contact the municipality in which your project is located for a "Component 1" minor subdivision planning module form.

- 1. Print the name of the proposed development and name, address, telephone number and email address of the person who is proposing the project. If planning module components are to be sent to a different person or address, include this information on the front of the mailer in the return address block.
- 2. a. Enter the county in which the project is located.
  - b. Enter the municipality in which the project is located.
  - c. Enter the road or address (if available) or street coordinates (example west side of T-235, 1 mile south of intersection of Rt 15 and T-235).
  - d. Enter the appropriate tax parcel identification number (if available) of the parcel proposed for subdivision.
  - e. Enter the name of the U.S. Geological Survey (USGS) 7.5-minute quadrangle map which contains the project area, and the location of the project area on that map in inches up and over from the lower right corner of the map to the approximate center of the project. (Example Centerville West quad, 7 inches up and 2.5 inches over from lower right corner of map.) Alternatively, include an original or a copy of the USGS quad map with the project area outlined on it.
  - f. If the proposed project is located within a special protection watershed, (i.e., watersheds with a stream classification of High Quality or Exceptional Value), check "yes." If not located in special protection watershed, check "no."
- 3. Check the box that best describes the intended use of the proposed land development project. **Residential** refers to single-family lots. **Multi-residential** includes apartments, condos, etc. **Commercial** includes retail centers, office parks, industrial development, etc. **Institutional** refers to schools, hospitals and the like. **Brownfield** Site

Redevelopment refers to projects proposing to recycle land. Some developments will involve more than one type of use, or will not fit comfortably into any of the classifications given. If this is the case, choose more than one category or explain under **Other**.

- 4. a. Enter the number of single family residential lots or Equivalent Dwelling Units (EDUs) proposed. An EDU is defined as that part of a multi-family dwelling or nonresidential project with flows equal to 400 gallons per day (gpd) (the assumed flow, for planning purposes, of a single family residential lot). To determine the number of EDUs, divide the proposed sewage flow of the project by 400 gpd.
  - b. Enter the total number of lots created from this parcel of land since May 15, 1972, including the lots being proposed at this time. (Onlot disposal proposals only.)
  - c. Enter the total project acreage and the acreage of any remaining land (land not proposed for development but under the same ownership and adjacent to the project area).
- 5. Enter the proposed total sewage flow from the project in gpd. See Title 25 of the Pennsylvania Code, Chapter 73, Section 73.17, (<a href="https://www.pacode.com">www.pacode.com</a>), or DEP's *Domestic Wastewater Facilities Manual*, DEP ID: 362-0300-001 available on DEP's website at www.depweb.state.pa.us, keyword: wastewater.
- 6. Choose the category (a, b, c or d) that describes the method of sewage disposal planned to serve the project and enter the information requested. Since this information could have an effect on the planning requirements for your project, be as accurate as possible. If more than one method of sewage disposal is planned, or if an interim method is planned, indicate it here.
  - a. Sewerage System

If an existing system is being extended to serve the proposed project, or if lots are to connect directly to an existing sewage collection system, check all boxes that describe the project. Write in the names of the existing collection systems that will be used, the interceptor sewer which will be used for conveyance and the treatment facility where the sewage flows will be treated. Include the National Pollutant Elimination Discharge System (NPDES) permit number for the treatment facility, where applicable.

b. Construction of Sewage Treatment Facility (with stream discharge or with spray irrigation as final disposal option)

Check the box corresponding to the chosen final disposal option (stream discharge or spray irrigation field). This category does **NOT** include individual residence spray irrigation systems (IRSIS) which are considered onlot sewage disposal systems ((c), below). For stream discharges, name the receiving waterbody. If the proposed facility is intended to replace an existing, malfunctioning onlot system, check the box marked "repair."

c. Onlot Sewage Disposal Systems (individual, community, or large-volume)

Check the box corresponding to the type of onlot sewage disposal systems proposed to serve the project. An **individual onlot sewage disposal system** is a system of piping, tanks or other facilities used for collecting, treating and disposing of sewage into a subsurface absorption area. This category also includes IRSIS. A **community onlot system** is a facility either publicly or privately owned which will collect and dispose of sewage from two or more lots or EDUs into a subsurface absorption area. A **large-volume onlot system** is an individual or community onlot system which is designed to treat flows in excess of 10,000 gpd.

The approving agency must be notified at least 10 days in advance of all soil testing activities (including those related to planning exemption requests - see 7(b)(5)(v)), so that its staff have the option of observing the tests.

d. Retaining Tanks (holding tanks or privies)

If retaining tanks are proposed as the method of sewage disposal, enter the number of holding tanks or privies which are proposed to serve the project.

7. Check this box if you desire to obtain your sewage facilities planning module forms from DEP's website. You will be provided with appropriate instructions, website addresses and DEP coding information in a letter rather than a package of paper forms.

8. Requests for Planning Exemption under the Sewage Facilities Act

You may request to be exempt from Act 537 planning requirements. Effective December 15, 1995, certain classes of subdivisions are no longer subject to the planning requirements of the Sewage Facilities Act. Completing Section 8 will help you and the approving agency determine if your project fits into one of these categories.

a. Protection of rare, endangered or threatened species.

DEP's technical guidance document "Policy for Pennsylvania Natural Diversity Inventory (PNDI) Coordination During Permit Review and Evaluation," (400-0200-001) requires DEP to ensure that requests for authorizations, are coordinated with the Department of Conservation and Natural Resources' (DCNR) Pennsylvania Natural Diversity Inventory (PNDI).

Conducting a search of the PNDI database and providing a copy of a "PNDI Project Environmental Review Receipt" for the proposed project and, if potential impacts are identified by the search, clearance or recommendation letters from the jurisdictional agency responsible for the particular species identified by a search, satisfies this requirement.

To avoid project delay, self-explanatory, self-conducted "PNDI Project Planning Environmental Review" searches are initiated at <a href="www.naturalheritage.state.pa.us">www.naturalheritage.state.pa.us</a>. This interactive, online search will ask questions about the proposed project and provide the appropriate receipt, instructions or additional information regarding coordination with jurisdictional agencies.

As an alternative to the self-conducted search, project sponsors may request DEP staff to conduct the search by providing a completed "PNDI Project Planning & Environmental Review Form" (PNDI Form). The form is available at <a href="www.naturalheritage.state.pa.us">www.naturalheritage.state.pa.us</a>. Individuals making this request should be aware that, due to the nature of the search software, DEP staff may need to contact them for additional information to successfully complete the search and that exclusive of any other items, their sewage planning exemption request is considered incomplete by DEP, until the appropriate receipt, clearance or recommendation letters are received.

For more information, see the "Policy for Pennsylvania Natural Diversity Inventory (PNDI) Coordination During Permit Review and Evaluation," (400-0200-001), available online in the eLibrary at DEP's website address www.depweb.state.pa.us.

- b. Attach a plot plan for the proposed project. The plan must depict anticipated lots to be created, either estimated sewer line runs (public sewer proposals), or site suitability test locations and Site Investigation and Percolation Test Reports (onlot proposals).
- c. Projects proposing use of onlot sewage disposal systems
  - (1) Information Required from the Municipality

The municipality in which the project is located (identified in Item 2.b. of the mailer) should determine if the municipality's Official Sewage Facilities Plan shows that the area planned for the project is to be served by onlot sewage disposal systems. If it is, the municipality should indicate this by having an authorized municipal official sign and date the form in the space provided. The official's name and title should be printed on the line below.

(2) Information Required from the Municipal Sewage Enforcement Officer (SEO)

The municipality's SEO must conduct personally, observe or otherwise confirm in a manner approved by DEP, site testing on each proposed lot in the subdivision (including any remaining land) to determine that separate sites are available for both a permitted primary onlot sewage disposal system and a replacement system (to be used if the original system fails in the future). If the SEO finds that each lot has been tested properly and fulfills these criteria, the SEO must indicate this by signing and dating the form in the space provided. His/her name and certification number should be printed on the line below.

(3) Information Required from the Applicant

The person proposing the subdivision, or his/her authorized agent, must determine if each lot in the subdivision (including the remaining land, if any) is at least one (1) acre in size. If they are, the applicant or his/her agent must indicate this by signing and dating the form in the space provided.

(4) Determinations Made by the Approving Agency

When the above listed information is received, the approving agency will determine the following:

- (a) If the geology of the project area is conducive to nitrate-nitrogen contamination of groundwater (determined from the topographic map location information); or
- (b) If elevated levels of nitrate-nitrogen are known to exist within one-quarter (1/4) mile of the proposed development (determined from agency groundwater sampling records in existence at the time of the application); or
- (c) If the area proposed for development is within an identified High Quality (HQ) or Exceptional Value (EV) watershed (determined from the topographic map location).

Following this investigation, the approving agency will render a decision on the exemption request within 10 working days of receiving the request for exemption. Both the applicant and municipality will be notified of the decision. If the request cannot be granted, the person named in the return address block will receive the proper planning module component forms (or instructions to obtain them from the DEP website) along with the notification of the decision, including the reason(s) that the request cannot be granted.

- d. Projects proposing use of public sewerage facilities (i.e., ownership by municipality or authority)
  - (1) Information Required from the Municipality

The municipality in which the proposed project is located (identified in Item 2.b. of the mailer) will determine the following from written documentation requested and obtained by the applicant from the facility permittee. This documentation MUST also be sent to the approving agency (DEP or delegated local agency) for evaluation.

- (a) Certification from the permittees of the collection, conveyance and treatment facilities proposed for use that capacity is available in these facilities to receive and treat the sewage flows from the proposed project; and
- (b) That these added flows will not cause an overload or 5-year projected overload in the facilities.

If the facilities proposed for use are owned and operated by an authority, or authorities, then attach a letter from each to the mailer.

If this written certification has been submitted by the applicant, an authorized municipal official should sign and date the form and print his/her name and title and the municipality name in the spaces provided.

NOTE: Since planning is a municipal responsibility, sewer authorities involved should make required information available but should **NOT** sign the mailer as the authorized municipal official.

(2) Determinations Made by the Approving Agency

When the above listed information is received by the approving agency, the approving agency will determine the following (from DEP records):

- (a) That the existing collection, conveyance and treatment facilities are in compliance;
- (b) That the existing facilities have no existing or 5-year projected overload;
- (c) That the municipality has a currently approved Official Sewage Facilities Plan which is being implemented; and
- (d) That the project does not propose service by facilities needing a new or modified permit from DEP under the Clean Streams Law.

Following this investigation, the approving agency will render a decision on the exemption request within 10 working days of receiving the request for exemption. Both the applicant and the municipality will be notified of the decision. If the request cannot be granted, the person named in the return address block will receive the proper planning module component forms (or instructions to obtain them from DEP's website) along with the notification of the decision, including the reason that the request cannot be granted.

If unsure of which local DEP office to contact, the following DEP regional offices will assist you in determining the appropriate local DEP office that serves your specific municipality.

If you need more information or assistance, please contact your local DEP office.

#### **DEP REGIONAL OFFICES**

#### **Northwest Region**

230 Chestnut St. Meadville, PA 16335-3481

Main Telephone: 814-332-6945 24-Hour Emergency: 800-373-3398

Counties: Armstrong, Butler, Clarion, Crawford, Elk, Erie, Forest, Indiana, Jefferson, Lawrence, McKean, Mercer, Venango, and Warren

#### **Southwest Region**

400 Waterfront Drive Pittsburgh, PA 15222-4745

Main Telephone: 412-442-4000 24-Hour Emergency: 412-442-4000

Counties: Allegheny, Beaver, Cambria, Fayette, Greene, Somerset, Washington, and Westmoreland

#### **North-central Region**

208 W. Third St., Suite 101 Williamsport, PA 17701-6448

Main Telephone: 570-327-3636 24-Hour Emergency: 570-327-3636

Counties: Bradford, Cameron, Clearfield, Centre, Clinton, Columbia, Lycoming, Montour, Northumberland, Potter, Snyder, Sullivan, Tioga, and Union

#### South-central Region

909 Elmerton Ave. Harrisburg, PA 17110-8200

Main Telephone: 717-705-4700 24-Hour Emergency: 866-825-0208

Counties: Adams, Bedford, Berks, Blair, Cumberland, Dauphin, Franklin, Fulton, Huntingdon, Juniata, Lancaster, Lebanon, Mifflin, Perry, and York

#### Northeast Region

2 Public Square Wilkes-Barre, PA 18701-1915 Main Telephone: 570-826-2511 24-Hour Emergency:570-826-2511

Counties: Carbon, Lackawanna, Lehigh, Luzerne, Monroe, Northampton, Pike, Schuylkill, Susquehanna, Wayne, and Wyoming

#### **Southeast Region**

2 E. Main St. Norristown, PA 19401-4915

Main Telephone: 484-250-5900 24-Hour Emergency: 484-250-5900

**Counties:** Bucks, Chester, Delaware, Montgomery, and Philadelphia

4	D	lopment	I.a.f.aa.	-4:

١.	De	veropinent information
	Na	me of Development Spotless Carwash
		veloper Name <u>Flagship Pennsylvania Propco, LLC c/o John</u> mbardo
		dress <u>950 Herndon Parkway, Suite 400</u>
		rndon, VA 20170
	Te	lephone # <u>610-952-5100</u>
	Em	nail <u>ilombardo@spotlessbrands.com</u>
2.	Lo	cation of Development
	a.	County Bucks
	b.	. ,
	C.	Address or Coordinates 545 West Butler Avenue, Chalfont, PA 18914
	d.	Tax Parcel # TMP's 26-006-101-001
	e.	USGS Quad Name <u>Doylestown</u>
		inches up <u>3.0</u> over <u>3.0</u>
	_	from bottom right corner of map.
	f.	Located in a High Quality/Exceptional Value watershed?  ☐ Yes ☐ No
3.	Ту	pe of Development Proposed (check appropriate box)
		Residential
	De	escribe
	De se	Commercial Institutional scribe Proposed 2,904 sf carwash with public water & wer
		Brownfield Site Redevelopment Other (specify)
4.	Siz	
Τ.		# of lots 1 # of EDUs 18
		# of lots since 5/15/72 1
		Development Acreage 0.65
		Remaining Acreage 0
5.	Se	wage Flows 4,464 gpd
3.		pposed Sewage Disposal Method (check applicable boxes)
<i>J</i> .		
	a.	<ul><li>☑ Sewerage System</li><li>☑ Existing (connection only)</li><li>☐ New (extension)</li></ul>
		☐ Private
		☐ Pump Station(s)/Force Main ☐ Gravity
		Name of existing system being extended
		0 , 0
		Interceptor Name
		Treatment Facility Name Chalfont-New Britain Township
		Wastewater Treatment Plant
		NPDES Permit #
	b.	☐ Construction of Treatment Facility
		☐ With Stream Discharge
		☐ With Land Application (not including IRSIS)
		Other
		Repair?
		Name of waterbody where point of discharge is proposed (if stream discharge)

			Onlot Sewage Disposal Systems	
			(check appropriate box) Individual onlot system(s) (including	IRSIS)
			Community onlot system	,
			Large-Volume onlot system	
	d.		Retaining tanks	
		Nun	nber of Holding Tanks	
		Nun	nber of Privies	
7.			quest Sewage Facilities Planni ctronic format	ng Module forms in
8.	Re	ques	st for Planning Exemption	
		Prof	tection of rare, endangered or threat	ened species
	⊠ or	The	"PNDI Project Environmental Revie	w Receipt" is attached.
	For the exe pro a "I	m," requempt cess PND cume	completed "PNDI Project Planning & (PNDI Form) is attached. I request uired PNDI search for my project. I for will be considered incompleting of my planning exemption request Project Environmental Review Recentation from jurisdictional agencies agencies of the project exercises.	t DEP staff to complete realize that my planning te and that the DEP est will be delayed, until ceipt" and all supporting ies (when necessary)
		Π.		sultant Initials
	b.	⊠ F	Plot Plan Attached	Site Reports Attached
	C.	Onlo	ot Disposal Systems	
		(1)	service area.	
			Signature of Municipal Official)	
		N	Name (Print)	/ Title
		Mur	nicipality (must be same as in 2.b.)	
	Т	elepl	none #	
			I certify that each lot in this subdand is suitable for both a prin sewage disposal system.	
		S	ignature of SEO)	/ Date
		Ū	ignature of GLO)	Dato
				1
		IN	lame (Print)	/ Certification #
				/ Certification #
			ephone #	
		Tele	ephone # I certify that each lot in this subdivi- size	sion is at least 1 acre in
		(3)	ephone # I certify that each lot in this subdivisize Signature of Project Applicant/Agent	sion is at least 1 acre in  / Date
	d.	(3)	ephone # I certify that each lot in this subdivi- size	sion is at least 1 acre in  / Date
	d.	(3) (5) Pub auth	I certify that each lot in this subdivisize  Signature of Project Applicant/Agent Sewerage Service (i.e., owners)	sion is at least 1 acre in  /  /  Date ship by municipality or certify that the facilities at no overload exists or
	d.	(3) (5) Pub auth Bas propis poi	I certify that each lot in this subdivisive  Signature of Project Applicant/Agent olic Sewerage Service (i.e., owners nority)  ed upon written documentation, I cosed for use have capacity and the	sion is at least 1 acre in  /  /  Date ship by municipality or certify that the facilities at no overload exists or
	d.	Tele (3)  (3)  Pub auth Bass propris proprie p	I certify that each lot in this subdivisually size  Signature of Project Applicant/Agent Signature of Project Applicant Signature of Proj	sion is at least 1 acre in  /  /  Date ship by municipality or certify that the facilities at no overload exists or uments.) /
	d.	Tele (3)  (\$\$(\$\$)  Pubbauth  Bass proprise proprietable proprise proprint proprise propriate proprise propriate proprint proprietable proprise propriate propr	I certify that each lot in this subdivisities  Signature of Project Applicant/Agent dic Sewerage Service (i.e., ownershority)  led upon written documentation, I documented by the cosed for use have capacity and the rojected within 5 years. (Attach documentation)	sion is at least 1 acre in  /  /  ) Date ship by municipality or certify that the facilities at no overload exists or uments.)  / Date / Title

Return Correspondence/Forms Van Cleef Engineering	to:
501 N Main Street	
Doylestown, PA 18901	
c/o Sean D. McGranahan	
	DEPARTMENT OF ENVIRONMENTAL PROTECTION
	DEP-SE Region
	2 E Main Street
	Norristown, PA 19401-4915
DEP USE	
Components Sent	
Onlot Disposal	
Collection and Treatment  Planning Agency Review	
Exempt from Planning	
Code Date	
	"Fold Here"



March 14, 2025

#### **Project Narrative – DEP Sewage Planning**

TMP No. 26-006-101-001

Location: 545 West Butler Avenue, Chalfont, PA 18914

Project Name: Spotless Carwash New Britain Township, Bucks County

VCEA #: 24-02-NBR

Existing Site Description – The subject property is located at 545 West Butler Avenue, Chalfont, PA 18914. It can also be identified as TMP no. 26-006-101-001. The property consists of 0.65 acres of land that is currently improved with a convenience store and paved parking.

<u>Proposed Improvements</u> – The applicant's proposal is to remove the convenience store and paved parking lot and construct a 2,904-sf carwash facility with the associated site features (parking, trash enclosure, etc.).

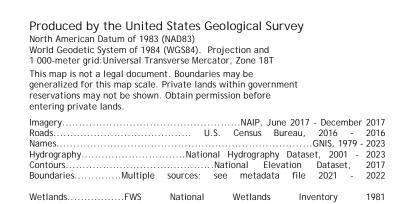
Public water and sewer are proposed for the carwash facility. Sewage flows will be transmitted to the sewer main in West Butler Avenue via gravity line, the intent is to utilize the existing 8" sewer lateral on site.

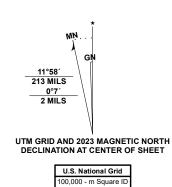
<u>Sewage Flows</u> – It is anticipated that construction will commence in Winter, 2025 and be completed within 18 months. No other phases are reserved for future development; the project will be constructed in a single phase. The projected sewage flows presented below are consistent with New Britain Township (1 EDU = 250 gpd):

18 EDUs at 250 gpd = 4,464 gpd (see calculations on next page).

215-345-1876

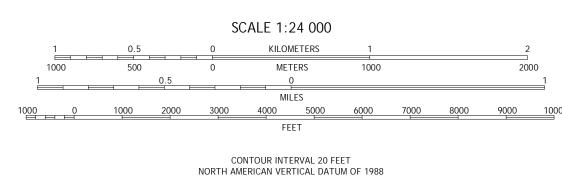




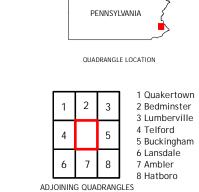


VK

Grid Zone Designation



This map was produced to conform with the National Geospatial Program US Topo Product Standard.





545 West Butler Avenue New Britain Township VCEA No. 24-02-NBR

#### **VEHICLE SEWAGE FLOW**

2. Gallons of water used per car -----> 399 cars/day 11.10 gallons/car

Total proposed = 4,433.52 gpd

#### **PROPOSED SEWAGE FLOW**

1. Toilet waste per person = 5 gpd/person (Ch. 73.17)

6 Staff x 5 gpd/ person = 30 gpd

Total proposed = 30 gpd

### **Proposed EDU's**

250 gpd = 1.0 edu per CNBJSA standards

**4,464 gpd/ 250 gpd/edu =** 17.85 edu's

Round-up to 18 edu's



Site (City / ST) Chalfont PA
Date 12/9/2024

#### **ESTIMATED WATER USAGE ANALYSIS**

APPLICATION			WATER		JET		APPLICATION	USE	FLOW RATE	RECLAIM	FRESH WATER	WELL WATER
NUMBER	FUNCTION	In Use?	TYPE	SIZE	QUANTITY	GPM/JET	TIME	FACTOR	GAL/MIN	GAL/CAR	GAL/CAR	GAL/CAR
1	Presoak Foam Sticks	yes	fresh	Foam Sticks	2	1.63	17.73	100.00%	3.25		1.0	
1	Presoak K-Nozzles	yes	fresh	K-Nozzles	4	0.56	17.73	100.00%	2.25		0.7	
2	Lava Sheet Foamer	yes	fresh	Foamer Tube	1	4	17.73	100.00%	4		1.2	
3	CTA #1	yes	fresh	SH	2	1	4.73	58.13%	2		0.1	
4	CTA #2	yes	fresh	SH	2	1	4.73	58.13%	2		0.1	
5	SF 63 Van High water	yes	reclaim	K-Sticks	2	1	3.55	100.00%	2	0.1		
6	Wrap #1 Foamer	yes	fresh	Elephant Ear	2	1	17.73	100.00%	2		0.6	
7	Wrap #1 Water	yes	reclaim	K-Sticks	2	1	3.55	100.00%	2	0.1		
8	Top Brush #1	yes	reclaim	Elephant Ear	1	1	17.73	100.00%	1	0.3		
9	Wrap #2 Foamer	yes	fresh	Elephant Ear	2	1	17.73	100.00%	2		0.6	
10	Wrap #2 Water	yes	reclaim	K-Sticks	2	1	3.55	100.00%	2	0.1		
11	SFB21 Rocker Water	yes	reclaim	K-Sticks	2	1	3.55	100.00%	2	0.1		
12	Triple Polish	yes	fresh	Elephant Ears	3	1.5	17.73	58.13%	4.5		0.8	
13	Undercarriage Flush	yes	reclaim	F.T.	12	0.2	17.73	58.13%	2.4	0.4		
14	Omni Top	yes	fresh	02	20	1	17.73	100.00%	20		5.9	
15	Omni Wheels	yes	reclaim	02	20	1	17.73	100.00%	20	5.9		
16	Omni Sides	yes	reclaim	02	20	1	17.73	100.00%	20	5.9		
21	Hot Wax	yes	fresh	Cyclone	3	2.67	17.73	41.87%	8		1.0	
22	Top Brush #2 Foam	yes	reclaim	Elephant Ear	1	1	17.73	100.00%	1	0.3		
23	Rain Repellent Arch	yes	fresh	Top Sealer	4	0.81	17.73	58.13%	3.25		0.6	
24	LP Mirror Rinse	yes	fresh	RAINBAR	1	4	2.36	100.00%	4		0.2	
26	Drying Agent	yes	fresh	RAINBAR	1	2	17.73	100.00%	2		0.6	
27	Sealer Wax	yes	fresh	RAINBAR	1	2	17.73	58.13%	2		0.3	
28	Final Rinse	yes	fresh	RAINBAR	1	4	17.73	100.00%	4		1.2	
33	Ceramic Shield Step 1	yes	fresh	Foamer Tube	1	4	17.73	41.87%	4		0.5	
34	Ceramic Shield Step 2	yes	fresh	Elephant Ear	2	1	17.73	41.87%	2		0.2	
35	Ceramic Shield Step 3	yes	fresh	Cyclone	1	8	17.73	41.87%	8		1.0	
35	Ceramic Shield Step 4	yes	fresh	Cyclone	1	8	17.73	41.87%	8		1.0	
		•							GAL/CAR	13.3	17.4	0.0

CONVEYOR SPEED (CPH)
AVERAGE APPLICATION TIME IN SECONDS:

145 17.73

AVERAGE CARS PER MONTH:

AVERAGE EVAPORATION AND CARRY OUT PER CAR ESTIMATED GALLONS PER MONTH AND PER DAY

ESTIMATED AVERAGE RECLAIMED PORTION:

43.32%

Washes per year; in year 3 145,787 Avg Cars/Mo Avg Cars/Day 12,149 399.42

6.3						
	TOTAL WATER	RECLAIMED	FRESH WATER	H2O LOSS/EVAP [	DISCHARGE TO SEWER	
GPC	30.70	13.30	17.40	6.3	11.10	
MONTHLY TOTAL	373,003.51	161,569.05	211,434.46	76,538.18	134,896.29	
DAILY TOTAL	12,263.13	5,311.86	6,951.27	2,516.32	4,434.95	

#### 1. PROJECT INFORMATION

Project Name: Spotless Carwash - 545 West Butler Avenue

Date of Review: 3/12/2025 03:04:34 PM

Project Category: Development, New commercial/industrial development (store, gas station, factory)

Project Area: 1.11 acres

County(s): **Bucks**; **Montgomery** 

Township/Municipality(s): New Britain Township

ZIP Code:

Quadrangle Name(s): DOYLESTOWN

Watersheds HUC 8: Crosswicks-Neshaminy

Watersheds HUC 12: West Branch Neshaminy Creek

Decimal Degrees: 40.266252, -75.228459

Degrees Minutes Seconds: 40° 15' 58.5058" N, 75° 13' 42.4523" W

#### 2. SEARCH RESULTS

Agency	Results	Response
PA Game Commission	No Known Impact	No Further Review Required
PA Department of Conservation and Natural Resources	No Known Impact	No Further Review Required
PA Fish and Boat Commission	No Known Impact	No Further Review Required
U.S. Fish and Wildlife Service	No Known Impact	No Further Review Required

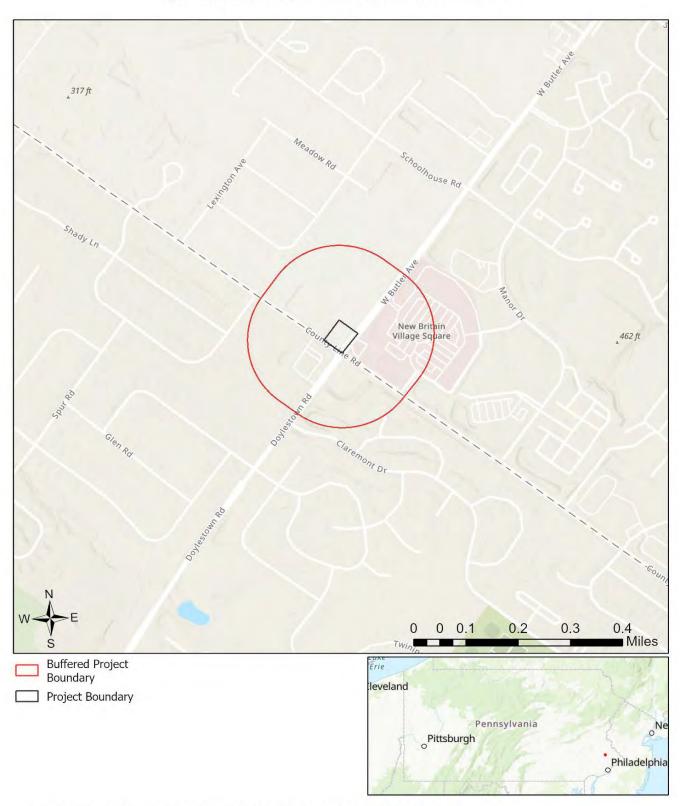
As summarized above, Pennsylvania Natural Diversity Inventory (PNDI) records indicate no known impacts to threatened and endangered species and/or special concern species and resources within the project area. Therefore, based on the information you provided, no further coordination is required with the jurisdictional agencies. This response does not reflect potential agency concerns regarding impacts to other ecological resources, such as wetlands.

## Spotless Carwash - 545 West Butler Avenue



Source: Esri, Maxar, Earthstar Geographics, and the GIS User Community Sources: Esri, TomTom, Garmin, FAO, NOAA, USGS, © OpenStreetMap contributors, and the GIS User Community

## Spotless Carwash - 545 West Butler Avenue



Sources: Esri, TomTom, Garmin, FAO, NOAA, USGS, © OpenStreetMap contributors, and the GIS User Community Sources: Esri, Maxar, Airbus DS, USGS, NGA, NASA, CGIAR, N Robinson, NCEAS, NLS, OS, NMA, Geodatastyrelsen, Rijkswaterstaat, GSA,

### RESPONSE TO QUESTION(S) ASKED

Q1: The proposed project is in the range of the Indiana bat. Describe how the project will affect bat habitat (forests, woodlots and trees) and indicate what measures will be taken in consideration of this. Round acreages up to the nearest acre (e.g., 0.2 acres = 1 acre).

Your answer is: No forests, woodlots or trees will be affected by the project.

**Q2:** Is tree removal, tree cutting or forest clearing of 40 acres or more necessary to implement all aspects of this project?

Your answer is: No

#### 3. AGENCY COMMENTS

Regardless of whether a DEP permit is necessary for this proposed project, any potential impacts to threatened and endangered species and/or special concern species and resources must be resolved with the appropriate jurisdictional agency. In some cases, a permit or authorization from the jurisdictional agency may be needed if adverse impacts to these species and habitats cannot be avoided.

These agency determinations and responses are **valid for two years** (from the date of the review), and are based on the project information that was provided, including the exact project location; the project type, description, and features; and any responses to questions that were generated during this search. If any of the following change: 1) project location, 2) project size or configuration, 3) project type, or 4) responses to the questions that were asked during the online review, the results of this review are not valid, and the review must be searched again via the PNDI Environmental Review Tool and resubmitted to the jurisdictional agencies. The PNDI tool is a primary screening tool, and a desktop review may reveal more or fewer impacts than what is listed on this PNDI receipt. The jurisdictional agencies **strongly advise against** conducting surveys for the species listed on the receipt prior to consultation with the agencies.

#### **PA Game Commission**

#### **RESPONSE:**

No Impact is anticipated to threatened and endangered species and/or special concern species and resources.

# PA Department of Conservation and Natural Resources RESPONSE:

No Impact is anticipated to threatened and endangered species and/or special concern species and resources.

# PA Fish and Boat Commission RESPONSE:

No Impact is anticipated to threatened and endangered species and/or special concern species and resources.

# U.S. Fish and Wildlife Service RESPONSE:

No impacts to **federally** listed or proposed species are anticipated. Therefore, no further consultation/coordination under the Endangered Species Act (87 Stat. 884, as amended; 16 U.S.C. 1531 et seq. is required. Because no take of federally listed species is anticipated, none is authorized. This response does not reflect potential Fish and Wildlife Service concerns under the Fish and Wildlife Coordination Act or other authorities.

Project Search ID: PNDI-836220

#### 4. DEP INFORMATION

The Pa Department of Environmental Protection (DEP) requires that a signed copy of this receipt, along with any required documentation from jurisdictional agencies concerning resolution of potential impacts, be submitted with applications for permits requiring PNDI review. Two review options are available to permit applicants for handling PNDI coordination in conjunction with DEP's permit review process involving either T&E Species or species of special concern. Under sequential review, the permit applicant performs a PNDI screening and completes all coordination with the appropriate jurisdictional agencies prior to submitting the permit application. The applicant will include with its application, both a PNDI receipt and/or a clearance letter from the jurisdictional agency if the PNDI Receipt shows a Potential Impact to a species or the applicant chooses to obtain letters directly from the jurisdictional agencies. Under concurrent review, DEP, where feasible, will allow technical review of the permit to occur concurrently with the T&E species consultation with the jurisdictional agency. The applicant must still supply a copy of the PNDI Receipt with its permit application. The PNDI Receipt should also be submitted to the appropriate agency according to directions on the PNDI Receipt. The applicant and the jurisdictional agency will work together to resolve the potential impact(s). See the DEP PNDI policy at <a href="https://conservationexplorer.dcnr.pa.gov/content/resources">https://conservationexplorer.dcnr.pa.gov/content/resources</a>.



### 5. ADDITIONAL INFORMATION

The PNDI environmental review website is a preliminary screening tool. There are often delays in updating species status classifications. Because the proposed status represents the best available information regarding the conservation status of the species, state jurisdictional agency staff give the proposed statuses at least the same consideration as the current legal status. If surveys or further information reveal that a threatened and endangered and/or special concern species and resources exist in your project area, contact the appropriate jurisdictional agency/agencies immediately to identify and resolve any impacts.

For a list of species known to occur in the county where your project is located, please see the species lists by county found on the PA Natural Heritage Program (PNHP) home page (<a href="www.naturalheritage.state.pa.us">www.naturalheritage.state.pa.us</a>). Also note that the PNDI Environmental Review Tool only contains information about species occurrences that have actually been reported to the PNHP.

#### 6. AGENCY CONTACT INFORMATION

# PA Department of Conservation and Natural Resources

Bureau of Forestry, Ecological Services Section 400 Market Street, PO Box 8552 Harrisburg, PA 17105-8552

Email: RA-HeritageReview@pa.gov

#### **PA Fish and Boat Commission**

Name:

Division of Environmental Services 595 E. Rolling Ridge Dr., Bellefonte, PA 16823

Email: RA-FBPACENOTIFY@pa.gov

#### U.S. Fish and Wildlife Service

Pennsylvania Field Office Endangered Species Section 110 Radnor Rd; Suite 101 State College, PA 16801 Email: <a href="mailto:IR1\_ESPenn@fws.gov">IR1\_ESPenn@fws.gov</a> NO Faxes Please

# PA Game Commission

Bureau of Wildlife Management Division of Environmental Review 2001 Elmerton Avenue, Harrisburg, PA 17110-9797

Email: RA-PGC PNDI@pa.gov

**NO Faxes Please** 

### 7. PROJECT CONTACT INFORMATION

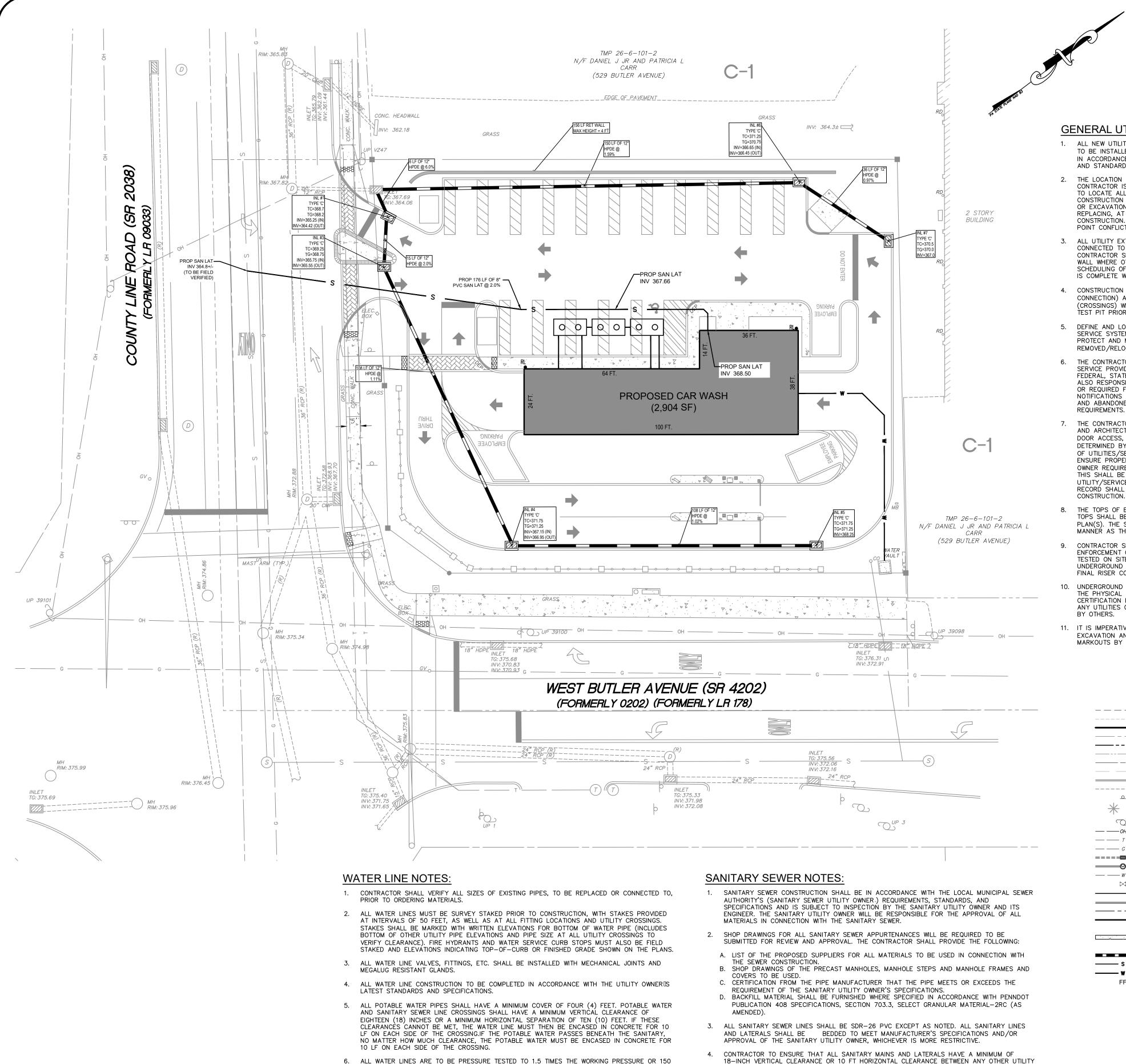
Company/Business Name:		
Address:	A Count Of the Count of the	
City, State, Zip:		5((a, 2))\a ( )
Phone:()	Fax:()	
Email:		100 Jan 18 18 18 18 18 18 18 18 18 18 18 18 18
B. CERTIFICATION		
size/configuration, project t	ject information contained in this receipt (includ type, answers to questions) is true, accurate ar on changes, or if the answers to any questions	nd complete. In addition, if the project type,
change, I agree to re-do the	e online environmental review.	
annlicant/project proponent	t signature	date

# Public Sewer System Available Capacity Determination

Developer Name: *
Project Name: *
Municipality & County: _*
Project Description: *
*
Project Flows: * gpd
The agent(s) responsible for completing the Chapter 94 report for the collection and/or the conveyance facilities is to sign below to affirm that the collection and/or conveyance facilities have adequate capacity and are able to provide service to the proposed development reference above.
Collection System
Name of Agency, Authority, or Municipality: * Chalfont-New Britain Township Joint Sewer Authority
Name of Responsible Agent: * John Larson
I do hereby certify that the sewerage facilities have capacity to receive and treat the sewage flows from the applicant's proposed development and that the additional wasteload from the proposed development will not create a hydraulic or organic overload or a 5-year projected overload.
Agent Signature: * Date: :* 3/14/2025
Conveyance System(s)
First Conveyance System
Name of Agency, Authority, or Municipality:* Chalfont-New Britain Township Joint Sewer Authority
Name of Responsible Agent: * John Larson
I do hereby certify that the sewerage facilities have capacity to receive and treat the sewage flows from the applicant's proposed development and that the additional wasteload from the proposed development will not create a hydraulic or organic overload or a 5-year projected overload.
Agent Signature: * Jhhhhhhhhhhhhhhhhhhhhhhhhhhhhhhhhhhh

# Public Sewer System Available Capacity Determination

# **Second Conveyance System** Name of Agency, Authority, or Municipality: \*\_\_\_\_ Name of Responsible Agent: \* I do hereby certify that the sewerage facilities have capacity to receive and treat the sewage flows from the applicant's proposed development and that the additional wasteload from the proposed development will not create a hydraulic or organic overload or a 5-year projected overload. Agent Signature: \* Date: :\* **Third Conveyance System** Name of Agency, Authority, or Municipality: \* Name of Responsible Agent: \* I do hereby certify that the sewerage facilities have capacity to receive and treat the sewage flows from the applicant's proposed development and that the additional wasteload from the proposed development will not create a hydraulic or organic overload or a 5-year projected overload. Agent Signature: \* Date: :\* **Treatment Facility** The treatment facility permittee must sign below to affirm that this facility has adequate treatment capacity and is able to provide wastewater treatment services for the proposed development referenced above. Name of Agency, Authority, or Municipality: \* Chalfont-New Britain Township Joint Sewer Authority Name of Responsible Agent: \* John Larson I do hereby certify that the sewerage facilities have capacity to receive and treat the sewage flows from the applicant's proposed development and that the additional wasteload from the proposed development will not create a hydraulic or organic overload or a 5-year projected overload. Agent Signature: \* 3/14/2025 Additional Information:



P.S.I., WHICHEVER IS GREATER, IN SECTIONS BETWEEN VALVES. THE TEST PRESSURE SHALL BE

MAINTAINED FOR A PERIOD NOT LESS THAN 2 HOURS. SHOULD ANY PRESSURE TEST DISCLOSE

AN INABILITY TO HOLD THE TEST PRESSURE, THE CONTRACTOR SHALL LOCATE AND CORRECT

7. THE INSTALLATION OR REPAIR OF ANY UNDERGROUND FACILITIES OR PIPING WHICH CONNECTS TO

OR FURNISHES WATER FOR THE FIRE PROTECTION SPRINKLER SYSTEM SHALL BE PERFORMED

ONLY BY A LICENSED UTILITY CONTRACTOR, FIRE PROTECTION SPRINKLER CONTRACTOR, OR

LICENSED PLUMBER. A COPY OF THE LICENSE OR CERTIFICATE OF COMPETENCY SHALL BE

ALL STORM CONVEYANCE PIPE SHALL BE SMOOTH LINED DOUBLE WALLED HIGH DENSITY

POLYETHYLENE PIPE (HDPE), UNLESS OTHERWISE SPECIFIED. ANY ROOF LEADERS SHALL BE

3. LANDSCAPING, FENCES AND STRUCTURES SHALL BE PLACED A MINIMUM OF 3 FEET AWAY FROM

IF A CONFLICT ARISES DURING THE INSTALLATION OF ANY PART OF THE STORM SEWER SYSTEM

DEFECTS AND RETEST TO THE SATISFACTION OF THE UTILITY OWNER

PROVIDED TO THE INSPECTOR AT THE FINAL INSPECTION.

THE ENGINEER IS TO BE NOTIFIED IMMEDIATELY IN WRITING.

STORM SEWERS OUTSIDE OF THE RIGHT OF WAY OF STREETS.

STORM SEWER NOTES:

- 18-INCH VERTICAL CLEARANCE OR 10 FT HORIZONTAL CLEARANCE BETWEEN ANY OTHER UTILITY CROSSING. OTHERWISE CONTRACTOR SHALL ENCASE MAIN OR LATERAL IN CONCRETE WITH A MINIMUM OF 10 LF ON EACH SIDE OF THE CROSSING. CLEANOUTS ARE TO BE PLACED AT MAXIMUM INTERVALS OF 100-FT AND AT ALL BENDS.
- 5. ALL SANITARY SEWER MANHOLES PROPOSED TO BE INSTALLED OR ADJUSTED SHALL BE INSTALLED WITH WATERTIGHT FRAMES AND COVERS. ANY MANHOLE TO BE INSTALLED OR ADJUSTED OUTSIDE OF PAVED AREAS SHALL BE INSTALLED A MINIMUM OF 6. ABOVE FINISH
- 6. CONTRACTOR SHALL VERIFY ALL SIZES, MATERIALS, AND CONDITION OF EXISTING PIPES, TO BE
- 7. ALL EXISTING MANHOLES SHALL BE TESTED PER SANITARY UTILITY OWNER STANDARDS TO ASSURE NO LEAKS EXIST WITHIN THE STRUCTURES.
- 8. ALL PROPOSED SANITARY SEWER FACILITIES WILL BE OWNED AND PERPETUALLY MAINTAINED BY THE PROPERTY OWNER IN PERPETUITY.
- 9. ALL MATERIALS USED, AND DETAILS OF CONSTRUCTION, INSTALLATION PROCEDURES, AND REQUIREMENTS WITH RESPECT TO THE SANITARY SEWERAGE FACILITIES SERVED BY THE LOCAL MUNICIPAL SEWER AUTHORITY'S (SANITARY SEWER UTILITY OWNER) SHALL BE IN ACCORDANCE WITH THE AUTHORITY'S STANDARD SPECIFICATIONS FOR CONSTRUCTION OF SANITARY SEWERS, FORCE MAINS, PUMPING STATIONS AND WASTEWATER TREATMENT.

### GENERAL UTILITIES NOTES:

- ALL NEW UTILITIES/SERVICES, INCLUDING ELECTRIC, TELEPHONE, CABLE TV, ETC. ARE TO BE INSTALLED UNDERGROUND. ALL NEW UTILITIES/SERVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE UTILITY/SERVICE PROVIDER INSTALLATION SPECIFICATIONS
- THE LOCATION OF ALL EXISTING AND PROPOSED SERVICES ARE APPROXIMATE. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING WITH LOCAL UTILITY COMPANIES TO LOCATE ALL PUBLIC OR PRIVATE UTILITIES WHICH LIE IN OR ADJACENT TO THE CONSTRUCTION SITE PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION, REMOVAL OR EXCAVATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING, AT HIS EXPENSE, ALL EXISTING UTILITIES DAMAGED DURING CONSTRUCTION. SANITARY SEWER AND ALL OTHER UTILITY SERVICE CONNECTION POINT CONFLICTS SHALL BE REPORTED IMMEDIATELY IN WRITING TO THE ENGINEER.
- 3. ALL UTILITY EXTENSIONS FROM THE BUILDING BY OTHER TRADES SHALL BE CONNECTED TO THE CONTINUATION UTILITY LINES ON THE SITE. THE GENERAL CONTRACTOR SHALL MAKE THE CONNECTIONS AT A POINT 50 FROM THE BUILDING WALL WHERE OTHER TRADES HAVE ENDED THEIR WORK. THE GC SHALL COORDINATE SCHEDULING OF THESE CONNECTIONS WITH OTHER TRADES TO ASSURE THE SYSTEM IS COMPLETE WHEN FLOW WILL BEGIN IN THE LINES.
- 4. CONSTRUCTION SHALL COMMENCE BEGINNING AT THE LOWEST INVERT (POINT OF CONNECTION) AND PROGRESS UP GRADIENT. PROPOSED INTERFACE POINTS (CROSSINGS) WITH EXISTING UNDERGROUND UTILITIES SHALL BE FIELD VERIFIED BY TEST PIT PRIOR TO COMMENCEMENT OF CONSTRUCTION.
- 5. DEFINE AND LOCATE VERTICALLY AND HORIZONTALLY ALL ACTIVE UTILITY AND/OR SERVICE SYSTEMS THAT ARE NOT TO REMAIN. THE CONTRACTOR IS RESPONSIBLE TO PROTECT AND MAINTAIN ALL ACTIVE SYSTEMS THAT ARE NOT BEING REMOVED/RELOCATED DURING SITE ACTIVITY.
- 6. THE CONTRACTOR SHALL FAMILIARIZE THEMSELVES WITH THE APPLICABLE UTILITY SERVICE PROVIDER REQUIREMENTS AND IS RESPONSIBLE FOR COMPLIANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS, CODES AND REGULATIONS.THE CONTRACTOR IS ALSO RESPONSIBLE FOR ALL COORDINATION REGARDING UTILITY WORK AS IDENTIFIED OR REQUIRED FOR PROJECT. THE CONTRACTOR SHALL PROVIDE THE OWNER WRITTEN NOTIFICATIONS THAT THE EXISTING UTILITIES AND SERVICES HAVE BEEN TERMINATED AND ABANDONED IN ACCORDANCE WITH JURISDICTION AND UTILITY COMPANY
- 7. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATION OF SITE PLAN DOCUMENTS AND ARCHITECTURAL DESIGN FOR EXACT BUILDING UTILITY CONNECTION LOCATIONS, DOOR ACCESS, AND EXTERIOR GRADING. (THE UTILITY SERVICE SIZES ARE TO BE DETERMINED BY ARCHITECT.) THE CONTRACTOR SHALL COORDINATE INSTALLATION OF UTILITIES/SERVICES WITH THE INDIVIDUAL COMPANIES TO AVOID CONFLICTS AND ENSURE PRÓPER DEPTHS ARE ACHIEVED. IN ADDITION TO MEETING THE UTILITY OWNER REQUIREMENTS, THE UTILITY TIE-INS/CONNECTIONS SHALL BE COORDINATED. THIS SHALL BE COMPLETED PRIOR TO CONNECTING TO THE EXISTING UTILITY/SERVICE. WHERE CONFLICTS EXIST WITH THESE PLANS, THE ENGINEER OF RECORD SHALL BE NOTIFIED IN WRITING TO RESOLVE SAID CONFLICTS PRIOR TO
- 8. THE TOPS OF EXISTING MANHOLES, INLET STRUCTURES, AND SANITARY CLEANOUT TOPS SHALL BE ADJUSTED, IF REQUIRED, TO MATCH THE GRADING AND UTILITY PLAN(S). THE SANITARY CLEANOUT TOPS SHALL BE REINSTALLED IN THE SAME MANNÈR AS THE ORIGINAL CLEANOUTS.
- CONTRACTOR SHALL CONTACT THE LOCAL (I.E. TOWNSHIP, BOROUGH, ETC.) CODE ENFORCEMENT OFFICE WHEN UNDERGROUND PIPING IS BEING HYDROSTATICALLY TESTED ON SITE. APPLICANTS ARE ALSO REMINDED THAT FLUSHING OF THE UNDERGROUND PIPING SHALL BE WITNESSED BY A TOWNSHIP OFFICIAL PRIOR TO FINAL RISER CONNECTIONS, PER NFPA 13.
- 10. UNDERGROUND UTILITY MARKOUTS BY THE UTILITY COMPANIES WERE ORDERED AND THE PHYSICAL LOCATION OF THESE MARKOUTS ARE SHOWN HEREON, HOWEVER, NO CERTIFICATION IS MADE BY VCEA AS TO THE ACTUAL UNDERGROUND POSITION OF ANY UTILITIES OR TO THE COMPLETENESS OR ACCURACY OF THE UTILITY MARKOUTS
- 11. IT IS IMPERATIVE THAT UTILITY COMPANIES ARE NOTIFIED PRIOR TO ANY EXCAVATION AND/OR CONSTRUCTION. CALL 1-800-242-1776 TO ORDER UTILITY MARKOUTS BY OTHERS.

EXISTING MINOR CONTOUR

EXISTING PROPERTY LINE

----- EXISTING ADJACENT PROPERTY LINE ---- EXISTING LEGAL RIGHT OF WAY

### **LEGEND** ---- EXISTING MAJOR CONTOUR

---- EXISTING EASEMENT

------ EXISTING ROAD CENTERLINE ---- EXISTING ROAD EXISTING CURB ---- EXISTING EDGE OF DRIVE EXISTING SIGN EXISTING TREES EXISTING UTILITY POLES — — OH — EXISTING OVERHEAD ELECTRIC — — T — EXISTING TELEPHONE — — G — EXISTING GAS MAIN ===== EXISTING STORM SEWER EXISTING SANITARY SEWER — — W — EXISTING WATER MAIN EXISTING WATER VALVE PROPOSED CONTOUR PROPOSED CURB ----- PROPOSED EASEMENT PROPOSED RIGHT OF WAY PROPOSED SETBACK LINE PROPOSED SIDEWALK

PROPOSED SIGN PROPOSED STORM SEWER PROPOSED WATER LATERAL FINISHED FLOOR ELEVATION PROPOSED SPOT ELEVATION

- THE CONTRACTOR, DURING THE PERFORMANCE OF ALL WORK ASSOCIATED WITH THE CONSTRUCTION OF THE PROJECT, IS RESPONSIBLE FOR COMPLIANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS, CODES AND REGULATIONS.
- 13. ANY DISCREPANCIES FOUND BETWEEN THE DRAWINGS AND SITE CONDITIONS OR ANY INCONSISTENCIES OR AMBIGUITIES IN DRAWINGS SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER, IN WRITING, WHO SHALL PROMPTLY ADDRESS SUCH INCONSISTENCIES OR AMBIGUITIES. WORK DONE BY THE CONTRACTOR AFTER HIS DISCOVERY OF SUCH DISCREPANCIES, INCONSISTENCIES OR AMBIGUITIES SHALL BE DONE AT THE CONTRACTOR'S RISK.
- 14. THE BUCKS COUNTY WATER AND SEWER AUTHORITY IS TO BE CONTACTED ONE WEEK PRIOR TO SANITARY SEWER CONSTRUCTION, AND 72 HOURS PRIOR TO EXCAVATING NEAR, AND CONNECTING INTO ANY EXISTING SANITARY SEWER.
- 15. ALL SANITARY SEWER CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE
- WITH THE BUCKS COUNTY WATER AND SEWER AUTHORITY (BCWSA) SPECIFICATIONS. a.) THE MINIMUM ALLOWABLE LATERAL SLOPE SHALL BE 0.02 FT/FT.
- PVC PIPE, TWO FEET (2') BELOW GRADE. c.) CONTRACTOR SHALL MAKE TESTS AS DIRECTED BY THE ENGINEER TO ASCERTAIN IF THE PIPE IS PROPERLY ALIGNED AND THE JOINTS ARE LIGHT. ENGINEER MUST WITNESS ALL TESTS OR THE CONTRACTOR WILL NEED TO RETEST IN THE PRESENCE OF THE ENGINEER AT THE SOLE COST OF THE CONTRACTOR.

b.) MAGNETIC - TYPE LOCATOR TAPE SHALL BE LAID DIRECTLY OVER THE TOP OF

- CONTRACTOR TO CONNECT NEW BUILDING UTILITY CONNECTIONS TO EXISTING WATER
- 17. FINAL DESIGN OF PROPOSED WATER AND SEWER FACILITIES ARE SUBJECT TO THE REVIEW AND APPROVAL BY THE SERVICING WATER AND SEWER AUTHORITY. ALL WATER MAIN CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH UPPER DUBLIN TOWNSHIP WATER DEPARTMENT SPECIFICATIONS.
- LATERALS ARE TO BE LOCATED AS SHOWN ON THE PLAN.

AND SEWER.

BE UTILIZED.

- WATER SERVICE IS TO BE LOCATED A MIN. OF 1.5 FEET ABOVE THE SANITARY LATERAL WITH A MINIMUM COVER OF 48".
- 20. NO GROUNDING OF ELECTRICAL SERVICE TO COPPER WATER SERVICE IS PERMITTED. CABLE AND TELECOMM UTILITIES TO FOLLOW ELECTRIC FACILITIES ALIGNMENTS,
- 18" VERTICAL CLEARANCE OR 10 FEET OF HORIZONTAL CLEARANCE SHALL BE PROVIDED BETWEEN THE SANITARY SEWER AND ALL OTHER UTILITIES. OTHERWISE, CONCRETE ENCASEMENT OF THE SANITARY SEWER SHALL BE PROVIDED.
- ALL DOWNSPOUT LOCATIONS TO BE OUTFITTED WITH A OVERFLOW / CLEANOUT. OVERFLOWS SHALL HAVE A SPLASHBLOCK INSTALLED AT THEIR OUTFALL LOCATION. 23. PROPOSED ROOF DRAIN CONNECTIONS SHOWN SCHEMATICALLY, FINAL DESIGN TO BE
- PROVIDED BY ARCHITECT AT TIME OF BUILDING PERMITS. 24. MATERIALS SHOWN HEREON, SUCH AS ADS N-12 PIPE, OR APPROVED EQUAL SHALL
- 25. REFER TO ARCHITECTURAL, MECHANICAL, ELECTRICAL AND PLUMBING PLANS FOR ADDITIONAL COORDINATION W/ IN 5' OF BUILDINGS.
- 26. PUBLIC SEWER SERVICE WILL BE PROVIDED BY BCSWA, AND CONSTRUCTION OF ALL SANITARY SEWER IMPROVEMENTS SHALL COMPLY WITH AUTHORITY STANDARDS.
- 27. ALL BURIED GRAVITY SANITARY SEWER MAINS AND LATERALS SHALL BE CLASS 50 DUCTILE IRON PIPE OR PVC SDR-26 OR SDR-35.
- 29. GENERAL CONTRACTOR (GC) TO COORDINATE LIGHT POLE LOCATIONS WITH TRADES.

28. THRUST BLOCKS SHALL BE PROVIDED AT ALL BENDS AND TEES.

# **GENERAL CONSTRUCTION NOTES:**

1. NO CERTIFICATION IS MADE BY VAN CLEEF ENGINEERING ASSOCIATES AS TO THE POSITION OF ANY UTILITIES OR TO THE COMPLETENESS OF ANY UTILITIES SHOWN. ALL CONTRACTORS WORKING ON THIS SITE SHALL COMPLY WITH THE REQUIREMENTS OF ACT 287 OF 1974 AS AMENDED BY ACT 50 OF 2017 (UNDERGROUND UTILITY LINE PROTECTION LAW). VCEA HAS REGISTERED THIS SITE WITH THE PA ONE-CALL SYSTEM ON NOVEMBER 14, 2022 (SERIAL NO. 20223183581).

THE FOLLOWING IS A LIST OF REGISTERED UTILITY USERS FOR NEW BRITAIN

CHALFONT, PA. 18914

ADDRESS: 190 SHOEMAKER RE

ADDRESS: 7000 WESTON PKWY

EMAIL: VICTOR.S.WOOD@VERIZON.COM

CONTACT: MIKE KIMBERLY

CONTACT: VICTOR WOOD

COMPANY: COMCAST

EMAIL: RCRESSMAN@NEWBRITAINTOWNSHIP.ORG

EMAIL: MIKE\_KIMBERLY@CABLE.COMCAST.COM

COMPANY VERIZON BUSINESS FORMERLY MCI

COMPANY: CHALFONT NEW BRITAIN TWP JT SEWAGE AUTH ADDRESS: 1645 UPPER STATE RD DOYLESTOWN, PA. 18901 CONTACT: JOHN SCHMIDT EMAIL: JSCHMIDT@CNBSA.ORG COMPANY: PECO AN EXELON COMPANY C/O USIC KING OF PRUSSIA, PA. 19406

EMAIL: NIKKIASIMPKINS@USICLLC.COM COMPANY: NORTH WALES WATER AUTHORITY ADDRESS: 200 W WALNUT ST PO BOX 1339 NORTH WALES, PA. 19454

CONTACT: JOSEPH MURPHY
EMAIL: JMURPHY@NWWATER.COM

2. IT IS IMPERATIVE THAT UTILITY COMPANIES ARE NOTIFIED PRIOR TO ANY EXCAVATION AND/OR CONSTRUCTION. CALL 1-800-242-1776 TO ORDER UTILITY MARKOUTS BY OTHERS.

- 3. THE LOCATIONS OF EXISTING UTILITIES AS SHOWN ARE APPROXIMATE ONLY. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL PUBLIC OR PRIVATE UTILITIES WHICH LIE IN OR ADJACENT TO THE CONSTRUCTION SITE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING, AT HIS EXPENSE, ALL EXISTING UTILITIES DAMAGED DURING CONSTRUCTION.
- 4. THE CONTRACTOR, DURING THE PERFORMANCE OF ALL WORK ASSOCIATED WITH THE CONSTRUCTION OF THE PROJECT, IS RESPONSIBLE FOR COMPLIANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS, CODES AND REGULATIONS.
- 5. ANY DISCREPANCIES FOUND BETWEEN THE DRAWINGS AND SITE CONDITIONS OR ANY INCONSISTENCIES OR AMBIGUITIES IN DRAWINGS SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER, IN WRITING, WHO SHALL PROMPTLY ADDRESS SUCH INCONSISTENCIES OR AMBIGUITIES. WORK DONE BY THE CONTRACTOR AFTER HIS DISCOVERY OF SUCH DISCREPANCIES, INCONSISTENCIES OR AMBIGUITIES SHALL BE DONE AT THE CONTRACTOR'S RISK.
- 6. ALL MATERIAL USED FOR FILL OR BACKFILL SHALL BE FREE OF WOOD, ROOTS, ROCKS, BOULDERS OR ANY OTHER NON-COMPACTIBLE SOIL TYPE MATERIAL. UNSATISFACTORY MATERIALS ALSO INCLUDE MAN-MADE FILLS AND REFUSE DEBRIS DERIVED FROM ANY SOURCE.
- 7. ALL EXCAVATIONS, INCLUDING TRENCHES, SHALL BE KEPT DRY TO PROTECT THEIR
- 8. MATERIALS USED TO CONSTRUCT EMBANKMENTS FOR ANY PURPOSE, BACKFILL AROUND DRAINAGE STRUCTURES OR IN UTILITY TRENCHES OR ANY OTHER DEPRESSIONS REQUIRING FILL OR BACKFILL SHALL BE COMPACTED TO AT LEAST 95% OF MAXIMUM DENSITY AS DETERMINED BY THE STANDARD PROCTOR TEST AS SET OUT IN ASTM STANDARD D-698 UNLESS SPECIFIED ELSEWHERE WITHIN THE CONSTRUCTION PLANS OR IF SPECIFIED DIFFERENTLY BY THE TOWNSHIP ENGINEER. CONTRACTOR TO VERIFY COMPACTION REQUIREMENTS WITH TOWNSHIP ENGINEER'S OFFICE PRIOR TO EXCAVATION.
- 9. ALL CONSTRUCTION DEBRIS INCLUDING EXCESS EXCAVATED MATERIAL, SCRAP WOOD, BRICKS, BLOCKS, ETC. SHALL BE DISPOSED OF BY THE CONTRACTOR IN ACCORDANCE WITH FEDERAL, STATE AND LOCAL REQUIREMENTS.
- 10. ELEVATIONS, DIMENSIONS, AND THE LOCATIONS OF LINEAR FEATURES SHALL BE FIELD VERIFIED BY THE CONTRACTOR.
- 11. MINIMUM SLOPE OF ALL GRASS AREAS SHALL NOT BE LESS THAN 2%, AND FOR PAVED AREAS, NOT LESS THAN 1%.

EV DESCRIPTION AUTH DATE **SERIAL NO. 20223183581** 

Before You Dig Anywhere



now what's below. STOP! CALL 1-800-242-1776 Call before you dig. PA Law requires 3 working days notice PA ONE CALL SYSTEM, INC.

**GRAPHIC SCALE** 10 20

1 INCH = 20 FT PLAN NOTATION

NLY THOSE PLANS WHICH CONTAIN A DIGI MPRESSED, OR COLORIZED INK SEAL OF TH RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERE ALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARE OR THE OWNER DESIGNATED HEREON. AN MODIFICATION, REVISION, DUPLICATION OR WITHOUT THE WRITTEN CONSENT OF VAN CL NGINEERING ASSOCIATES IS PROHIBITE RELIANCE ON THIS PLAN FOR ANY PURPO THER THAN THAT WHICH IS INTENDED SHALL AT THE SOLE DISCRETION AND LIABILITY OF APPLICABLE PARTY.



AN DAVID McGRANAHAN

NSYLVANIA PROFESSIONA NEER NUMBER 085090

VAN CLEEF ENGINEERING ASSOCIATES, LLC 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM PHONE (215) 345-1876

**UTILITY PLAN** 

FOR

SPOTLESS CARWASH

MARCH 21, 20

DRAWN BY

HECKED BY

**B NUMBER** 

TMP'S 26-006-101-001

**NEW BRTIAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 

6 of 16



April 3, 2025 Revised June 6, 2025

Dan Fox Township Manager New Britain Township 207 Park Avenue Chalfont, PA 18914

RE: Flagship Car Wash (Spotless Brands)

545 West Butler Avenue, Chalfont, PA 18914

VCEA No. 24-02-NBR

Dear Mr. Fox:

In conjunction with the Land Development Application for the property (PARID No. 26-006-101-001) address is 545 West Butler Avenue, Chalfont, PA 18914; Please consider the below written waiver requests from requirements in the New Britain Township Subdivision/Land Development Ordinance (SALDO).

#### **WAIVER REQUESTS:**

1. SALDO §22-403 - Preliminary Plan Submission and Review Procedure

The applicant is proposing to combine the Preliminary and Final plan stages therefore shortening the review period required.

2. <u>SALDO §22-706.1.D & SALDO §22-708.6.1</u> – Requiring all proposed curbs shall have a minimum radius of five (5) feet.

In lieu of pavement gore stripping curbing was utilized to delineate the parking area uses, utilizing curb radii less than the required 5 feet.

3. <u>SALDO §22-708.2.F.</u> – Requiring a six (6) foot minimum width sidewalk or walkway when adjacent to parking stalls.

Due to the lot size and configuration of the site, a sidewalk of 4.33 feet was provided along the front of the proposed building in lieu of the required 6 feet width.

4. <u>SALDO §22-711.4.C.</u> – Requiring the top of bottom edge of slopes shall be a minimum of five (5) feet from property lines in order to permit normal rounding of the edge without encroaching onto the abutting property.

#### **OFFICE LOCATIONS**

### www.vancleefengineering.com



Due to the lot size and configuration of the site, the grading associated with the site features extends to the adjoiner property lines.

5. <u>SALDO §22-712.5.E.</u> – Requiring storm sewers to be reinforced concrete pipe with a minimum diameter of 15 inches or equivalent. Storm sewer located outside a public ultimate right-of-way or easement shall be high density corrugated polyethylene.

Due to the lot size and anticipated stormwater runoff, only a maximum pipe size of 12 inches is necessary to convey stormwater for the site.

6. <u>SALDO §22-722.4.C.(1)(a)&(b)</u>, and (2) – Requiring setbacks from Butler Avenue to be 50 feet from principal and accessory buildings. 25 feet from surface parking areas and interior drives and setbacks from all other street lines shall be 50 feet except parking areas and interior drives may be located up to 10 feet from all other street lines.

Due to the lot size and configuration of the site, the interior drive along Bulter Avenue are setback 8 feet from the property line.

7. <u>SALDO §22-722.4.C.(7)(b)</u> – Requiring all street trees to be planted in the planting strip located between the curbline and the sidewalk.

The plantings have been proposed between the sidewalk and the drive-through per the Township's direction.

8. <u>SALDO §22-722.4.D.(6)</u> – Requiring bicycle parking.

Due to the nature of the business, we do not believe bicycle parking is necessary.

# PRELIMINARY / FINAL LAND DEVELOPMENT PLANS

**FOR** 

# FLAGSHIP CARWASH

PREPARED FOR

# SPOTLESS BRANDS

SITUATED IN

# NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA

**OWNER OF RECORD:** 

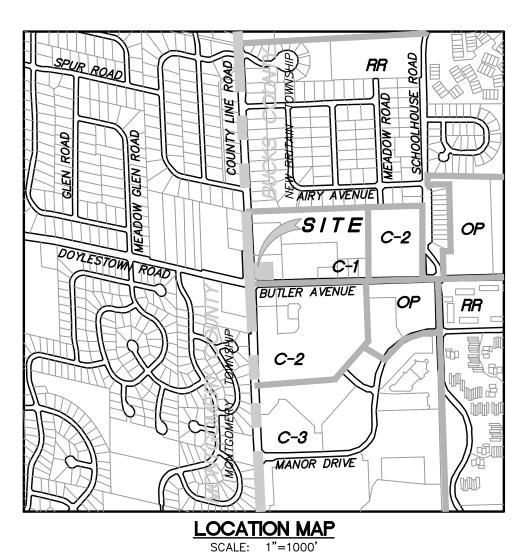
FLAGSHIP PENNSYLVANIA PROPCO, LLC

SITE ADDRESS: 545 WEST BUTLER AVENUE CHALFONT, PA 18914

PARID 26-006-101-001 INSTRUMENT NUMBER 2025004671

**SITE AREA:** 

28,414 SQ. FT. OR 0.652 ACRES



GRAPHIC SCALE

500 1000 2000

1 INCH = 1000 FT

DATE: APRIL 3, 2025 REVISED: JUNE 3, 2025



VAN CLEEF ENGINEERING ASSOCIATES, LLC 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM PHONE (215) 345-1876

# **INDEX OF SHEETS**

# SHEET No. TITLE 1 COVER SHEET

SITE PLAN (SHEET 1 OF 1 TO BE RECORDED)

EXISTING FEATURES PLAN

4 AERIAL PLAN

5 GRADING PLAN

6 UTILITY PLAN

7 LANDSCAPE PLAN 8 LIGHTING PLAN

9 EROSION AND SEDIMENTATION CONTROL PLAN

10 CONSTRUCTION DETAILS

11 UTILITY DETAILS

12 EROSION AND SEDIMENTATION CONTROL NOTES

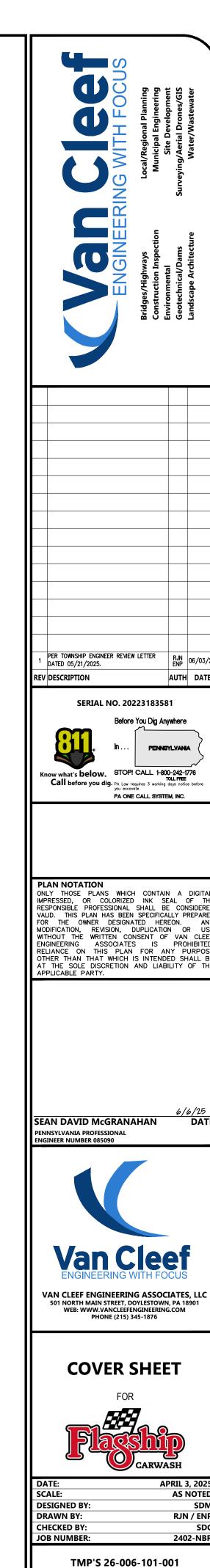
13 EROSION AND SEDIMENTATION CONTROL DETAILS
14 CONSTRUCTION DETAILS

15 CONSTRUCTION DETAILS

16 ADA DETAILS

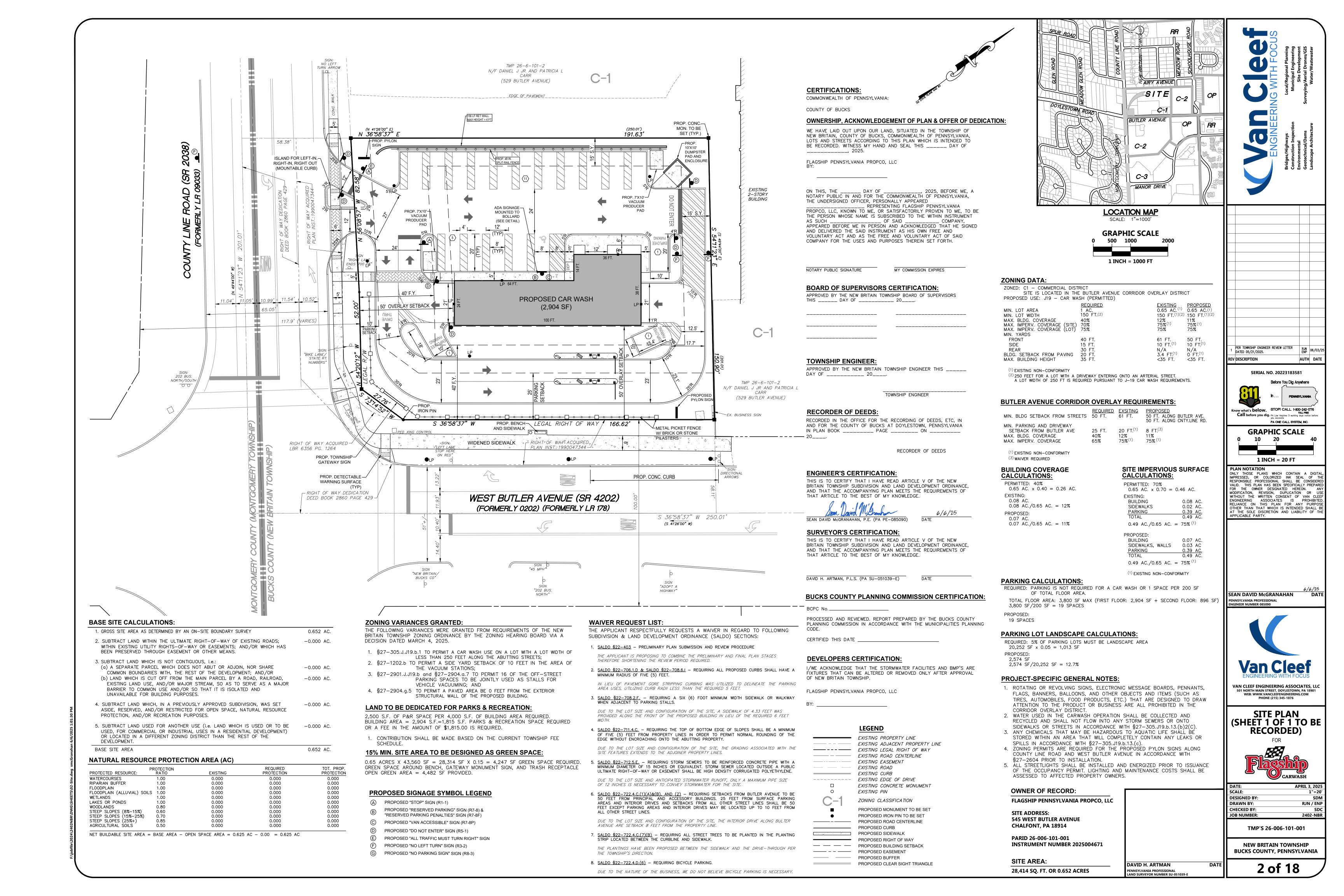
17 UTILITY PROFILES

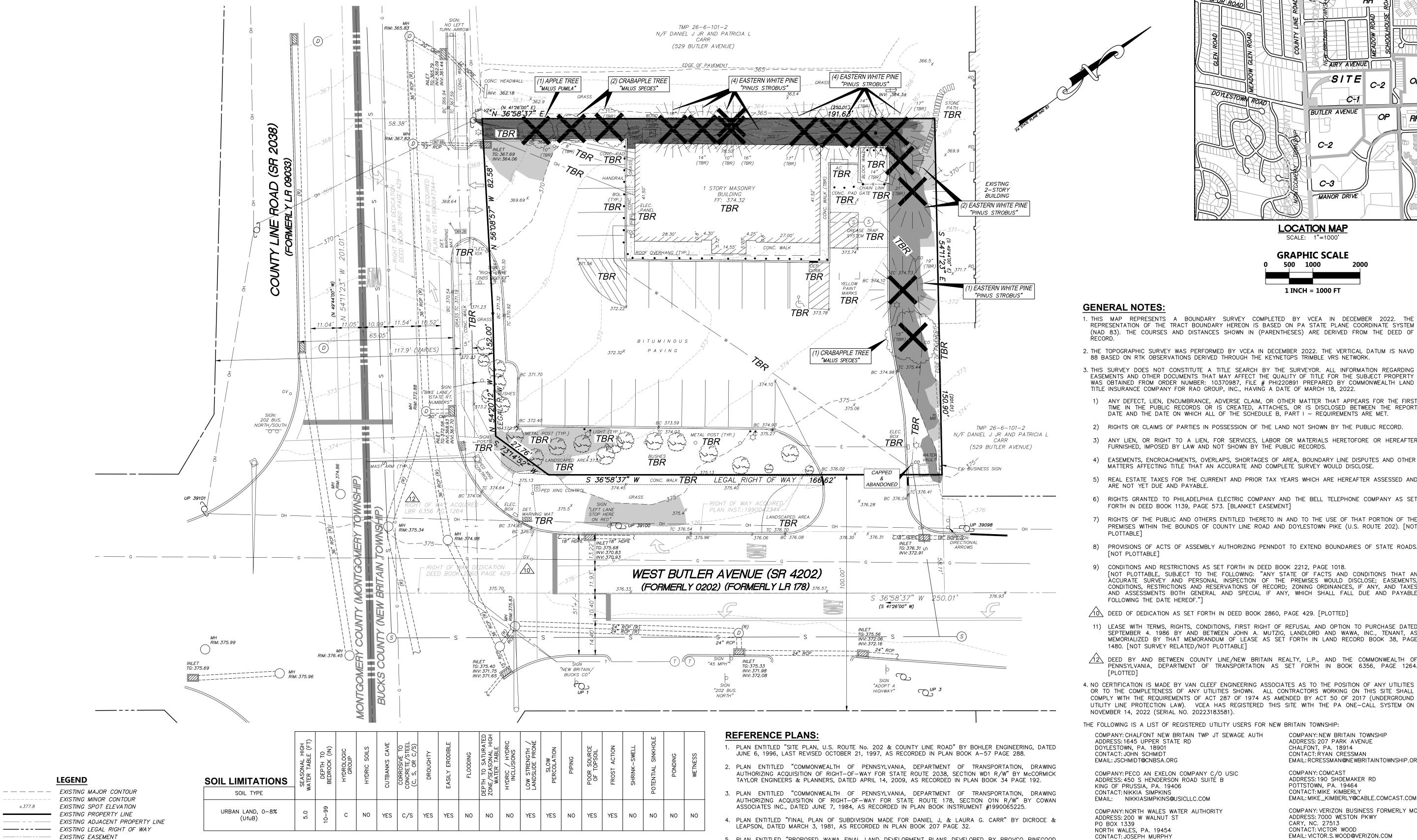
18 VEHICLE NAVIGATION PLAN



**NEW BRITAIN TOWNSHIP** 

**BUCKS COUNTY, PENNSYLVANIA** 





# **GENERAL NOTES:**

\_\_\_\_\_

====== EXISTING CURB

---- EXISTING EDGE OF DRIVE

EXISTING ROAD CENTERLINE

EXISTING SOILS LIMIT AND SOILS TYPE

EXISTING ROAD

EXISTING SIGN

EXISTING TREES

— — OH — EXISTING OVERHEAD ELECTRIC

— — T — EXISTING TELEPHONE

==== EXISTING STORM SEWER

— — W — EXISTING WATER MAIN

EXISTING SANITARY SEWER

— — G — EXISTING GAS MAIN

EXISTING UTILITY POLES

EXISTING WATER VALVE

EXISTING SLOPE 8 TO 15 PERCENT

EXISTING SLOPE 15 TO 25 PERCENT

EXISTING SLOPE 25< PERCENT

EXISTING ITEM TO BE REMOVED

EXISTING TREE TO BE REMOVED

1. SOILS SHOWN HEREON ARE AS MAPPED BY THE NATURAL RESOURCES CONSERVATION SERVICES (NRCS) WEBSITE FOR BUCKS COUNTY ON SEPTEMBER 26, 2023. 2. THE SITE FLOWS DRAIN TO THE WEST BRANCH OF THE NESHAMINY CREEK (WWF, MF) WITHIN THE NESHAMINY CREEK WATERSHED. THE WATERS DESIGNATED/EXISTING USES ARE CLASSIFIED BY THE PA CODE CHAPTER 93.

# SOIL RESOLUTIONS:

WHEN THE ABOVE SOILS LIMITATIONS ARE ENCOUNTERED, UTILIZE THE FOLLOWING SOILS RESOLUTIONS:

- DEPTH TO BEDROCK WHEN DEPTH TO BEDROCK IS ENCOUNTERED, CONTRACTOR MAY BLAST IF NECESSARY.
- CUTBANKS CAVE ALL APPLICABLE OSHA STANDARDS AND REGULATIONS WITH REGARD TO UTILITY AND RETAINING WALL CONSTRUCTION MUST BE IMPLEMENTED AT ALL TIMES. LIMIT SLOPES TO 3:1 MAXIMUM.
- CORROSIVE TO CONCRETE/STEEL ALL UNDERGROUND CONCRETE AND/OR STEEL MATERIALS TO BE BACKFILLED WITH APPROPRIATE MATERIAL WHEN CORROSIVENESS IS LIMITED. FOR BUILDINGS, CONTRACTOR TO COMPLY WITH THE INTERNAL RESIDENTIAL CODE (IRC) BUILDING CODE.
- DROUGHTY PROVIDE WATER TRUCK FOR IRRIGATION IN NEEDED. WHERE LANDSCAPING IS PROPOSED WITHIN DROUGHTY SOILS, APPLYING COMPOST MAY BE NECESSARY TO HELP
- EASILY ERODIBLE SOILS TO BE STABILIZED WITH APPROPRIATE COVER AND/OR MATTING.
- LOW STRENGTH/LANDSLIDE PRONE PRECAUTIONS SHOULD BE TAKEN TO PREVENT SLOPE FAILURES DUE TO IMPROPER CONSTRUCTION PRACTICES, SUCH AS OVER-STEEPENING AND OVERLOADING OF SLOPES, REMOVAL OF LATERAL SUPPORT, AND FAILURE TO PREVENT SATURATION OF SLOPES. ROAD FILL MATERIAL MAY NEED TO BE IMPORTED IN AREAS WHERE SOILS HAVE LOW STRENGTH.
- SLOW PERCOLATION UTILIZE STRUCTURAL BMP'S THAT DO NOT REQUIRE INFILTRATION (I.E. AMENDED SOILS, SLOW RELEASE BASIN). NO INFILTRATION BMP'S ARE PROPOSED. • POOR SOURCE OF TOPSOIL - CONTRACTOR TO SUPPLY TOPSOIL AS NEEDED.
- FROST ACTION PRECAUTIONS ARE NEEDED TO PREVENT DAMAGE, ESPECIALLY TO ROADWAYS.

- 5. PLAN ENTITLED "PROPOSED WAWA FINAL LAND DEVELOPMENT PLANS DEVELOPED BY PROVCO PINEGOOD CHALFONT, LLC" BY BOHLER ENGINEERING, DATED MAY 18, 2018, LAST REVISED NOVEMBER 1, 2019, AS RECORDED IN PLAN BOOK INSTRUMENT # 2020034823.
- 6. PLAN ENTITLED "SHOWALTER SUBDIVISION MINOR SUBDIVISION PLAN" BY SHOWALTER & ASSOCIATES, DATED MARCH 26, 1984, LAST REVISED APRIL 10, 1990, AS RECORDED IN PLAN BOOK 256 PAGE 82.

# SUMMARY OF ADVERSE FEE-SIMPLE CONVEYANCES:

50,244 S.F. DEED INSTRUMENT NO. 2022054655 -12,566 S.F. DEED BK, 2860 PG, 429 - 8,050 S.F. PLAN INSTRUMENT NO. 1990047344 - 1,214 S.F. DEED LRB 6356 PG. 1264 28,414 S.F. CURRENT SURVEY

ZATRY AVENUE *≤SITE* BUTLER AVENUE RR C-3 **LOCATION MAP** 

**GRAPHIC SCALE** 500 1000

1 INCH = 1000 FT

### **GENERAL NOTES:**

[NOT PLOTTABLE]

1. THIS MAP REPRESENTS A BOUNDARY SURVEY COMPLETED BY VCEA IN DECEMBER 2022. THE REPRESENTATION OF THE TRACT BOUNDARY HEREON IS BASED ON PA STATE PLANE COORDINATE SYSTEM (NAD 83). THE COURSES AND DISTANCES SHOWN IN (PARENTHESES) ARE DERIVED FROM THE DEED OF

88 BASED ON RTK OBSERVATIONS DERIVED THROUGH THE KEYNETGPS TRIMBLE VRS NETWORK. 3. THIS SURVEY DOES NOT CONSTITUTE A TITLE SEARCH BY THE SURVEYOR. ALL INFORMATION REGARDING EASEMENTS AND OTHER DOCUMENTS THAT MAY AFFECT THE QUALITY OF TITLE FOR THE SUBJECT PROPERTY WAS OBTAINED FROM ORDER NUMBER: 10370987, FILE # PHI220891 PREPARED BY COMMONWEALTH LAND

- TITLE INSURANCE COMPANY FOR RAO GROUP, INC., HAVING A DATE OF MARCH 18, 2022. 1) ANY DEFECT, LIEN, ENCUMBRANCE, ADVERSE CLAIM, OR OTHER MATTER THAT APPEARS FOR THE FIRST
- TIME IN THE PUBLIC RECORDS OR IS CREATED, ATTACHES, OR IS DISCLOSED BETWEEN THE REPORT DATE AND THE DATE ON WHICH ALL OF THE SCHEDULE B, PART I - REQUIREMENTS ARE MET.
- 3) ANY LIEN, OR RIGHT TO A LIEN, FOR SERVICES, LABOR OR MATERIALS HERETOFORE OR HEREAFTER
- FURNISHED, IMPOSED BY LAW AND NOT SHOWN BY THE PUBLIC RECORDS. 4) EASEMENTS, ENCROACHMENTS, OVERLAPS, SHORTAGES OF AREA, BOUNDARY LINE DISPUTES AND OTHER
- 5) REAL ESTATE TAXES FOR THE CURRENT AND PRIOR TAX YEARS WHICH ARE HEREAFTER ASSESSED AND
- ARE NOT YET DUE AND PAYABLE. 6) RIGHTS GRANTED TO PHILADELPHIA ELECTRIC COMPANY AND THE BELL TELEPHONE COMPANY AS SET

MATTERS AFFECTING TITLE THAT AN ACCURATE AND COMPLETE SURVEY WOULD DISCLOSE.

- FORTH IN DEED BOOK 1139, PAGE 573. [BLANKET EASEMENT] 7) RIGHTS OF THE PUBLIC AND OTHERS ENTITLED THERETO IN AND TO THE USE OF THAT PORTION OF THE PREMISES WITHIN THE BOUNDS OF COUNTY LINE ROAD AND DOYLESTOWN PIKE (U.S. ROUTE 202). [NOT
- 8) PROVISIONS OF ACTS OF ASSEMBLY AUTHORIZING PENNDOT TO EXTEND BOUNDARIES OF STATE ROADS.
- 9) CONDITIONS AND RESTRICTIONS AS SET FORTH IN DEED BOOK 2212, PAGE 1018. [NOT PLOTTABLE, SUBJECT TO THE FOLLOWING: "ANY STATE OF FACTS AND CONDITIONS THAT AN ACCURATE SURVEY AND PERSONAL INSPECTION OF THE PREMISES WOULD DISCLOSE; EASEMENTS, CONDITIONS, RESTRICTIONS AND RESERVATIONS OF RECORD; ZONING ORDINANCES, IF ANY, AND TAXES AND ASSESSMENTS BOTH GENERAL AND SPECIAL IF ANY, WHICH SHALL FALL DUE AND PAYABLE FOLLOWING THE DATE HEREOF."]
- 10 DEED OF DEDICATION AS SET FORTH IN DEED BOOK 2860, PAGE 429. [PLOTTED]
- 11) LEASE WITH TERMS, RIGHTS, CONDITIONS, FIRST RIGHT OF REFUSAL AND OPTION TO PURCHASE DATED SEPTEMBER 4. 1986 BY AND BETWEEN JOHN A. MUTZIG, LANDLORD AND WAWA, INC., TENANT, AS MEMORIALIZED BY THAT MEMORANDUM OF LEASE AS SET FORTH IN LAND RECORD BOOK 38, PAGE
- 1480. [NOT SURVEY RELATED/NOT PLOTTABLE] DEED BY AND BETWEEN COUNTY LINE/NEW BRITAIN REALTY, L.P., AND THE COMMONWEALTH OF PENNSYLVANIA, DEPARTMENT OF TRANSPORTATION AS SET FORTH IN BOOK 6356, PAGE 1264.
- 4. NO CERTIFICATION IS MADE BY VAN CLEEF ENGINEERING ASSOCIATES AS TO THE POSITION OF ANY UTILITIES OR TO THE COMPLETENESS OF ANY UTILITIES SHOWN. ALL CONTRACTORS WORKING ON THIS SITE SHALL COMPLY WITH THE REQUIREMENTS OF ACT 287 OF 1974 AS AMENDED BY ACT 50 OF 2017 (UNDERGROUND UTILITY LINE PROTECTION LAW). VCEA HAS REGISTERED THIS SITE WITH THE PA ONE-CALL SYSTEM ON
- NOVEMBER 14, 2022 (SERIAL NO. 20223183581).

THE FOLLOWING IS A LIST OF REGISTERED UTILITY USERS FOR NEW BRITAIN TOWNSHIP: COMPANY: CHALFONT NEW BRITAIN TWP JT SEWAGE AUTH ADDRESS: 1645 UPPER STATE RD DOYLESTOWN, PA. 18901 CONTACT: JOHN SCHMIDT

COMPANY: PECO AN EXELON COMPANY C/O USIC ADDRESS: 450 S HENDERSON ROAD SUITE B KING OF PRUSSIA, PA. 19406 CONTACT: NIKKIA SIMPKINS

COMPANY: NORTH WALES WATER AUTHORITY ADDRESS: 200 W WALNUT ST PO BOX 1339

NORTH WALES, PA. 19454 CONTACT: JOSEPH MURPHY EMAIL: JMURPHY@NWWATER.COM

5. THIS SURVEY DOES NOT CERTIFY AS TO THE PRESENCE OR ABSENCE OF UNDERGROUND IMPROVEMENTS OR

6. THIS SURVEY DOES NOT CERTIFY AS TO THE PRESENCE OR ABSENCE OF WETLANDS OR WATERS.

7. THE PARCEL SHOWN HEREON IS NOT LOCATED IN A SPECIAL FLOOD HAZARD AREA (SFHA), PER FLOOD INSURANCE RATE MAPS (FIRM) PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA), BEING PANEL 288 OF 532 MAP NUMBER 42017C0288K, EFFECTIVE DATE MARCH 21, 2017.

> OWNER OF RECORD: FLAGSHIP PENNSYLVANIA PROPCO, LLC

SITE ADDRESS: **545 WEST BUTLER AVENUE** CHALFONT, PA 18914

PARID 26-006-101-001 **INSTRUMENT NUMBER 2025004671** 

SITE AREA:

28,414 SQ. FT. OR 0.652 ACRES

OAVID H. ARTMAN

COMPANY: NEW BRITAIN TOWNSHIP

EMAIL: RCRESSMAN@NEWBRITAINTOWNSHIP.ORG

EMAIL: MIKE\_KIMBERLY@CABLE.COMCAST.COM

COMPANY: VERIZON BUSINESS FORMERLY MCI

ADDRESS: 207 PARK AVENUE

ADDRESS: 190 SHOEMAKER RD

ADDRESS: 7000 WESTON PKWY

EMAIL: VICTOR.S.WOOD@VERIZON.COM

CHALFONT, PA. 18914 CONTACT: RYAN CRESSMAN

POTTSTOWN, PA. 19464

CONTACT: MIKE KIMBERLY

COMPANY: COMCAST

CARY, NC. 27513

CONTACT: VICTOR WOOD

PER TOWNSHIP ENGINEER REVIEW LETTER DATED 05/21/2025.

EV DESCRIPTION AUTH DATE **SERIAL NO. 20223183581** 



PA ONE CALL SYSTEM, INC. **GRAPHIC SCALE** 

0 10 20 1 INCH = 20 FT

PLAN NOTATION NLY THOSE PLANS WHICH CONTAIN A DIG MPRESSED, OR COLORIZED INK SEAL OF T RESPONSIBLE PROFESSIONAL SHALL BE CONSIDER VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPART FOR THE OWNER DESIGNATED HEREON. AN MODIFICATION, REVISION, DUPLICATION OR U
WITHOUT THE WRITTEN CONSENT OF VAN CLE
ENGINEERING ASSOCIATES IS PROHIBITE
RELIANCE ON THIS PLAN FOR ANY PURPO THER THAN THAT WHICH IS INTENDED SHALL AT THE SOLE DISCRETION AND LIABILITY OF APPLICABLE PARTY.

EAN DAVID McGRANAHAN NSYLVANIA PROFESSIONAL NEER NUMBER 085090



VAN CLEEF ENGINEERING ASSOCIATES, LLC 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM PHONE (215) 345-1876



HECKED BY **B NUMBER** 

RAWN BY

TMP'S 26-006-101-001

RJN / ENP

**NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 



ENGINEERING WITH FOCUS
Bridges/Highways
Construction Inspection
Environmental
Geotechnical/Dams
Surveying/Aerial Drones/GIS

PER TOWNSHIP ENGINEER REVIEW LETTER DATED 05/21/2025.

DESCRIPTION

AUTH

DA

SERIAL NO. 20223183581

Before You Dig Anywi

In ...

PENNSYL

W what's below. STOPI CALL 1-800-2

Call before you dig. PA Law requires 3 working day

below. STOPI CALL 1-800-242-7776
TOLL FREE
pre you dig. PA Low requires 3 working doys notice before
you excavate
PA ONE CALL SYSTEM, INC.

**GRAPHIC SCALE**25 50 100

1 INCH = 50 FT

PLAN NOTATION

ONLY THOSE PLANS WHICH CONTAIN A DIGITAL, IMPRESSED, OR COLORIZED INK SEAL OF THE RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERED VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARED FOR THE OWNER DESIGNATED HEREON. ANY MODIFICATION, REVISION, DUPLICATION OR USE WITHOUT THE WRITTEN CONSENT OF VAN CLEEF ENGINEERING ASSOCIATES IS PROHIBITED. RELIANCE ON THIS PLAN FOR ANY PURPOSE OTHER THAN THAT WHICH IS INTENDED SHALL BE AT THE SOLE DISCRETION AND LIABILITY OF THE APPLICABLE PARTY.

APPLICABLE PARIT.

6/6
SEAN DAVID McGRANAHAN
PENNSYLVANIA PROFESSIONAL
ENGINEER NUMBER 085090



VAN CLEEF ENGINEERING ASSOCIATES, LLC
501 NORTH MAIN STREET, DOYLESTOWN, PA 18901
WEB: WWW.VANCLEEFENGINEERING.COM
PHONE (215) 345-1876

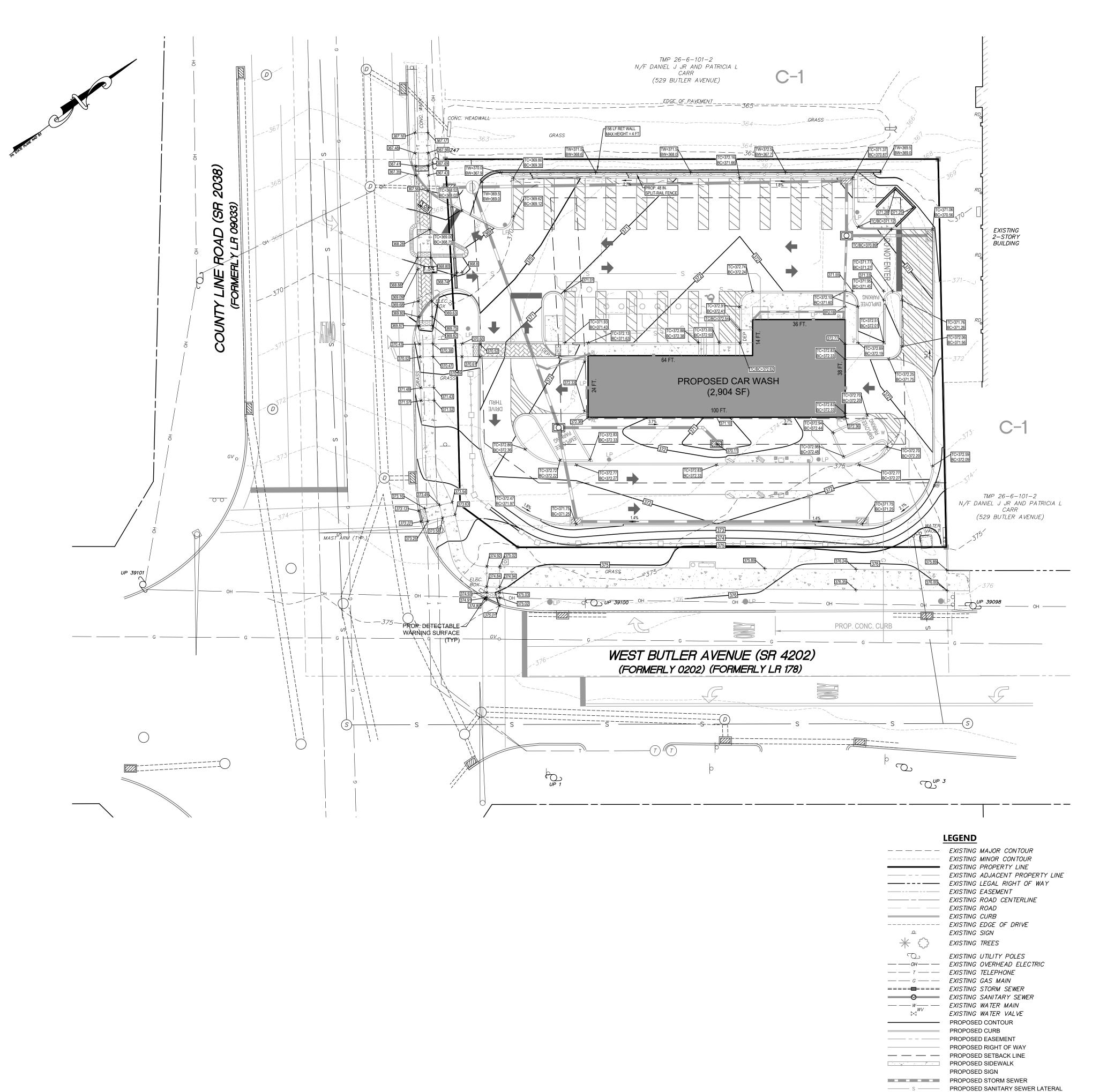
AERIAL PLAN



DATE: APRIL 3, 2025
SCALE: 1"=50'
DESIGNED BY: SDM
DRAWN BY: RJN / ENP
CHECKED BY: SDC
JOB NUMBER: 2402-NBR

TMP'S 26-006-101-001

NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA



### **GRADING GENERAL NOTES:**

- 1. VERTICAL DATUM IS NAVD 1988 AND ESTABLISHED BY OBSERVATIONS REFERENCED TO THE KEYSTONE VRS NETWORK (KEYNETGPS).
- 2. THE MINIMUM SLOPE IN GRASSED AREAS SHALL NOT BE LESS THAN 2.0% AND THE MINIMUM IN PAVED AREAS SHOULD NOT BE LESS THAN 1.0%.
- 3. PAVEMENT SHALL BE SAW CUT IN STRAIGHT LINES TO THE FULL DEPTH OF THE EXISTING PAVEMENT. ALL DEBRIS FROM REMOVAL OPERATIONS SHALL BE REMOVED FROM THE SITE AT THE TIME OF EXCAVATION. STOCKPILING OF DEBRIS WILL NOT BE PERMITTED.
- 4. IN CASE OF DISCREPANCIES BETWEEN PLANS, THE SITE / RECORD PLAN WILL SUPERSEDE IN ALL CASES. THE ENGINEER OF RECORD MUST BE IMMEDIATELY NOTIFIED IN WRITING OF ANY CONFLICTS.
- 5. THE CONTRACTOR SHALL BE REQUIRED TO SECURE ALL NECESSARY PERMITS (INCLUDING DEP, ETC.) FOR ALL OFF—SITE HAUL AND/OR BORROW SITES. CONTRACTOR SHALL SUPPLY A COPY OF APPROVALS TO DESIGN ENGINEER AND OWNER PRIOR TO INITIATING WORK.
- 6. EXISTING INLETS AND STORM SEWER INDICATED AS FILLED W/DEBRIS SHALL BE CLEANED AND FLUSHED.

  NEW INLETS AND PIPES SHALL BE CHECKED FOR SILT/DEBRIS AFTER CONSTRUCTION AND FLUSHED/CLEANED IF NECESSARY.
- 7. DEPTH OF EXISTING UTILITIES IN PORTIONS OF THE SITE ARE UNKNOWN. WHERE EXISTING UTILITIES ARE TO REMAIN AND ARE FOUND TO HAVE INADEQUATE GROUND COVER AFTER FINAL PROPOSED GRADES HAVE BEEN ESTABLISHED, THE DESIGN ENGINEER SHALL BE CONTACTED IMMEDIATELY AND PRIOR TO FURTHER CONSTRUCTION ACTIVITIES IN THE AREA OF SAID CONFLICT.
- 8. ALL DESIGNERS AND CONTRACTORS UTILIZING THIS PLAN AND THE INFORMATION CONTAINED THEREON ARE CAUTIONED TO COMPLY WITH THE REQUIREMENTS OF PENNSYLVANIA ACT 287. LOCATION OF EXISTING AND PROPOSED UNDERGROUND UTILITIES AND FACILITIES SHOWN ON THE DRAWINGS HAVE BEEN DEVELOPED FROM INFORMATION MADE AVAILABLE. COMPLETENESS AND ACCURACY OF LOCATION AND DEPTH OF UTILITIES AND FACILITIES CANNOT BE GUARANTEED. THE CONTRACTOR IS TO VERIFY THE DEPTH AND LOCATION OF ALL UTILITIES AND FACILITIES BEFORE THE START OF WORK. UTILIZE HAND EXCAVATION AS REQUIRED. WORK IS ALSO TO BE DONE IN ACCORDANCE WITH THE STANDARDS OF THE UTILITY COMPANIES WHOSE FACILITIES ARE IN THE PROXIMITY OF THE WORK. OTHER UTILITIES MAY BE REQUIREMENTS OF PENNSYLVANIA ACT 38 (1991), THE CONTRACTOR SHALL CONTACT THE PENNSYLVANIA ONE CALL SYSTEM AT 1-800-242-1776, AT LEAST 3 DAYS PRIOR TO EXCAVATION.
- 9. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE FEDERAL, STATE AND LOCAL CODES, AND ALL REGULATIONS APPURTENANT TO THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970. ALL WORK SHALL BE DONE IN ACCORDANCE WITH PROJECT SPECIFICATIONS INCLUDING CURRENT APPLICABLE STANDARDS AND REQUIREMENTS. WHERE ANY STANDARDS SEEM IN CONFLICT WITH THESE DRAWINGS, NOTIFY THE DESIGN ENGINEER AND CONSTRUCTION MANAGER FOR DIRECTION PRIOR TO PROCEEDING WITH WORK.
- 10. CONTRACTORS SHALL HAVE ALL REQUIRED SUBMITTAL APPROVALS PRIOR TO BEGINNING WORK OR ORDERING
- 11. CONTRACTORS SHALL VERIFY ALL DIMENSIONS, INVERTS, ELEVATIONS, AND EXISTING CONDITIONS PRIOR TO PROCEEDING WITH THE WORK OR PROCUREMENT OF MATERIALS. VARIATIONS BETWEEN DRAWINGS AND ACTUAL FIELD CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER AND RESOLVED PRIOR TO PROCEEDING WITH THE WORK
- 12. CONTRACTOR SHALL BE RESPONSIBLE FOR SAFETY, PROCEDURES, MEANS AND METHODS, SEQUENCING, AND COORDINATION.
- 13. ALL WORK SHALL BE PERFORMED BY QUALIFIED, EXPERIENCED PERSONNEL.
- 14. CONTRACTOR SHALL NOTIFY THE OWNER OF PREEXISTING CONDITIONS OF DETERIORATION IN AREAS OF WORK THAT ARE UNCOVERED OR EXPOSED DURING THE WORK.
- 15. FIELD CHANGES REQUIRE PRIOR DESIGN ENGINEERING REVIEW AND WRITTEN CONFIRMATION.
- 16. CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR TO ANY SIDEWALKS, LAWN, TREES, PAVING, AND OTHER IMPROVEMENTS DISTURBED OR DAMAGED BY DEMOLITION ACTIVITIES PROPOSED HEREIN.
- 17. CONTRACTOR SHALL PROVIDE PROPER TEMPORARY BRACING AND SHORING OF ALL CONSTRUCTION TO REMAIN OR DEMOLITION WORK IN PROGRESS.
- 18. CONTRACTOR SHALL PROVIDE LAYOUT, LINE AND GRADE UNLESS OTHERWISE NOTED.
- 19. CONTRACTOR SHALL NOTIFY THE OWNER OF ANY DISCREPANCIES WITHIN THE DRAWINGS, SPECIFICATIONS, CODES OR STANDARDS FOR CORRECTIVE ACTION PRIOR TO START OF WORK.
- 20. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE AND PROTECTION OF VEHICULAR AND PEDESTRIAN TRAFFIC. ALL TRAFFIC CONTROL MEASURES SHALL BE IN ACCORDANCE WITH LOCAL, PENNDOT & OSHA REGULATIONS.
- 21. CONTRACTOR SHALL PROVIDE PROTECTION FOR EXISTING UTILITIES UNLESS OTHERWISE NOTED. THE CONTRACTOR SHALL IMMEDIATELY REPAIR ANY UTILITY LINE INTERRUPTION AT NO ADDITIONAL CONTRACT COST. THE CONTRACTOR SHALL PROVIDE ADEQUATE PROTECTION AND SUPPORT FOR ALL UTILITIES EXPOSED DURING THE WORK TO INSURE AGAINST DAMAGE AT NO ADDITIONAL COST.
- 22. CONTRACTOR SHALL PROVIDE TEMPORARY DEWATERING OF EXCAVATIONS THROUGHOUT THE DURATION OF CONTRACT AT NO ADDITIONAL COST.
- 23. EXISTING UTILITIES SHOWN ARE BASED ON AVAILABLE DATA. DUE TO THE POTENTIAL LACK OF COMPLETE OR ACCURATE DATA REGARDING EXISTING ONSITE AND OFFSITE UTILITIES, THE CONTRACTOR SHALL ASSESSAVAILABLE DATA, SHALL REQUEST UTILITY COMPANY MARKOUTS, SHALL COORDINATE WITH AFFECTED UTILITY COMPANIES, AND SHALL DIG TEST PITS AT ALL PROPOSED UTILITY CROSSING LOCATIONS SO THAT ELEVATIONS CAN BE TAKEN TO ASSESS POTENTIAL CONFLICTING PIPES/UTILITIES PRIOR TO ANY PROPOSED UTILITY CONSTRUCTION. IF UNEXPECTED UTILITY LOCATIONS OR ELEVATIONS OR PIPE CONFLICTS ARE ENCOUNTERED DURING CONSTRUCTION, OR IF TEST PITS REVEAL POTENTIAL CONFLICT, DESIGN ENGINEER SHALL BE NOTIFIED SO THAT ELEVATIONS AND LOCATIONS (WHERE NECESSARY) OF AFFECTED UTILITIES CAN BE OBTAINED TO FACILITATE NECESSARY DESIGN ADJUSTMENTS.
- 24. REMOVAL OF EXISTING UTILITIES SHALL BE COORDINATED WITH THE APPROPRIATE UTILITY COMPANIES.
- 25. REFER TO THE SITE / RECORD PLAN FOR ADDITIONAL NOTES.
- 26. ALL SIDEWALKS, CROSSWALK, TRAILS, ENTRANCES, AND RAMPS TO BE BUILT IN ACCORDANCE WITH ADA STANDARDS. MAXIMUM LONGITUDINAL SLOPE TO BE 5%. MAXIMUM CROSS SLOPE TO BE 2%. IT IS RECOMMENDED TO CONSTRUCT CROSS SLOPES AT 1.5% AND LONGITUDINAL SLOPES AT 4.9% TO ALLOW FOR CONSTRUCTION TOLERANCE.
- 27. REFER TO ADA CURB RAMP CONSTRUCTION DETAILS AND ADA ACCESSIBILITY PLAN FOR ADDITIONAL INFORMATION SPECIFIC TO CURB RAMP AND ACCESSIBLE ROUTE GRADING.
- 28. AFTER FINAL GRADING THERE SHALL BE A MINIMUM OF EIGHT INCHES OF TOPSOIL ON THE ENTIRE SITE OTHER THAN THAT PORTION OF THE SITE WHERE THERE ARE BUILDINGS OR OTHER IMPERVIOUS SURFACE COVERAGE. THE DEVELOPER IS NOT REQUIRED TO IMPORT TOPSOIL TO THE SITE IF AN EIGHT—INCH FINAL DEPTH CANNOT BE ACHIEVED. IN CASES WHERE THE ORIGINAL SITE HAS LESS THAN EIGHT INCHES OF TOPSOIL AT THE START OF GRADING ACTIVITIES, THE TOTAL AMOUNT AVAILABLE WILL BECOME THE MINIMUM TOPSOIL REQUIREMENT. NO TOPSOIL WILL BE ALLOWED TO BE RELEASED FROM THE SITE.
- 29. ALL AREAS SHALL BE PROPERLY GRADED TO ENSURE DRAINAGE FLOW AWAY FROM THE PROPOSED BUILDING WITHOUT PONDING OR OBSTRUCTION.
- 30. DURING GRADING OPERATIONS, NECESSARY MEASURES FOR DUST CONTROL SHALL BE EXERCISED.
- 31. THE CHALFONT NEW BRITAIN JOINT SEWER AUTHORITY IS TO BE CONTACTED ONE WEEK PRIOR TO SANITARY SEWER CONSTRUCTION, AND 72 HOURS PRIOR TO EXCAVATING NEAR, AND CONNECTING INTO ANY EXISTING SANITARY SEWER.
- 32. ALL SANITARY SEWER CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THE NORTH WALES WATER AUTHORITY SPECIFICATIONS.
- 33. PUBLIC WATER AND SEWER CURRENTLY SERVE THE EXISTING BUILDING. THE PROPOSED WATER SERVICE
- SHALL CONNECT TO THE EXISTING WATER VAULT.
- 34. PROPOSED HANDICAP PARKING SPACES SHALL BE PROVIDED WITH POST-MOUNTED SIGNS.
- 35. CONTRACTOR TO CONNECT NEW BUILDING UTILITY CONNECTIONS TO EXISTING WATER AND SEWER.36. CONTRACTOR SHALL VERIFY COMPLIANCE WITH ADA STANDARDS WITH PROJECT ARCHITECT FOR RAMP AREAS
- PROPOSED ADJACENT TO DOORWAYS.
- 37. ALL UTILITY SERVICES TO THE PROPOSED BUILDING SHALL BE INSTALLED UNDERGROUND.
- 38. TOPSOIL MAY NOT BE REMOVED FROM THE SITE WITHOUT APPROVAL FROM THE TOWNSHIP.39. FINAL DESIGN OF PROPOSED WATER AND SEWER FACILITIES ARE SUBJECT TO THE REVIEW AND APPROVAL

# RETAINING WALL NOTES:

PROPOSED WATER LATERAL FINISHED FLOOR ELEVATION

PROPOSED SPOT ELEVATION

507.30

BY THE SERVICING WATER AND SEWER AUTHORITY.

- . RETAINING WALL SPECIFICATIONS AND DESIGN DETAILS SHALL BE APPROVED BY THE TOWNSHIP ENGINEER PRIOR TO FINAL PLAN APPROVAL.
- 2. WALL STYLE, FINISH AND COLOR SHALL BE COORDINATED WITH PROPOSED BUILDING ARCHITECTURE.



PER TOWNSHIP ENGINEER REVIEW LETTER BATED 05/21/2025. RJN CP/21/2025. RJN CP/21/2025.

SERIAL NO. 20223183581



EV DESCRIPTION

Call before you dig. PA Low requires 3 working days notice you exceeded PA ONE CALL SYSTEM, INC.

10 20 4

1 INCH = 20 FT
PLAN NOTATION

ONLY THOSE PLANS WHICH CONTAIN A DIGITAL IMPRESSED, OR COLORIZED INK SEAL OF TH RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERE VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARE FOR THE OWNER DESIGNATED HEREON. AN MODIFICATION, REVISION, DUPLICATION OR US WITHOUT THE WRITTEN CONSENT OF VAN CLEE ENGINEERING ASSOCIATES IS PROHIBITED RELIANCE ON THIS PLAN FOR ANY PURPOS OTHER THAN THAT WHICH IS INTENDED SHALL BE AT THE SOLE DISCRETION AND LIABILITY OF THAPPLICABLE PARTY.

EAN DAVID McGRANAHAN DENNSYLVANIA PROFESSIONAL



VAN CLEEF ENGINEERING ASSOCIATES, LLC
501 NORTH MAIN STREET, DOYLESTOWN, PA 18901
WEB: WWW.VANCLEEFENGINEERING.COM

PHONE (215) 345-1876

GRADING PLAN



DATE: APRIL 3, 2025

SCALE: 1"=20"

DESIGNED BY: SDM

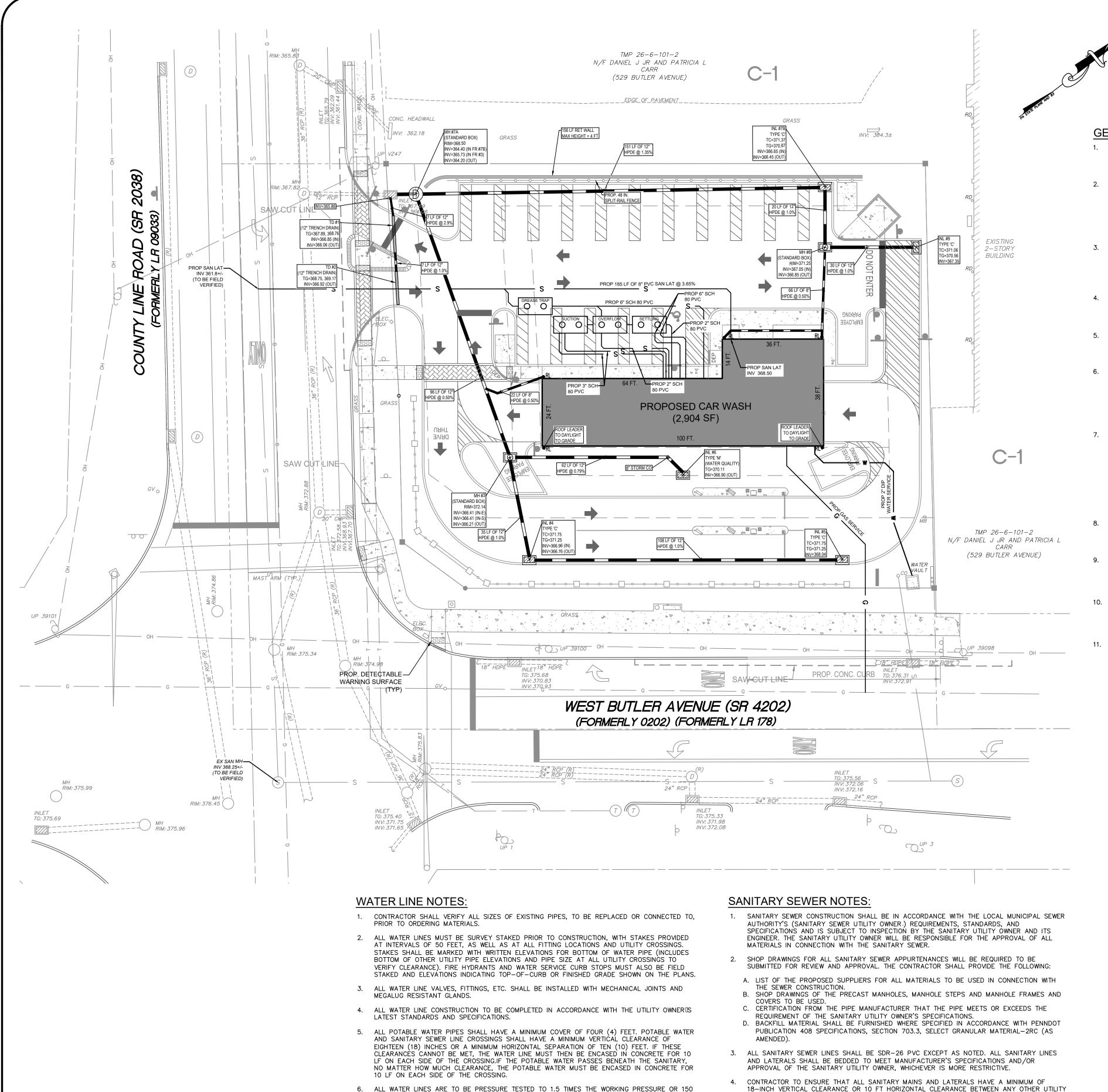
DRAWN BY: RJN / ENP

CHECKED BY: SDC

JOB NUMBER: 2402-NBR

TMP'S 26-006-101-001

NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA



P.S.I., WHICHEVER IS GREATER, IN SECTIONS BETWEEN VALVES. THE TEST PRESSURE SHALL BE

MAINTAINED FOR A PERIOD NOT LESS THAN 2 HOURS. SHOULD ANY PRESSURE TEST DISCLOSE

AN INABILITY TO HOLD THE TEST PRESSURE, THE CONTRACTOR SHALL LOCATE AND CORRECT

7. THE INSTALLATION OR REPAIR OF ANY UNDERGROUND FACILITIES OR PIPING WHICH CONNECTS TO

OR FURNISHES WATER FOR THE FIRE PROTECTION SPRINKLER SYSTEM SHALL BE PERFORMED

ONLY BY A LICENSED UTILITY CONTRACTOR, FIRE PROTECTION SPRINKLER CONTRACTOR, OR

LICENSED PLUMBER. A COPY OF THE LICENSE OR CERTIFICATE OF COMPETENCY SHALL BE

ALL STORM CONVEYANCE PIPE SHALL BE SMOOTH LINED DOUBLE WALLED HIGH DENSITY

POLYETHYLENE PIPE (HDPE), UNLESS OTHERWISE SPECIFIED. ANY ROOF LEADERS SHALL BE

3. LANDSCAPING, FENCES AND STRUCTURES SHALL BE PLACED A MINIMUM OF 3 FEET AWAY FROM

IF A CONFLICT ARISES DURING THE INSTALLATION OF ANY PART OF THE STORM SEWER SYSTEM

DEFECTS AND RETEST TO THE SATISFACTION OF THE UTILITY OWNER

PROVIDED TO THE INSPECTOR AT THE FINAL INSPECTION.

THE ENGINEER IS TO BE NOTIFIED IMMEDIATELY IN WRITING.

STORM SEWERS OUTSIDE OF THE RIGHT OF WAY OF STREETS.

STORM SEWER NOTES:

### GENERAL UTILITIES NOTES:

- ALL NEW UTILITIES/SERVICES, INCLUDING ELECTRIC, TELEPHONE, CABLE TV, ETC. ARE TO BE INSTALLED UNDERGROUND. ALL NEW UTILITIES/SERVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE UTILITY/SERVICE PROVIDER INSTALLATION SPECIFICATIONS
- THE LOCATION OF ALL EXISTING AND PROPOSED SERVICES ARE APPROXIMATE. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING WITH LOCAL UTILITY COMPANIES TO LOCATE ALL PUBLIC OR PRIVATE UTILITIES WHICH LIE IN OR ADJACENT TO THE CONSTRUCTION SITE PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION, REMOVAL OR EXCAVATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING, AT HIS EXPENSE, ALL EXISTING UTILITIES DAMAGED DURING CONSTRUCTION. SANITARY SEWER AND ALL OTHER UTILITY SERVICE CONNECTION POINT CONFLICTS SHALL BE REPORTED IMMEDIATELY IN WRITING TO THE ENGINEER.
- 3. ALL UTILITY EXTENSIONS FROM THE BUILDING BY OTHER TRADES SHALL BE CONNECTED TO THE CONTINUATION UTILITY LINES ON THE SITE. THE GENERAL CONTRACTOR SHALL MAKE THE CONNECTIONS AT A POINT 50 FROM THE BUILDING WALL WHERE OTHER TRADES HAVE ENDED THEIR WORK. THE GC SHALL COORDINATE SCHEDULING OF THESE CONNECTIONS WITH OTHER TRADES TO ASSURE THE SYSTEM IS COMPLETE WHEN FLOW WILL BEGIN IN THE LINES.
- 4. CONSTRUCTION SHALL COMMENCE BEGINNING AT THE LOWEST INVERT (POINT OF CONNECTION) AND PROGRESS UP GRADIENT. PROPOSED INTERFACE POINTS (CROSSINGS) WITH EXISTING UNDERGROUND UTILITIES SHALL BE FIELD VERIFIED BY TEST PIT PRIOR TO COMMENCEMENT OF CONSTRUCTION.
- 5. DEFINE AND LOCATE VERTICALLY AND HORIZONTALLY ALL ACTIVE UTILITY AND/OR SERVICE SYSTEMS THAT ARE NOT TO REMAIN. THE CONTRACTOR IS RESPONSIBLE TO PROTECT AND MAINTAIN ALL ACTIVE SYSTEMS THAT ARE NOT BEING REMOVED/RELOCATED DURING SITE ACTIVITY.
- 6. THE CONTRACTOR SHALL FAMILIARIZE THEMSELVES WITH THE APPLICABLE UTILITY SERVICE PROVIDER REQUIREMENTS AND IS RESPONSIBLE FOR COMPLIANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS, CODES AND REGULATIONS.THE CONTRACTOR IS ALSO RESPONSIBLE FOR ALL COORDINATION REGARDING UTILITY WORK AS IDENTIFIED OR REQUIRED FOR PROJECT. THE CONTRACTOR SHALL PROVIDE THE OWNER WRITTEN NOTIFICATIONS THAT THE EXISTING UTILITIES AND SERVICES HAVE BEEN TERMINATED AND ABANDONED IN ACCORDANCE WITH JURISDICTION AND UTILITY COMPANY
- 7. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATION OF SITE PLAN DOCUMENTS AND ARCHITECTURAL DESIGN FOR EXACT BUILDING UTILITY CONNECTION LOCATIONS, DOOR ACCESS, AND EXTERIOR GRADING. (THE UTILITY SERVICE SIZES ARE TO BE DETERMINED BY ARCHITECT.) THE CONTRACTOR SHALL COORDINATE INSTALLATION OF UTILITIES/SERVICES WITH THE INDIVIDUAL COMPANIES TO AVOID CONFLICTS AND ENSURE PRÓPER DEPTHS ARE ACHIEVED. IN ADDITION TO MEETING THE UTILITY OWNER REQUIREMENTS, THE UTILITY TIE-INS/CONNECTIONS SHALL BE COORDINATED. THIS SHALL BE COMPLETED PRIOR TO CONNECTING TO THE EXISTING UTILITY/SERVICE. WHERE CONFLICTS EXIST WITH THESE PLANS, THE ENGINEER OF RECORD SHALL BE NOTIFIED IN WRITING TO RESOLVE SAID CONFLICTS PRIOR TO CONSTRUCTION.
- 8. THE TOPS OF EXISTING MANHOLES, INLET STRUCTURES, AND SANITARY CLEANOUT TOPS SHALL BE ADJUSTED, IF REQUIRED, TO MATCH THE GRADING AND UTILITY PLAN(S). THE SANITARY CLEANOUT TOPS SHALL BE REINSTALLED IN THE SAME MANNÈR AS THE ORIGINAL CLEANOUTS.
- CONTRACTOR SHALL CONTACT THE LOCAL (I.E. TOWNSHIP, BOROUGH, ETC.) CODE ENFORCEMENT OFFICE WHEN UNDERGROUND PIPING IS BEING HYDROSTATICALLY TESTED ON SITE. APPLICANTS ARE ALSO REMINDED THAT FLUSHING OF THE UNDERGROUND PIPING SHALL BE WITNESSED BY A TOWNSHIP OFFICIAL PRIOR TO FINAL RISER CONNECTIONS, PER NFPA 13.
- 10. UNDERGROUND UTILITY MARKOUTS BY THE UTILITY COMPANIES WERE ORDERED AND THE PHYSICAL LOCATION OF THESE MARKOUTS ARE SHOWN HEREON, HOWEVER, NO CERTIFICATION IS MADE BY VCEA AS TO THE ACTUAL UNDERGROUND POSITION OF ANY UTILITIES OR TO THE COMPLETENESS OR ACCURACY OF THE UTILITY MARKOUTS
- 11. IT IS IMPERATIVE THAT UTILITY COMPANIES ARE NOTIFIED PRIOR TO ANY EXCAVATION AND/OR CONSTRUCTION. CALL 1-800-242-1776 TO ORDER UTILITY MARKOUTS BY OTHERS.

### ---- EXISTING MAJOR CONTOUR EXISTING MINOR CONTOUR

**LEGEND** 

EXISTING PROPERTY LINE ----- EXISTING ADJACENT PROPERTY LINE ---- EXISTING LEGAL RIGHT OF WAY ---- EXISTING EASEMENT ------ EXISTING ROAD CENTERLINE ---- EXISTING ROAD EXISTING CURB ---- EXISTING EDGE OF DRIVE EXISTING SIGN EXISTING TREES EXISTING UTILITY POLES — — OH — EXISTING OVERHEAD ELECTRIC — — T — EXISTING TELEPHONE — — G — EXISTING GAS MAIN

CROSSING. OTHERWISE CONTRACTOR SHALL ENCASE MAIN OR LATERAL IN CONCRETE WITH A

MINIMUM OF 10 LF ON EACH SIDE OF THE CROSSING. CLEANOUTS ARE TO BE PLACED AT

INSTALLED WITH WATERTIGHT FRAMES AND COVERS. ANY MANHOLE TO BE INSTALLED OR

6. CONTRACTOR SHALL VERIFY ALL SIZES, MATERIALS, AND CONDITION OF EXISTING PIPES, TO BE

8. ALL PROPOSED SANITARY SEWER FACILITIES WILL BE OWNED AND PERPETUALLY MAINTAINED BY

REQUIREMENTS WITH RESPECT TO THE SANITARY SEWERAGE FACILITIES SERVED BY THE LOCAL

MUNICIPAL SEWER AUTHORITY'S (SANITARY SEWER UTILITY OWNER) SHALL BE IN ACCORDANCE

WITH THE AUTHORITY'S STANDARD SPECIFICATIONS FOR CONSTRUCTION OF SANITARY SEWERS,

7. ALL EXISTING MANHOLES SHALL BE TESTED PER SANITARY UTILITY OWNER STANDARDS TO

9. ALL MATERIALS USED, AND DETAILS OF CONSTRUCTION, INSTALLATION PROCEDURES. AND

ADJUSTED OUTSIDE OF PAVED AREAS SHALL BE INSTALLED A MINIMUM OF 6" ABOVE FINISH

5. ALL SANITARY SEWER MANHOLES PROPOSED TO BE INSTALLED OR ADJUSTED SHALL BE

MAXIMUM INTERVALS OF 100-FT AND AT ALL BENDS.

ASSURE NO LEAKS EXIST WITHIN THE STRUCTURES.

THE PROPERTY OWNER IN PERPETUITY.

REPLACED OR CONNECTED TO. PRIOR TO ORDERING MATERIALS.

FORCE MAINS, PUMPING STATIONS AND WASTEWATER TREATMENT.

===== EXISTING STORM SEWER EXISTING SANITARY SEWER — — w — EXISTING WATER MAIN EXISTING WATER VALVE PROPOSED CONTOUR PROPOSED CURB ----- PROPOSED EASEMENT PROPOSED RIGHT OF WAY — — PROPOSED SETBACK LINE PROPOSED SIDEWALK PROPOSED SIGN PROPOSED STORM SEWER ----- S ------ PROPOSED SANITARY SEWER LATERAL PROPOSED WATER LATERAL

- THE CONTRACTOR, DURING THE PERFORMANCE OF ALL WORK ASSOCIATED WITH THE CONSTRUCTION OF THE PROJECT, IS RESPONSIBLE FOR COMPLIANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS, CODES AND REGULATIONS.
- 13. ANY DISCREPANCIES FOUND BETWEEN THE DRAWINGS AND SITE CONDITIONS OR ANY INCONSISTENCIES OR AMBIGUITIES IN DRAWINGS SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER, IN WRITING, WHO SHALL PROMPTLY ADDRESS SUCH INCONSISTENCIES OR AMBIGUITIES. WORK DONE BY THE CONTRACTOR AFTER HIS DISCOVERY OF SUCH DISCREPANCIES, INCONSISTENCIES OR AMBIGUITIES SHALL BE DONE AT THE CONTRACTOR'S RISK.
- 14. THE CHALFONT NEW BRITAIN JOINT SEWER AUTHORITY IS TO BE CONTACTED ONE WEEK PRIOR TO SANITARY SEWER CONSTRUCTION, AND 72 HOURS PRIOR TO EXCAVATING NEAR, AND CONNECTING INTO ANY EXISTING SANITARY SEWER.
- 15. ALL SANITARY SEWER CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THE CHALFONT NEW BRITAIN JOINT SEWER AUTHORITY (CNBSA)
- a.) THE MINIMUM ALLOWABLE LATERAL SLOPE SHALL BE 0.02 FT/FT. b.) MAGNETIC - TYPE LOCATOR TAPE SHALL BE LAID DIRECTLY OVER THE TOP OF PVC PIPE, TWO FEET (2') BELOW GRADE. c.) CONTRACTOR SHALL MAKE TESTS AS DIRECTED BY THE ENGINEER TO ASCERTAIN IF THE PIPE IS PROPERLY ALIGNED AND THE JOINTS ARE LIGHT. ENGINEER MUST WITNESS ALL TESTS OR THE CONTRACTOR WILL NEED TO RETEST IN THE PRESENCE OF THE ENGINEER AT THE SOLE COST OF THE CONTRACTOR.
- CONTRACTOR TO CONNECT NEW BUILDING UTILITY CONNECTIONS TO EXISTING WATER AND SEWER.
- 17. FINAL DESIGN OF PROPOSED WATER AND SEWER FACILITIES ARE SUBJECT TO THE REVIEW AND APPROVAL BY THE SERVICING WATER AND SEWER AUTHORITY. ALL WATER MAIN CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH NORTH WALES WATER AUTHORITY SPECIFICATIONS.
- 18. LATERALS ARE TO BE LOCATED AS SHOWN ON THE PLAN.

PROVIDED BY ARCHITECT AT TIME OF BUILDING PERMITS.

- WATER SERVICE IS TO BE LOCATED A MIN. OF 1.5 FEET ABOVE THE SANITARY LATERAL WITH A MINIMUM COVER OF 48".
- NO GROUNDING OF ELECTRICAL SERVICE TO COPPER WATER SERVICE IS PERMITTED. CABLE AND TELECOMM UTILITIES TO FOLLOW ELECTRIC FACILITIES ALIGNMENTS,
- 18" VERTICAL CLEARANCE OR 10 FEET OF HORIZONTAL CLEARANCE SHALL BE PROVIDED BETWEEN THE SANITARY SEWER AND ALL OTHER UTILITIES. OTHERWISE, CONCRETE ENCASEMENT OF THE SANITARY SEWER SHALL BE PROVIDED.
- 22. ALL DOWNSPOUT LOCATIONS TO BE OUTFITTED WITH A OVERFLOW / CLEANOUT. OVERFLOWS SHALL HAVE A SPLASHBLOCK INSTALLED AT THEIR OUTFALL LOCATION.
- 23. PROPOSED ROOF DRAIN CONNECTIONS SHOWN SCHEMATICALLY, FINAL DESIGN TO BE
- 24. MATERIALS SHOWN HEREON, SUCH AS ADS N-12 PIPE, OR APPROVED EQUAL SHALL BE UTILIZED.
- 25. REFER TO ARCHITECTURAL, MECHANICAL, ELECTRICAL AND PLUMBING PLANS FOR
- ADDITIONAL COORDINATION W/ IN 5' OF BUILDINGS. PUBLIC SEWER SERVICE WILL BE PROVIDED BY BCSWA, AND CONSTRUCTION OF ALL
- SANITARY SEWER IMPROVEMENTS SHALL COMPLY WITH AUTHORITY STANDARDS. 27. ALL BURIED GRAVITY SANITARY SEWER MAINS AND LATERALS SHALL BE CLASS 50
- DUCTILE IRON PIPE OR PVC SDR-26 OR SDR-35. THRUST BLOCKS SHALL BE PROVIDED AT ALL BENDS AND TEES.
- 29. GENERAL CONTRACTOR (GC) TO COORDINATE LIGHT POLE LOCATIONS WITH TRADES.

# **GENERAL CONSTRUCTION NOTES:**

CONTACT: JOSEPH MURPHY
EMAIL: JMURPHY@NWWATER.COM

1. NO CERTIFICATION IS MADE BY VAN CLEEF ENGINEERING ASSOCIATES AS TO THE POSITION OF ANY UTILITIES OR TO THE COMPLETENESS OF ANY UTILITIES SHOWN. ALL CONTRACTORS WORKING ON THIS SITE SHALL COMPLY WITH THE REQUIREMENTS OF ACT 287 OF 1974 AS AMENDED BY ACT 50 OF 2017 (UNDERGROUND UTILITY LINE PROTECTION LAW). VCEA HAS REGISTERED THIS SITE WITH THE PA ONE-CALL SYSTEM ON NOVEMBER 14, 2022 (SERIAL NO. 20223183581).

THE FOLLOWING IS A LIST OF REGISTERED UTILITY USERS FOR NEW BRITAIN

COMPANY: CHALFONT NEW BRITAIN TWP JT SEWAGE AUTH ADDRESS: 1645 UPPER STATE RD DOYLESTOWN, PA. 18901 CONTACT: JOHN SCHMIDT CHALFONT, PA. 18914 EMAIL: RCRESSMAN@NEWBRITAINTOWNSHIP.ORG EMAIL: JSCHMIDT@CNBSA.ORG COMPANY: COMCAST COMPANY: PECO AN EXELON COMPANY C/O USIC ADDRESS: 190 SHOEMAKER RE KING OF PRUSSIA, PA. 19406 CONTACT: MIKE KIMBERLY EMAIL: MIKE\_KIMBERLY@CABLE.COMCAST.COM EMAIL: NIKKIASIMPKINS@USICLLC.COM COMPANY: VERIZON BUSINESS FORMERLY MCI COMPANY: NORTH WALES WATER AUTHORITY ADDRESS: 7000 WESTON PKWY ADDRESS: 200 W WALNUT ST PO BOX 1339 NORTH WALES, PA. 19454 CONTACT: VICTOR WOOD

2. IT IS IMPERATIVE THAT UTILITY COMPANIES ARE NOTIFIED PRIOR TO ANY EXCAVATION AND/OR CONSTRUCTION. CALL 1-800-242-1776 TO ORDER UTILITY MARKOUTS BY OTHERS.

EMAIL: VICTOR.S.WOOD@VERIZON.COM

- 3. THE LOCATIONS OF EXISTING UTILITIES AS SHOWN ARE APPROXIMATE ONLY. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL PUBLIC OR PRIVATE UTILITIES WHICH LIE IN OR ADJACENT TO THE CONSTRUCTION SITE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING, AT HIS EXPENSE, ALL EXISTING UTILITIES DAMAGED DURING CONSTRUCTION.
- 4. THE CONTRACTOR, DURING THE PERFORMANCE OF ALL WORK ASSOCIATED WITH THE CONSTRUCTION OF THE PROJECT, IS RESPONSIBLE FOR COMPLIANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS, CODES AND REGULATIONS.
- 5. ANY DISCREPANCIES FOUND BETWEEN THE DRAWINGS AND SITE CONDITIONS OR ANY INCONSISTENCIES OR AMBIGUITIES IN DRAWINGS SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER, IN WRITING, WHO SHALL PROMPTLY ADDRESS SUCH INCONSISTENCIES OR AMBIGUITIES. WORK DONE BY THE CONTRACTOR AFTER HIS DISCOVERY OF SUCH DISCREPANCIES, INCONSISTENCIES OR AMBIGUITIES SHALL BE DONE AT THE CONTRACTOR'S RISK.
- 6. ALL MATERIAL USED FOR FILL OR BACKFILL SHALL BE FREE OF WOOD, ROOTS, ROCKS, BOULDERS OR ANY OTHER NON-COMPACTIBLE SOIL TYPE MATERIAL. UNSATISFACTORY MATERIALS ALSO INCLUDE MAN-MADE FILLS AND REFUSE DEBRIS DERIVED FROM ANY SOURCE.
- 7. ALL EXCAVATIONS, INCLUDING TRENCHES, SHALL BE KEPT DRY TO PROTECT THEIR
- 8. MATERIALS USED TO CONSTRUCT EMBANKMENTS FOR ANY PURPOSE, BACKFILL AROUND DRAINAGE STRUCTURES OR IN UTILITY TRENCHES OR ANY OTHER DEPRESSIONS REQUIRING FILL OR BACKFILL SHALL BE COMPACTED TO AT LEAST 95% OF MAXIMUM DENSITY AS DETERMINED BY THE STANDARD PROCTOR TEST AS SET OUT IN ASTM STANDARD D-698 UNLESS SPECIFIED ELSEWHERE WITHIN THE CONSTRUCTION PLANS OR IF SPECIFIED DIFFERENTLY BY THE TOWNSHIP ENGINEER. CONTRACTOR TO VERIFY COMPACTION REQUIREMENTS WITH TOWNSHIP ENGINEER'S OFFICE PRIOR TO EXCAVATION.
- 9. ALL CONSTRUCTION DEBRIS INCLUDING EXCESS EXCAVATED MATERIAL, SCRAP WOOD, BRICKS, BLOCKS, ETC. SHALL BE DISPOSED OF BY THE CONTRACTOR IN ACCORDANCE WITH FEDERAL, STATE AND LOCAL REQUIREMENTS.
- 10. ELEVATIONS, DIMENSIONS, AND THE LOCATIONS OF LINEAR FEATURES SHALL BE FIELD VERIFIED BY THE CONTRACTOR.
- 11. MINIMUM SLOPE OF ALL GRASS AREAS SHALL NOT BE LESS THAN 2%, AND FOR PAVED AREAS, NOT LESS THAN 1%.

PER TOWNSHIP ENGINEER REVIEW LETTER DATED 05/21/2025. EV DESCRIPTION AUTH DATE

> **SERIAL NO. 20223183581** Before You Dig Anywhere

PENNSYLVAN now what's below. STOP! CALL 1-800-242-1776 Call before you dig. PA Law requires 3 working days notice PA ONE CALL SYSTEM, INC.

**GRAPHIC SCALE** 10 20

1 INCH = 20 FT

PLAN NOTATION NLY THOSE PLANS WHICH CONTAIN A DIGI MPRESSED, OR COLORIZED INK SEAL OF TH RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERE ALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARE OR THE OWNER DESIGNATED HEREON. AN MODIFICATION, REVISION, DUPLICATION OR WITHOUT THE WRITTEN CONSENT OF VAN CL NGINEERING ASSOCIATES IS PROHIBITE RELIANCE ON THIS PLAN FOR ANY PURPO THER THAN THAT WHICH IS INTENDED SHALL AT THE SOLE DISCRETION AND LIABILITY OF APPLICABLE PARTY.

EAN DAVID McGRANAHAN NSYLVANIA PROFESSIONA NEER NUMBER 085090



VAN CLEEF ENGINEERING ASSOCIATES, LLC 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM

PHONE (215) 345-1876

UTILITY PLAN

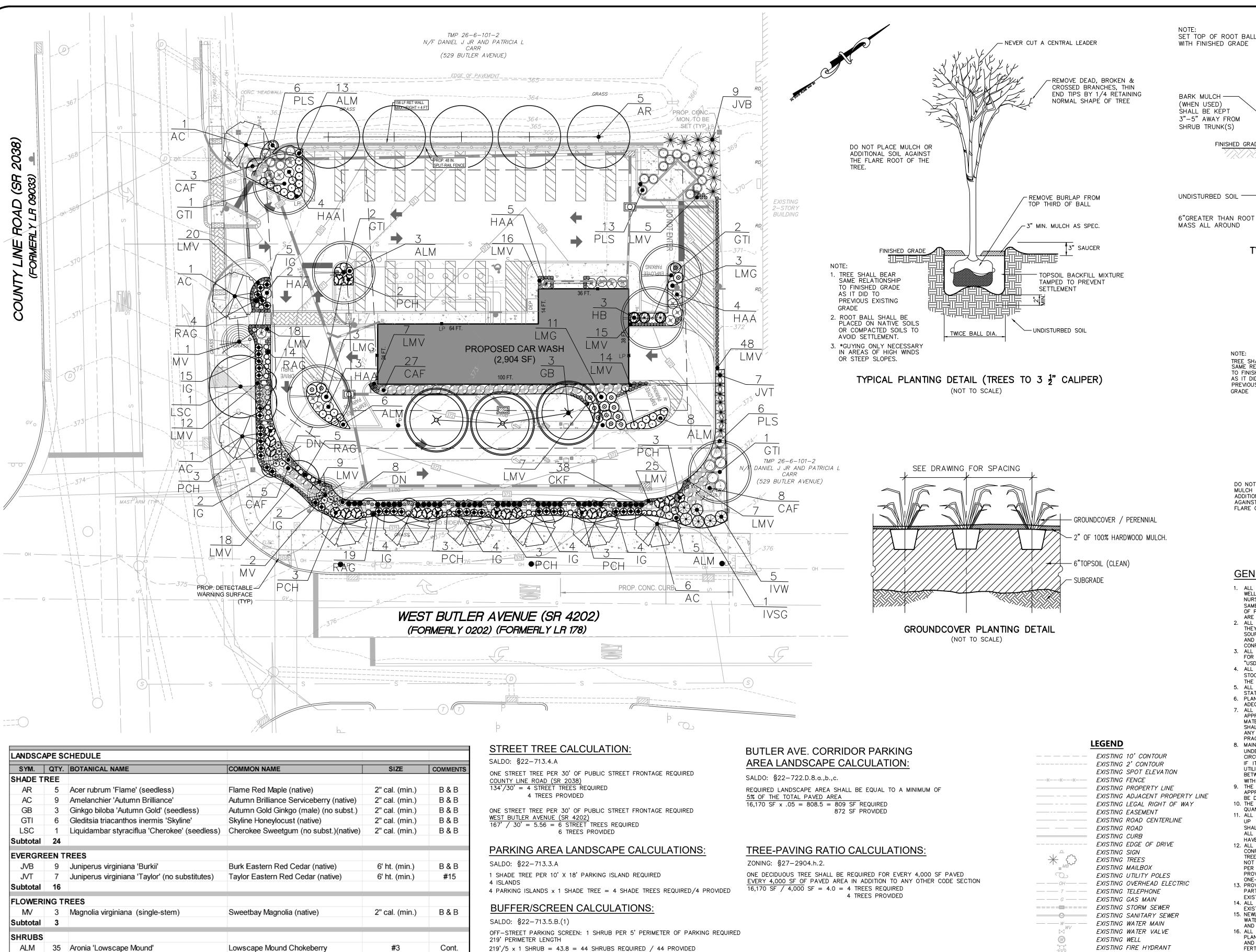


RJN / ENP RAWN BY HECKED BY **B NUMBER** 

CARWASH

TMP'S 26-006-101-001

**NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 



CAF

DN

HAA

HB

**IVW** 

**IVSG** 

LMG

PCH

PLS

Subtotal 224

**PERENNIALS** 

Subtotal 302

TOTAL 569

44 Cornus sericea 'Arctic Fire' (native)

18 Hydrangea arborescens 'Annabelle'

20 Prunus laurocerasus 'Chestnut Hill'

26 Prunus laurocerasus 'Schipkaensis'

43 Rudbeckia fulgida 'American Gold Rush'

llex verticillata 'Southern Gentleman' (male)

40 llex glabra 'Compacta' (native)

5 llex verticillata 'Winter Red'

17 Lonicera pliata 'Moss Green'

15 Deutzia gracilis 'Nikko'

Hydrangea 'Bobo'

CKF 38 Calamagrostis 'Karl Foerster'

221 Liriope muscari 'Variegata'

Arctic Fire Redosier/Redtwig Dogwood

Annabelle Smooth Hydrangea

Winter Red Winterberry

Compact Inkberry Holly (evergreen)

Moss Green Prostrate Honeysuckle

Chestnut Hill Cherry Laurel (evergreen)

Southern Gentleman Winterberry

Schip / Skip Laurel (evergreen)

Karl Foerster Feather Reed Grass

Amer. Gold Rush Black-Eyed Susan

Variegated Liriope (evergreen)

Nikko Deutzia

Bobo Hydrangea

30" ht. (min.)

#3

30" ht. (min.)

2 Gal.

1 Gal.

1 Gal.

#2

Cont.

24" o.c.

18" o.c.

24" o.c.

APPLICATION RATE

FERTILIZER APPL. RATE

FERTILIZER TYPE

LIMING RATE

MULCH TYPE

MULCH RATE

PERMANENT:

% PURE LIVE SEED

FERTILIZER TYPE

ANCHOR MATERIAL

ANCHORING METHOD

RATE OF ANCHOR MATERIAL APPL. N/A

LIMING RATE

MULCH TYPE MULCH RATE

APPLICATION RATE

FERTILIZER APPL. RATE

SPECIES

GRASS SEEDING SPECIFICATIONS:

ANNUAL RYEGRASS

COMMERCIAL 10-20-10

KENTUCKY BLUEGRASS / CREEPING

41 / 20 / 14 / 20 LB/ACRE

COMMERCIAL 10-20-10

FESCUE / PERENNIAL RYEGRASS / CHEWING'S

APRIL 15 - MAY 30, AUGUST 15 - OCTOBER 15

87.0 LB/ACRE

1,100 LB/ACRE

2 T/ACRE

3 T/ACRE

RED FESCUE

1,100 LB/ACRE

HAY OR STRAW

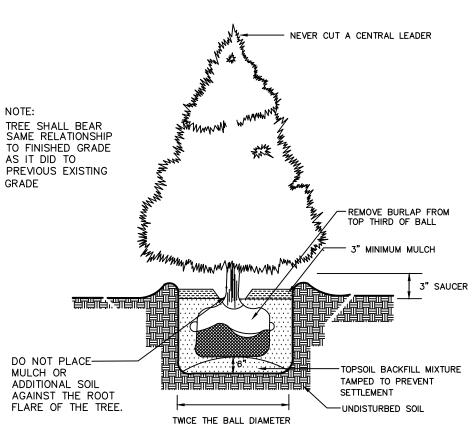
3 T/ACRE

STRAW

TREE SHALL BEAR SAME RELATIONSHIP TO FINISHED GRADE PREVIOUS EXISTING GRADE TOP THIRD OF BALL 3" MINIMUM MULCH DO NOT PLACE-TOPSOIL BACKFILL MIXTUR MULCH OR ADDITIONAL SOIL TAMPED TO PREVENT SETTLEMENT AGAINST THE ROOT -UNDISTURBED SOIL TYPICAL EVERGREEN PLANTING DETAIL **GENERAL LANDSCAPING NOTES** ALL PLANTS SHALL BE TYPICAL OF THEIR SPECIES OR VARIETY; AND SHALL HAVE NORMAL WELL-DEVELOPED BRANCHES AND VIGOROUS FIBROUS ROOT SYSTEMS. ALL PLANTS SHALL B NURSERY-GROWN UNLESS OTHERWISE STATED; THEY SHALL HAVE BEEN GROWING UNDER TH SAME CLIMATE CONDITIONS AS THE MUNICIPALITY FOR AT LEAST TWO (2) YEARS PRIOR TO DATE OF PLANTING. ALL PLANTS WHICH ARE FOUND UNSUITABLE IN GROWTH OR CONDITION OR WHICH ARE NOT TRUE TO NAME SHALL BE REMOVED AND REPLACED WITH ACCEPTABLE PLANTS. 2. ALL PLANT MATERIAL SHALL BE TWICE TRANSPLANTED, NURSERY-GROWN OF SPECIMEN QUALITY. THEY SHALL BE OF SYMMETRICAL GROWTH OR TYPICAL OF THE VARIETY AND SUPPLIED FROM SOURCES IN THE SAME HARDINESS ZONE AS THE DEVELOPMENT IS LOCATED AND FREE OF INSECT AND DISEASE PROBLEMS OR OBJECTIONABLE DISFIGUREMENTS. ALL PLANT MATERIAL SHALL CONFORM TO THE STANDARDS OF THE AMERICAN ASSOCIATION OF NURSERYMEN. 3. ALL PRECAUTIONS CUSTOMARY IN GOOD TRADE PRACTICE SHALL BE TAKEN IN PREPARING PLANTS FOR MOVING. ALL BALLED AND BURLAPPED PLANTS SHALL BE DUG TO MEET OR EXCEED THE "USDA STANDARDS FOR NURSERY STOCK" 4. ALL PLANT MATERIAL SHALL MEET THE STANDARDS OF AMERICAN STANDARD FOR NURSERY STOCK BY THE AMERICAN ASSOCIATION OF NURSERYMEN (1994), OR MOST RECENT EDITION, AND THE HEIGHT, SPREAD AND/OR CALIPER FOR TREES AND SHRUBS. 5. ALL PLANT MATERIAL SHALL BE INSTALLED IN ACCORDANCE WITH THE PLANTING PRACTICES STATED IN CHAPTER 3 OF TREE MAINTENANCE BY P. P. PIRONE, FIFTH OR MOST RECENT EDITION. 6. PLANTS SHALL BE PACKED, TRANSPORTED AND HANDLED WITH UTMOST CARE TO INSURE ADEQUATE PROTECTION AGAINST INJURY. 7. ALL PLANT MATERIALS SHALL BE QUARANTEED FOR A PERIOD OF 18 MONTHS FOLLOWING FINAL APPROVAL BY THE TOWNSHIP, EXCEPT WHERE OTHERWISE NOTED IN THIS SECTION. ANY PLANT MATERIAL THAT HAS 25% OR GREATER DEAD BRANCHES SHALL BE CONSIDERED DEAD. A TREE SHALL BE CONSIDERED DEAD WHEN THE MAIN LEADER HAS DIED OR 25% OF THE CROWN IS DEAD. ANY DEAD MATERIAL SHALL BE REPLACED AND INSTALLED ACCORDING TO APPROVED PLANTING 8. MAINTAIN A MINIMUM FIFTEEN (15) FOOT SEPARATION DISTANCE BETWEEN TREES AND UNDERGROUND UTILITIES. THIS MINIMUM MAY BE REDUCED TO TEN (10) FEET IN CERTAIN CIRCUMSTANCES WITH APPROVAL OF THE TOWNSHIP ENGINEER. HOWEVER, DURING CONSTRUCTION, IF IT IS DEEMED NECESSARY TO FIELD ADJUST THE LOCATIONS OF ANY PLANTINGS AND/OR UTILITY CONNECTIONS DUE TO THE NEED TO PROVIDE THE MINIMUM TEN FOOT SEPARATION BETWEEN THOSE PLAN ELEMENTS, THIS ADJUSTMENT SHOULD BE UNDERTAKEN IN CONSULTATION WITH NEW BRITAIN TOWNSHIP STAFF/PROFESSIONALS. 9. THE LOCATIONS OF ALL PLANT MATERIAL INDICATED ON THE LANDSCAPE PLANS ARE APPROXIMATE. THE FINAL LOCATION OF ALL PLANT MATERIAL AND PLANTING BED LINES SHALL BE DETERMINED IN THE FIELD UNDER THE DIRECTION OF THE LANDSCAPE ARCHITECT. 10. THE PLANTING PLAN SHALL TAKE PRECEDENCE OVER THE PLANT SCHEDULE SHOULD ANY PLANT 11. ALL SHADE TREES PLANTED NEAR PEDESTRIAN OR VEHICULAR ACCESS SHOULD BE CLEAR TRUNK UP TO 7'-0" ABOVE GRADE. ALL SHRUBBERY MATERIAL LOCATED WITHIN SIGHT TRIANGLES SHALL NOT EXCEED A MATURE HEIGHT OF 3' ABOVE THE ELEVATION OF THE ADJACENT CURB. ALL SHADE TREES PLANTED OR EXISTING IN SIGHT TRIANGLES SHALL BE PRUNED SO AS NOT TO HAVE BRANCHES BELOW 7'-0". 12. ALL PLANT MATERIAL SHALL BE PROPERLY GUYED, STAKED, WRAPPED AND PLANTED IN CONFORMANCE WITH THE TYPICAL PLANTING DETAILS. GUY WIRES SHALL BE ATTACHED TO THE TREE AT TWO—THIRDS THE HEIGHT OF THE TREE AND SHOULD BE LOCATED AT POINTS SO AS NOT TO SPLIT THE TRUNKS OF MULTI-STEMMED TREES. PROVIDE TWO TO THREE TREE STAKES PER TREE AS NOTED ON THE PLANS. INSTALL ALL PLANT MATERIAL ON UNDISTURBED GRADE. PROVIDE BURLAP WRAPPING WITH A 50% OVERLAP. CUT AND REMOVE BURLAP FROM TOP ONE-THIRD OF THE ROOT BALL 13. PROVIDE PLANTING PITS AS INDICATED ON PLANTING DETAILS. BACKFILL PLANTING PITS WITH ONE PART EACH OF TOPSOIL, PEAT MOSS AND PARENT MATERIAL. IF WET OR CLAY SOIL CONDITIONS EXIST, INSTALL A 4" PERFORATED DRAIN. RUN TO GRADE OR CATCH BASIN 14. ALL PLANT MATERIAL SHALL BEAR THE SAME RELATION TO FINISHED GRADE AS IT DID TO EXISTING GRADE. 15. NEWLY INSTALLED PLANT MATERIAL SHALL BE WATERED AT THE TIME OF INSTALLATION. REGULAR WATERING ALL PLANT MATERIAL SHALL BE PROVIDED TO ENSURE THE ESTABLISHMENT, GROWTH AND SURVIVAL OF ALL PLANTS. 16. ALL DISTURBED LAWN AREAS SHALL BE STABILIZED WITH SEED AS INDICATED ON THE LANDSCAPE PLANS. SEED MIXTURE SHALL BE AS LISTED IN THE SEEDING SCHEDULE ON THE SOIL EROSION AND SEDIMENT CONTROL NARRATIVE. ALL DISTURBED LAWN AREAS SHALL BE TOPSOILED, LIMED, FERTILIZED AND FINE GRADED PRIOR TO LAWN INSTALLATION. 17. ALL PLANTING BEDS SHALL RECEIVE MINIMUM 3" OF SHREDDED HARDWOOD BARK.
18. ALL SHRUB MASSES SHALL BE PLANTED IN CONTINUOUS MULCHED BEDS. PROPOSED CONTOUR 19. ALL EXISTING TREES AND SHRUBS TO BE PRESERVED ON SITE SHALL BE PROTECTED AGAINST CONSTRUCTION DAMAGE BY SNOW FENCING. ALL FENCING SHALL BE PLACED A MINIMUM OF ONE PROPOSED EASEMENT (1) FOOT OUTSIDE THE INDIVIDUAL TREE DRIPLINE, OR AT THE SPECIFIC LOCATION DETERMINED PROPOSED RIGHT OF WAY APPROPRIATE BY THE LANDSCAPE ARCHITECT. ALL TREES TO REMAIN SHALL BE IDENTIFIED IN THE PROPOSED SETBACK LINE

SET TOP OF ROOT BALL EVEN REMOVE CONTAINER AND MAKE 2 SHALLOW CUTS THE LENGTH OF THE ROOT MASS OR LOOSEN ROOTS. 3" MULCH - SHREDDED BARK MULCH 3" MIN. RIM OF WATER SAUCER -SOIL BACKFILL MIXTURE: SPECIFIED MIX OR 1/3 TOPSOIL, 1/3 PEAT MOSS, 1/3 SAND - SCARIFY SUBGRADE

TYPICAL SHRUB PLANTING DETAIL



FIELD PRIOR TO COMMENCEMENT OF CONSTRUCTION, GRADING OR CLEARING. ALL EXISTING VEGETATION BEING PRESERVED AND LOCATED AT THE EDGE OF THE NEW TREELINE SHALL BE PRUNED AND TRIMMED TO REMOVE DEAD OR DAMAGED BRANCHES.

20. THE QUANTITIES SHOWN ON THE PLAN SHALL HAVE PRECEDENCE OVER THE PLANT SCHEDULE IN THE EVENT OF ANY QUANTITY DISCREPANCIES.

PROPOSED CURB

PROPOSED SIDEWALK

PROPOSED WATER LATERAL

PROPOSED LANDSCAPING

PROPOSED SANITARY SEWER LATERAL

PROPOSED SIGN

PROPOSED LIGHT

PROPOSED STORM SEWER

- - - -

21. A CLEAR-SIGHT TRIANGLE OF TEN FEET (10') MEASURED FROM THE POINT OF INTERSECTION OF THE STREET LINE AND THE EDGE OF THE ACCESSWAY SHALL BE MAINTAINED, WITHIN WHICH VEGETATION AND OTHER VISUAL OBSTRUCTIONS SHALL BE LIMITED TO A HEIGHT OF NOT MORE THAN TWO FEET (2') ABOVE THE STREET GRADE.

> RAWN BY HECKED BY OB NUMBER **BUCKS COUNTY, PENNSYLVANIA**

THIS PLAN IS FOR LANDSCAPE PURPOSES ONLY. PENNSYLVANIA REGISTERED

7 of 18

TMP'S 26-006-101-001

**NEW BRITAIN TOWNSHIP** 

PER TOWNSHIP ENGINEER REVIEW LETTER

**SERIAL NO. 20223183581** 

now what's below. STOPI CALL 1-800-242-1770

**GRAPHIC SCALE** 

1 INCH = 20 FT

ONLY THOSE PLANS WHICH CONTAIN A DIGIT MPRESSED, OR COLORIZED INK SEAL OF TH RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERE

ALID. THIS PLAN HAS BEEN SPECIFICALLY PREPART OR THE OWNER DESIGNATED HEREON. AN

MODIFICATION, REVISION, DUPLICATION OR WITHOUT THE WRITTEN CONSENT OF VAN CL

NGINEERING ASSOCIATES IS PROHIBITE RELIANCE ON THIS PLAN FOR ANY PURPO

THER THAN THAT WHICH IS INTENDED SHALL

AT THE SOLE DISCRETION AND LIABILITY OF APPLICABLE PARTY.

EAN DAVID McGRANAHAN

VAN CLEEF ENGINEERING ASSOCIATES, LLC

501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM

PHONE (215) 345-1876

LANDSCAPE

**PLAN** 

DESIGNED BY

INSYLVANIA PROFESSIONAL

NEER NUMBER 085090

Call before you dig. PA Law requires 3 working days noti

10 20

PLAN NOTATION

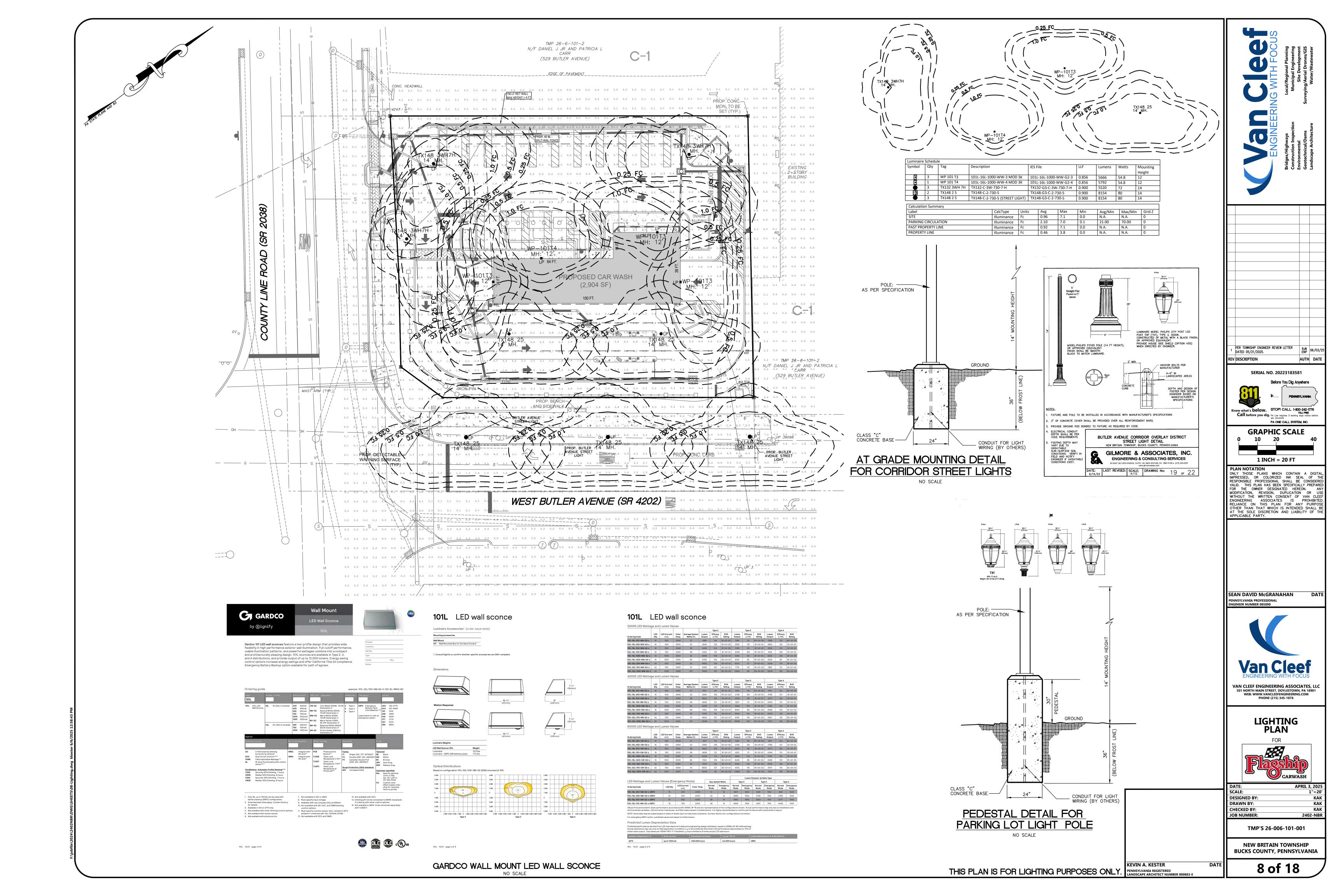
Before You Dig Anywhere

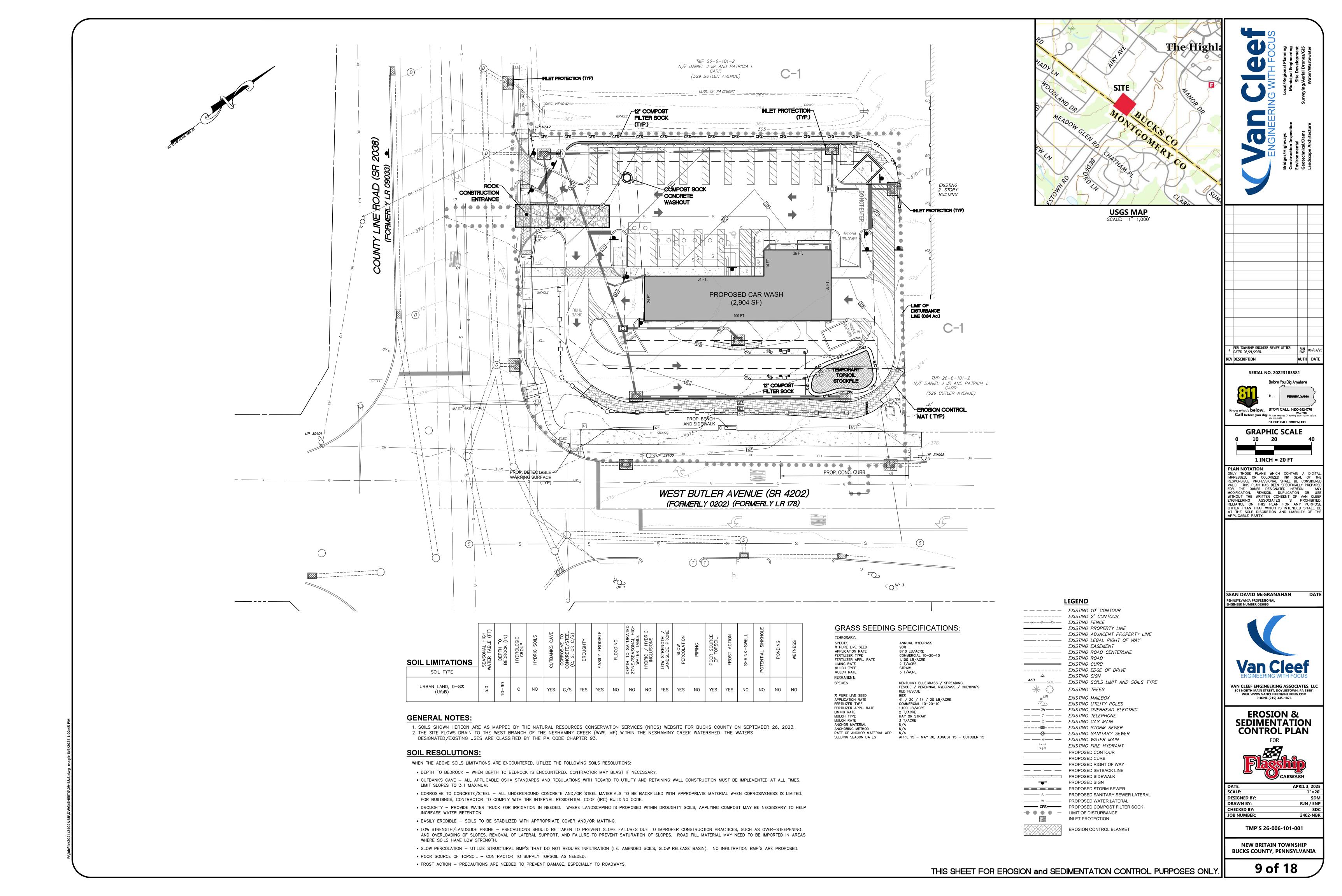
PA ONE CALL SYSTEM, INC.

AUTH DATE

DATED 05/21/2025.

V DESCRIPTION





# GENERAL EROSION & SEDIMENT CONTROL NOTES

- 1. ALL EARTH DISTURBANCES, INCLUDING CLEARING AND GRUBBING AS WELL AS CUTS AND FILLS SHALL BE DONE IN ACCORDANCE WITH THE APPROVED E&S PLAN. A COPY OF THE APPROVED DRAWINGS (STAMPED, SIGNED AND DATED BY THE REVIEWING AGENCY) MUST BE AVAILABLE AT THE PROJECT SITE AT ALL TIMES. THE REVIEWING AGENCY SHALL BE NOTIFIED OF ANY CHANGES TO THE APPROVED PLAN PRIOR TO IMPLEMENTATION OF THOSE CHANGES. THE REVIEWING AGENCY MAY REQUIRE A WRITTEN SUBMITTAL OF THOSE CHANGES FOR REVIEW AND APPROVAL AT ITS DISCRETION.
- 2. AT LEAST 7 DAYS PRIOR TO STARTING ANY EARTH DISTURBANCE ACTIVITIES, INCLUDING CLEARING AND GRUBBING, THE OWNER AND/OR OPERATOR SHALL INVITE ALL CONTRACTORS, THE LANDOWNER, APPROPRIATE MUNICIPAL OFFICIALS, THE E&S PLAN PREPARER, THE PCSM PLAN PREPARER, THE LICENSED PROFESSIONAL RESPONSIBLE FOR OVERSIGHT OF CRITICAL STAGES OF IMPLEMENTATION OF THE PCSM PLAN, AND A REPRESENTATIVE FROM THE LOCAL CONSERVATION DISTRICT TO AN ON-SITE PRECONSTRUCTION MEETING.
- 3. AT LEAST 3 DAYS PRIOR TO STARTING ANY EARTH DISTURBANCE ACTIVITIES, OR EXPANDING INTO AN AREA PREVIOUSLY UNMARKED, THE PENNSYLVANIA ONE CALL SYSTEM INC. SHALL BE NOTIFIED AT 1-800-242-1776 FOR THE LOCATION OF EXISTING UNDERGROUND UTILITIES.
- 4. ALL EARTH DISTURBANCE ACTIVITIES SHALL PROCEED IN ACCORDANCE WITH THE SEQUENCE PROVIDED ON THE PLAN DRAWINGS. DEVIATION FROM
- THAT SEQUENCE MUST BE APPROVED IN WRITING FROM THE LOCAL CONSERVATION DISTRICT OR BY THE DEPARTMENT PRIOR TO IMPLEMENTATION.

  5. AREAS TO BE FILLED ARE TO BE CLEARED, GRUBBED, AND STRIPPED OF TOPSOIL TO REMOVE TREES, VEGETATION, ROOTS AND OTHER
- OBJECTIONABLE MATERIAL.

  6. CLEARING, GRUBBING, AND TOPSOIL STRIPPING SHALL BE LIMITED TO THOSE AREAS DESCRIBED IN EACH STAGE OF THE CONSTRUCTION SEQUENCE.
  GENERAL SITE CLEARING, GRUBBING AND TOPSOIL STRIPPING MAY NOT COMMENCE IN ANY STAGE OR PHASE OF THE PROJECT UNTIL THE E&S
- 7. AT NO TIME SHALL CONSTRUCTION VEHICLES BE ALLOWED TO ENTER AREAS OUTSIDE THE LIMIT OF DISTURBANCE BOUNDARIES SHOWN ON THE PLAN MAPS. THESE AREAS MUST BE CLEARLY MARKED AND FENCED OFF BEFORE CLEARING AND GRUBBING OPERATIONS BEGIN.

BMPS SPECIFIED BY THE BMP SEQUENCE FOR THAT STAGE OR PHASE HAVE BEEN INSTALLED AND ARE FUNCTIONING AS DESCRIBED IN THIS E&S

- 8. TOPSOIL REQUIRED FOR THE ESTABLISHMENT OF VEGETATION SHALL BE STOCKPILED AT THE LOCATION(S) SHOWN ON THE PLAN MAPS(S) IN THE AMOUNT NECESSARY TO COMPLETE THE FINISH GRADING OF ALL EXPOSED AREAS THAT ARE TO BE STABILIZED BY VEGETATION. EACH STOCKPILE SHALL BE PROTECTED IN THE MANNER SHOWN ON THE PLAN DRAWINGS. STOCKPILE HEIGHTS SHALL NOT EXCEED 35 FEET. STOCKPILE SLOPES SHALL BE 2H:1V OR FLATTER.
- 9. IMMEDIATELY UPON DISCOVERING UNFORESEEN CIRCUMSTANCES POSING THE POTENTIAL FOR ACCELERATED EROSION AND/OR SEDIMENT POLLUTION, THE OPERATOR SHALL IMPLEMENT APPROPRIATE BEST MANAGEMENT PRACTICES TO ELIMINATE POTENTIAL FOR EROSION AND SEDIMENT POLLUTION AND NOTIFY THE LOCAL CONSERVATION DISTRICT AND/OR THE REGIONAL OFFICE OF THE DEPARTMENT.
- 10. ALL BUILDING MATERIAL AND WASTES MUST BE REMOVED FROM THE SITE AND RECYCLED IN ACCORDANCE WITH DEP'S SOLID WASTE REGULATIONS (25 PA CODE 260.1 ET SEQ., 271.1 ET SEQ., AND 287.1 ET SEQ.), AND/OR ANY ADDITIONAL LOCAL, STATE OR FEDERAL REGULATIONS. NO BUILDING MATERIALS (USED OR UNUSED) OR WASTE MATERIALS SHALL BE BURNED, BURIED, DUMPED OR DISCHARGED AT THE SITE.
- 11. ALL OFF-SITE WASTE AND BORROW AREAS MUST HAVE AN E&S PLAN APPROVED BY THE LOCAL CONSERVATION DISTRICT OR THE DEPARTMENT FULLY IMPLEMENTED PRIOR TO BEING ACTIVATED.
- 12. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT ANY MATERIAL BROUGHT ON SITE IS CLEAN FILL. FORM FP-001 MUST BE RETAINED BY THE PROPERTY OWNER FOR ANY FILL MATERIAL AFFECTED BY A SPILL OR RELEASE OF A REGULATED SUBSTANCE BUT QUALIFYING AS CLEAN FILL DUE TO ANALYTICAL TESTING.
- 13. ALL PUMPING OF WATER FROM ANY WORK AREA SHALL BE DONE ACCORDING TO THE PROCEDURE DESCRIBED IN THIS PLAN, OVER UNDISTURBED VEGETATED AREAS.
- 14. UNTIL THE SITE IS STABILIZED, ALL EROSION AND SEDIMENT BMPS SHALL BE MAINTAINED PROPERLY. MAINTENANCE SHALL INCLUDE INSPECTIONS OF ALL EROSION AND SEDIMENT BMPS AFTER EACH RUNOFF EVENT AND ON A WEEKLY BASIS. ALL PREVENTATIVE AND REMEDIAL MAINTENANCE WORK, INCLUDING CLEAN OUT, REPAIR, REPLACEMENT, REGRADING, RESEEDING, REMULCHING AND RENETTING MUST BE PERFORMED IMMEDIATELY. IF THE E&S BMPS FAIL TO PERFORM AS EXPECTED, REPLACEMENT BMPS, OR MODIFICATIONS OF THOSE INSTALLED WILL BE REQUIRED.
- 15. A LOG SHOWING DATES THAT E&S BMPS WERE INSPECTED AS WELL AS ANY DEFICIENCIES FOUND AND THE DATE THEY WERE CORRECTED SHALL BE MAINTAINED ON THE SITE AND BE MADE AVAILABLE TO REGULATORY AGENCY OFFICIALS AT THE TIME OF INSPECTION. WRITTEN DOCUMENTATION OF INSPECTION AND REPLACEMENT OF BMP'S BY CONTRACTOR.
- 16. SEDIMENT TRACKED ONTO ANY PUBLIC ROADWAY OR SIDEWALK SHALL BE IMMEDIATELY RETURNED TO THE CONSTRUCTION SITE AND DISPOSED IN THE MANNER DESCRIBED IN THIS PLAN. IN NO CASE SHALL THE SEDIMENT BE WASHED, SHOVELED, OR SWEPT INTO ANY ROADSIDE DITCH, STORM SEWER, OR SURFACE WATER.
- 17. ALL SEDIMENT REMOVED FROM BMPS SHALL BE DISPOSED OF IN THE MANNER DESCRIBED ON THE PLAN DRAWINGS.
- 18. AREAS WHICH ARE TO BE TOPSOILED SHALL BE SCARIFIED TO A MINIMUM DEPTH OF 8" OR TO THE DEPTH ENCOUNTERED WITHIN THE SITE PRIOR TO PLACEMENT OF TOPSOIL. AREAS TO BE VEGETATED SHALL HAVE A MINIMUM 8" OF TOPSOIL (OR TO THE DEPTH ENCOUNTERED WITHIN THE SITE, WHICHEVER IS GREATER) IN PLACE PRIOR TO SEEDING AND MULCHING.
- 19. ALL FILLS SHALL BE COMPACTED AS REQUIRED TO REDUCE EROSION, SLIPPAGE, SETTLEMENT, SUBSIDENCE OR OTHER RELATED PROBLEMS. FILL INTENDED TO SUPPORT BUILDINGS, STRUCTURES AND CONDUITS, ETC. SHALL BE COMPACTED IN ACCORDANCE WITH LOCAL REQUIREMENTS OR CODES.
- 20. ALL EARTHEN FILLS SHALL BE PLACED IN COMPACTED LAYERS NOT TO EXCEED 9 INCHES IN THICKNESS.
- 21. FILL MATERIALS SHALL BE FREE OF FROZEN PARTICLES, BRUSH, ROOTS, SOD, OR OTHER FOREIGN OR OBJECTIONABLE MATERIALS THAT WOULD INTERFERE WITH OR PREVENT CONSTRUCTION OF SATISFACTORY FILLS.
- 22. FROZEN MATERIALS OR SOFT, MUCKY, OR HIGHLY COMPRESSIBLE MATERIALS SHALL NOT BE INCORPORATED INTO FILLS.
- 23. FILL SHALL NOT BE PLACED ON SATURATED OR FROZEN SURFACES.
- 24. SEEPS OR SPRINGS ENCOUNTERED DURING CONSTRUCTION SHALL BE HANDLED IN ACCORDANCE WITH THE STANDARD AND SPECIFICATION FOR SUBSURFACE DRAIN OR OTHER APPROVED METHOD.
- 25. ALL GRADED AREAS SHALL BE PERMANENTLY STABILIZED IMMEDIATELY UPON REACHING FINISHED GRADE. CUT SLOPES IN COMPETENT BEDROCK AND ROCK FILLS NEED NOT BE VEGETATED. SEEDED AREAS WITHIN 50 FEET OF A SURFACE WATER, OR AS OTHERWISE SHOWN ON THE PLAN DRAWINGS, SHALL BE BLANKETED ACCORDING TO THE STANDARDS OF THIS PLAN.
- 26. IMMEDIATELY AFTER EARTH DISTURBANCE ACTIVITIES CEASE IN ANY AREA OR SUBAREA OF THE PROJECT, THE OPERATOR SHALL STABILIZE ALL DISTURBED AREAS. DURING NON-GERMINATING MONTHS, MULCH OR PROTECTIVE BLANKETING SHALL BE APPLIED AS DESCRIBED IN THE PLAN. AREAS NOT AT FINISHED GRADE, WHICH WILL BE REACTIVATED WITHIN 1 YEAR, MAY BE STABILIZED IN ACCORDANCE WITH THE TEMPORARY STABILIZATION SPECIFICATIONS. THOSE AREAS WHICH WILL NOT BE REACTIVATED WITHIN 1 YEAR SHALL BE STABILIZED IN ACCORDANCE WITH THE PERMANENT STABILIZATION SPECIFICATIONS.
- 27. PERMANENT STABILIZATION IS DEFINED AS A MINIMUM UNIFORM, PERENNIAL 70% VEGETATIVE COVER OR OTHER PERMANENT NON-VEGETATIVE COVER WITH A DENSITY SUFFICIENT TO RESIST ACCELERATED EROSION. CUT AND FILL SLOPES SHALL BE CAPABLE OF RESISTING FAILURE DUE TO SLUMPING, SLIDING, OR OTHER MOVEMENTS.
- 28. E&S BMPS SHALL REMAIN FUNCTIONAL AS SUCH UNTIL ALL AREAS TRIBUTARY TO THEM ARE PERMANENTLY STABILIZED OR UNTIL THEY ARE REPLACED BY ANOTHER BMP APPROVED BY THE LOCAL CONSERVATION DISTRICT OR THE DEPARTMENT.
- 29. UPON COMPLETION OF ALL EARTH DISTURBANCE ACTIVITIES AND PERMANENT STABILIZATION OF ALL DISTURBED AREAS, THE OWNER AND/OR OPERATOR SHALL CONTACT THE LOCAL CONSERVATION DISTRICT FOR AN INSPECTION PRIOR TO REMOVAL/CONVERSION OF THE E&S BMPS.
- 30. AFTER FINAL SITE STABILIZATION HAS BEEN ACHIEVED, TEMPORARY EROSION AND SEDIMENT BMPS MUST BE REMOVED OR CONVERTED TO PERMANENT POST CONSTRUCTION STORMWATER MANAGEMENT BMPS. AREAS DISTURBED DURING REMOVAL OR CONVERSION OF THE BMPS SHALL BE STABILIZED IMMEDIATELY. IN ORDER TO ENSURE RAPID REVEGETATION OF DISTURBED AREAS, SUCH REMOVAL/CONVERSIONS ARE TO BE DONE ONLY DURING THE GERMINATING SEASON.
- 31. FAILURE TO CORRECTLY INSTALL E&S BMPS, FAILURE TO PREVENT SEDIMENT—LADEN RUNOFF FROM LEAVING THE CONSTRUCTION SITE, OR FAILURE TO TAKE IMMEDIATE CORRECTIVE ACTION TO RESOLVE FAILURE OF E&S BMPS MAY RESULT IN ADMINISTRATIVE, CIVIL, AND/OR CRIMINAL PENALTIES BEING INSTITUTED BY THE DEPARTMENT AS DEFINED IN SECTION 602 OF THE PENNSYLVANIA CLEAN STREAMS LAW. THE CLEAN STREAMS LAW PROVIDES FOR UP TO \$10,000 PER DAY IN CIVIL PENALTIES, UP TO \$10,000 IN SUMMARY CRIMINAL PENALTIES, AND UP TO \$25,000 IN MISDEMEANOR CRIMINAL PENALTIES FOR EACH VIOLATION.
- 32. FILL MATERIAL FOR EMBANKMENTS SHALL BE FREE OF ROOTS, OR OTHER WOODY VEGETATION, ORGANIC MATERIAL, LARGE STONES, AND OTHER OBJECTIONABLE MATERIALS.
- 33. STOCKPILE HEIGHTS MUST NOT EXCEED 35 FEET; STOCKPILE SLOPES MUST NOT EXCEED 2.1.
- 34. THE OPERATOR/RESPONSIBLE PERSON (O/RP) ON SITE SHALL ASSURE THAT THE APPROVED EROSION AND SEDIMENT CONTROL PLAN IS PROPERLY AND COMPLETELY IMPLEMENTED.
- 35. THE O/RP SHALL ASSURE THAT AN EROSION AND SEDIMENT CONTROL PLAN HAS BEEN PREPARED, APPROVED BY THE BUCKS COUNTY CONSERVATION DISTRICT AND IS BEING IMPLEMENTED AND MAINTAINED FOR ALL SOIL AND/OR ROCK SPOIL AND BORROW AREAS REGARDLESS OF THEIR LOCATIONS.
- 36. ALL PUMPING OF SEDIMENT-LADEN WATER SHALL BE THROUGH A SEDIMENT CONTROL BMP SUCH AS A PUMPED WATER FILTER BAG DISCHARGING OVER UNDISTURBED AREAS.
- 37. A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN MUST BE AVAILABLE ON THE PROJECT SITE AT ALL TIMES.
- 38. EROSION AND SEDIMENT BMPS MUST BE CONSTRUCTED, STABILIZED AND FUNCTIONAL BEFORE SITE DISTURBANCE BEGINS WITHIN THE TRIBUTARY AREAS OF THOSE BMPS.
- 39. DISTURBED AREAS THAT ARE AT FINISHED GRADE OR WHICH WILL NOT BE RE-DISTURBED WITHIN ONE YEAR MUST BE STABILIZED IN ACCORDANCE WITH PERMANENT VEGETATIVE STABILIZATION SPECIFICATIONS.
- 40. AN AREA SHALL BE CONSIDERED TO HAVE ACHIEVED FINAL STABILIZATION WHEN IT HAS A MINIMUM UNIFORM 70% (PERCENT) VEGETATIVE OR OTHER PERMANENT NON-VEGETATIVE COVER WITH A DENSITY SUFFICIENT TO RESIST ACCELERATED SURFACE EROSION AND SUBSURFACE CHARACTERISTICS SUFFICIENT TO RESIST SLIDING AND OTHER MOVEMENTS.
- 41. SEDIMENT REMOVED FROM BMPS SHALL BE DISPOSED OF ON-SITE IN LANDSCAPED AREAS OUTSIDE OF STEEP SLOPES, WETLANDS, FLOODPLAINS OR DRAINAGE SWALES AND IMMEDIATELY STABILIZED OR PLACED IN SOIL STOCKPILES AND STABILIZED.
- 42. BMPs SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER MEASURABLE STORM EVENTS (I.E. AT LEAST 0.25 INCH).
- 43. TEMPORARY STABILIZATION SHALL BE PROVIDED TO ALL BARE SOIL AREAS THAT WILL REMAIN UNDISTURBED FOR 4 OR MORE DAYS IN THE FORM OF 3 TON/ACRE STRAW MULCH OR EQUIVALENT.

### CONSTRUCTION SEQUENCE

- 1. AT LEAST 7 DAYS PRIOR TO START OF WORK OR EARTH DISTURBANCE ACTIVITIES, A PRE—CONSTRUCTION MEETING SHALL BE HELD INCLUDING THE OWNER, SITE CONTRACTOR(S), TOWNSHIP ENGINEER(S), COUNTY CONSERVATION DISTRICT, AND THE PLAN DESIGNER.
- 2. THE LIMITS OF DISTURBANCE SHALL BE DELINEATED WITH SURVEY STAKES OR SIMILAR, PRIOR TO ANY DISTURBANCE AND SHALL NOT BE DISTURBED DURING SITE CONSTRUCTION EXCEPT FOR TEMPORARY IMPACTS FOR MITIGATION OR RESTORATION PER THE PLAN.
- 3. INSTALL THE ROCK CONSTRUCTION ENTRANCE PER THE PLAN.
- 4. CONSTRUCTION FENCING SHALL BE INSTALLED ALONG THE SOUTH AND EASTERN PROPERTY LINES. PLACE COMPOST FILTER SOCK AND INLET PROTECTION AS DELINEATED ON THE PLAN AND DOWNSTREAM OF ALL EXCAVATED OR TOPSOIL STOCKPILING AREAS TO BE DISTURBED. INSTALL CONCRETE WASHOUT FACILITY PER THE PLAN.
- 5. DEMOLISH EXISTING BUILDING.
- 6. STRIP TOPSOIL WITHIN AREAS OF PROPOSED EARTHWORK AND STOCKPILE PER THE PLAN. CESSATION OF GRADING ACTIVITIES FOR FOUR (4) DAYS OR LONGER REQUIRES TEMPORARY SEEDING.
- 7. BEGIN CONSTRUCTION OF PROPOSED RETAINING WALL, STORM SEWERS, SEWER/GAS/WATER LATERALS, BUILDING FEATURES, DRIVEWAY AND PARKING AREAS, FLAGSHIP CARWASH SIGNAGE, SIDEWALK AND ADA FEATURES AND ANY OTHER ASSOCIATED FEATURES.
  7.1. CONSTRUCTION OF THE SANITARY SEWER LATERAL WILL REQUIRE TEMPORARY REMOVAL OF THE ROCK CONSTRUCTION ENTRANCE. UPON COMPLETION OF THE SANITARY SEWER LATERAL CONNECTION WORK, THE ROCK CONSTRUCTION ENTRANCE SHALL BE REINSTALLED IN THE
- LOCATION DEPICTED ON THE PLANS. CARE SHALL BE TAKEN AS TO NOT TRACK SEDIMENT ONTO COUNTY LINE ROAD DURING THE CONSTRUCTION OF THE SANITARY SEWER LATERAL CONNECTION.
- 9. FINISH GRADE AND SPREAD TOPSOIL. SEED AND MULCH EACH AREA OF DISTURBANCE IMMEDIATELY AFTER CONSTRUCTION IS COMPLETED. AS DISTURBED AREAS WITHIN A PROJECT APPROACH FINAL GRADE, PREPARATIONS SHOULD BE MADE FOR SEEDING AND MULCHING TO BEGIN (I.E. ANTICIPATE COMPLETION DATE AND SCHEDULE SEEDING). PRIOR TO PLACING TOPSOIL, SUBSOIL SHALL BE SCARIFIED. PLANT LANDSCAPING AS DEPICTED ON THE LANDSCAPE AND LIGHTING PLAN.
- 10. REMOVE TEMPORARY CONTROL MEASURES AFTER UNIFORM EROSION RESISTANT PERENNIAL VEGETATION HAS BEEN ESTABLISHED, MINIMUM OF UNIFORM COVERAGE OR A DENSITY OF 70% ACROSS THE DISTURBED AREA, TO THE POINT WHERE THE SURFACE SOIL IS CAPABLE OF RESISTING EROSION DURING RUNOFF EVENTS AND STABILIZATION OF THE SITE IS COMPLETE TO THE SATISFACTION OF THE COUNTY CONSERVATION DISTRICT. AREAS DISTURBED DURING THE REMOVAL OF THE CONTROLS MUST BE STABILIZED. THE COUNTY CONSERVATION DISTRICT AND TOWNSHIP SHALL BE NOTIFIED PRIOR TO REMOVAL OF ANY EROSION CONTROLS.

### PROJECT CONSTRUCTION WASTES

- 1. TREES CHIPPED AND REMOVED OFFSITE FOR MULCH.
- 2. BUILDING MATERIALS SHALL BE REMOVED AND DISPOSED OF AT PROPER RECYCLING FACILITY AS PER NOTE #6 OF THE GENERAL EROSION & SEDIMENT CONTROL NOTES.
- 3. ROCK WASTE ALL ROCKS THAT ARE SUITABLE FOR LANDSCAPING USES SHALL BE USED ON SITE OR AT ANOTHER FACILITY.
- 4. SOIL WASTE ALL EXCAVATED SOIL SHALL BE USED ON SITE, AS FILL MATERIAL WHERE POSSIBLE. ANY EXCESS MATERIAL NOT USED ON SITE MAY ONLY BE DISPOSED AT A FACILITY WITH A VALID EARTH DISTURBANCE AND NPDES PERMIT APPROVED AND WERE ALL BMPS ARE PROPERLY INSTALLED. ALL SEDIMENT REMOVED DURING MAINTENANCE AND CLEANING OF EROSION AND SEDIMENTATION CONTROLS DEVICES WILL BE INCORPORATED INTO THE FINE GRADING UPSTREAM FROM A SEDIMENT CONTROL DEVICE. EXCESS MATERIAL THAT IS REMOVED FROM EROSION AND SEDIMENTATION CONTROL MEASURES AND CANNOT BE INCORPORATED INTO THE FINE GRADING STABILIZATION SHALL BE REMOVED FROM THE SITE AND MAY ONLY BE DISPOSED AT A FACILITY WITH A VALID EARTH DISTURBANCE AND NPDES PERMIT APPROVAL AND WERE ALL BMPS ARE PROPERLY INSTALLED.
- 5. ACCUMULATED SEDIMENT FROM COMPOST FILTER SOCKS TO BE RETURNED TO SITE UPSTREAM OF ALL PROPOSED BMP'S.

### MAINTENANCE OF FACILITIES

1. SILT FENCE/FILTER SOCK SHOULD BE INSPECTED AND MAINTAINED ON A DAILY BASIS.

8. ALL 3:1 SLOPES SHALL BE STABILIZED AND LINED WITH TEMPORARY EROSION CONTROL MATTING.

2. UNTIL THE SITE IS STABILIZED, ALL EROSION AND SEDIMENT CONTROL DEVICES MUST BE MAINTAINED PROPERLY. MAINTENANCE MUST INCLUDE INSPECTIONS OF ALL EROSION AND SEDIMENT CONTROLS AFTER EACH RUNOFF EVENT AND ON A WEEKLY BASIS. ALL PREVENTATIVE AND REMEDIAL MAINTENANCE WORK, INCLUDING CLEAN OUT, REPAIR, REPLACEMENT, REGRADING, RESEEDING, REMULCHING, AND RENETTING MUST BE PERFORMED IMMEDIATELY. IF E&S CONTROL BMPS FAIL TO PERFORM AS EXPECTED, REPLACEMENT BMPS OR MODIFICATIONS OF INSTALLED MEASURES WILL BE

3. CONTRACTOR SHALL MAINTAIN AND MAKE AVAILABLE TO THE COUNTY CONSERVATION DISTRICT COMPLETE, WRITTEN INSPECTION LOGS OF THE ABOVE NOTED INSPECTION AND MAINTENANCE.

- 4. SEEDING, MULCHING AND FERTILIZING SHALL BE IN ACCORDANCE WITH THE SEEDING AND MULCHING SCHEDULE.
- 5. THE CONTRACTOR SHALL HAVE AVAILABLE WATER TRUCKS OR OTHER MEANS OF CONTROLLING EXCESSIVE DUST AND AIRBORNE DEBRIS.
- 6. ALL AREAS OF CONCENTRATED SURFACE DRAINAGE SHALL BE SEEDED AND MULCHED, AND PROTECTED WITH TEMPORARY TURF REINFORCEMENT MAT: NORTH AMERICAN GREEN #D575 (OR EQUAL). IF AREAS ARE TO BE SODDED, TURF REINFORCEMENT IS NOT REQUIRED.
- 7. SEEDED AREAS THAT HAVE WASHED AWAY SHALL BE FILLED AND GRADED AS NECESSARY AND THEN RESEEDED. A BURLAP OR STRAW COVER WILL BE APPLIED TO RETAIN THE SEED UNTIL IT HAS A CHANCE TO ROOT PROPERLY.
- 8. THE ABOVE PROCEDURE SHALL BE REPEATED AFTER EACH SIZABLE STORM UNTIL NO MORE SIGNS OF EROSION ARE EVIDENT. AT MONTHLY INTERVALS THEREAFTER, INSPECTIONS AND NECESSARY CLEANING WILL BE DONE. TRASH THAT IS REMOVED FROM ANY OF THE CONTROL DEVICES SHALL BE DISPOSED OF AT AN APPROVED DISPOSAL AREA. SILT THAT HAS ACCUMULATED SHALL BE REMOVED AND ALLOWED TO DRY AND USED AS FILL WHEREVER REQUIRED ON THE SITE.
- 9. IMMEDIATELY UPON DISCOVERING UNFORESEEN CIRCUMSTANCES POSING THE POTENTIAL FOR ACCELERATED EROSION AND/OR SEDIMENT POLLUTION, THE CONTRACTOR SHALL IMPLEMENT APPROPRIATE MEASURES TO ADDRESS SAID CIRCUMSTANCES.

# FILL MATERIALS

IF THE SITE WILL NEED TO HAVE FILL IMPORTED FROM AN OFF SITE LOCATION, THE RESPONSIBILITY FOR PERFORMING ENVIRONMENTAL DUE DILIGENCE AND THE DETERMINATION OF CLEAN FILL WILL RESIDE WITH THE OPERATOR.

CLEAN FILL: IS DEFINED AS UNCONTAMINATED, NON-WATER SOLUBLE, ON-DECOMPOSABLE, INERT, SOLID MATERIAL. THE TERM INCLUDES SOIL, ROCK, STONE, DREDGED MATERIAL, USED ASPHALT, AND BRICK, BLOCK OR CONCRETE FROM CONSTRUCTION AND DEMOLITION ACTIVITIES HAT IS SEPARATE FROM OTHER WASTE AND IS RECOGNIZABLE AS SUCH. THE TERM DOES NOT INCLUDE MATERIALS PLACED IN OR ON THE WATERS OF THE COMMONWEALTH UNLESS OTHERWISE AUTHORIZED. (THE ERM "USED ASPHALT" DOES NOT INCLUDE MILLED ASPHALT OR ASPHALT THAT HAS BEEN PROCESSED FOR RE-USE.)

ENVIRONMENTAL DUE DILIGENCE: INVESTIGATIVE TECHNIQUES, INCLUDING, BUT NOT LIMITED TO, VISUAL PROPERTY INSPECTIONS, ELECTRONIC DATA BASE SEARCHES, REVIEW OF PROPERTY OWNERSHIP, REVIEW OF PROPERTY USE HISTORY, SANBORN MAPS, ENVIRONMENTAL QUESTIONNAIRES, TRANSACTION SCREENS, ANALYTICAL TESTING, ENVIRONMENTAL ASSESSMENTS OR AUDITS. ANALYTICAL TESTING IS NOT A REQUIRED PART OF DUE DILIGENCE UNLESS VISUAL INSPECTION AND/OR REVIEW OF THE PAST LAND USE OF THE PROPERTY INDICATES THAT THE FILL MAY HAVE BEEN SUBJECTED TO A SPILL OR RELEASE OF REGULATED SUBSTANCE, IF THE FILL MAY HAVE BEEN AFFECTED BY A SPILL OR RELEASE OF A REGULATED SUBSTANCE, IT MUST BE TESTED TO DETERMINE IF IT QUALIFIES AS CLEAN FILL. TESTING SHOULD BE PERFORMED IN ACCORDANCE WITH APPENDIX A OF THE DEPARTMENT'S POLICY "MANAGEMENT OF FILL".

FILL MATERIAL THAT DOES NOT QUALIFY AS CLEAN FILL IS REGULATED FILL. REGULATED FILL IS WASTE AND MUST BE MANAGED IN ACCORDANCE WITH THE DEPARTMENT'S MUNICIPAL OR RESIDUAL WASTE REGULATIONS BASED ON 25 PA. CODE CHAPTERS 287 RESIDUAL WASTE MANAGEMENT OR 271 MUNICIPAL WASTE MANAGEMENT, WHICHEVER IS APPLICABLE.

# E&S PLAN PLANNING & DESIGN NOTES

- 1. THE CONTRACTOR SHALL MINIMIZE THE EXTENT AND DURATION OF THE EARTH DISTURBANCE BY LIMITING THE CLEARING, GRUBBING, AND TOPSOIL STRIPPING TO AREAS IN EACH PHASE OF THE CONSTRUCTION SEQUENCE. GENERAL SITE CLEARING, GRUBBING AND TOPSOIL STRIPPING MAY NOT COMMENCE IN ANY STAGE OR PHASE OF THE PROJECT UNTIL THE E&S BMP'S SPECIFIED BY THE CONSTRUCTION SEQUENCE FOR THAT PHASE HAVE BEEN INSTALLED AND ARE FUNCTIONING AS DESCRIBED IN THE E&S PLAN. IMMEDIATELY AFTER EARTH DISTURBANCE ACTIVITIES CEASE, THE CONTRACTOR SHALL STABILIZE AND AREAS DISTURBED BY ANY ACTIVITIES.
- 2. TO MAXIMIZE THE PROTECTION OF EXISTING DRAINAGE FEATURES AND VEGETATION, E&S BMP'S MUST BE CONSTRUCTED, STABILIZED, AND FUNCTIONAL BEFORE SITE DISTURBANCE BEGINS WITHIN THE TRIBUTARY AREAS TO THOSE BMP'S. E&S BMP'S SHALL REMAIN FUNCTIONAL UNTIL ALL AREAS TRIBUTARY TO THEM ARE PERMANENTLY STABILIZED. AT NO TIME WILL CONSTRUCTION VEHICLES BE ALLOWED TO ENTER AREAS OUTSIDE THE LIMIT OF DISTURBANCE BOUNDARY. SEDIMENT TRACKED ONTO ANY PUBLIC ROADWAYS SHALL BE RETURNED TO THE CONSTRUCTION SITE BY THE END OF EACH WORK DAY. IN NO CASE SHALL THE SEDIMENT BE WASHED, SHOVELED, OR SWEPT INTO ANY ROADSIDE DITCH, STORM SEWER, OR SURFACE WATER. ALL GRADED AREAS SHALL BE PERMANENTLY STABILIZED IMMEDIATELY UPON REACHING FINISHED GRADE.
- 3. TO MINIMIZE SOIL COMPACTION, NO LAND CLEARING AND/OR GRADING SHALL BE PERFORMED OUTSIDE OF LIMIT OF DISTURBANCE FENCING.
  PROPER CARE SHALL BE TAKEN WITHIN UNDERGROUND INFILTRATION TRENCH TO MINIMIZE SOIL COMPACTION.
- 4. THE FOLLOWING MEASURES OR CONTROLS UTILIZED TO MINIMIZE GENERATION OF INCREASED STORMWATER RUNOFF: A ROCK CONSTRUCTION ENTRANCE, COMPOST FILTER SOCKS AROUND THE DISTURBANCE, A CONCRETE WASHOUT, EROSION CONTROL BLANKET, AND TEMPORARY/PERMANENT STABILIZATION OF ALL LOT FEATURES.
- 5. THERE ARE NO PROPOSED INFILTRATION BMP'S OUTSIDE PROPOSED GRADING AREAS.
- 6. THERE ARE NO EXISTING/PROPOSED RIPARIAN FOREST BUFFERS WITHIN THE LIMITS OF DISTURBANCE THEREFORE NO ASSOCIATED REQUIREMENTS ARE SHOWN ON THE PLANS.
- 7. THIS PROJECT IS NOT LOCATED IN A SPECIAL PROTECTION (HQ/EV) WATERSHED, THEREFORE NO ANTIDEGRADATION ANALYSIS IS REQUIRED.
- 8. THERE ARE NO NATURALLY OCCURRING GEOLOGIC FORMATIONS (KARST SOILS) THAT MAY CAUSE POLLUTION WITHIN THE PROJECT LIMIT OF DISTURBANCE; THEREFORE, MEASURES TO AVOID/MINIMIZE/OR MITIGATE ARE NOT NECESSARY.

# GRASS SEEDING SPECIFICATIONS:

TEMPORARY:

ANNUAL RYEGRASS % PURE LIVE SEED APPLICATION RATE 87.0 LB/ACRE FERTILIZER TYPE COMMERCIAL 10-20-10 FERTILIZER APPL, RATE 1,100 LB/ACRE LIMING RATE 2 T/ACRE MULCH TYPE STRAW MULCH RATE 3 T/ACRE PERMANENT: KENTUCKY BLUEGRASS / SPREADING SPECIES FESCUE / PERENNIAL RYEGRASS / CHEWING'S RED FESCUE % PURE LIVE SEE APPLICATION RATE 41 / 20 / 14 / 20 LB/ACRE FERTILIZER TYPE COMMERCIAL 10-20-10 FERTILIZER APPL, RATE 1,100 LB/ACRE LIMING RATE MULCH TYPE HAY OR STRAW MULCH RATE 3 T/ACRE ANCHOR MATERIAL ANCHORING METHOD RATE OF ANCHOR MATERIAL APPL. N/A APRIL 15 - MAY 30, AUGUST 15 - OCTOBER 15 SEEDING SEASON DATES

THIS SHEET FOR EROSION and SEDIMENTATION CONTROL PURPOSES ONLY.



1 PER TOWNSHIP ENGINEER REVIEW LETTER RJN 06/03/
REV DESCRIPTION AUTH DAT

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PLAN NOTATION

ONLY THOSE PLANS WHICH CONTAIN A DIGITA
IMPRESSED, OR COLORIZED INK SEAL OF THE
RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERE
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ENGINEERING ASSOCIATES IS PROHIBITE
RELIANCE ON THIS PLAN FOR ANY PURPOS

THER THAN THAT WHICH IS INTENDED SHALL

THE SOLE DISCRETION AND LIABILITY OF

APPLICABLE PARTY.

SEAN DAVID McGRANAHAN

ISYLVANIA PROFESSIONA



VAN CLEEF ENGINEERING ASSOCIATES, LLC
501 NORTH MAIN STREET, DOYLESTOWN, PA 18901
WEB: WWW.VANCLEEFENGINEERING.COM
PHONE (215) 345-1876

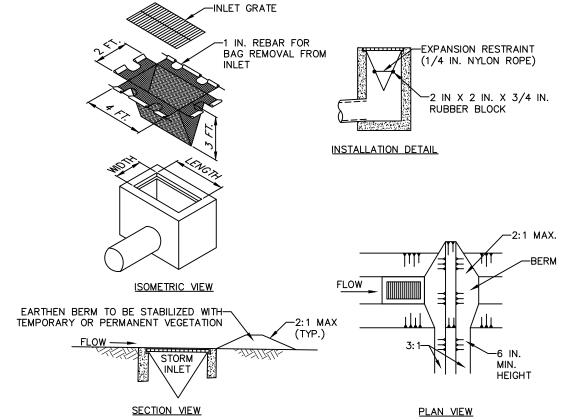
EROSION &



DATE: APRIL 3, 2025
SCALE: NO SCALE
DESIGNED BY: SDM
DRAWN BY: RJN / ENP
CHECKED BY: SDC
JOB NUMBER: 2402-NBR

TMP'S 26-006-101-001

NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA



NOTES:

MAXIMUM DRAINAGE AREA = 1/2 ACRE.

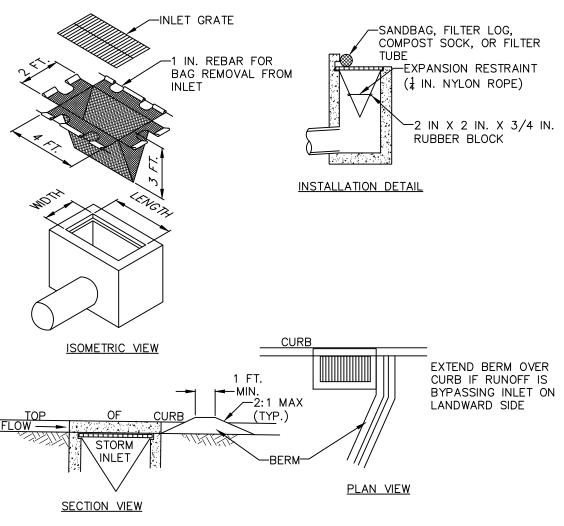
INLET PROTECTION SHALL NOT BE REQUIRED FOR INLET TRIBUTARY TO SEDIMENT BASIN OR TRAP. BERMS SHALL BE REQUIRED FOR ALL INSTALLATIONS. ROLLED EARTHEN BERM IN ROADWAY SHALL BE MAINTAINED UNTIL ROADWAY IS STONED. ROAD SUBBASE BERM ON ROADWAY SHALL BE MAINTAINED UNTIL ROADWAY IS PAVED. EARTHEN BERM IN CHANNEL SHALL BE MAINTAINED UNTIL PERMANENT STABILIZATION IS COMPLETED OR REMAIN PERMANENTLY.

AT A MINIMUM, THE FABRIC SHALL HAVE A MINIMUM GRAB TENSILE STRENGTH OF 120 LBS., A MINIMUM BURST STRENGTH OF 200 PSI, AND A MINIMUM TRAPEZOIDAL TEAR STRENGTH OF 50 LBS. FILTER BAGS SHALL BE CAPABLE OF TRAPPING ALL PARTICLES NOT PASSING A NO. 40 SIEVE.

INLET FILTER BAGS SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EACH RUNOFF EVENT. BAGS SHALL BE EMPTIED AND RINSED OR REPLACED WHEN HALF FULL OR WHEN FLOW CAPACITY HAS BEEN REDUCED SO AS TO CAUSE FLOODING OR BYPASSING OF THE INLET. DAMAGED OR CLOGGED BAGS SHALL BE REPLACED. A SUPPLY SHALL BE MAINTAINED ON SITE FOR REPLACEMENT OF BAGS. ALL NEEDED REPAIRS SHALL BE INITIATED IMMEDIATELY AFTER THE INSPECTION. DISPOSE ACCUMULATED SEDIMENT AS WELL AS ALL USED BAGS ACCORDING TO THE PLAN NOTES.

DO NOT USE ON MAJOR PAVED ROADWAYS WHERE PONDING MAY CAUSE TRAFFIC HAZARDS.

### FILTER BAG INLET PROTECTION - TYPE M INLET (NOT TO SCALE)



MAXIMUM DRAINAGE AREA = 1/2 ACRE.

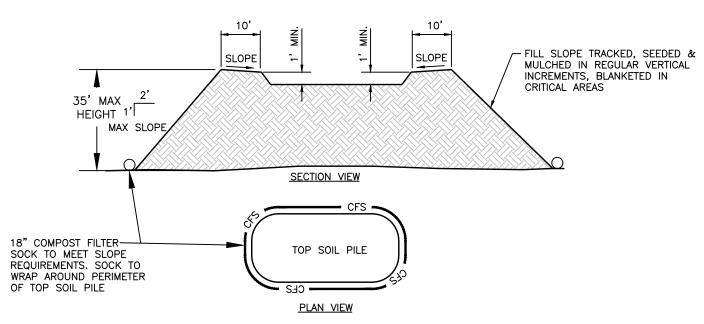
INLET PROTECTION SHALL NOT BE REQUIRED FOR INLET TRIBUTARY TO SEDIMENT BASIN OR TRAP. BERMS SHALL BE REQUIRED FOR ALL INSTALLATIONS.

ROLLED EARTHEN BERM SHALL BE MAINTAINED UNTIL ROADWAY IS STONED. ROAD SUBBASE BERM SHALL BE MAINTAINED UNTIL ROADWAY IS PAVED. SIX INCH MINIMUM HEIGHT ASPHALT BERM SHALL BE MAINTAINED UNTIL ROADWAY SURFACE RECEIVES FINAL COAT. AT A MINIMUM, THE FABRIC SHALL HAVE A MINIMUM GRAB TENSILE STRENGTH OF 120 LBS, A MINIMUM BURST STRENGTH OF 200 PSI, AND A MINIMUM TRAPEZOIDAL TEAR STRENGTH OF 50 LBS. FILTER BAGS SHALL BE CAPABLE OF TRAPPING ALL PARTICLES NOT PASSING A NO. 40

INLET FILTER BAGS SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EACH RUNOFF EVENT. BAGS SHALL BE EMPTIED AND RINSED OR REPLACED WHEN HALF FULL OR WHEN FLOW CAPACITY HAS BEEN REDUCED SO AS TO CAUSE FLOODING OR BYPASSING OF THE INLET. DAMAGED OR CLOGGED BAGS SHALL BE REPLACED. A SUPPLY SHALL BE MAINTAINED ON SITE FOR REPLACEMENT OF BAGS. ALL NEEDED REPAIRS SHALL BE INITIATED IMMEDIATELY AFTER THE INSPECTION. DISPOSE OF ACCUMULATED SEDIMENT AS WELL AS ALL USED BAGS ACCORDING TO THE PLAN NOTES.

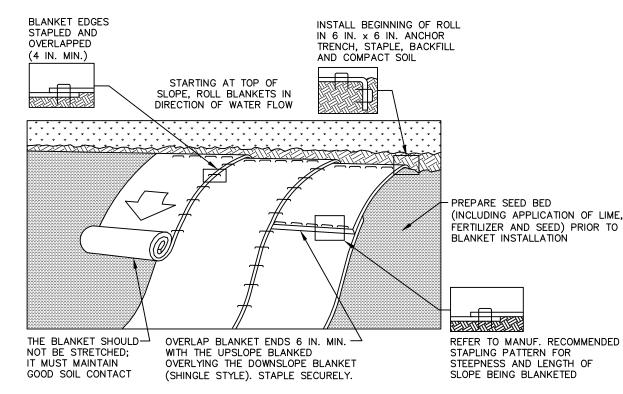
DO NOT USE ON MAJOR PAVED ROADWAYS WHERE PONDING MAY CAUSE TRAFFIC HAZARDS.

### FILTER BAG INLET PROTECTION - TYPE C INLET (NOT TO SCALE)



1. TEMPORARY BERMS TO BE PLACED, MAINTAINED, AND ADJUSTED CONTINUOUSLY UNTIL 90% VEGETATIVE GROWTH IS ESTABLISHED ON THE EXTERIOR SLOPES WITH PERMANENT STORM DRAINAGE FACILITIES FUNCTIONING. 2. BERMS MUST OUTLET TO TEMPORARY SLOPE PIPES, PERMANENT SLOPE PIPES, TEMPORARY CHANNELS, OR PERMANENT CHANNELS. TOPSOIL STOCKPILE AREA

(NOT TO SCALE)

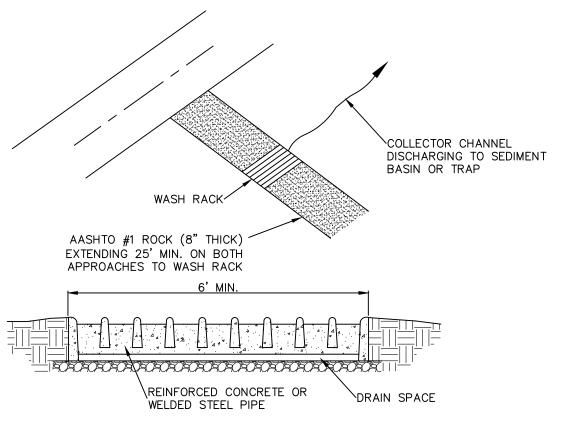


SEED AND SOIL AMENDMENTS SHALL BE APPLIED ACCORDING TO THE RATES IN THE PLAN DRAWINGS PRIOR TO PROVIDE ANCHOR TRENCH AT TOE OF SLOPE IN SIMILAR FASHION AS AT TOP OF SLOPE. SLOPE SURFACE SHALL BE FREE OF ROCKS, CLODS, STICKS, AND GRASS. BLANKET SHALL HAVE GOOD CONTINUOUS CONTACT WITH UNDERLYING SOIL THROUGHOUT ENTIRE LENGTH. LAY

BLANKET LOOSELY AND STAKE OR STAPLE TO MAINTAIN DIRECT CONTACT WITH SOIL. DO NOT STRETCH

THE BLANKET SHALL BE STAPLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. BLANKETED AREAS SHALL BE INSPECTED WEEKLY AND AFTER EACH RUNOFF EVENT UNTIL PERENNIAL VEGETATION IS ESTABLISHED TO A MINIMUM UNIFORM 70% COVERAGE THROUGHOUT THE BLANKETED AREA. DAMAGED OR DISPLACED BLANKETS SHALL BE RESTORED OR REPLACED WITHIN 4 CALENDAR DAYS.

# EROSION CONTROL BLANKET INSTALLATION DETAIL



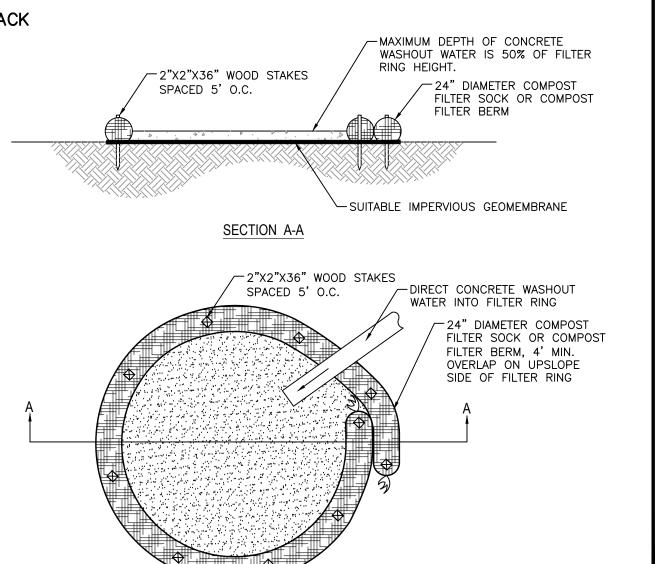
WASH RACK SHALL BE 20 FEET (MIN.) WIDE OR TOTAL WIDTH OF ACCESS. WASH RACK SHALL BE DESIGNED AND CONSTRUCTED TO ACCOMMODATE ANTICIPATED CONSTRUCTION VEHICULAR TRAFFIC.

A WATER SUPPLY SHALL BE MADE AVAILABLE TO WASH THE WHEELS OF ALL VEHICLES EXITING

MAINTENANCE: ROCK CONSTRUCTION ENTRANCE THICKNESS SHALL BE CONSTANTLY MAINTAINED TO THE SPECIFIED DIMENSIONS BY ADDING ROCK. A STOCKPILE OF ROCK MATERIAL SHALL BE MAINTAINED ON SITE FOR THIS PURPOSE, DRAIN SPACE UNDER WASH RACK SHALL BE KEPT OPEN AT ALL TIMES. DAMAGE TO THE WASH RACK SHALL BE REPAIRED PRIOR TO FURTHER USE OF THE RACK. ALL SEDIMENT DEPOSITED ON ROADWAYS SHALL BE REMOVED AND RETURNED TO THE CONSTRUCTION SITE IMMEDIATELY. WASHING THE ROADWAY OR SWEEPING THE DEPOSITS INTO ROADWAY DITCHES, SEWERS, CULVERTS, OR OTHER DRAINAGE COURSES IS

# ROCK CONSTRUCTION ACCESS WITH WASH RACK

(NOT TO SCALE)



COMPOST SOCK CONCRETE WASHOUT

4. ACCUMULATED MATERIALS TO BE REMOVED WHEN THEY REACH 75% CAPACITY.

(NOT TO SCALE)

1. INSTALL ON FLAT GRADE (2% MAXIMUM SLOPE) FOR OPTIMAL PERFORMANCE

2. 18" DIAMETER FILTER SOCK MAY BE STACKED ONTO DOUBLE 24" DIAMETER FILTER

3. ALL CONCRETE WASHOUT FACILITIES SHOULD BE INSPECTED DAILY. COMPOST SOCK MUST REMAIN IN CONTINUOUS CONTACT WITH THE GEOMEMBRANE AT ALL LOCATIONS.

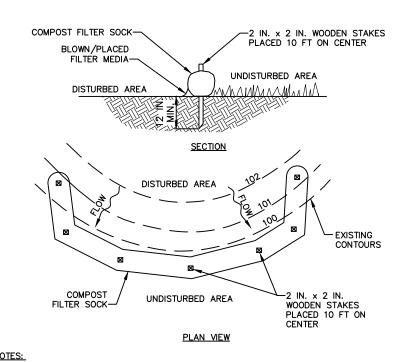
DAMAGED OR LEAKING WASHOUTS SHOULD BE DEACTIVATED AND REPAIRED OR

WELL VEGETATED, GRASSY AREA TDISCHARGE HOSE -HEAVY DUTY LIFTING STRAPS COMPOST FILTER -DISCHARGE HOSE CLAMP (TYP.) FILTER MEDIA BLOWN/PLACED FILTER MEDIA ELEVATION VIEW

LOW VOLUME FILTER BAGS SHALL BE MADE FROM NON-WOVEN GEOTEXTILE MATERIAL SEWN WITH HIGH STRENGTH, DOUBLE STITCHED "J" TYPE SEAMS. THEY SHALL BE CAPABLE OF TRAPPING PARTICLES LARGER THAN 150 MICRONS. HIGH VOLUME FILTER BAGS SHALL BE MADE FROM WOVEN GEOTEXTILES THAT MEET THE

A SUITABLE MEANS OF ACCESSING THE BAG WITH MACHINERY REQUIRED FOR DISPOSAL PURPOSES SHALL BE PROVIDED. FILTER BAGS SHALL BE REPLACED WHEN THEY BECOME 1/2 FULL OF SEDIMENT. SPARE BAGS SHALL BE KEPT AVAILABLE FOR REPLACEMENT OF THOSE THAT HAVE FAILED OR ARE FILLED. BAGS SHALL BE PLACED ON STRAPS TO FACILITATE REMOVAL UNLESS BAGS COME WITH LIFTING STRAPS ALREADY ATTACHED. BAGS SHALL BE LOCATED IN WELL—VEGETATED (GRASSY) AREA, AND DISCHARGE ONTO STABLE, EROSION RESISTANT AREAS. WHERE THIS IS NOT POSSIBLE, A GEOTEXTILE UNDERLAYMENT AND FLOW PATH SHALL BE PROVIDED. BAGS MAY BE PLACED ON FILTER STONE TO INCREASE DISCHARGE CAPACITY. BAGS SHALL NOT BE PLACED ON SLOPES GREATER THAN 5%. FOR SLOPES EXCEEDING 5%, CLEAN ROCK OR OTHER NON-ERODIBLE AND NON-POLLUTING MATERIAL MAY BE PLACED UNDER THE BAG TO REDUCE SLOPE STEEPNESS. NO DOWNSLOPE SEDIMENT BARRIER IS REQUIRED FOR MOST INSTALLATIONS. COMPOST BERM OR COMPOST FILTER SOCK SHALL BE INSTALLED BELOW BAGS LOCATED IN HQ OR EV WATERSHEDS, WITHIN 50 FEET OF ANY RECEIVING SURFACE WATER OR WHERE GRASSY AREA IS NOT AVAILABLE. THE PUMP DISCHARGE HOSE SHALL BE INSERTED INTO THE BAGS IN THE MANNER SPECIFIED BY THE MANUFACTURER AND SECURELY CLAMPED. A PIECE OF PVC PIPE IS RECOMMENDED FOR THIS PURPOSE. THE PUMPING RATE SHALL BE NO GREATER THAN 750 GPM OR 1/2 THE MAXIMUM SPECIFIED BY THE MANUFACTURER, WHICHEVER IS LESS. PUMP INTAKES SHALL BE FLOATING AND SCREENED. FILTER BAGS SHALL BE INSPECTED DAILY. IF ANY PROBLEM IS DETECTED, PUMPING SHALL CEASE IMMEDIATELY AND NOT RESUME UNTIL THE PROBLEM IS CORRECTED.

### PUMPED WATER FILTER BAG (NOT TO SCALE)



SOCK FABRIC SHALL MEET STANDARDS OF TABLE 4.1 OF THE PA DEP EROSION CONTROL COMPOST FILTER SOCK SHALL BE PLACED AT EXISTING LEVEL GRADE. BOTH ENDS OF THE BARRIER SHALL BE EXTENDED AT LEAST 8 FEET UP SLOPE AT 45 DEGREES TO THE MAIN BARRIER ALIGNMENT. MAXIMUM SLOPE LENGTH ABOVE ANY BARRIER SHALL NOT EXCEED THAT SPECIFIED FOR THE SIZE OF THE SOCK AND THE SLOPE OF ITS TRIBUTARY AREA. TRAFFIC SHALL NOT BE PERMITTED TO CROSS COMPOST FILTER SOCKS. ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN IT REACHES 1/2 THE ABOVE GROUND HEIGHT OF THE BARRIER AND DISPOSED IN THE MANNER DESCRIBED ELSEWHERE IN THE PLAN. COMPOST FILTER SOCKS SHALL BE INSPECTED WEEKLY AND AFTER EACH RUNOFF EVENT. DAMAGED SOCKS SHALL BE REPAIRED ACCORDING TO MANUFACTURER'S SPECIFICATIONS OR REPLACED WITHIN 24 HOURS OF INSPECTION. BIODEGRADABLE COMPOST FILTER SOCKS SHALL BE REPLACED AFTER 6 MONTHS; PHOTODEGRADABLE SOCKS AFTER 1 YEAR. POLYPROPYLENE SOCKS SHALL BE REPLACED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS. UPON STABILIZATION OF THE AREA TRIBUTARY TO THE SOCK, STAKES SHALL BE REMOVED. THE SOCK MAY BE LEFT IN PLACE AND VEGETATED OR REMOVED. IN THE LATTER CASE, THE MESH SHALL BE CUT OPEN AND THE MULCH SPREAD AS A SOIL SUPPLEMENT.

### COMPOST FILTER SOCK (NOT TO SCALE)

VAN CLEEF ENGINEERING ASSOCIATES, LLC **CONTROL DETAILS** 

> RJN / ENP DRAWN BY HECKED BY **DB NUMBER**

DESIGNED BY

501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM

PHONE (215) 345-1876

**EROSION &** 

**SEDIMENTATION** 

PER TOWNSHIP ENGINEER REVIEW LETTER

**SERIAL NO. 20223183581** 

(now what's below. STOP! CALL 1-800-242-1776 Call before you dig. PA Law requires 3 working days notice

ONLY THOSE PLANS WHICH CONTAIN A DIGIT

IMPRESSED, OR COLORIZED INK SEAL OF TH RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERE

VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPART FOR THE OWNER DESIGNATED HEREON. AN

MODIFICATION, REVISION, DUPLICATION OR US
WITHOUT THE WRITTEN CONSENT OF VAN CLEE
ENGINEERING ASSOCIATES IS PROHIBITE
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THER THAN THAT WHICH IS INTENDED SHALL

AT THE SOLE DISCRETION AND LIABILITY OF APPLICABLE PARTY.

EAN DAVID McGRANAHAN

NSYLVANIA PROFESSIONAL

NEER NUMBER 085090

Before You Dig Anywhere

PA ONE CALL SYSTEM, INC.

PENNSYLVANI

AUTH DATE

DATED 05/21/2025.

PLAN NOTATION

EV DESCRIPTION

TMP'S 26-006-101-001

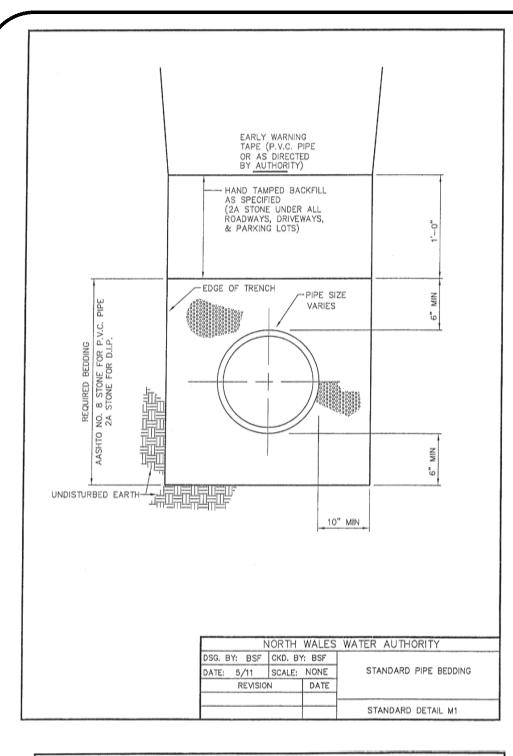
**NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 

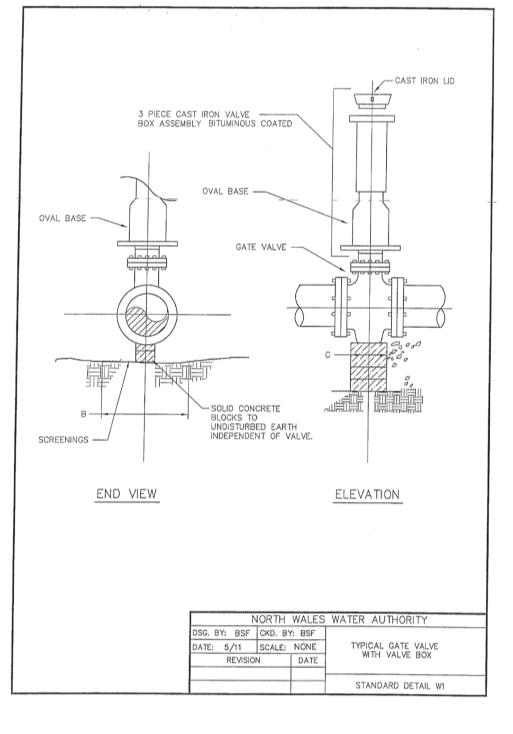
11 of 18

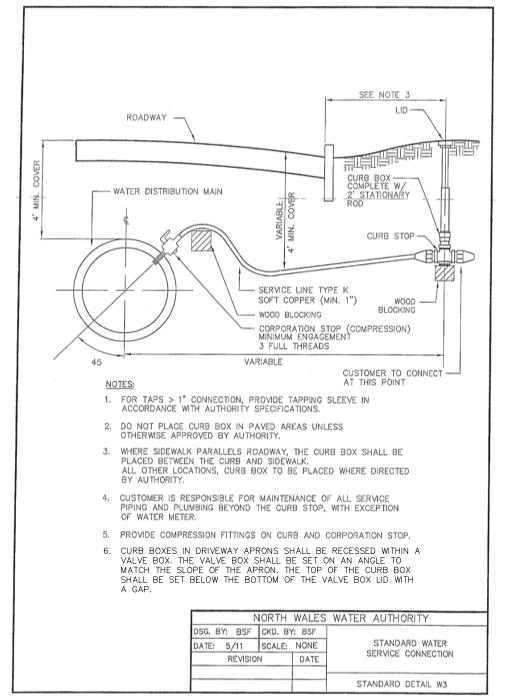
THIS SHEET FOR EROSION and SEDIMENTATION CONTROL PURPOSES ONLY

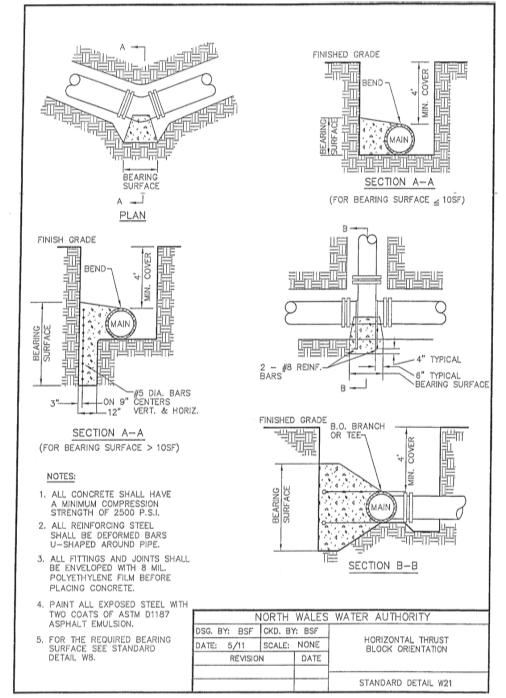
SOCKS IN PYRAMIDAL CONFIGURATION FOR ADDED HEIGHT

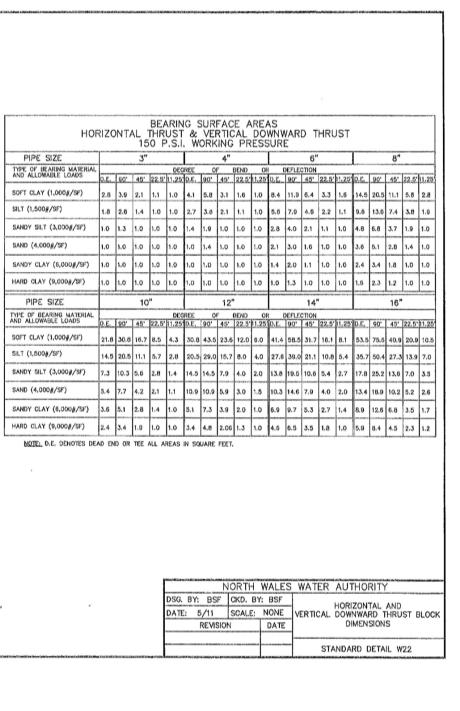
REPLACED IMMEDIATELY.



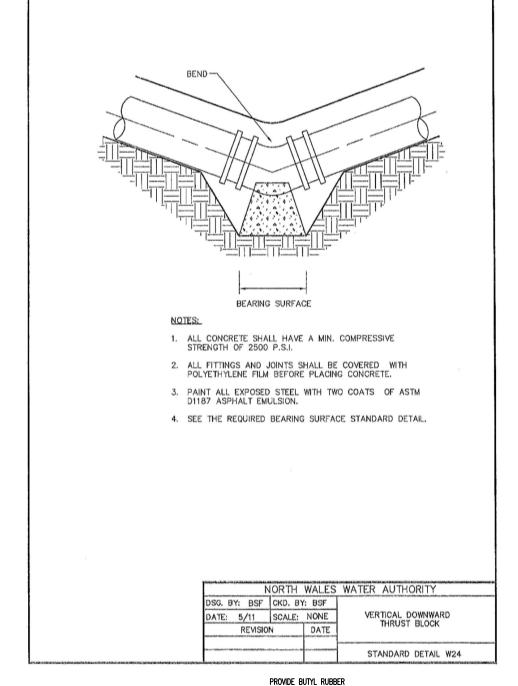


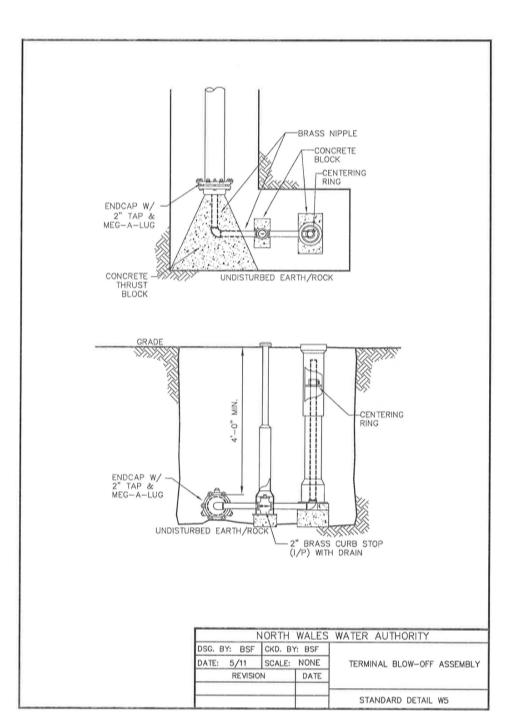


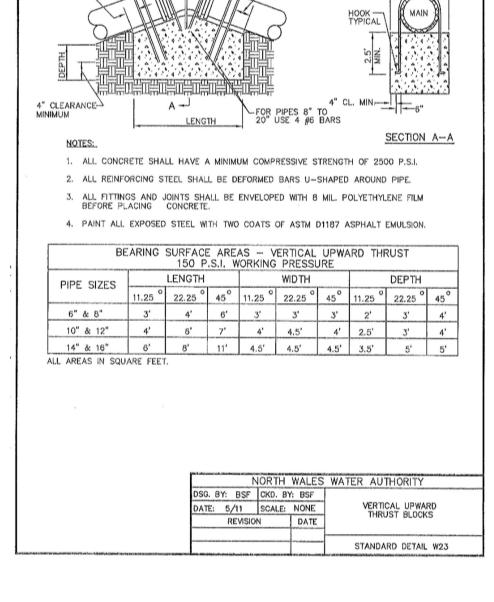


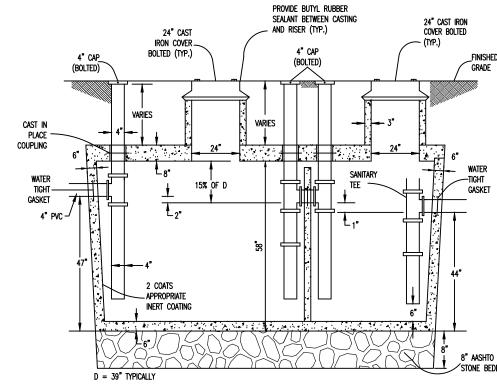


FOR 24" AND LARGER PIPES USE 7 #6 BARS AS SHOWN









ALL INLET AND OUTLET PIPES SHALL BE INSTALLED NO MORE THAN 6" FROM THE BOTTOM OF THE GREASE TRAP. TANK TAPERS TOP TO BOTTOM AND IS TRAPEZOIDAL IN CROSS SECTION.

TANK IS 5000 PSI CONCRETE-STEEL REINFORCED (@ 28 DAYS) CONCRETE CONFORMS TO ACI 318-16-4.5.1 AND 318-16-4.5.2, ASTM A615 AND A185

-DIMENSION: 151" INTERIOR/ 163" EXTERIOR LENGTH x 72" INTERIOR/ 84" EXTERIOR WIDTH

- 1) WHEN LOCATED IN DRIVEWAYS OR PAVED AREAS, GREASE TRAP TO BE DESIGNED FOR APPROPRIATE LOAD BEARING CONDITIONS. GREASE TRAP SHALL BE CAPABLE OF WITHSTANDING HS-20 LOADING.

  2) ALL PIPE PENETRATIONS SHALL BE WATERTIGHT.

  3) COPPASE TRAP SHALL BE PROVIDED WITH CASTICHT MANHOLE COVERS IN ACCORDANCE WITH TOWNISHIP STANDARD.
- 2) ALL PIPE PENEURATIONS SHALL BE WATERTIGHT.

  3) GREASE TRAP SHALL BE PROVIDED WITH GAS—TIGHT MANHOLE COVERS, IN ACCORDANCE WITH TOWNSHIP STANDARD SPECIFICATIONS.

  4) PRECAST CONCRETE SHALL HAVE A MINIMUM 28—DAY COMPRESSIVE STRENGTH 5000 PSI.
- JOINT SHALL BE SEALED WITH BUTYL RUBBER SEALANT (KENT SEAL #2 OR APPROVED EQUIVALENT) AND THE EXTERIOR OF THE JOINT SHALL BE SEALED WITH NON-SHRINK GROUT IN CONFORMANCE WITH THE TOWNSHIP STANDARD GREASE TRAP DETAIL.

  7) TANK SHALL BE TESTED FOR WATER TIGHTNESS BY FILLING FOR 24 HRS. TO SOAK, THEN TOPPED OFF, AND THEN

6) SPECIFIC SEALANT DETAIL AT CONCRETE RISER TO CONCRETE VAULT INTERFACE SHALL BE WATERTIGHT. AT A MINIMUM, THE

EXTERIOR CONCRETE SURFACES BELOW GRADE SHALL HAVE 2 COATS OF COAL TAR EPOXY.

10) INLET AND OUTLET EQUIPPED WITH PIPE SEALS.

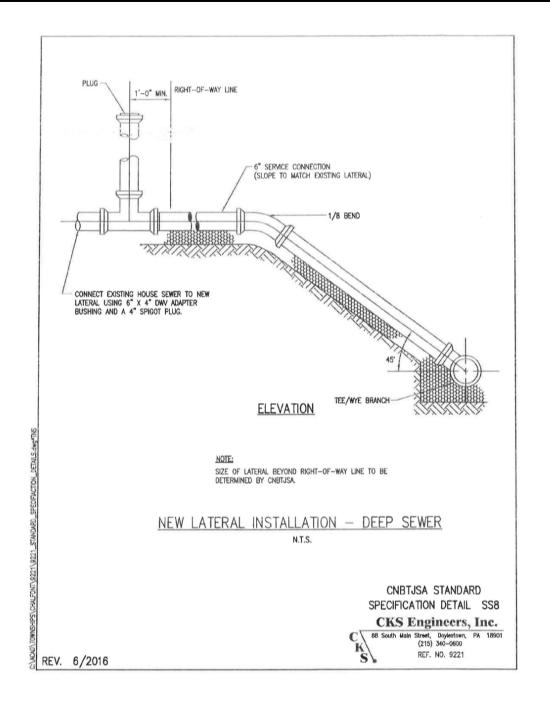
WATCHED FOR 24 HRS. NO DROP IN WATER IS ALLOWED.

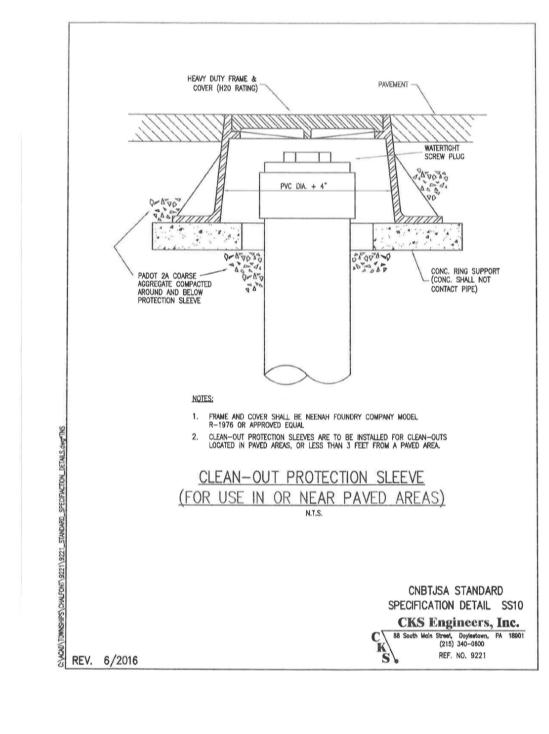
8) CAST IRON SHALL BE BOLTED TO CONCRETE WITH MASTIC TAPE (KENT SEAL OR APPROVED EQUIVALENT) SEALANT.

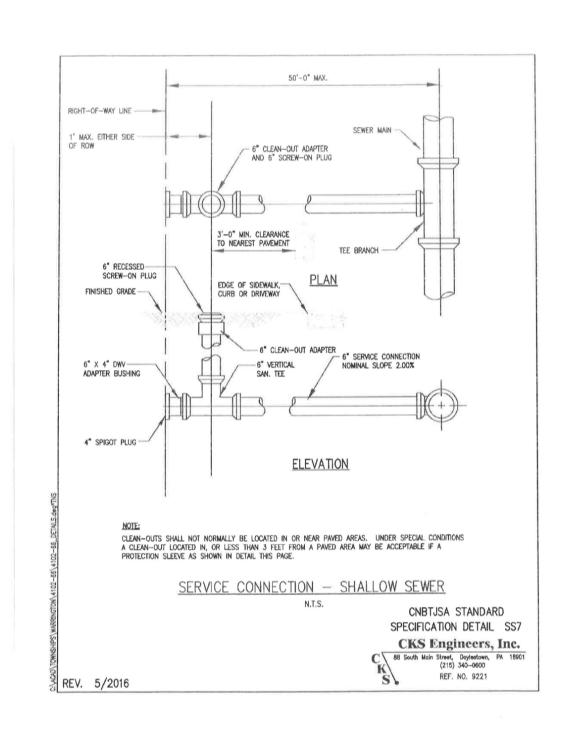
9) MAXIMUM EARTH COVER=5.0', HS-20 LOADING.

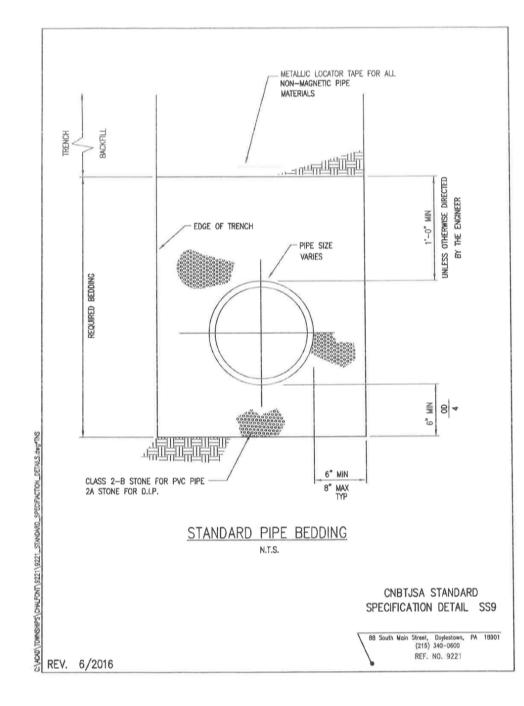
GREASE TRAP (1,500 GALLON - 2 COMPARTMENT)

(NOT TO SCALE)

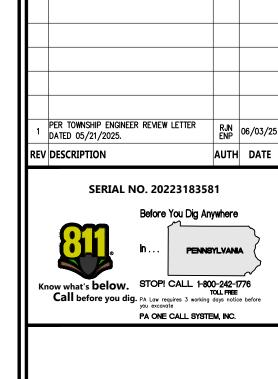


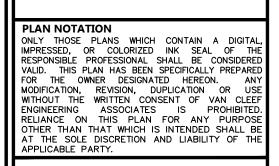












SEAN DAVID McGRANAHAN

INEER NUMBER 085090



VAN CLEEF ENGINEERING ASSOCIATES, LLC
501 NORTH MAIN STREET, DOYLESTOWN, PA 18901
WEB: WWW.VANCLEEFENGINEERING.COM
PHONE (215) 345-1876

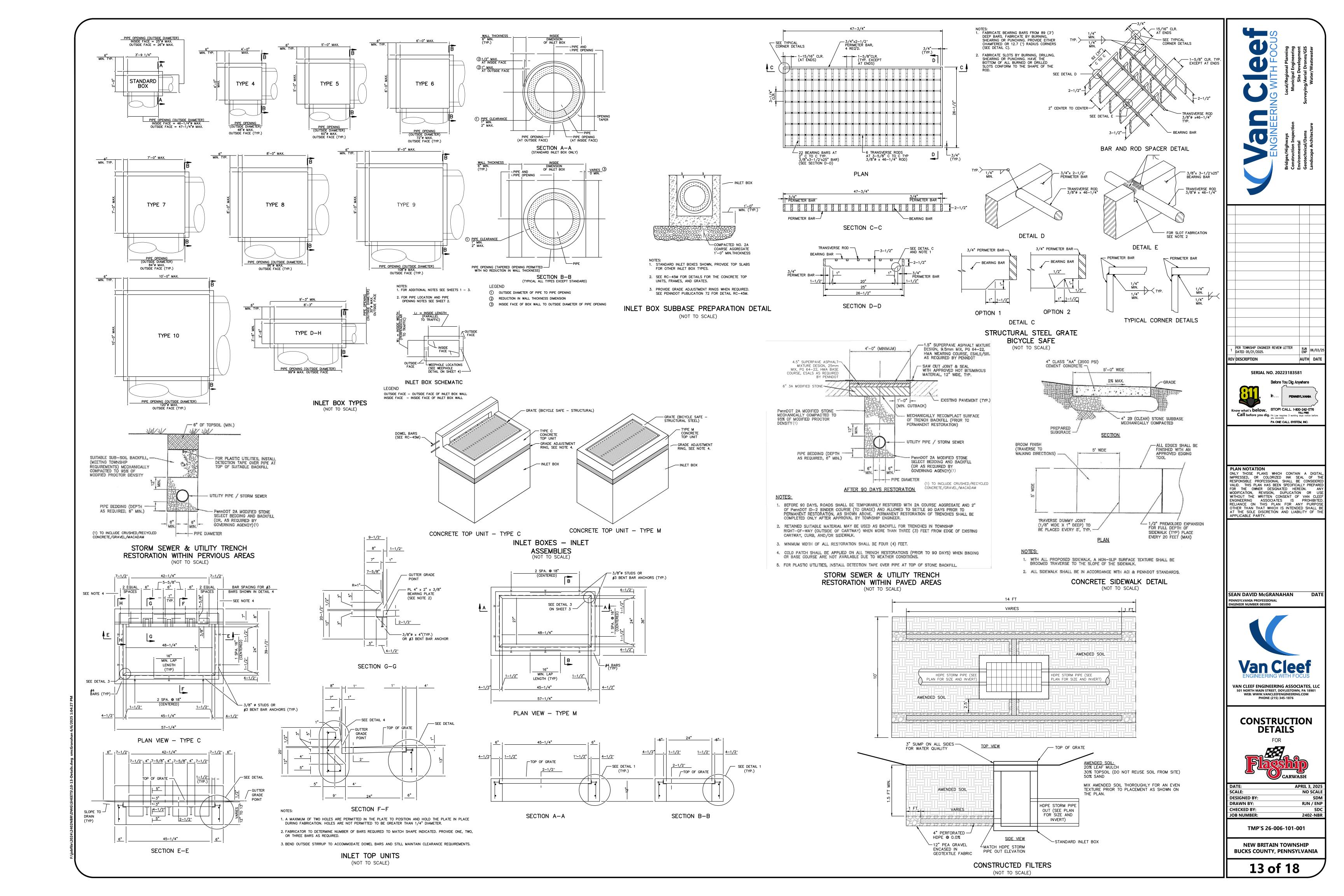
UTILITY DETAILS

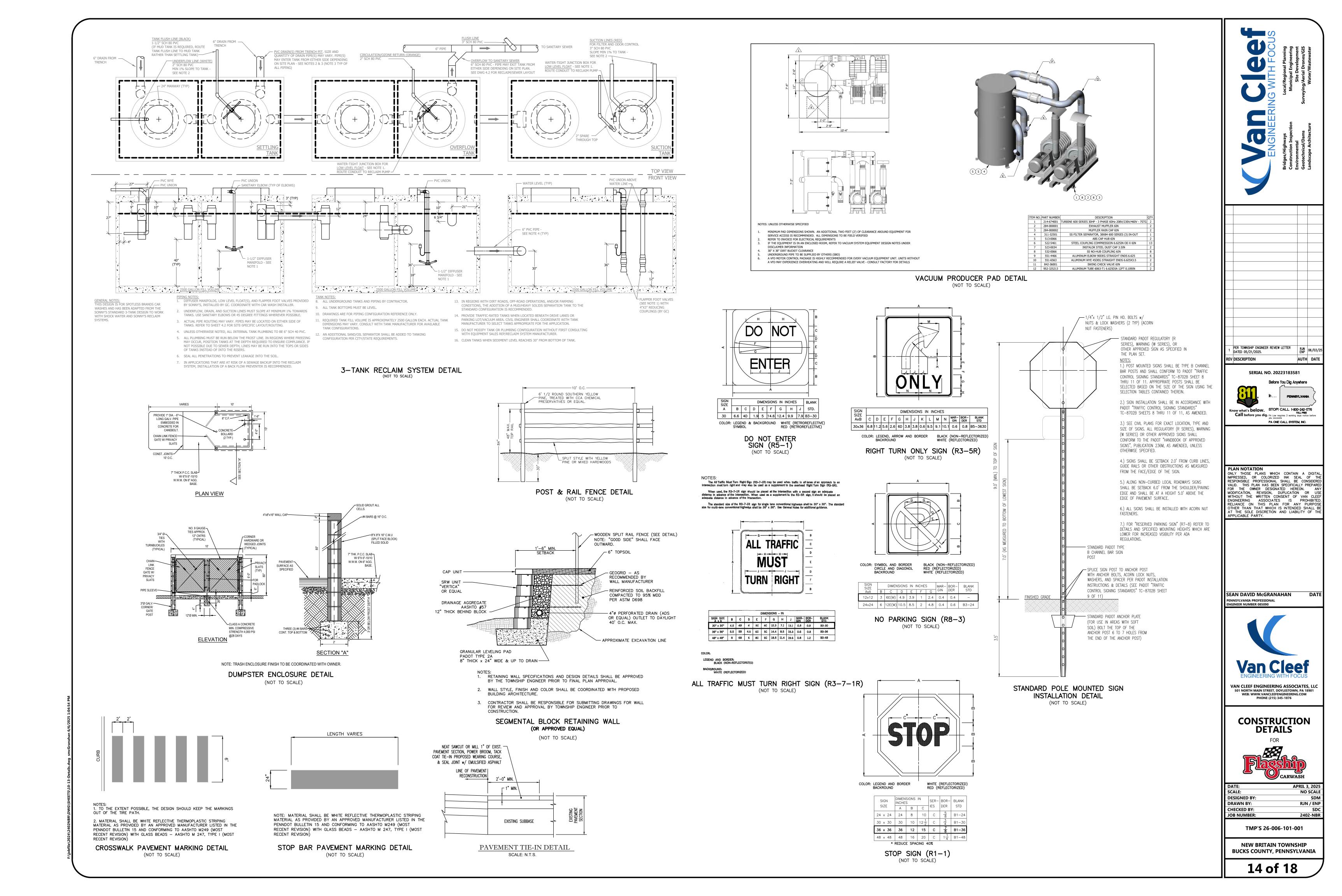


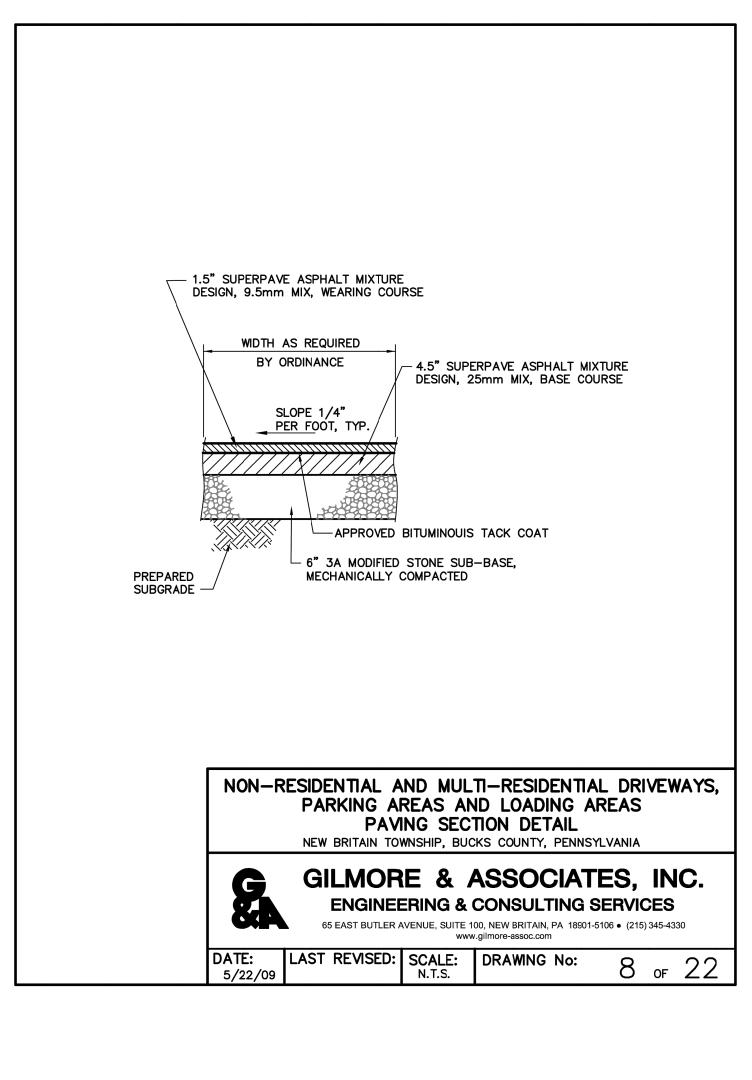
DATE:	APRIL 3, 202
SCALE:	NO SCAL
DESIGNED BY:	SDI
DRAWN BY:	RJN / EN
CHECKED BY:	SD
JOB NUMBER:	2402-NB

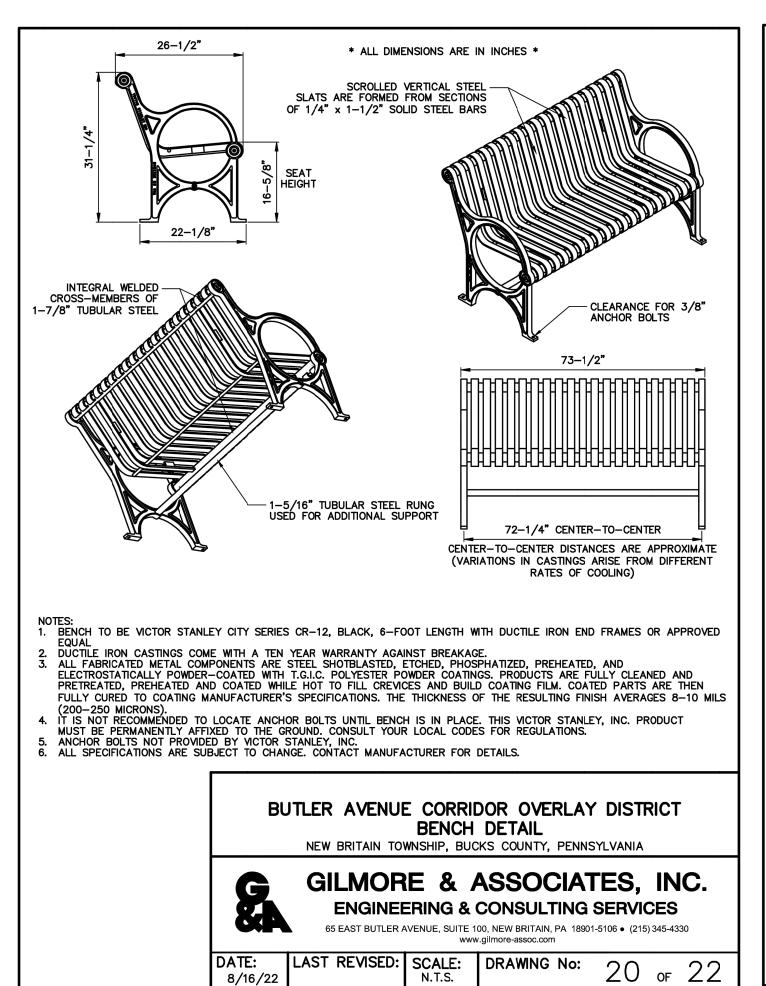
TMP'S 26-006-101-001

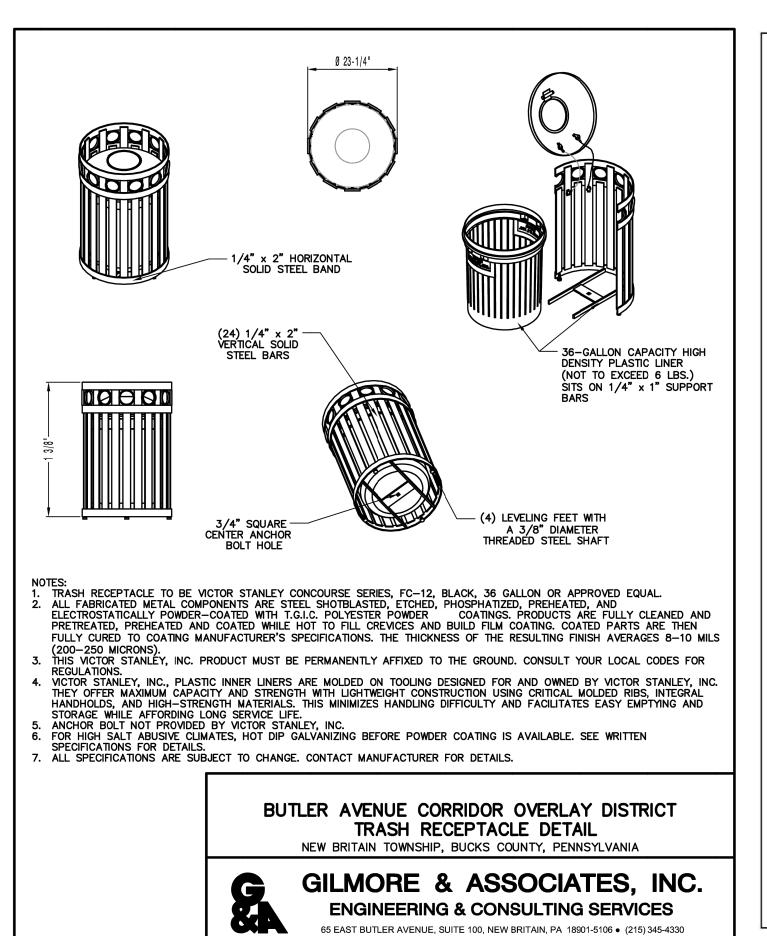
NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA







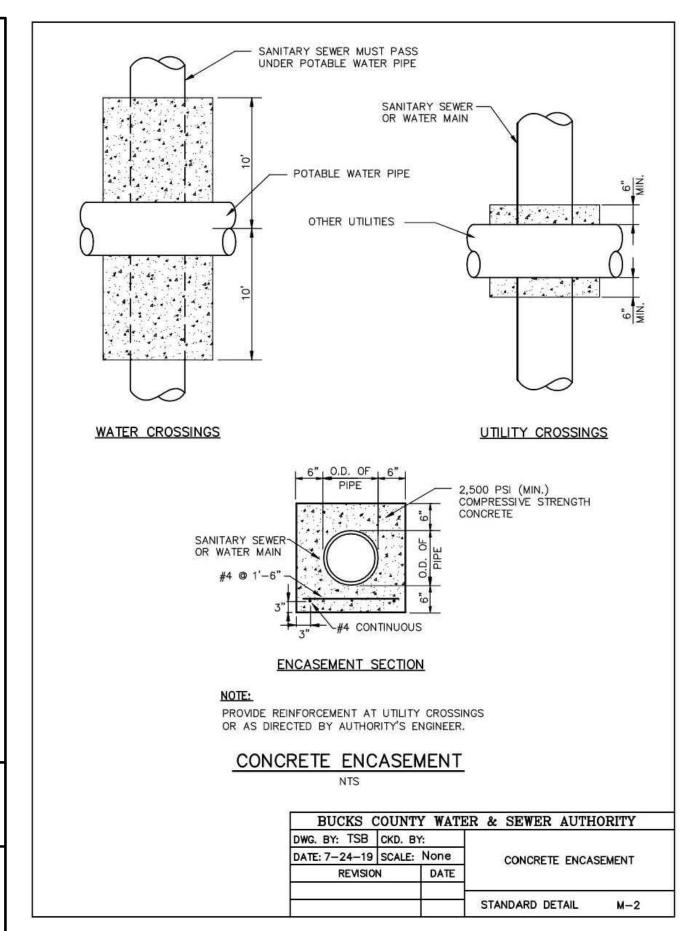


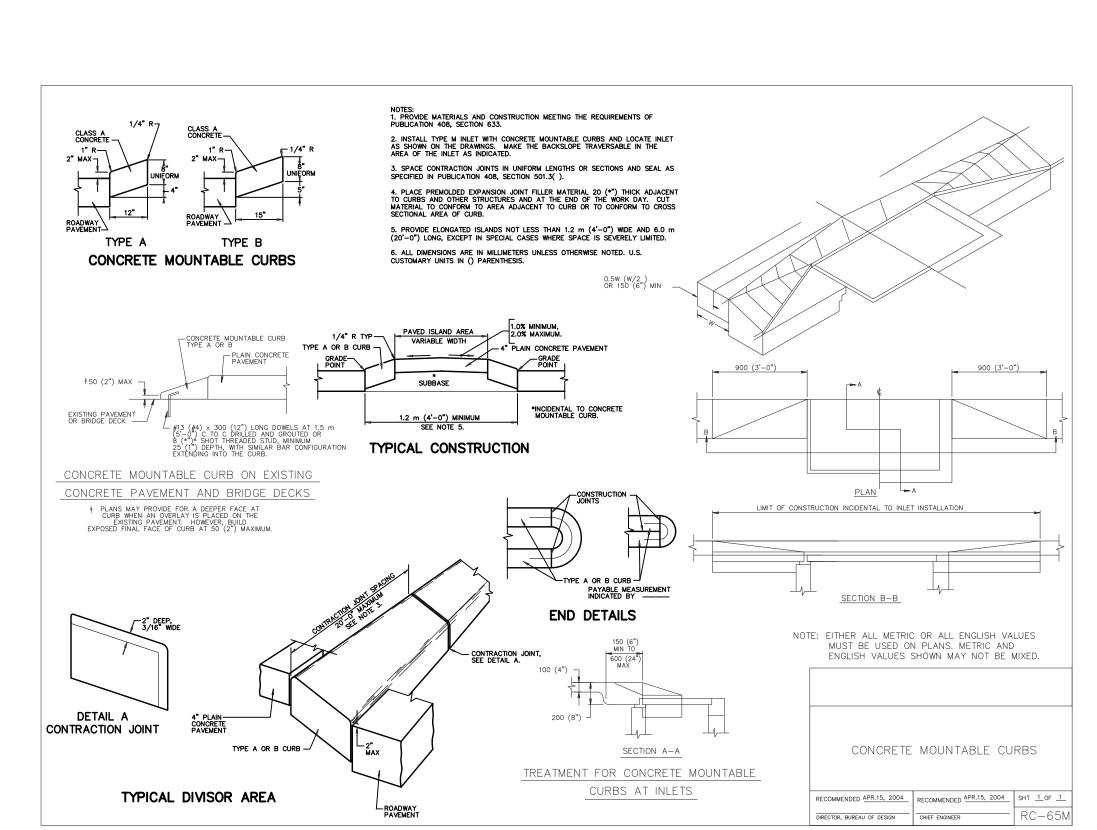


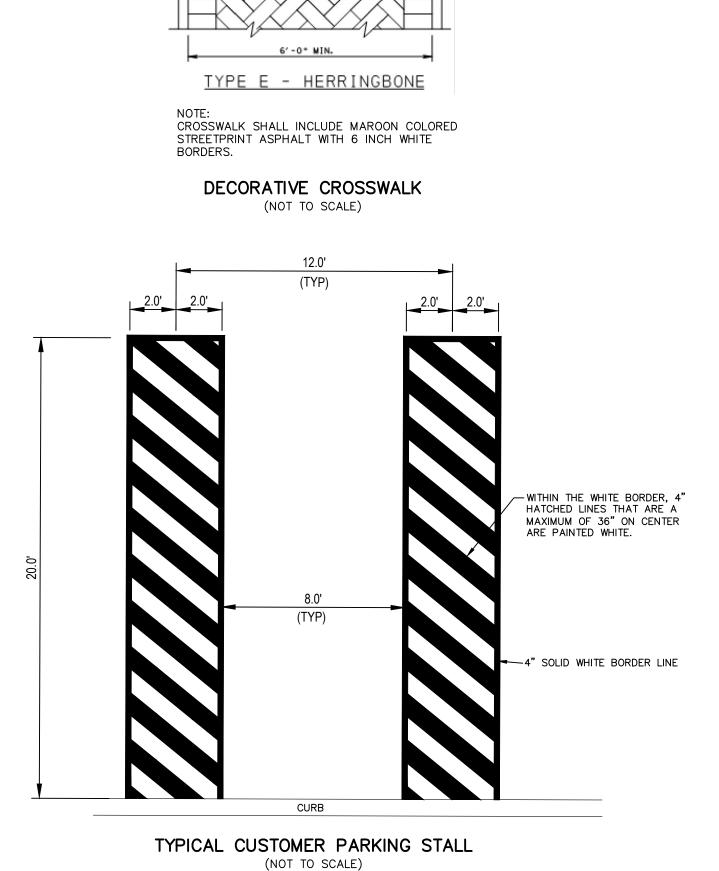
LAST REVISED: | SCALE: | DRAWING No:

8/16/22

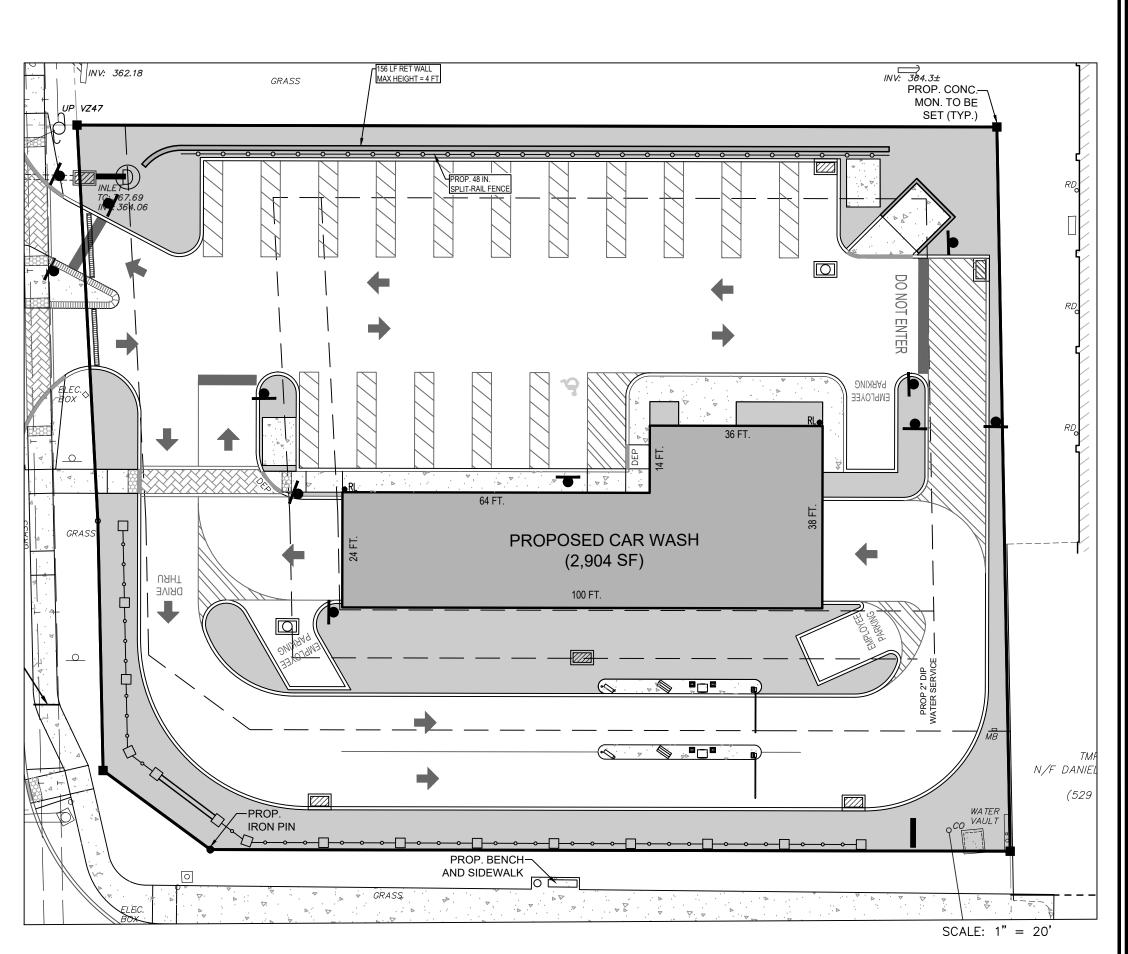
21 of 22







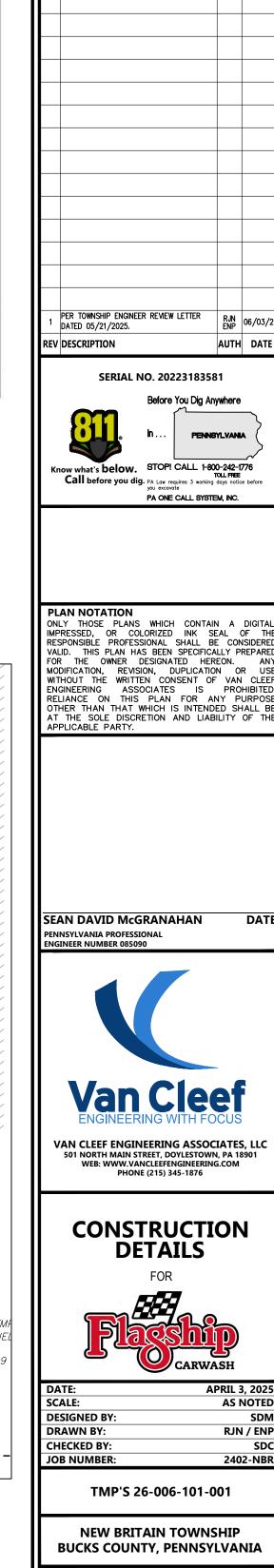
BORDER (REQUIRED) EACH SIDE.

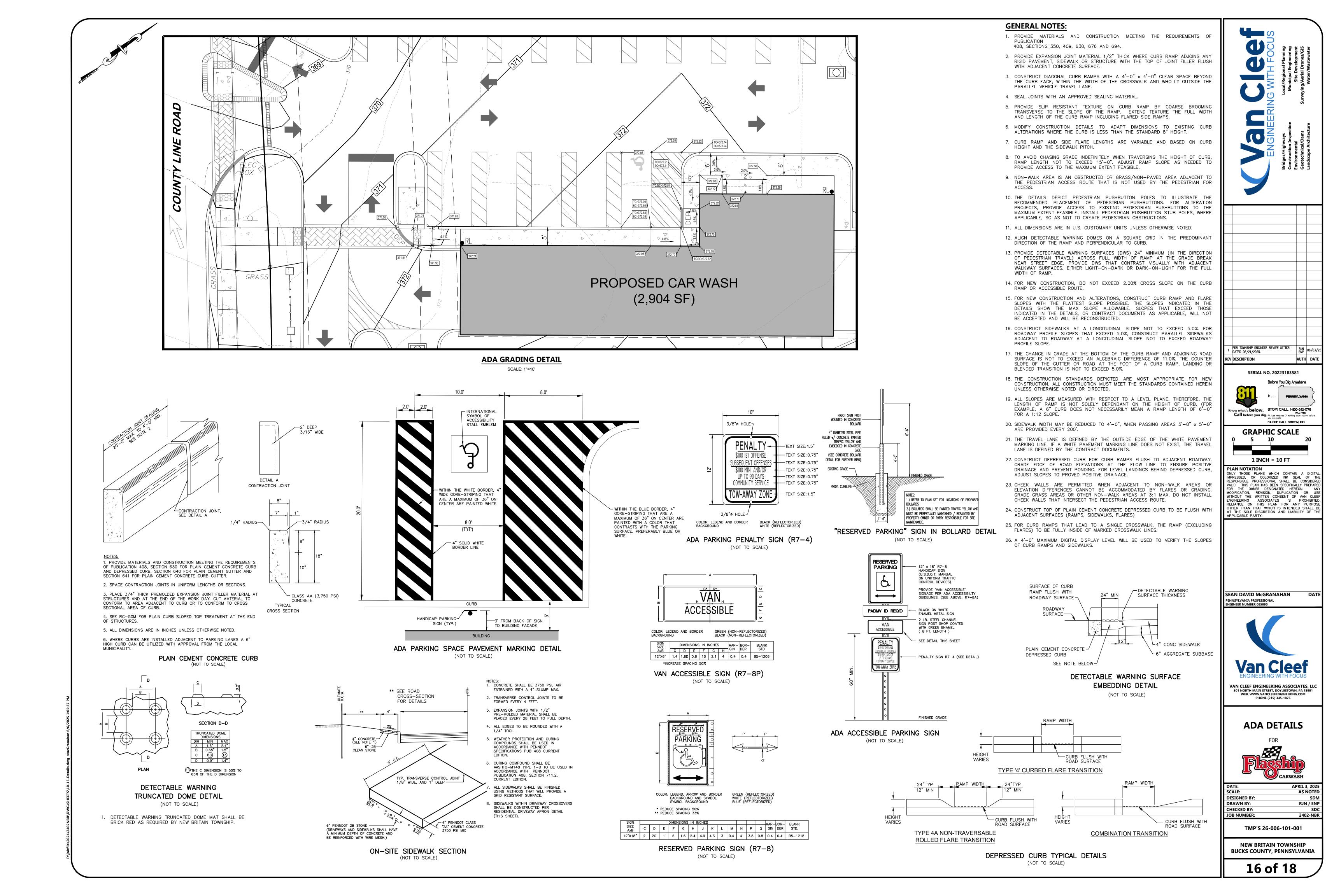


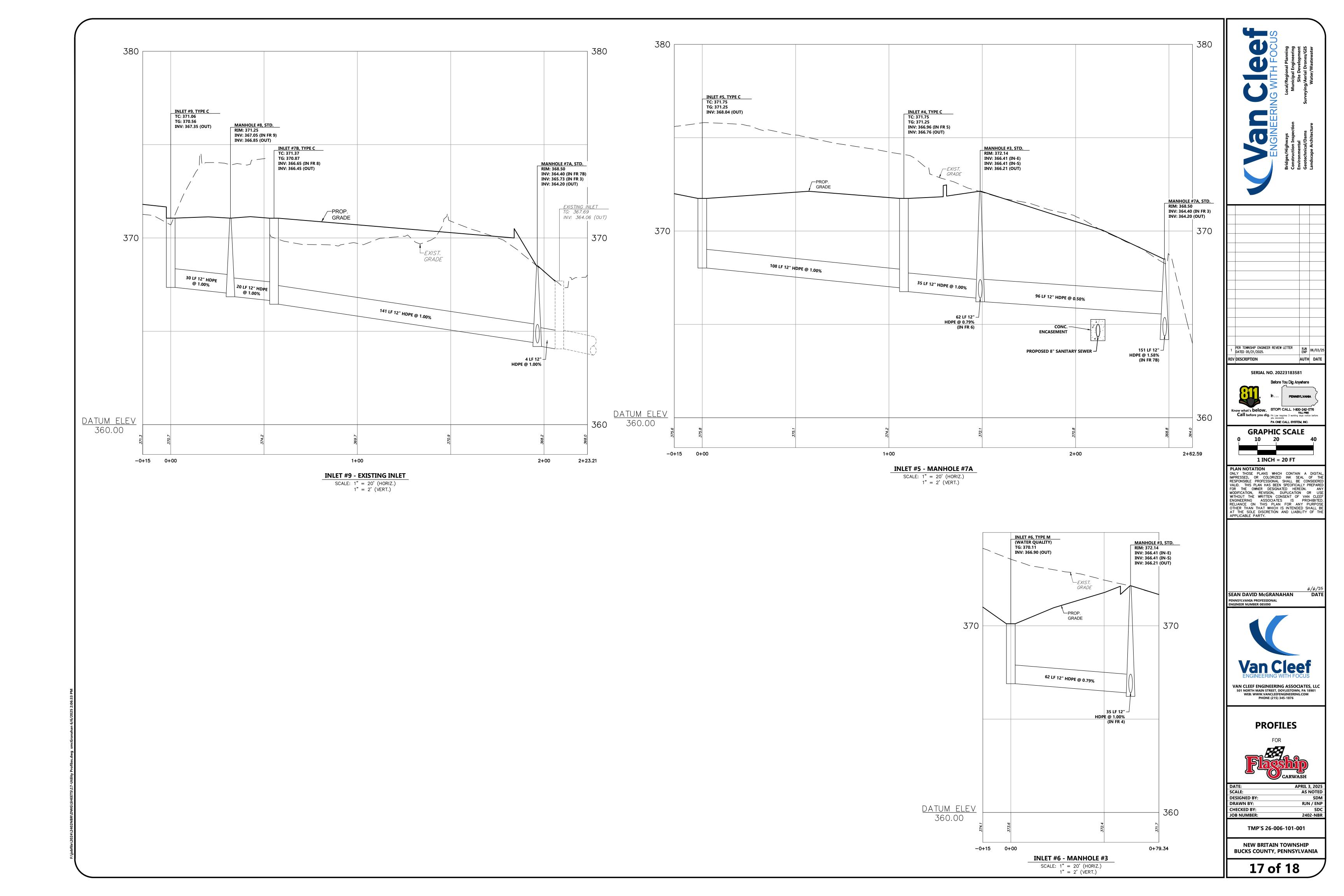
15% MIN. SITE AREA TO BE DESIGNED AS GREEN SPACE:

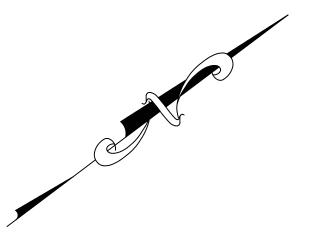
OPEN GREEN AREA = 7,112 SF PROVIDED.

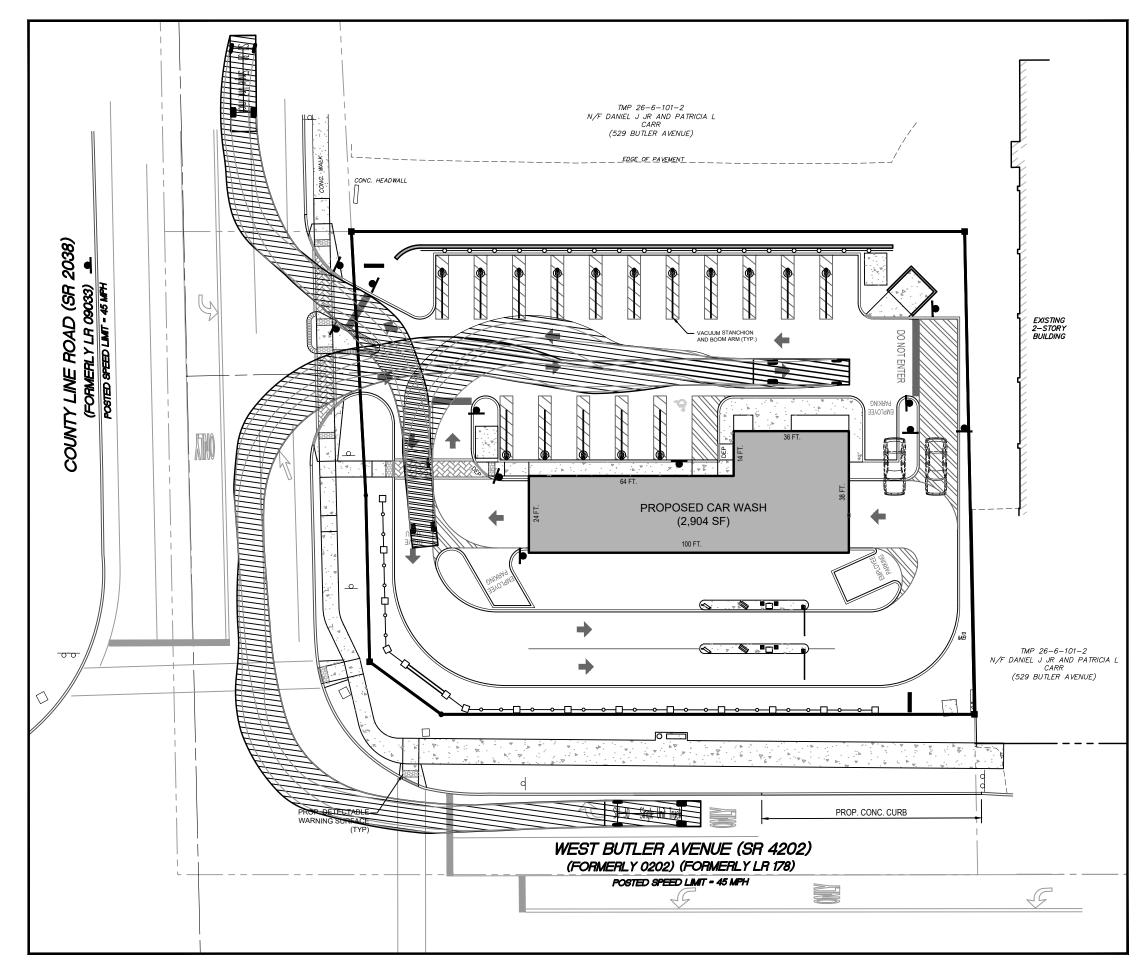
0.65 ACRES X 43,560 SF = 28,314 SF X 0.15 = 4,247 SF GREEN SPACE REQUIRED. GREEN SPACE AROUND BENCH, GATEWAY MONUMENT SIGN, AND TRASH RECEPTACLE

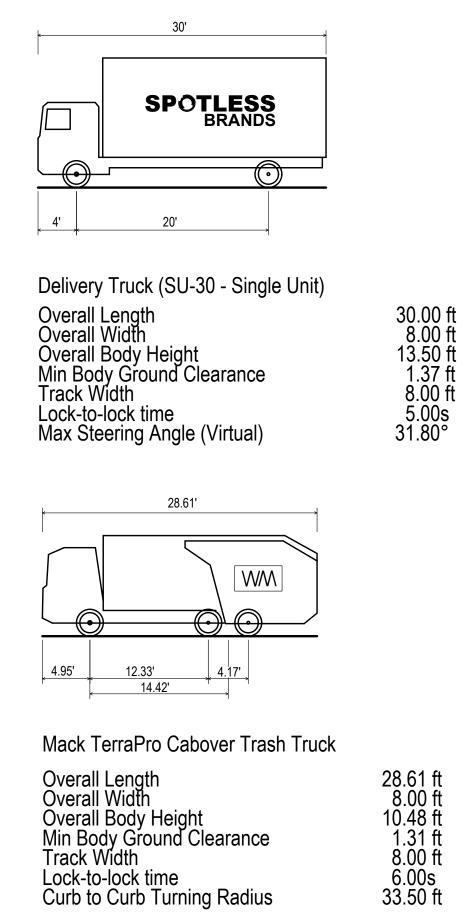


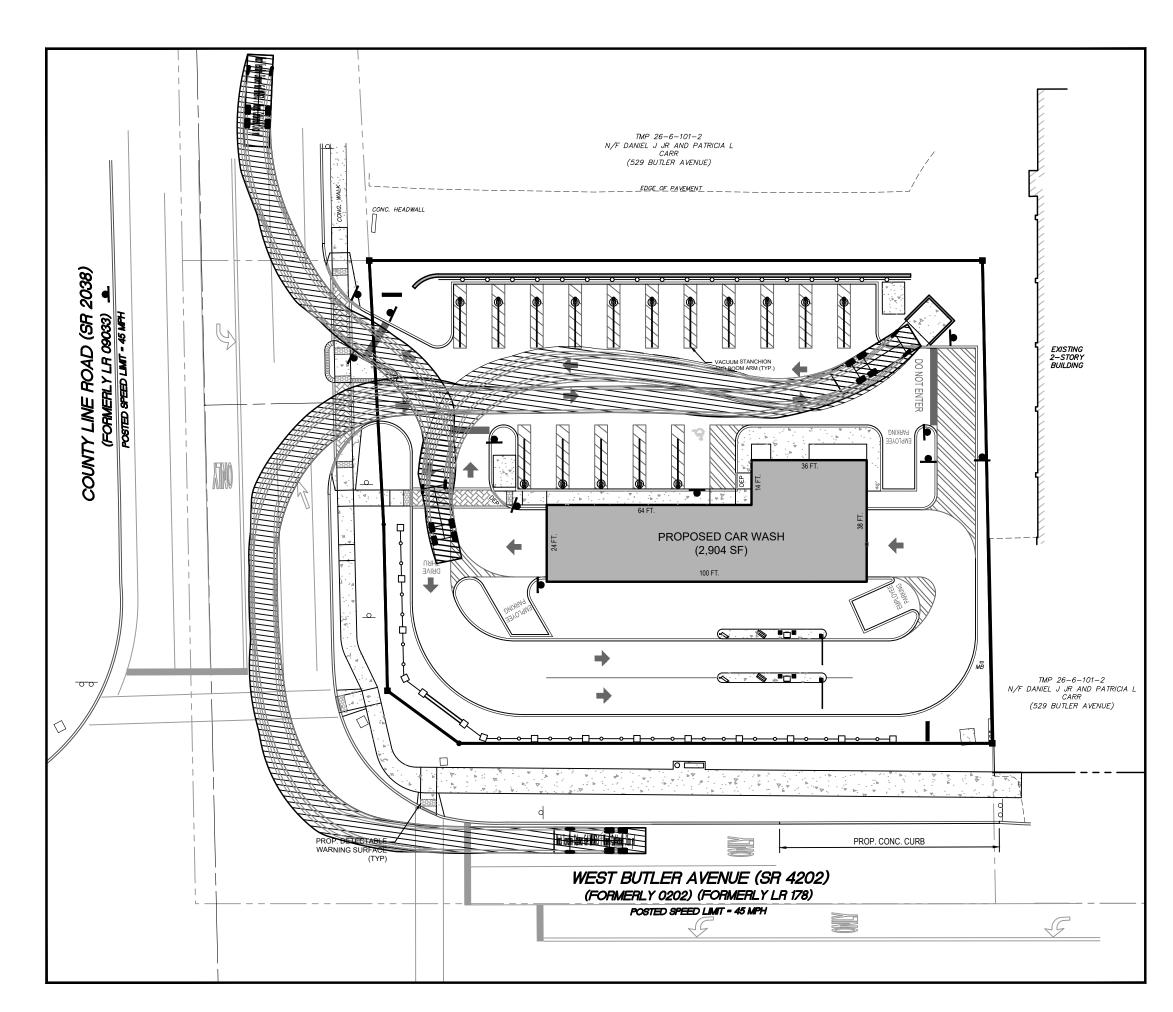






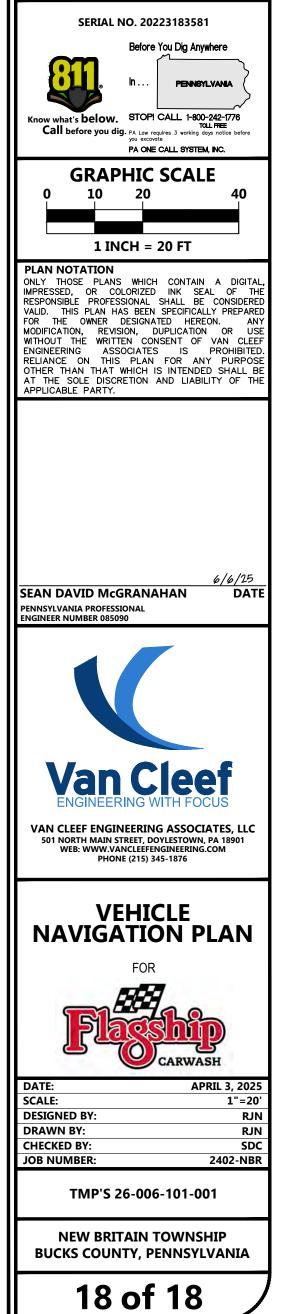






TRASH TRUCK (MACK TERRAPRO CABOVER)

SCALE: 1"=20'



PER TOWNSHIP ENGINEER REVIEW LETTER DATED 05/21/2025.

AUTH DATE

REV DESCRIPTION

DELIVERY TRUCK (SU-30)

# PRELIMINARY / FINAL LAND DEVELOPMENT PLANS

**FOR** 

# FLAGSHIP CARWASH

PREPARED FOR

# SPOTLESS **BRANDS**

SITUATED IN

# **NEW BRITAIN TOWNSHIP** BUCKS COUNTY, PENNSYLVANIA

**OWNER OF RECORD:** 

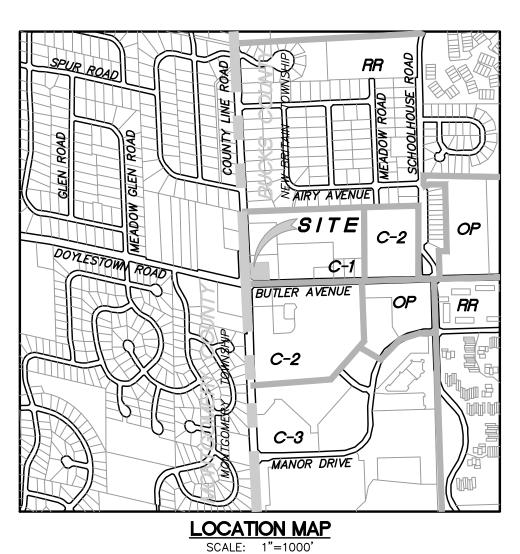
FLAGSHIP PENNSYLVANIA PROPCO, LLC

SITE ADDRESS: **545 WEST BUTLER AVENUE CHALFONT, PA 18914** 

PARID 26-006-101-001 **INSTRUMENT NUMBER 2025004671** 

**SITE AREA:** 

28,414 SQ. FT. OR 0.652 ACRES



**GRAPHIC SCALE** 1 INCH = 1000 FT

**DATE: APRIL 3, 2025 REVISED: JUNE 3, 2025** 



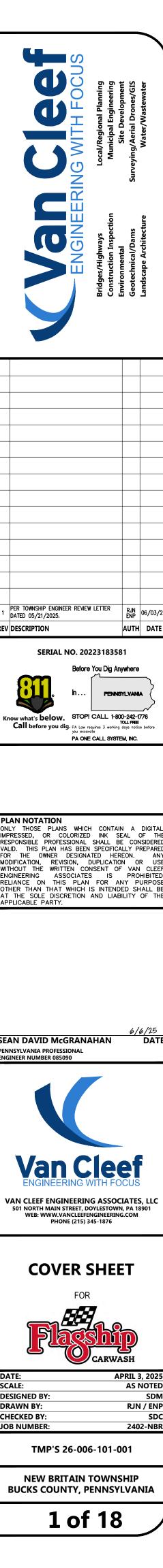
VAN CLEEF ENGINEERING ASSOCIATES, LLC **501 NORTH MAIN STREET, DOYLESTOWN, PA 18901** WEB: WWW.VANCLEEFENGINEERING.COM PHONE (215) 345-1876

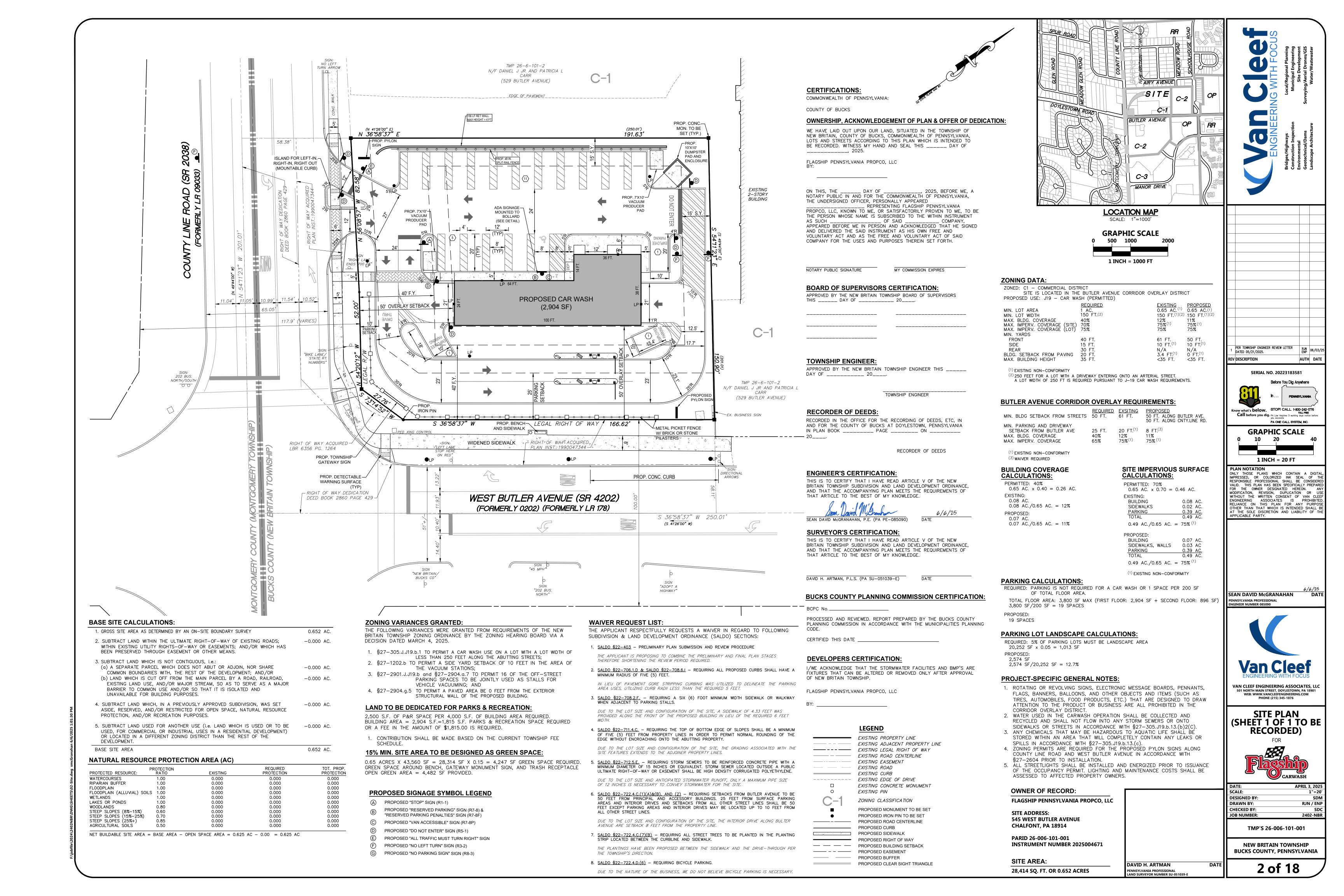
INDEX OF SHEETS			
SHEET No.	TITLE		
1	COVER SHEET		
2	SITE PLAN (SHEET 1 OF 1 TO BE RECORDED)		
3	EXISTING FEATURES PLAN		
4	AERIAL PLAN		
5	GRADING PLAN		
6	UTILITY PLAN		
7	LANDSCAPE PLAN		
8	LIGHTING PLAN		
9	<b>EROSION AND SEDIMENTATION CONTROL PLAN</b>		
10	CONSTRUCTION DETAILS		
11	UTILITY DETAILS		
12	<b>EROSION AND SEDIMENTATION CONTROL NOTES</b>		

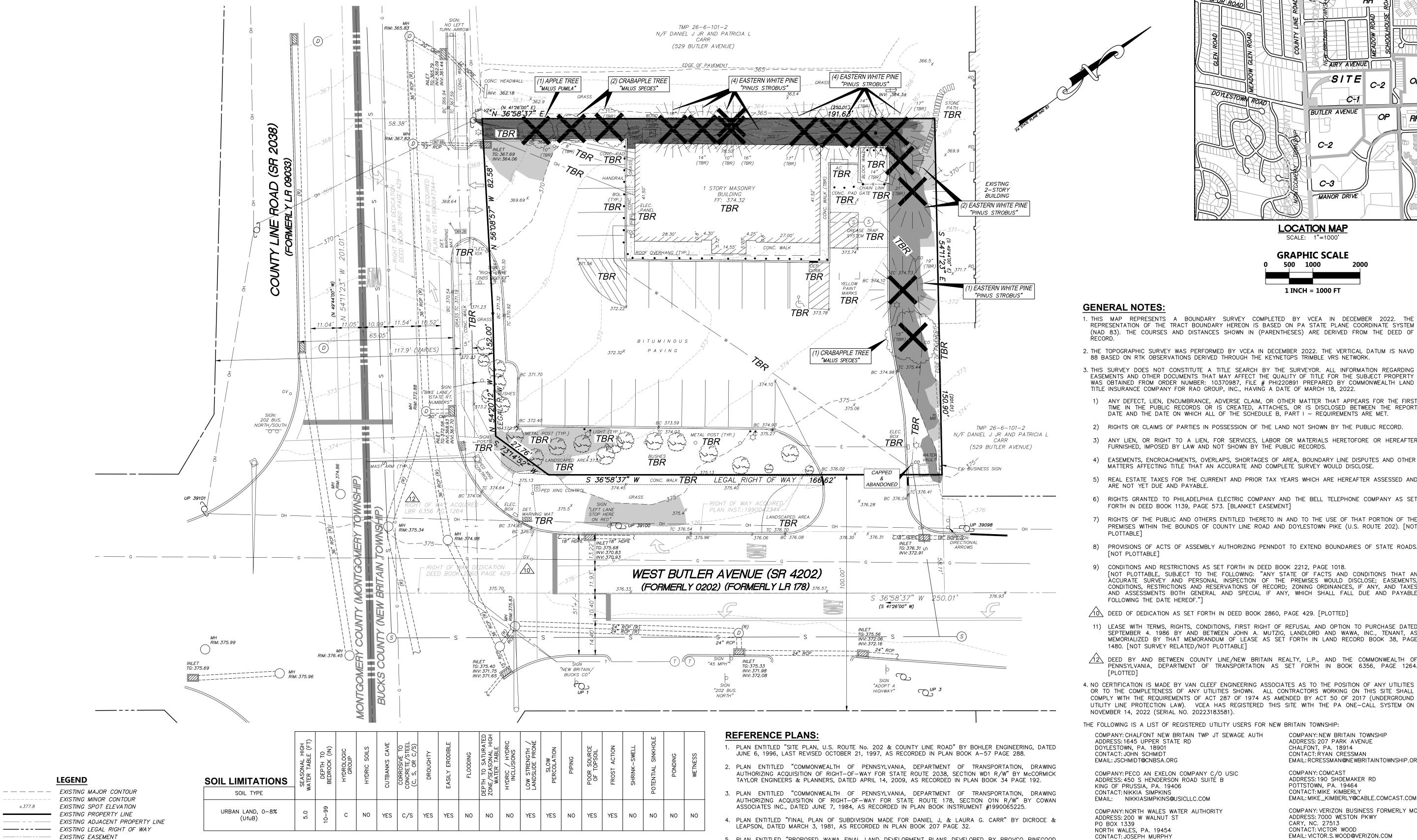
**CONSTRUCTION DETAILS ADA DETAILS UTILITY PROFILES VEHICLE NAVIGATION PLAN** 

**CONSTRUCTION DETAILS** 

**EROSION AND SEDIMENTATION CONTROL DETAILS** 







# **GENERAL NOTES:**

\_\_\_\_\_

====== EXISTING CURB

---- EXISTING EDGE OF DRIVE

EXISTING ROAD CENTERLINE

EXISTING SOILS LIMIT AND SOILS TYPE

EXISTING ROAD

EXISTING SIGN

EXISTING TREES

— — OH — EXISTING OVERHEAD ELECTRIC

— — T — EXISTING TELEPHONE

==== EXISTING STORM SEWER

— — W — EXISTING WATER MAIN

EXISTING SANITARY SEWER

— — G — EXISTING GAS MAIN

EXISTING UTILITY POLES

EXISTING WATER VALVE

EXISTING SLOPE 8 TO 15 PERCENT

EXISTING SLOPE 15 TO 25 PERCENT

EXISTING SLOPE 25< PERCENT

EXISTING ITEM TO BE REMOVED

EXISTING TREE TO BE REMOVED

1. SOILS SHOWN HEREON ARE AS MAPPED BY THE NATURAL RESOURCES CONSERVATION SERVICES (NRCS) WEBSITE FOR BUCKS COUNTY ON SEPTEMBER 26, 2023. 2. THE SITE FLOWS DRAIN TO THE WEST BRANCH OF THE NESHAMINY CREEK (WWF, MF) WITHIN THE NESHAMINY CREEK WATERSHED. THE WATERS DESIGNATED/EXISTING USES ARE CLASSIFIED BY THE PA CODE CHAPTER 93.

# SOIL RESOLUTIONS:

WHEN THE ABOVE SOILS LIMITATIONS ARE ENCOUNTERED, UTILIZE THE FOLLOWING SOILS RESOLUTIONS:

- DEPTH TO BEDROCK WHEN DEPTH TO BEDROCK IS ENCOUNTERED, CONTRACTOR MAY BLAST IF NECESSARY.
- CUTBANKS CAVE ALL APPLICABLE OSHA STANDARDS AND REGULATIONS WITH REGARD TO UTILITY AND RETAINING WALL CONSTRUCTION MUST BE IMPLEMENTED AT ALL TIMES. LIMIT SLOPES TO 3:1 MAXIMUM.
- CORROSIVE TO CONCRETE/STEEL ALL UNDERGROUND CONCRETE AND/OR STEEL MATERIALS TO BE BACKFILLED WITH APPROPRIATE MATERIAL WHEN CORROSIVENESS IS LIMITED. FOR BUILDINGS, CONTRACTOR TO COMPLY WITH THE INTERNAL RESIDENTIAL CODE (IRC) BUILDING CODE.
- DROUGHTY PROVIDE WATER TRUCK FOR IRRIGATION IN NEEDED. WHERE LANDSCAPING IS PROPOSED WITHIN DROUGHTY SOILS, APPLYING COMPOST MAY BE NECESSARY TO HELP
- EASILY ERODIBLE SOILS TO BE STABILIZED WITH APPROPRIATE COVER AND/OR MATTING.
- LOW STRENGTH/LANDSLIDE PRONE PRECAUTIONS SHOULD BE TAKEN TO PREVENT SLOPE FAILURES DUE TO IMPROPER CONSTRUCTION PRACTICES, SUCH AS OVER-STEEPENING AND OVERLOADING OF SLOPES, REMOVAL OF LATERAL SUPPORT, AND FAILURE TO PREVENT SATURATION OF SLOPES. ROAD FILL MATERIAL MAY NEED TO BE IMPORTED IN AREAS WHERE SOILS HAVE LOW STRENGTH.
- SLOW PERCOLATION UTILIZE STRUCTURAL BMP'S THAT DO NOT REQUIRE INFILTRATION (I.E. AMENDED SOILS, SLOW RELEASE BASIN). NO INFILTRATION BMP'S ARE PROPOSED. • POOR SOURCE OF TOPSOIL - CONTRACTOR TO SUPPLY TOPSOIL AS NEEDED.
- FROST ACTION PRECAUTIONS ARE NEEDED TO PREVENT DAMAGE, ESPECIALLY TO ROADWAYS.

- 5. PLAN ENTITLED "PROPOSED WAWA FINAL LAND DEVELOPMENT PLANS DEVELOPED BY PROVCO PINEGOOD CHALFONT, LLC" BY BOHLER ENGINEERING, DATED MAY 18, 2018, LAST REVISED NOVEMBER 1, 2019, AS RECORDED IN PLAN BOOK INSTRUMENT # 2020034823.
- 6. PLAN ENTITLED "SHOWALTER SUBDIVISION MINOR SUBDIVISION PLAN" BY SHOWALTER & ASSOCIATES, DATED MARCH 26, 1984, LAST REVISED APRIL 10, 1990, AS RECORDED IN PLAN BOOK 256 PAGE 82.

# SUMMARY OF ADVERSE FEE-SIMPLE CONVEYANCES:

50,244 S.F. DEED INSTRUMENT NO. 2022054655 -12,566 S.F. DEED BK, 2860 PG, 429 - 8,050 S.F. PLAN INSTRUMENT NO. 1990047344 - 1,214 S.F. DEED LRB 6356 PG. 1264 28,414 S.F. CURRENT SURVEY

ZATRY AVENUE *≤SITE* BUTLER AVENUE RR C-3 **LOCATION MAP** 

**GRAPHIC SCALE** 500 1000

1 INCH = 1000 FT

### **GENERAL NOTES:**

1. THIS MAP REPRESENTS A BOUNDARY SURVEY COMPLETED BY VCEA IN DECEMBER 2022. THE REPRESENTATION OF THE TRACT BOUNDARY HEREON IS BASED ON PA STATE PLANE COORDINATE SYSTEM (NAD 83). THE COURSES AND DISTANCES SHOWN IN (PARENTHESES) ARE DERIVED FROM THE DEED OF

88 BASED ON RTK OBSERVATIONS DERIVED THROUGH THE KEYNETGPS TRIMBLE VRS NETWORK. 3. THIS SURVEY DOES NOT CONSTITUTE A TITLE SEARCH BY THE SURVEYOR. ALL INFORMATION REGARDING EASEMENTS AND OTHER DOCUMENTS THAT MAY AFFECT THE QUALITY OF TITLE FOR THE SUBJECT PROPERTY WAS OBTAINED FROM ORDER NUMBER: 10370987, FILE # PHI220891 PREPARED BY COMMONWEALTH LAND

- TITLE INSURANCE COMPANY FOR RAO GROUP, INC., HAVING A DATE OF MARCH 18, 2022. 1) ANY DEFECT, LIEN, ENCUMBRANCE, ADVERSE CLAIM, OR OTHER MATTER THAT APPEARS FOR THE FIRST
- TIME IN THE PUBLIC RECORDS OR IS CREATED, ATTACHES, OR IS DISCLOSED BETWEEN THE REPORT DATE AND THE DATE ON WHICH ALL OF THE SCHEDULE B, PART I - REQUIREMENTS ARE MET.
- 3) ANY LIEN, OR RIGHT TO A LIEN, FOR SERVICES, LABOR OR MATERIALS HERETOFORE OR HEREAFTER
- FURNISHED, IMPOSED BY LAW AND NOT SHOWN BY THE PUBLIC RECORDS. 4) EASEMENTS, ENCROACHMENTS, OVERLAPS, SHORTAGES OF AREA, BOUNDARY LINE DISPUTES AND OTHER
- 5) REAL ESTATE TAXES FOR THE CURRENT AND PRIOR TAX YEARS WHICH ARE HEREAFTER ASSESSED AND
- ARE NOT YET DUE AND PAYABLE. 6) RIGHTS GRANTED TO PHILADELPHIA ELECTRIC COMPANY AND THE BELL TELEPHONE COMPANY AS SET

MATTERS AFFECTING TITLE THAT AN ACCURATE AND COMPLETE SURVEY WOULD DISCLOSE.

- FORTH IN DEED BOOK 1139, PAGE 573. [BLANKET EASEMENT] 7) RIGHTS OF THE PUBLIC AND OTHERS ENTITLED THERETO IN AND TO THE USE OF THAT PORTION OF THE PREMISES WITHIN THE BOUNDS OF COUNTY LINE ROAD AND DOYLESTOWN PIKE (U.S. ROUTE 202). [NOT
- 8) PROVISIONS OF ACTS OF ASSEMBLY AUTHORIZING PENNDOT TO EXTEND BOUNDARIES OF STATE ROADS.
- [NOT PLOTTABLE] 9) CONDITIONS AND RESTRICTIONS AS SET FORTH IN DEED BOOK 2212, PAGE 1018. [NOT PLOTTABLE, SUBJECT TO THE FOLLOWING: "ANY STATE OF FACTS AND CONDITIONS THAT AN ACCURATE SURVEY AND PERSONAL INSPECTION OF THE PREMISES WOULD DISCLOSE; EASEMENTS, CONDITIONS, RESTRICTIONS AND RESERVATIONS OF RECORD; ZONING ORDINANCES, IF ANY, AND TAXES AND ASSESSMENTS BOTH GENERAL AND SPECIAL IF ANY, WHICH SHALL FALL DUE AND PAYABLE
- FOLLOWING THE DATE HEREOF."] 10 DEED OF DEDICATION AS SET FORTH IN DEED BOOK 2860, PAGE 429. [PLOTTED]
- 11) LEASE WITH TERMS, RIGHTS, CONDITIONS, FIRST RIGHT OF REFUSAL AND OPTION TO PURCHASE DATED SEPTEMBER 4. 1986 BY AND BETWEEN JOHN A. MUTZIG, LANDLORD AND WAWA, INC., TENANT, AS MEMORIALIZED BY THAT MEMORANDUM OF LEASE AS SET FORTH IN LAND RECORD BOOK 38, PAGE
- 1480. [NOT SURVEY RELATED/NOT PLOTTABLE] DEED BY AND BETWEEN COUNTY LINE/NEW BRITAIN REALTY, L.P., AND THE COMMONWEALTH OF PENNSYLVANIA, DEPARTMENT OF TRANSPORTATION AS SET FORTH IN BOOK 6356, PAGE 1264.
- 4. NO CERTIFICATION IS MADE BY VAN CLEEF ENGINEERING ASSOCIATES AS TO THE POSITION OF ANY UTILITIES OR TO THE COMPLETENESS OF ANY UTILITIES SHOWN. ALL CONTRACTORS WORKING ON THIS SITE SHALL COMPLY WITH THE REQUIREMENTS OF ACT 287 OF 1974 AS AMENDED BY ACT 50 OF 2017 (UNDERGROUND UTILITY LINE PROTECTION LAW). VCEA HAS REGISTERED THIS SITE WITH THE PA ONE-CALL SYSTEM ON
- NOVEMBER 14, 2022 (SERIAL NO. 20223183581).

THE FOLLOWING IS A LIST OF REGISTERED UTILITY USERS FOR NEW BRITAIN TOWNSHIP: COMPANY: CHALFONT NEW BRITAIN TWP JT SEWAGE AUTH ADDRESS: 1645 UPPER STATE RD DOYLESTOWN, PA. 18901 CONTACT: JOHN SCHMIDT

COMPANY: PECO AN EXELON COMPANY C/O USIC ADDRESS: 450 S HENDERSON ROAD SUITE B KING OF PRUSSIA, PA. 19406 CONTACT: NIKKIA SIMPKINS

COMPANY: NORTH WALES WATER AUTHORITY ADDRESS: 200 W WALNUT ST PO BOX 1339 NORTH WALES, PA. 19454

CONTACT: JOSEPH MURPHY EMAIL: JMURPHY@NWWATER.COM

- 5. THIS SURVEY DOES NOT CERTIFY AS TO THE PRESENCE OR ABSENCE OF UNDERGROUND IMPROVEMENTS OR
- 6. THIS SURVEY DOES NOT CERTIFY AS TO THE PRESENCE OR ABSENCE OF WETLANDS OR WATERS.
- 7. THE PARCEL SHOWN HEREON IS NOT LOCATED IN A SPECIAL FLOOD HAZARD AREA (SFHA), PER FLOOD INSURANCE RATE MAPS (FIRM) PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA), BEING PANEL 288 OF 532 MAP NUMBER 42017C0288K, EFFECTIVE DATE MARCH 21, 2017.

OWNER OF RECORD: FLAGSHIP PENNSYLVANIA PROPCO, LLC

SITE ADDRESS: **545 WEST BUTLER AVENUE** CHALFONT, PA 18914

PARID 26-006-101-001 **INSTRUMENT NUMBER 2025004671** 

SITE AREA:

28,414 SQ. FT. OR 0.652 ACRES

COMPANY: NEW BRITAIN TOWNSHIP

EMAIL: RCRESSMAN@NEWBRITAINTOWNSHIP.ORG

EMAIL: MIKE\_KIMBERLY@CABLE.COMCAST.COM

COMPANY: VERIZON BUSINESS FORMERLY MCI

ADDRESS: 207 PARK AVENUE

ADDRESS: 190 SHOEMAKER RD

ADDRESS: 7000 WESTON PKWY

EMAIL: VICTOR.S.WOOD@VERIZON.COM

CHALFONT, PA. 18914 CONTACT: RYAN CRESSMAN

POTTSTOWN, PA. 19464

CONTACT: MIKE KIMBERLY

COMPANY: COMCAST

CARY, NC. 27513

CONTACT: VICTOR WOOD

OAVID H. ARTMAN

PHONE (215) 345-1876 **EXISTING FEATURES DEMOLITION PLAN** 

VAN CLEEF ENGINEERING ASSOCIATES, LLC

501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM

PER TOWNSHIP ENGINEER REVIEW LETTER

**SERIAL NO. 20223183581** 

now what's below. STOPI CALL 1-800-242-1776

**GRAPHIC SCALE** 

1 INCH = 20 FT

NLY THOSE PLANS WHICH CONTAIN A DIG MPRESSED, OR COLORIZED INK SEAL OF T RESPONSIBLE PROFESSIONAL SHALL BE CONSIDER

VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPART FOR THE OWNER DESIGNATED HEREON. AN

MODIFICATION, REVISION, DUPLICATION OR U
WITHOUT THE WRITTEN CONSENT OF VAN CLE
ENGINEERING ASSOCIATES IS PROHIBITE
RELIANCE ON THIS PLAN FOR ANY PURPO

THER THAN THAT WHICH IS INTENDED SHALL

AT THE SOLE DISCRETION AND LIABILITY OF APPLICABLE PARTY.

EAN DAVID McGRANAHAN NSYLVANIA PROFESSIONAL

NEER NUMBER 085090

Call before you dig. PA Law requires 3 working days notice

0 10 20

PLAN NOTATION

Before You Dig Anywhere

PA ONE CALL SYSTEM, INC.

AUTH DATE

DATED 05/21/2025.

EV DESCRIPTION

RJN / ENP RAWN BY HECKED BY **B NUMBER** 

TMP'S 26-006-101-001

**NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 



ENGINEERING WITH FOCUS
Bridges/Highways
Construction Inspection
Environmental
Geotechnical/Dams
Surveying/Aerial Drones/GIS

PER TOWNSHIP ENGINEER REVIEW LETTER DATED 05/21/2025.

DESCRIPTION

AUTH

DA

SERIAL NO. 20223183581

Before You Dig Anywi

In ...

PENNSYL

W what's below. STOPI CALL 1-800-2

Call before you dig. PA Law requires 3 working day

below. STOPI CALL 1-800-242-7776
TOLL FREE
pre you dig. PA Low requires 3 working doys notice before
you excavate
PA ONE CALL SYSTEM, INC.

**GRAPHIC SCALE**25 50 100

1 INCH = 50 FT

PLAN NOTATION

ONLY THOSE PLANS WHICH CONTAIN A DIGITAL, IMPRESSED, OR COLORIZED INK SEAL OF THE RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERED VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARED FOR THE OWNER DESIGNATED HEREON. ANY MODIFICATION, REVISION, DUPLICATION OR USE WITHOUT THE WRITTEN CONSENT OF VAN CLEEF ENGINEERING ASSOCIATES IS PROHIBITED. RELIANCE ON THIS PLAN FOR ANY PURPOSE OTHER THAN THAT WHICH IS INTENDED SHALL BE AT THE SOLE DISCRETION AND LIABILITY OF THE APPLICABLE PARTY.

APPLICABLE PARIT.

6/6
SEAN DAVID McGRANAHAN
PENNSYLVANIA PROFESSIONAL
ENGINEER NUMBER 085090



VAN CLEEF ENGINEERING ASSOCIATES, LLC
501 NORTH MAIN STREET, DOYLESTOWN, PA 18901
WEB: WWW.VANCLEEFENGINEERING.COM
PHONE (215) 345-1876

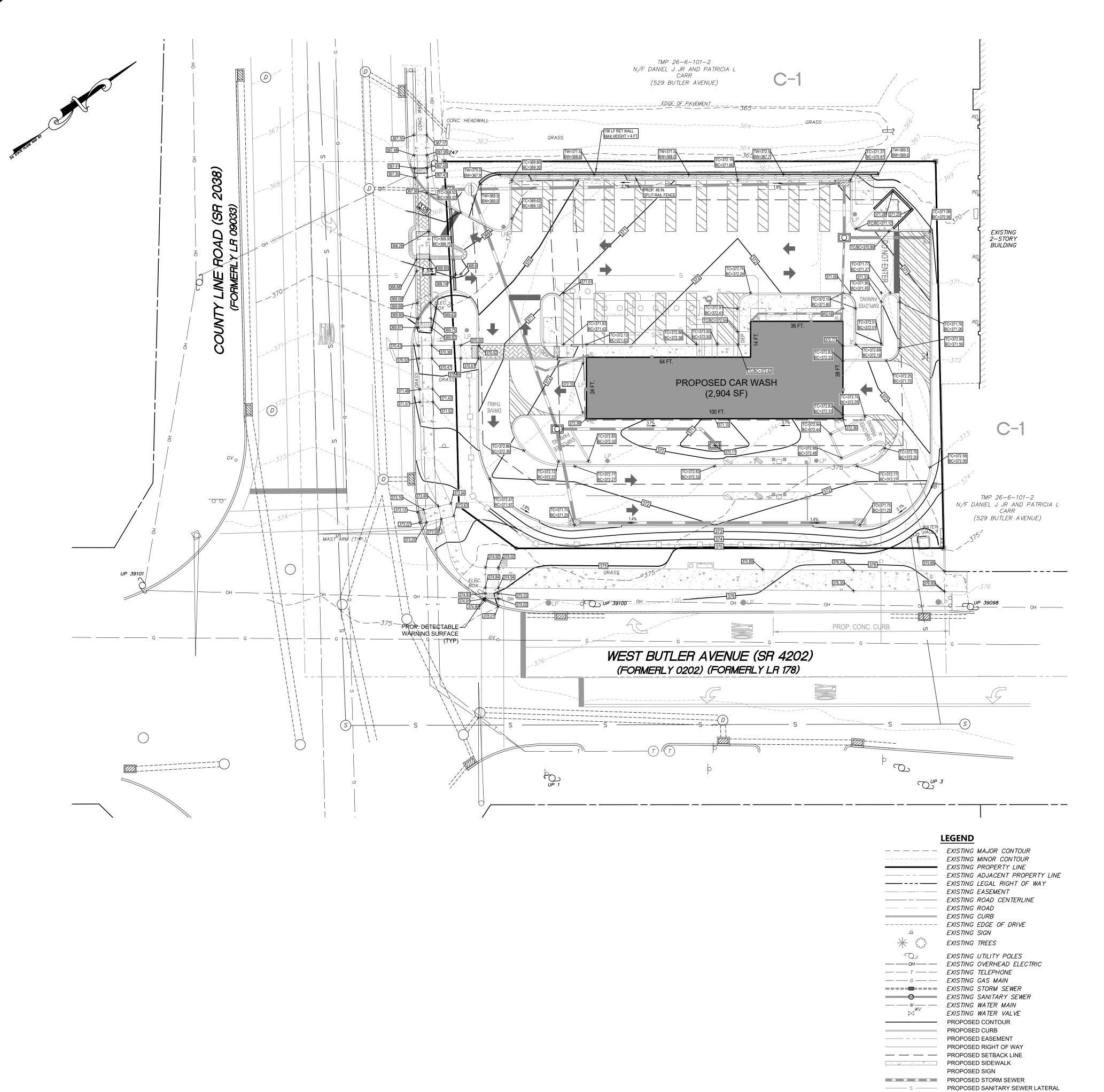
AERIAL PLAN



DATE: APRIL 3, 2025
SCALE: 1"=50'
DESIGNED BY: SDM
DRAWN BY: RJN / ENP
CHECKED BY: SDC
JOB NUMBER: 2402-NBR

TMP'S 26-006-101-001

NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA



### **GRADING GENERAL NOTES:**

- 1. VERTICAL DATUM IS NAVD 1988 AND ESTABLISHED BY OBSERVATIONS REFERENCED TO THE KEYSTONE VRS NETWORK (KEYNETGPS).
- 2. THE MINIMUM SLOPE IN GRASSED AREAS SHALL NOT BE LESS THAN 2.0% AND THE MINIMUM IN PAVED AREAS SHOULD NOT BE LESS THAN 1.0%.
- 3. PAVEMENT SHALL BE SAW CUT IN STRAIGHT LINES TO THE FULL DEPTH OF THE EXISTING PAVEMENT. ALL DEBRIS FROM REMOVAL OPERATIONS SHALL BE REMOVED FROM THE SITE AT THE TIME OF EXCAVATION. STOCKPILING OF DEBRIS WILL NOT BE PERMITTED.
- 4. IN CASE OF DISCREPANCIES BETWEEN PLANS, THE SITE / RECORD PLAN WILL SUPERSEDE IN ALL CASES. THE ENGINEER OF RECORD MUST BE IMMEDIATELY NOTIFIED IN WRITING OF ANY CONFLICTS.
- 5. THE CONTRACTOR SHALL BE REQUIRED TO SECURE ALL NECESSARY PERMITS (INCLUDING DEP, ETC.) FOR ALL OFF—SITE HAUL AND/OR BORROW SITES. CONTRACTOR SHALL SUPPLY A COPY OF APPROVALS TO DESIGN ENGINEER AND OWNER PRIOR TO INITIATING WORK.
- 6. EXISTING INLETS AND STORM SEWER INDICATED AS FILLED W/DEBRIS SHALL BE CLEANED AND FLUSHED. NEW INLETS AND PIPES SHALL BE CHECKED FOR SILT/DEBRIS AFTER CONSTRUCTION AND FLUSHED/CLEANED IF NECESSARY.
- 7. DEPTH OF EXISTING UTILITIES IN PORTIONS OF THE SITE ARE UNKNOWN. WHERE EXISTING UTILITIES ARE TO REMAIN AND ARE FOUND TO HAVE INADEQUATE GROUND COVER AFTER FINAL PROPOSED GRADES HAVE BEEN ESTABLISHED, THE DESIGN ENGINEER SHALL BE CONTACTED IMMEDIATELY AND PRIOR TO FURTHER CONSTRUCTION ACTIVITIES IN THE AREA OF SAID CONFLICT.
- 8. ALL DESIGNERS AND CONTRACTORS UTILIZING THIS PLAN AND THE INFORMATION CONTAINED THEREON ARE CAUTIONED TO COMPLY WITH THE REQUIREMENTS OF PENNSYLVANIA ACT 287. LOCATION OF EXISTING AND PROPOSED UNDERGROUND UTILITIES AND FACILITIES SHOWN ON THE DRAWINGS HAVE BEEN DEVELOPED FROM INFORMATION MADE AVAILABLE. COMPLETENESS AND ACCURACY OF LOCATION AND DEPTH OF UTILITIES AND FACILITIES CANNOT BE GUARANTEED. THE CONTRACTOR IS TO VERIFY THE DEPTH AND LOCATION OF ALL UTILITIES AND FACILITIES BEFORE THE START OF WORK. UTILIZE HAND EXCAVATION AS REQUIRED. WORK IS ALSO TO BE DONE IN ACCORDANCE WITH THE STANDARDS OF THE UTILITY COMPANIES WHOSE FACILITIES ARE IN THE PROXIMITY OF THE WORK. OTHER UTILITIES MAY BE REQUIREMENTS OF PENNSYLVANIA ACT 38 (1991), THE CONTRACTOR SHALL CONTACT THE PENNSYLVANIA ONE CALL SYSTEM AT 1-800-242-1776, AT LEAST 3 DAYS PRIOR TO EXCAVATION.
- 9. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE FEDERAL, STATE AND LOCAL CODES, AND ALL REGULATIONS APPURTENANT TO THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970. ALL WORK SHALL BE DONE IN ACCORDANCE WITH PROJECT SPECIFICATIONS INCLUDING CURRENT APPLICABLE STANDARDS AND REQUIREMENTS. WHERE ANY STANDARDS SEEM IN CONFLICT WITH THESE DRAWINGS, NOTIFY THE DESIGN ENGINEER AND CONSTRUCTION MANAGER FOR DIRECTION PRIOR TO PROCEEDING WITH WORK.
- 10. CONTRACTORS SHALL HAVE ALL REQUIRED SUBMITTAL APPROVALS PRIOR TO BEGINNING WORK OR ORDERING
- 11. CONTRACTORS SHALL VERIFY ALL DIMENSIONS, INVERTS, ELEVATIONS, AND EXISTING CONDITIONS PRIOR TO PROCEEDING WITH THE WORK OR PROCUREMENT OF MATERIALS. VARIATIONS BETWEEN DRAWINGS AND ACTUAL FIELD CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER AND RESOLVED PRIOR TO PROCEEDING WITH THE WORK.
- 12. CONTRACTOR SHALL BE RESPONSIBLE FOR SAFETY, PROCEDURES, MEANS AND METHODS, SEQUENCING, AND COORDINATION.
- 13. ALL WORK SHALL BE PERFORMED BY QUALIFIED, EXPERIENCED PERSONNEL.
- 14. CONTRACTOR SHALL NOTIFY THE OWNER OF PREEXISTING CONDITIONS OF DETERIORATION IN AREAS OF WORK THAT ARE UNCOVERED OR EXPOSED DURING THE WORK.
- 15. FIELD CHANGES REQUIRE PRIOR DESIGN ENGINEERING REVIEW AND WRITTEN CONFIRMATION.
- 16. CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR TO ANY SIDEWALKS, LAWN, TREES, PAVING, AND OTHER IMPROVEMENTS DISTURBED OR DAMAGED BY DEMOLITION ACTIVITIES PROPOSED HEREIN.
- 17. CONTRACTOR SHALL PROVIDE PROPER TEMPORARY BRACING AND SHORING OF ALL CONSTRUCTION TO REMAIN OR DEMOLITION WORK IN PROGRESS.
- 18. CONTRACTOR SHALL PROVIDE LAYOUT, LINE AND GRADE UNLESS OTHERWISE NOTED.
- 19. CONTRACTOR SHALL NOTIFY THE OWNER OF ANY DISCREPANCIES WITHIN THE DRAWINGS, SPECIFICATIONS, CODES OR STANDARDS FOR CORRECTIVE ACTION PRIOR TO START OF WORK.
- 20. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE AND PROTECTION OF VEHICULAR AND PEDESTRIAN TRAFFIC. ALL TRAFFIC CONTROL MEASURES SHALL BE IN ACCORDANCE WITH LOCAL, PENNDOT & OSHA REGULATIONS.
- 21. CONTRACTOR SHALL PROVIDE PROTECTION FOR EXISTING UTILITIES UNLESS OTHERWISE NOTED. THE CONTRACTOR SHALL IMMEDIATELY REPAIR ANY UTILITY LINE INTERRUPTION AT NO ADDITIONAL CONTRACT COST. THE CONTRACTOR SHALL PROVIDE ADEQUATE PROTECTION AND SUPPORT FOR ALL UTILITIES EXPOSED DURING THE WORK TO INSURE AGAINST DAMAGE AT NO ADDITIONAL COST.
- 22. CONTRACTOR SHALL PROVIDE TEMPORARY DEWATERING OF EXCAVATIONS THROUGHOUT THE DURATION OF CONTRACT AT NO ADDITIONAL COST.
- 23. EXISTING UTILITIES SHOWN ARE BASED ON AVAILABLE DATA. DUE TO THE POTENTIAL LACK OF COMPLETE OR ACCURATE DATA REGARDING EXISTING ONSITE AND OFFSITE UTILITIES, THE CONTRACTOR SHALL ASSESSAVAILABLE DATA, SHALL REQUEST UTILITY COMPANY MARKOUTS, SHALL COORDINATE WITH AFFECTED UTILITY COMPANIES, AND SHALL DIG TEST PITS AT ALL PROPOSED UTILITY CROSSING LOCATIONS SO THAT ELEVATIONS CAN BE TAKEN TO ASSESS POTENTIAL CONFLICTING PIPES/UTILITIES PRIOR TO ANY PROPOSED UTILITY CONSTRUCTION. IF UNEXPECTED UTILITY LOCATIONS OR ELEVATIONS OR PIPE CONFLICTS ARE ENCOUNTERED DURING CONSTRUCTION, OR IF TEST PITS REVEAL POTENTIAL CONFLICT, DESIGN ENGINEER SHALL BE NOTIFIED SO THAT ELEVATIONS AND LOCATIONS (WHERE NECESSARY) OF AFFECTED UTILITIES CAN BE OBTAINED TO FACILITATE NECESSARY DESIGN ADJUSTMENTS.
- 24. REMOVAL OF EXISTING UTILITIES SHALL BE COORDINATED WITH THE APPROPRIATE UTILITY COMPANIES.
- 25. REFER TO THE SITE  $\!\!\!/$  RECORD PLAN FOR ADDITIONAL NOTES.
- 26. ALL SIDEWALKS, CROSSWALK, TRAILS, ENTRANCES, AND RAMPS TO BE BUILT IN ACCORDANCE WITH ADA STANDARDS. MAXIMUM LONGITUDINAL SLOPE TO BE 5%. MAXIMUM CROSS SLOPE TO BE 2%. IT IS RECOMMENDED TO CONSTRUCT CROSS SLOPES AT 1.5% AND LONGITUDINAL SLOPES AT 4.9% TO ALLOW FOR CONSTRUCTION TOLERANCE.
- 27. REFER TO ADA CURB RAMP CONSTRUCTION DETAILS AND ADA ACCESSIBILITY PLAN FOR ADDITIONAL INFORMATION SPECIFIC TO CURB RAMP AND ACCESSIBLE ROUTE GRADING.
- 28. AFTER FINAL GRADING THERE SHALL BE A MINIMUM OF EIGHT INCHES OF TOPSOIL ON THE ENTIRE SITE OTHER THAN THAT PORTION OF THE SITE WHERE THERE ARE BUILDINGS OR OTHER IMPERVIOUS SURFACE COVERAGE. THE DEVELOPER IS NOT REQUIRED TO IMPORT TOPSOIL TO THE SITE IF AN EIGHT—INCH FINAL DEPTH CANNOT BE ACHIEVED. IN CASES WHERE THE ORIGINAL SITE HAS LESS THAN EIGHT INCHES OF TOPSOIL AT THE START OF GRADING ACTIVITIES, THE TOTAL AMOUNT AVAILABLE WILL BECOME THE MINIMUM TOPSOIL REQUIREMENT. NO TOPSOIL WILL BE ALLOWED TO BE RELEASED FROM THE SITE.
- 29. ALL AREAS SHALL BE PROPERLY GRADED TO ENSURE DRAINAGE FLOW AWAY FROM THE PROPOSED BUILDING WITHOUT PONDING OR OBSTRUCTION.
- 30. DURING GRADING OPERATIONS, NECESSARY MEASURES FOR DUST CONTROL SHALL BE EXERCISED.
- 31. THE CHALFONT NEW BRITAIN JOINT SEWER AUTHORITY IS TO BE CONTACTED ONE WEEK PRIOR TO SANITARY SEWER CONSTRUCTION, AND 72 HOURS PRIOR TO EXCAVATING NEAR, AND CONNECTING INTO ANY EXISTING SANITARY SEWER.
- 32. ALL SANITARY SEWER CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THE NORTH WALES WATER AUTHORITY SPECIFICATIONS.
- 33. PUBLIC WATER AND SEWER CURRENTLY SERVE THE EXISTING BUILDING. THE PROPOSED WATER SERVICE
- SHALL CONNECT TO THE EXISTING WATER VAULT.
- 34. PROPOSED HANDICAP PARKING SPACES SHALL BE PROVIDED WITH POST—MOUNTED SIGNS.35. CONTRACTOR TO CONNECT NEW BUILDING UTILITY CONNECTIONS TO EXISTING WATER AND SEWER.
- 36. CONTRACTOR SHALL VERIFY COMPLIANCE WITH ADA STANDARDS WITH PROJECT ARCHITECT FOR RAMP AREAS PROPOSED ADJACENT TO DOORWAYS.
- 37. ALL UTILITY SERVICES TO THE PROPOSED BUILDING SHALL BE INSTALLED UNDERGROUND.
- 38. TOPSOIL MAY NOT BE REMOVED FROM THE SITE WITHOUT APPROVAL FROM THE TOWNSHIP.
- 39. FINAL DESIGN OF PROPOSED WATER AND SEWER FACILITIES ARE SUBJECT TO THE REVIEW AND APPROVAL BY THE SERVICING WATER AND SEWER AUTHORITY.

# RETAINING WALL NOTES:

PROPOSED WATER LATERAL FINISHED FLOOR ELEVATION

PROPOSED SPOT ELEVATION

507.30

- 1. RETAINING WALL SPECIFICATIONS AND DESIGN DETAILS SHALL BE APPROVED BY THE TOWNSHIP ENGINEER PRIOR TO FINAL PLAN APPROVAL.
- 2. WALL STYLE, FINISH AND COLOR SHALL BE COORDINATED WITH PROPOSED BUILDING ARCHITECTURE.



PER TOWNSHIP ENGINEER REVIEW LETTER DATED 05/21/2025. RJN 06/03/2

SERIAL NO. 20223183581



GRAPHIC SCALE

PA ONE CALL SYSTEM, INC.

1 INCH = 20 FT

PLAN NOTATION

ONLY THOSE PLANS WHICH CONTAIN A DIGITA IMPRESSED, OR COLORIZED INK SEAL OF THE RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERE VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARE FOR THE OWNER DESIGNATED HEREON. AN MODIFICATION, REVISION, DUPLICATION OR US WITHOUT THE WRITTEN CONSENT OF VAN CLEENGINEERING ASSOCIATES IS PROHIBITEI RELIANCE ON THIS PLAN FOR ANY PURPOS OTHER THAN THAT WHICH IS INTENDED SHALL BAT THE SOLE DISCRETION AND LIABILITY OF THAPPLICABLE PARTY.

6/6 EAN DAVID McGRANAHAN D

NSYLVANIA PROFESSIONAL



VAN CLEEF ENGINEERING ASSOCIATES, LLC 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM PHONE (215) 345-1876

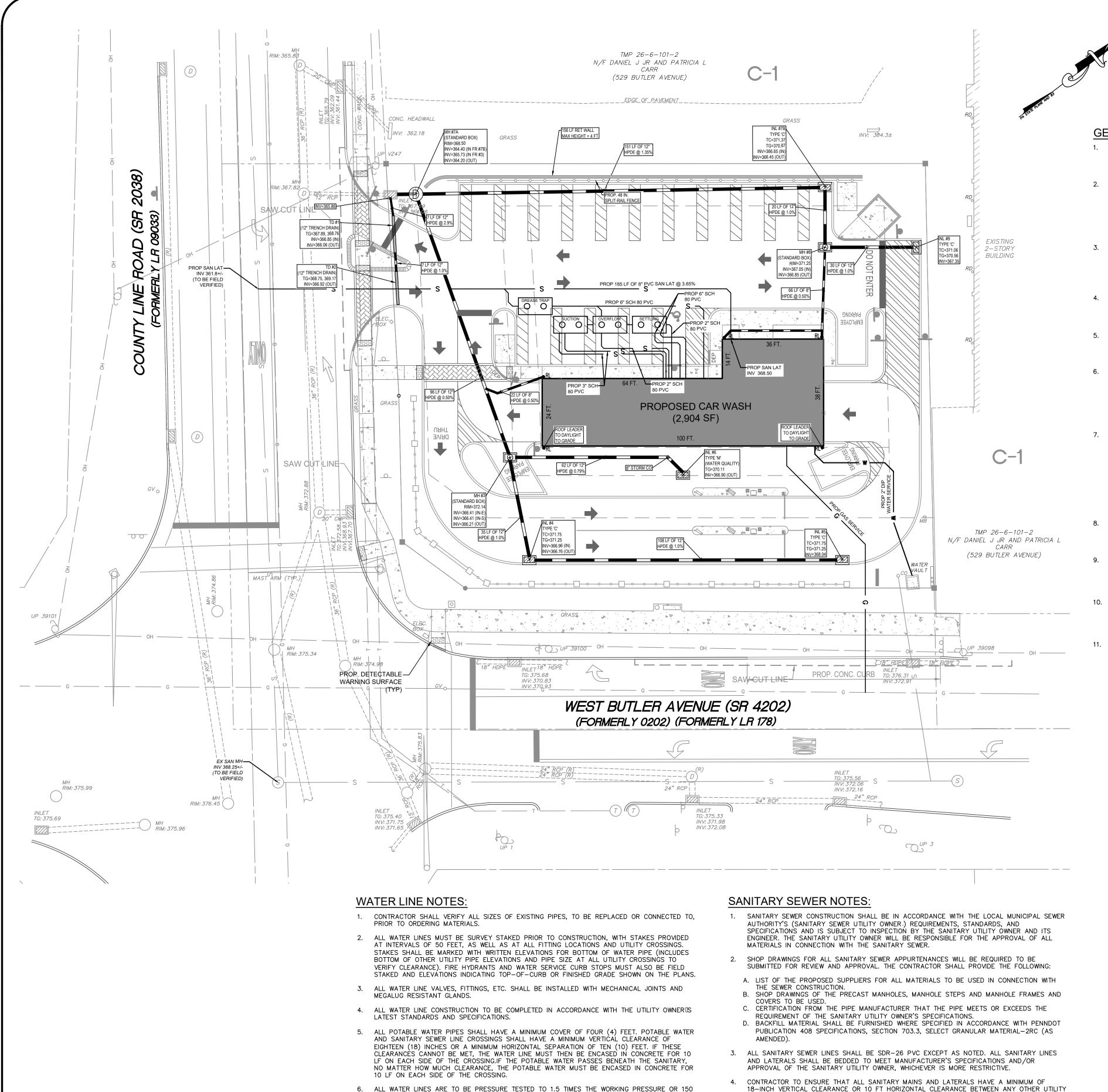
GRADING PLAN



DATE: APRIL 3, 2025
SCALE: 1"=20"
DESIGNED BY: SDM
DRAWN BY: RJN / ENP
CHECKED BY: SDC
JOB NUMBER: 2402-NBR

TMP'S 26-006-101-001

NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA



P.S.I., WHICHEVER IS GREATER, IN SECTIONS BETWEEN VALVES. THE TEST PRESSURE SHALL BE

MAINTAINED FOR A PERIOD NOT LESS THAN 2 HOURS. SHOULD ANY PRESSURE TEST DISCLOSE

AN INABILITY TO HOLD THE TEST PRESSURE, THE CONTRACTOR SHALL LOCATE AND CORRECT

7. THE INSTALLATION OR REPAIR OF ANY UNDERGROUND FACILITIES OR PIPING WHICH CONNECTS TO

OR FURNISHES WATER FOR THE FIRE PROTECTION SPRINKLER SYSTEM SHALL BE PERFORMED

ONLY BY A LICENSED UTILITY CONTRACTOR, FIRE PROTECTION SPRINKLER CONTRACTOR, OR

LICENSED PLUMBER. A COPY OF THE LICENSE OR CERTIFICATE OF COMPETENCY SHALL BE

ALL STORM CONVEYANCE PIPE SHALL BE SMOOTH LINED DOUBLE WALLED HIGH DENSITY

POLYETHYLENE PIPE (HDPE), UNLESS OTHERWISE SPECIFIED. ANY ROOF LEADERS SHALL BE

3. LANDSCAPING, FENCES AND STRUCTURES SHALL BE PLACED A MINIMUM OF 3 FEET AWAY FROM

IF A CONFLICT ARISES DURING THE INSTALLATION OF ANY PART OF THE STORM SEWER SYSTEM

DEFECTS AND RETEST TO THE SATISFACTION OF THE UTILITY OWNER

PROVIDED TO THE INSPECTOR AT THE FINAL INSPECTION.

THE ENGINEER IS TO BE NOTIFIED IMMEDIATELY IN WRITING.

STORM SEWERS OUTSIDE OF THE RIGHT OF WAY OF STREETS.

STORM SEWER NOTES:

### GENERAL UTILITIES NOTES:

- ALL NEW UTILITIES/SERVICES, INCLUDING ELECTRIC, TELEPHONE, CABLE TV, ETC. ARE TO BE INSTALLED UNDERGROUND. ALL NEW UTILITIES/SERVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE UTILITY/SERVICE PROVIDER INSTALLATION SPECIFICATIONS
- THE LOCATION OF ALL EXISTING AND PROPOSED SERVICES ARE APPROXIMATE. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING WITH LOCAL UTILITY COMPANIES TO LOCATE ALL PUBLIC OR PRIVATE UTILITIES WHICH LIE IN OR ADJACENT TO THE CONSTRUCTION SITE PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION, REMOVAL OR EXCAVATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING, AT HIS EXPENSE, ALL EXISTING UTILITIES DAMAGED DURING CONSTRUCTION. SANITARY SEWER AND ALL OTHER UTILITY SERVICE CONNECTION POINT CONFLICTS SHALL BE REPORTED IMMEDIATELY IN WRITING TO THE ENGINEER.
- 3. ALL UTILITY EXTENSIONS FROM THE BUILDING BY OTHER TRADES SHALL BE CONNECTED TO THE CONTINUATION UTILITY LINES ON THE SITE. THE GENERAL CONTRACTOR SHALL MAKE THE CONNECTIONS AT A POINT 50 FROM THE BUILDING WALL WHERE OTHER TRADES HAVE ENDED THEIR WORK. THE GC SHALL COORDINATE SCHEDULING OF THESE CONNECTIONS WITH OTHER TRADES TO ASSURE THE SYSTEM IS COMPLETE WHEN FLOW WILL BEGIN IN THE LINES.
- 4. CONSTRUCTION SHALL COMMENCE BEGINNING AT THE LOWEST INVERT (POINT OF CONNECTION) AND PROGRESS UP GRADIENT. PROPOSED INTERFACE POINTS (CROSSINGS) WITH EXISTING UNDERGROUND UTILITIES SHALL BE FIELD VERIFIED BY TEST PIT PRIOR TO COMMENCEMENT OF CONSTRUCTION.
- 5. DEFINE AND LOCATE VERTICALLY AND HORIZONTALLY ALL ACTIVE UTILITY AND/OR SERVICE SYSTEMS THAT ARE NOT TO REMAIN. THE CONTRACTOR IS RESPONSIBLE TO PROTECT AND MAINTAIN ALL ACTIVE SYSTEMS THAT ARE NOT BEING REMOVED/RELOCATED DURING SITE ACTIVITY.
- 6. THE CONTRACTOR SHALL FAMILIARIZE THEMSELVES WITH THE APPLICABLE UTILITY SERVICE PROVIDER REQUIREMENTS AND IS RESPONSIBLE FOR COMPLIANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS, CODES AND REGULATIONS.THE CONTRACTOR IS ALSO RESPONSIBLE FOR ALL COORDINATION REGARDING UTILITY WORK AS IDENTIFIED OR REQUIRED FOR PROJECT. THE CONTRACTOR SHALL PROVIDE THE OWNER WRITTEN NOTIFICATIONS THAT THE EXISTING UTILITIES AND SERVICES HAVE BEEN TERMINATED AND ABANDONED IN ACCORDANCE WITH JURISDICTION AND UTILITY COMPANY
- 7. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATION OF SITE PLAN DOCUMENTS AND ARCHITECTURAL DESIGN FOR EXACT BUILDING UTILITY CONNECTION LOCATIONS, DOOR ACCESS, AND EXTERIOR GRADING. (THE UTILITY SERVICE SIZES ARE TO BE DETERMINED BY ARCHITECT.) THE CONTRACTOR SHALL COORDINATE INSTALLATION OF UTILITIES/SERVICES WITH THE INDIVIDUAL COMPANIES TO AVOID CONFLICTS AND ENSURE PRÓPER DEPTHS ARE ACHIEVED. IN ADDITION TO MEETING THE UTILITY OWNER REQUIREMENTS, THE UTILITY TIE-INS/CONNECTIONS SHALL BE COORDINATED. THIS SHALL BE COMPLETED PRIOR TO CONNECTING TO THE EXISTING UTILITY/SERVICE. WHERE CONFLICTS EXIST WITH THESE PLANS, THE ENGINEER OF RECORD SHALL BE NOTIFIED IN WRITING TO RESOLVE SAID CONFLICTS PRIOR TO CONSTRUCTION.
- 8. THE TOPS OF EXISTING MANHOLES, INLET STRUCTURES, AND SANITARY CLEANOUT TOPS SHALL BE ADJUSTED, IF REQUIRED, TO MATCH THE GRADING AND UTILITY PLAN(S). THE SANITARY CLEANOUT TOPS SHALL BE REINSTALLED IN THE SAME MANNÈR AS THE ORIGINAL CLEANOUTS.
- CONTRACTOR SHALL CONTACT THE LOCAL (I.E. TOWNSHIP, BOROUGH, ETC.) CODE ENFORCEMENT OFFICE WHEN UNDERGROUND PIPING IS BEING HYDROSTATICALLY TESTED ON SITE. APPLICANTS ARE ALSO REMINDED THAT FLUSHING OF THE UNDERGROUND PIPING SHALL BE WITNESSED BY A TOWNSHIP OFFICIAL PRIOR TO FINAL RISER CONNECTIONS, PER NFPA 13.
- 10. UNDERGROUND UTILITY MARKOUTS BY THE UTILITY COMPANIES WERE ORDERED AND THE PHYSICAL LOCATION OF THESE MARKOUTS ARE SHOWN HEREON, HOWEVER, NO CERTIFICATION IS MADE BY VCEA AS TO THE ACTUAL UNDERGROUND POSITION OF ANY UTILITIES OR TO THE COMPLETENESS OR ACCURACY OF THE UTILITY MARKOUTS
- 11. IT IS IMPERATIVE THAT UTILITY COMPANIES ARE NOTIFIED PRIOR TO ANY EXCAVATION AND/OR CONSTRUCTION. CALL 1-800-242-1776 TO ORDER UTILITY MARKOUTS BY OTHERS.

### ---- EXISTING MAJOR CONTOUR EXISTING MINOR CONTOUR

**LEGEND** 

EXISTING PROPERTY LINE ----- EXISTING ADJACENT PROPERTY LINE ---- EXISTING LEGAL RIGHT OF WAY ---- EXISTING EASEMENT ------ EXISTING ROAD CENTERLINE ---- EXISTING ROAD EXISTING CURB ---- EXISTING EDGE OF DRIVE EXISTING SIGN EXISTING TREES EXISTING UTILITY POLES — — OH — EXISTING OVERHEAD ELECTRIC — — T — EXISTING TELEPHONE — — G — EXISTING GAS MAIN

CROSSING. OTHERWISE CONTRACTOR SHALL ENCASE MAIN OR LATERAL IN CONCRETE WITH A

MINIMUM OF 10 LF ON EACH SIDE OF THE CROSSING. CLEANOUTS ARE TO BE PLACED AT

INSTALLED WITH WATERTIGHT FRAMES AND COVERS. ANY MANHOLE TO BE INSTALLED OR

6. CONTRACTOR SHALL VERIFY ALL SIZES, MATERIALS, AND CONDITION OF EXISTING PIPES, TO BE

8. ALL PROPOSED SANITARY SEWER FACILITIES WILL BE OWNED AND PERPETUALLY MAINTAINED BY

REQUIREMENTS WITH RESPECT TO THE SANITARY SEWERAGE FACILITIES SERVED BY THE LOCAL

MUNICIPAL SEWER AUTHORITY'S (SANITARY SEWER UTILITY OWNER) SHALL BE IN ACCORDANCE

WITH THE AUTHORITY'S STANDARD SPECIFICATIONS FOR CONSTRUCTION OF SANITARY SEWERS,

7. ALL EXISTING MANHOLES SHALL BE TESTED PER SANITARY UTILITY OWNER STANDARDS TO

9. ALL MATERIALS USED, AND DETAILS OF CONSTRUCTION, INSTALLATION PROCEDURES. AND

ADJUSTED OUTSIDE OF PAVED AREAS SHALL BE INSTALLED A MINIMUM OF 6" ABOVE FINISH

5. ALL SANITARY SEWER MANHOLES PROPOSED TO BE INSTALLED OR ADJUSTED SHALL BE

MAXIMUM INTERVALS OF 100-FT AND AT ALL BENDS.

ASSURE NO LEAKS EXIST WITHIN THE STRUCTURES.

THE PROPERTY OWNER IN PERPETUITY.

REPLACED OR CONNECTED TO. PRIOR TO ORDERING MATERIALS.

FORCE MAINS, PUMPING STATIONS AND WASTEWATER TREATMENT.

===== EXISTING STORM SEWER EXISTING SANITARY SEWER — — w — EXISTING WATER MAIN EXISTING WATER VALVE PROPOSED CONTOUR PROPOSED CURB ----- PROPOSED EASEMENT PROPOSED RIGHT OF WAY — — PROPOSED SETBACK LINE PROPOSED SIDEWALK PROPOSED SIGN PROPOSED STORM SEWER ----- S ------ PROPOSED SANITARY SEWER LATERAL PROPOSED WATER LATERAL

- THE CONTRACTOR, DURING THE PERFORMANCE OF ALL WORK ASSOCIATED WITH THE CONSTRUCTION OF THE PROJECT, IS RESPONSIBLE FOR COMPLIANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS, CODES AND REGULATIONS.
- 13. ANY DISCREPANCIES FOUND BETWEEN THE DRAWINGS AND SITE CONDITIONS OR ANY INCONSISTENCIES OR AMBIGUITIES IN DRAWINGS SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER, IN WRITING, WHO SHALL PROMPTLY ADDRESS SUCH INCONSISTENCIES OR AMBIGUITIES. WORK DONE BY THE CONTRACTOR AFTER HIS DISCOVERY OF SUCH DISCREPANCIES, INCONSISTENCIES OR AMBIGUITIES SHALL BE DONE AT THE CONTRACTOR'S RISK.
- 14. THE CHALFONT NEW BRITAIN JOINT SEWER AUTHORITY IS TO BE CONTACTED ONE WEEK PRIOR TO SANITARY SEWER CONSTRUCTION, AND 72 HOURS PRIOR TO EXCAVATING NEAR, AND CONNECTING INTO ANY EXISTING SANITARY SEWER.
- 15. ALL SANITARY SEWER CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THE CHALFONT NEW BRITAIN JOINT SEWER AUTHORITY (CNBSA)
- a.) THE MINIMUM ALLOWABLE LATERAL SLOPE SHALL BE 0.02 FT/FT. b.) MAGNETIC - TYPE LOCATOR TAPE SHALL BE LAID DIRECTLY OVER THE TOP OF PVC PIPE, TWO FEET (2') BELOW GRADE. c.) CONTRACTOR SHALL MAKE TESTS AS DIRECTED BY THE ENGINEER TO ASCERTAIN IF THE PIPE IS PROPERLY ALIGNED AND THE JOINTS ARE LIGHT. ENGINEER MUST WITNESS ALL TESTS OR THE CONTRACTOR WILL NEED TO RETEST IN THE PRESENCE OF THE ENGINEER AT THE SOLE COST OF THE CONTRACTOR.
- CONTRACTOR TO CONNECT NEW BUILDING UTILITY CONNECTIONS TO EXISTING WATER AND SEWER.
- 17. FINAL DESIGN OF PROPOSED WATER AND SEWER FACILITIES ARE SUBJECT TO THE REVIEW AND APPROVAL BY THE SERVICING WATER AND SEWER AUTHORITY. ALL WATER MAIN CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH NORTH WALES WATER AUTHORITY SPECIFICATIONS.
- 18. LATERALS ARE TO BE LOCATED AS SHOWN ON THE PLAN.

PROVIDED BY ARCHITECT AT TIME OF BUILDING PERMITS.

- WATER SERVICE IS TO BE LOCATED A MIN. OF 1.5 FEET ABOVE THE SANITARY LATERAL WITH A MINIMUM COVER OF 48".
- NO GROUNDING OF ELECTRICAL SERVICE TO COPPER WATER SERVICE IS PERMITTED. CABLE AND TELECOMM UTILITIES TO FOLLOW ELECTRIC FACILITIES ALIGNMENTS,
- 18" VERTICAL CLEARANCE OR 10 FEET OF HORIZONTAL CLEARANCE SHALL BE PROVIDED BETWEEN THE SANITARY SEWER AND ALL OTHER UTILITIES. OTHERWISE, CONCRETE ENCASEMENT OF THE SANITARY SEWER SHALL BE PROVIDED.
- 22. ALL DOWNSPOUT LOCATIONS TO BE OUTFITTED WITH A OVERFLOW / CLEANOUT. OVERFLOWS SHALL HAVE A SPLASHBLOCK INSTALLED AT THEIR OUTFALL LOCATION.
- 23. PROPOSED ROOF DRAIN CONNECTIONS SHOWN SCHEMATICALLY, FINAL DESIGN TO BE
- 24. MATERIALS SHOWN HEREON, SUCH AS ADS N-12 PIPE, OR APPROVED EQUAL SHALL BE UTILIZED.
- 25. REFER TO ARCHITECTURAL, MECHANICAL, ELECTRICAL AND PLUMBING PLANS FOR ADDITIONAL COORDINATION W/ IN 5' OF BUILDINGS.
- PUBLIC SEWER SERVICE WILL BE PROVIDED BY BCSWA, AND CONSTRUCTION OF ALL
- SANITARY SEWER IMPROVEMENTS SHALL COMPLY WITH AUTHORITY STANDARDS. 27. ALL BURIED GRAVITY SANITARY SEWER MAINS AND LATERALS SHALL BE CLASS 50 DUCTILE IRON PIPE OR PVC SDR-26 OR SDR-35.
- THRUST BLOCKS SHALL BE PROVIDED AT ALL BENDS AND TEES.
- 29. GENERAL CONTRACTOR (GC) TO COORDINATE LIGHT POLE LOCATIONS WITH TRADES.

# **GENERAL CONSTRUCTION NOTES:**

CONTACT: JOSEPH MURPHY
EMAIL: JMURPHY@NWWATER.COM

1. NO CERTIFICATION IS MADE BY VAN CLEEF ENGINEERING ASSOCIATES AS TO THE POSITION OF ANY UTILITIES OR TO THE COMPLETENESS OF ANY UTILITIES SHOWN. ALL CONTRACTORS WORKING ON THIS SITE SHALL COMPLY WITH THE REQUIREMENTS OF ACT 287 OF 1974 AS AMENDED BY ACT 50 OF 2017 (UNDERGROUND UTILITY LINE PROTECTION LAW). VCEA HAS REGISTERED THIS SITE WITH THE PA ONE-CALL SYSTEM ON NOVEMBER 14, 2022 (SERIAL NO. 20223183581).

THE FOLLOWING IS A LIST OF REGISTERED UTILITY USERS FOR NEW BRITAIN

COMPANY: CHALFONT NEW BRITAIN TWP JT SEWAGE AUTH ADDRESS: 1645 UPPER STATE RD DOYLESTOWN, PA. 18901 CONTACT: JOHN SCHMIDT CHALFONT, PA. 18914 EMAIL: RCRESSMAN@NEWBRITAINTOWNSHIP.ORG EMAIL: JSCHMIDT@CNBSA.ORG COMPANY: COMCAST COMPANY: PECO AN EXELON COMPANY C/O USIC ADDRESS: 190 SHOEMAKER RE KING OF PRUSSIA, PA. 19406 CONTACT: MIKE KIMBERLY EMAIL: MIKE\_KIMBERLY@CABLE.COMCAST.COM EMAIL: NIKKIASIMPKINS@USICLLC.COM COMPANY: VERIZON BUSINESS FORMERLY MCI COMPANY: NORTH WALES WATER AUTHORITY ADDRESS: 7000 WESTON PKWY ADDRESS: 200 W WALNUT ST PO BOX 1339 NORTH WALES, PA. 19454 CONTACT: VICTOR WOOD

- 2. IT IS IMPERATIVE THAT UTILITY COMPANIES ARE NOTIFIED PRIOR TO ANY EXCAVATION AND/OR CONSTRUCTION. CALL 1-800-242-1776 TO ORDER UTILITY MARKOUTS BY OTHERS.
- 3. THE LOCATIONS OF EXISTING UTILITIES AS SHOWN ARE APPROXIMATE ONLY. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL PUBLIC OR PRIVATE UTILITIES WHICH LIE IN OR ADJACENT TO THE CONSTRUCTION SITE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING, AT HIS EXPENSE, ALL EXISTING UTILITIES DAMAGED DURING CONSTRUCTION.

EMAIL: VICTOR.S.WOOD@VERIZON.COM

- 4. THE CONTRACTOR, DURING THE PERFORMANCE OF ALL WORK ASSOCIATED WITH THE CONSTRUCTION OF THE PROJECT, IS RESPONSIBLE FOR COMPLIANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS, CODES AND REGULATIONS.
- 5. ANY DISCREPANCIES FOUND BETWEEN THE DRAWINGS AND SITE CONDITIONS OR ANY INCONSISTENCIES OR AMBIGUITIES IN DRAWINGS SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER, IN WRITING, WHO SHALL PROMPTLY ADDRESS SUCH INCONSISTENCIES OR AMBIGUITIES. WORK DONE BY THE CONTRACTOR AFTER HIS DISCOVERY OF SUCH DISCREPANCIES, INCONSISTENCIES OR AMBIGUITIES SHALL BE DONE AT THE CONTRACTOR'S RISK.
- 6. ALL MATERIAL USED FOR FILL OR BACKFILL SHALL BE FREE OF WOOD, ROOTS, ROCKS, BOULDERS OR ANY OTHER NON-COMPACTIBLE SOIL TYPE MATERIAL. UNSATISFACTORY MATERIALS ALSO INCLUDE MAN-MADE FILLS AND REFUSE DEBRIS DERIVED FROM ANY SOURCE.
- 7. ALL EXCAVATIONS, INCLUDING TRENCHES, SHALL BE KEPT DRY TO PROTECT THEIR
- 8. MATERIALS USED TO CONSTRUCT EMBANKMENTS FOR ANY PURPOSE, BACKFILL AROUND DRAINAGE STRUCTURES OR IN UTILITY TRENCHES OR ANY OTHER DEPRESSIONS REQUIRING FILL OR BACKFILL SHALL BE COMPACTED TO AT LEAST 95% OF MAXIMUM DENSITY AS DETERMINED BY THE STANDARD PROCTOR TEST AS SET OUT IN ASTM STANDARD D-698 UNLESS SPECIFIED ELSEWHERE WITHIN THE CONSTRUCTION PLANS OR IF SPECIFIED DIFFERENTLY BY THE TOWNSHIP ENGINEER. CONTRACTOR TO VERIFY COMPACTION REQUIREMENTS WITH TOWNSHIP ENGINEER'S OFFICE PRIOR TO EXCAVATION.
- 9. ALL CONSTRUCTION DEBRIS INCLUDING EXCESS EXCAVATED MATERIAL, SCRAP WOOD, BRICKS, BLOCKS, ETC. SHALL BE DISPOSED OF BY THE CONTRACTOR IN ACCORDANCE WITH FEDERAL, STATE AND LOCAL REQUIREMENTS.
- 10. ELEVATIONS, DIMENSIONS, AND THE LOCATIONS OF LINEAR FEATURES SHALL BE FIELD VERIFIED BY THE CONTRACTOR.
- 11. MINIMUM SLOPE OF ALL GRASS AREAS SHALL NOT BE LESS THAN 2%, AND FOR PAVED AREAS, NOT LESS THAN 1%.

PER TOWNSHIP ENGINEER REVIEW LETTER DATED 05/21/2025. EV DESCRIPTION AUTH DATE

**SERIAL NO. 20223183581** 

Before You Dig Anywhere PENNSYLVAN

now what's below. STOP! CALL 1-800-242-1776 Call before you dig. PA Law requires 3 working days notice PA ONE CALL SYSTEM, INC.

**GRAPHIC SCALE** 10 20

1 INCH = 20 FT

PLAN NOTATION NLY THOSE PLANS WHICH CONTAIN A DIGI MPRESSED, OR COLORIZED INK SEAL OF TH RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERE ALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARE OR THE OWNER DESIGNATED HEREON. AN MODIFICATION, REVISION, DUPLICATION OR WITHOUT THE WRITTEN CONSENT OF VAN CL NGINEERING ASSOCIATES IS PROHIBITE RELIANCE ON THIS PLAN FOR ANY PURPO THER THAN THAT WHICH IS INTENDED SHALL AT THE SOLE DISCRETION AND LIABILITY OF APPLICABLE PARTY.

EAN DAVID McGRANAHAN

NSYLVANIA PROFESSIONA

NEER NUMBER 085090

VAN CLEEF ENGINEERING ASSOCIATES, LLC 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM

PHONE (215) 345-1876

UTILITY PLAN

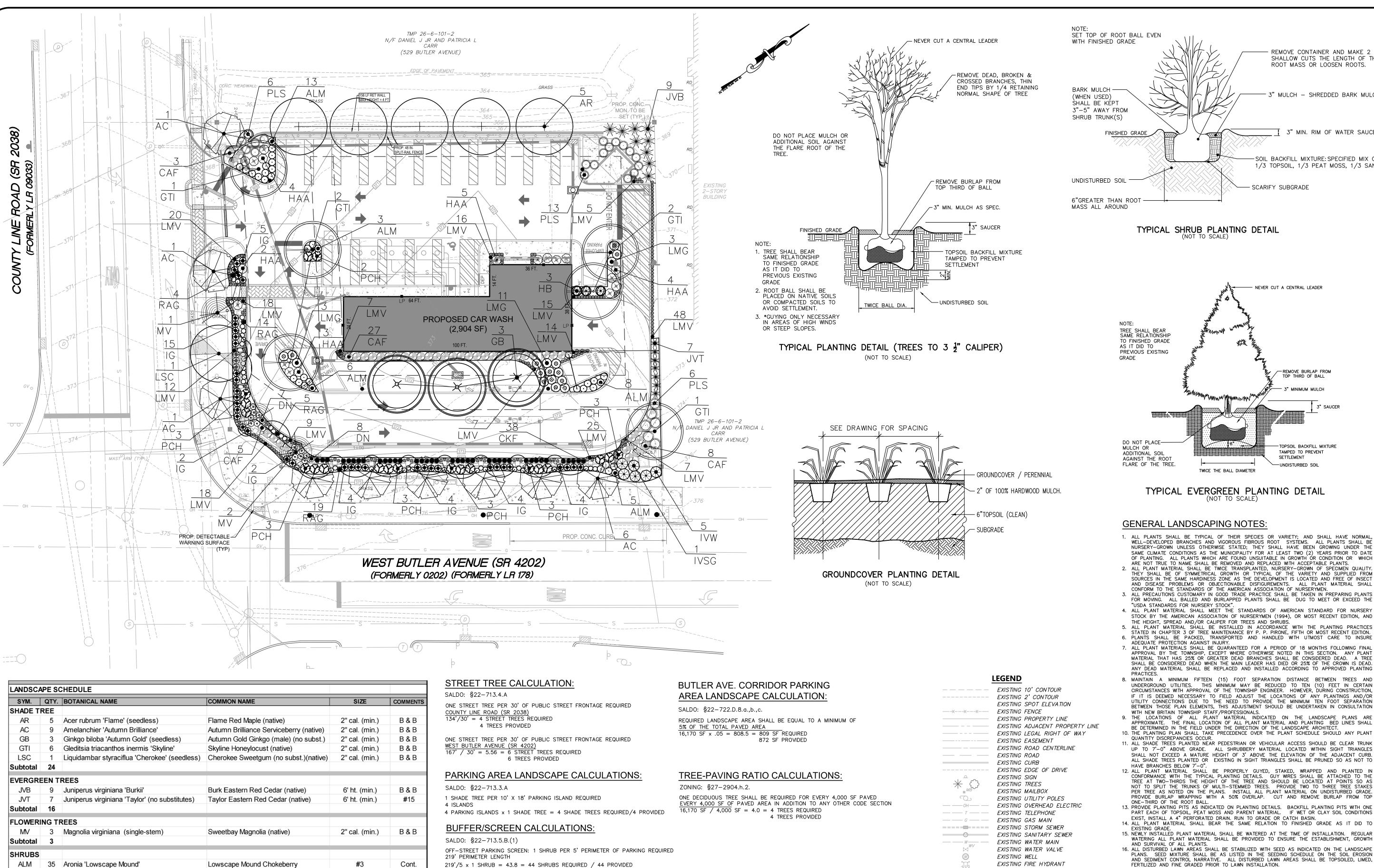


RJN / ENP RAWN BY HECKED BY **B NUMBER** 

CARWASH

TMP'S 26-006-101-001

**NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 



CAF

DN

HAA

HB

**IVW** 

**IVSG** 

LMG

PCH

PLS

Subtotal 224

**PERENNIALS** 

Subtotal 302

TOTAL 569

44 Cornus sericea 'Arctic Fire' (native)

18 Hydrangea arborescens 'Annabelle'

20 Prunus laurocerasus 'Chestnut Hill'

26 Prunus laurocerasus 'Schipkaensis'

43 Rudbeckia fulgida 'American Gold Rush'

llex verticillata 'Southern Gentleman' (male)

40 llex glabra 'Compacta' (native)

5 llex verticillata 'Winter Red'

17 Lonicera pliata 'Moss Green'

15 Deutzia gracilis 'Nikko'

Hydrangea 'Bobo'

CKF 38 Calamagrostis 'Karl Foerster'

221 Liriope muscari 'Variegata'

Arctic Fire Redosier/Redtwig Dogwood

Annabelle Smooth Hydrangea

Winter Red Winterberry

Compact Inkberry Holly (evergreen)

Moss Green Prostrate Honeysuckle

Chestnut Hill Cherry Laurel (evergreen)

Southern Gentleman Winterberry

Schip / Skip Laurel (evergreen)

Karl Foerster Feather Reed Grass

Amer. Gold Rush Black-Eyed Susan

Variegated Liriope (evergreen)

Nikko Deutzia

Bobo Hydrangea

30" ht. (min.)

#3

30" ht. (min.)

2 Gal.

1 Gal.

1 Gal.

#2

Cont.

24" o.c.

18" o.c.

24" o.c.

APPLICATION RATE

FERTILIZER APPL. RATE

FERTILIZER TYPE

LIMING RATE

MULCH TYPE

MULCH RATE

PERMANENT:

% PURE LIVE SEED

FERTILIZER TYPE

ANCHOR MATERIAL

ANCHORING METHOD

RATE OF ANCHOR MATERIAL APPL. N/A

LIMING RATE

MULCH TYPE MULCH RATE

APPLICATION RATE

FERTILIZER APPL. RATE

SPECIES

GRASS SEEDING SPECIFICATIONS:

ANNUAL RYEGRASS

COMMERCIAL 10-20-10

KENTUCKY BLUEGRASS / CREEPING

41 / 20 / 14 / 20 LB/ACRE

COMMERCIAL 10-20-10

FESCUE / PERENNIAL RYEGRASS / CHEWING'S

APRIL 15 - MAY 30, AUGUST 15 - OCTOBER 15

87.0 LB/ACRE

1,100 LB/ACRE

2 T/ACRE

3 T/ACRE

RED FESCUE

1,100 LB/ACRE

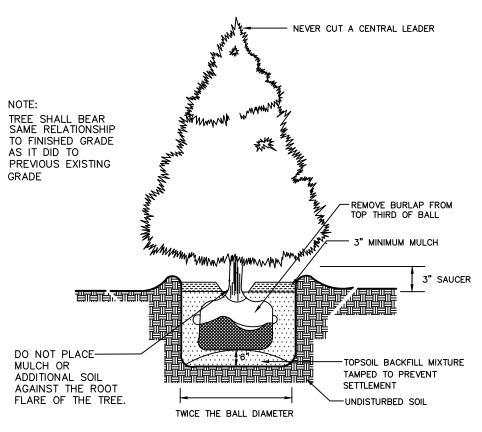
HAY OR STRAW

3 T/ACRE

STRAW

SET TOP OF ROOT BALL EVEN WITH FINISHED GRADE REMOVE CONTAINER AND MAKE 2 SHALLOW CUTS THE LENGTH OF THE ROOT MASS OR LOOSEN ROOTS. 3" MULCH - SHREDDED BARK MULCH 3" MIN. RIM OF WATER SAUCER -SOIL BACKFILL MIXTURE: SPECIFIED MIX OR 1/3 TOPSOIL, 1/3 PEAT MOSS, 1/3 SAND - SCARIFY SUBGRADE 6"GREATER THAN ROOT

TYPICAL SHRUB PLANTING DETAIL



TYPICAL EVERGREEN PLANTING DETAIL

# **GENERAL LANDSCAPING NOTES**

- ALL PLANTS SHALL BE TYPICAL OF THEIR SPECIES OR VARIETY; AND SHALL HAVE NORMAL WELL-DEVELOPED BRANCHES AND VIGOROUS FIBROUS ROOT SYSTEMS. ALL PLANTS SHALL B NURSERY-GROWN UNLESS OTHERWISE STATED; THEY SHALL HAVE BEEN GROWING UNDER TH SAME CLIMATE CONDITIONS AS THE MUNICIPALITY FOR AT LEAST TWO (2) YEARS PRIOR TO DATE OF PLANTING. ALL PLANTS WHICH ARE FOUND UNSUITABLE IN GROWTH OR CONDITION OR WHICH ARE NOT TRUE TO NAME SHALL BE REMOVED AND REPLACED WITH ACCEPTABLE PLANTS. 2. ALL PLANT MATERIAL SHALL BE TWICE TRANSPLANTED, NURSERY-GROWN OF SPECIMEN QUALITY. THEY SHALL BE OF SYMMETRICAL GROWTH OR TYPICAL OF THE VARIETY AND SUPPLIED FROM SOURCES IN THE SAME HARDINESS ZONE AS THE DEVELOPMENT IS LOCATED AND FREE OF INSECT AND DISEASE PROBLEMS OR OBJECTIONABLE DISFIGUREMENTS. ALL PLANT MATERIAL SHALL CONFORM TO THE STANDARDS OF THE AMERICAN ASSOCIATION OF NURSERYMEN.
- 3. ALL PRECAUTIONS CUSTOMARY IN GOOD TRADE PRACTICE SHALL BE TAKEN IN PREPARING PLANTS FOR MOVING. ALL BALLED AND BURLAPPED PLANTS SHALL BE DUG TO MEET OR EXCEED THE "USDA STANDARDS FOR NURSERY STOCK" 4. ALL PLANT MATERIAL SHALL MEET THE STANDARDS OF AMERICAN STANDARD FOR NURSERY STOCK BY THE AMERICAN ASSOCIATION OF NURSERYMEN (1994), OR MOST RECENT EDITION, AND
- THE HEIGHT, SPREAD AND/OR CALIPER FOR TREES AND SHRUBS. 5. ALL PLANT MATERIAL SHALL BE INSTALLED IN ACCORDANCE WITH THE PLANTING PRACTICES STATED IN CHAPTER 3 OF TREE MAINTENANCE BY P. P. PIRONE, FIFTH OR MOST RECENT EDITION. 6. PLANTS SHALL BE PACKED, TRANSPORTED AND HANDLED WITH UTMOST CARE TO INSURE ADEQUATE PROTECTION AGAINST INJURY. 7. ALL PLANT MATERIALS SHALL BE QUARANTEED FOR A PERIOD OF 18 MONTHS FOLLOWING FINAL APPROVAL BY THE TOWNSHIP, EXCEPT WHERE OTHERWISE NOTED IN THIS SECTION. ANY PLANT MATERIAL THAT HAS 25% OR GREATER DEAD BRANCHES SHALL BE CONSIDERED DEAD. A TREE SHALL BE CONSIDERED DEAD WHEN THE MAIN LEADER HAS DIED OR 25% OF THE CROWN IS DEAD.
- 8. MAINTAIN A MINIMUM FIFTEEN (15) FOOT SEPARATION DISTANCE BETWEEN TREES AND UNDERGROUND UTILITIES. THIS MINIMUM MAY BE REDUCED TO TEN (10) FEET IN CERTAIN CIRCUMSTANCES WITH APPROVAL OF THE TOWNSHIP ENGINEER. HOWEVER, DURING CONSTRUCTION, IF IT IS DEEMED NECESSARY TO FIELD ADJUST THE LOCATIONS OF ANY PLANTINGS AND/OR UTILITY CONNECTIONS DUE TO THE NEED TO PROVIDE THE MINIMUM TEN FOOT SEPARATION BETWEEN THOSE PLAN ELEMENTS, THIS ADJUSTMENT SHOULD BE UNDERTAKEN IN CONSULTATION WITH NEW BRITAIN TOWNSHIP STAFF/PROFESSIONALS.
- 9. THE LOCATIONS OF ALL PLANT MATERIAL INDICATED ON THE LANDSCAPE PLANS ARE APPROXIMATE. THE FINAL LOCATION OF ALL PLANT MATERIAL AND PLANTING BED LINES SHALL BE DETERMINED IN THE FIELD UNDER THE DIRECTION OF THE LANDSCAPE ARCHITECT. 10. THE PLANTING PLAN SHALL TAKE PRECEDENCE OVER THE PLANT SCHEDULE SHOULD ANY PLANT 11. ALL SHADE TREES PLANTED NEAR PEDESTRIAN OR VEHICULAR ACCESS SHOULD BE CLEAR TRUNK UP TO 7'-0" ABOVE GRADE. ALL SHRUBBERY MATERIAL LOCATED WITHIN SIGHT TRIANGLES SHALL NOT EXCEED A MATURE HEIGHT OF 3' ABOVE THE ELEVATION OF THE ADJACENT CURB. ALL SHADE TREES PLANTED OR EXISTING IN SIGHT TRIANGLES SHALL BE PRUNED SO AS NOT TO HAVE BRANCHES BELOW 7'-0".
- 12. ALL PLANT MATERIAL SHALL BE PROPERLY GUYED, STAKED, WRAPPED AND PLANTED IN CONFORMANCE WITH THE TYPICAL PLANTING DETAILS. GUY WIRES SHALL BE ATTACHED TO THE TREE AT TWO—THIRDS THE HEIGHT OF THE TREE AND SHOULD BE LOCATED AT POINTS SO AS NOT TO SPLIT THE TRUNKS OF MULTI-STEMMED TREES. PROVIDE TWO TO THREE TREE STAKES PER TREE AS NOTED ON THE PLANS. INSTALL ALL PLANT MATERIAL ON UNDISTURBED GRADE. PROVIDE BURLAP WRAPPING WITH A 50% OVERLAP. CUT AND REMOVE BURLAP FROM TOP ONE-THIRD OF THE ROOT BALL
- 13. PROVIDE PLANTING PITS AS INDICATED ON PLANTING DETAILS. BACKFILL PLANTING PITS WITH ONE PART EACH OF TOPSOIL, PEAT MOSS AND PARENT MATERIAL. IF WET OR CLAY SOIL CONDITIONS EXIST, INSTALL A 4" PERFORATED DRAIN. RUN TO GRADE OR CATCH BASIN
- 14. ALL PLANT MATERIAL SHALL BEAR THE SAME RELATION TO FINISHED GRADE AS IT DID TO EXISTING GRADE. 15. NEWLY INSTALLED PLANT MATERIAL SHALL BE WATERED AT THE TIME OF INSTALLATION. REGULAR WATERING ALL PLANT MATERIAL SHALL BE PROVIDED TO ENSURE THE ESTABLISHMENT, GROWTH AND SURVIVAL OF ALL PLANTS.
- 16. ALL DISTURBED LAWN AREAS SHALL BE STABILIZED WITH SEED AS INDICATED ON THE LANDSCAPE PLANS. SEED MIXTURE SHALL BE AS LISTED IN THE SEEDING SCHEDULE ON THE SOIL EROSION AND SEDIMENT CONTROL NARRATIVE. ALL DISTURBED LAWN AREAS SHALL BE TOPSOILED, LIMED, FERTILIZED AND FINE GRADED PRIOR TO LAWN INSTALLATION. 17. ALL PLANTING BEDS SHALL RECEIVE MINIMUM 3" OF SHREDDED HARDWOOD BARK.
  18. ALL SHRUB MASSES SHALL BE PLANTED IN CONTINUOUS MULCHED BEDS.
- 19. ALL EXISTING TREES AND SHRUBS TO BE PRESERVED ON SITE SHALL BE PROTECTED AGAINST CONSTRUCTION DAMAGE BY SNOW FENCING. ALL FENCING SHALL BE PLACED A MINIMUM OF ONE (1) FOOT OUTSIDE THE INDIVIDUAL TREE DRIPLINE, OR AT THE SPECIFIC LOCATION DETERMINED APPROPRIATE BY THE LANDSCAPE ARCHITECT. ALL TREES TO REMAIN SHALL BE IDENTIFIED IN THE FIELD PRIOR TO COMMENCEMENT OF CONSTRUCTION, GRADING OR CLEARING. ALL EXISTING VEGETATION BEING PRESERVED AND LOCATED AT THE EDGE OF THE NEW TREELINE SHALL BE PRUNED AND TRIMMED TO REMOVE DEAD OR DAMAGED BRANCHES.

  20. THE QUANTITIES SHOWN ON THE PLAN SHALL HAVE PRECEDENCE OVER THE PLANT SCHEDULE IN THE EVENT OF ANY QUANTITY DISCREPANCIES.
- 21. A CLEAR-SIGHT TRIANGLE OF TEN FEET (10') MEASURED FROM THE POINT OF INTERSECTION OF THE STREET LINE AND THE EDGE OF THE ACCESSWAY SHALL BE MAINTAINED, WITHIN WHICH VEGETATION AND OTHER VISUAL OBSTRUCTIONS SHALL BE LIMITED TO A HEIGHT OF NOT MORE THAN TWO FEET (2') ABOVE THE STREET GRADE.

PROPOSED WATER LATERAL PROPOSED LIGHT

- - - -

PROPOSED LANDSCAPING

PROPOSED SANITARY SEWER LATERAL

PROPOSED CONTOUR

PROPOSED EASEMENT

PROPOSED SIDEWALK

PROPOSED SIGN

PROPOSED STORM SEWER

PROPOSED RIGHT OF WAY

PROPOSED SETBACK LINE

PROPOSED CURB

PENNSYLVANIA REGISTERED

1 INCH = 20 FT PLAN NOTATION ONLY THOSE PLANS WHICH CONTAIN A DIGIT MPRESSED, OR COLORIZED INK SEAL OF TH RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERE ALID. THIS PLAN HAS BEEN SPECIFICALLY PREPART OR THE OWNER DESIGNATED HEREON. AN MODIFICATION, REVISION, DUPLICATION OR WITHOUT THE WRITTEN CONSENT OF VAN CL NGINEERING ASSOCIATES IS PROHIBITE RELIANCE ON THIS PLAN FOR ANY PURPO THER THAN THAT WHICH IS INTENDED SHALL AT THE SOLE DISCRETION AND LIABILITY OF APPLICABLE PARTY. EAN DAVID McGRANAHAN INSYLVANIA PROFESSIONAL NEER NUMBER 085090

PER TOWNSHIP ENGINEER REVIEW LETTER

**SERIAL NO. 20223183581** 

now what's below. STOPI CALL 1-800-242-1770

**GRAPHIC SCALE** 

Call before you dig. PA Law requires 3 working days noti

10 20

Before You Dig Anywhere

PA ONE CALL SYSTEM, INC.

AUTH DATE

DATED 05/21/2025.

V DESCRIPTION

HECKED BY OB NUMBER

DESIGNED BY

RAWN BY

TMP'S 26-006-101-001

7 of 18

VAN CLEEF ENGINEERING ASSOCIATES, LLC

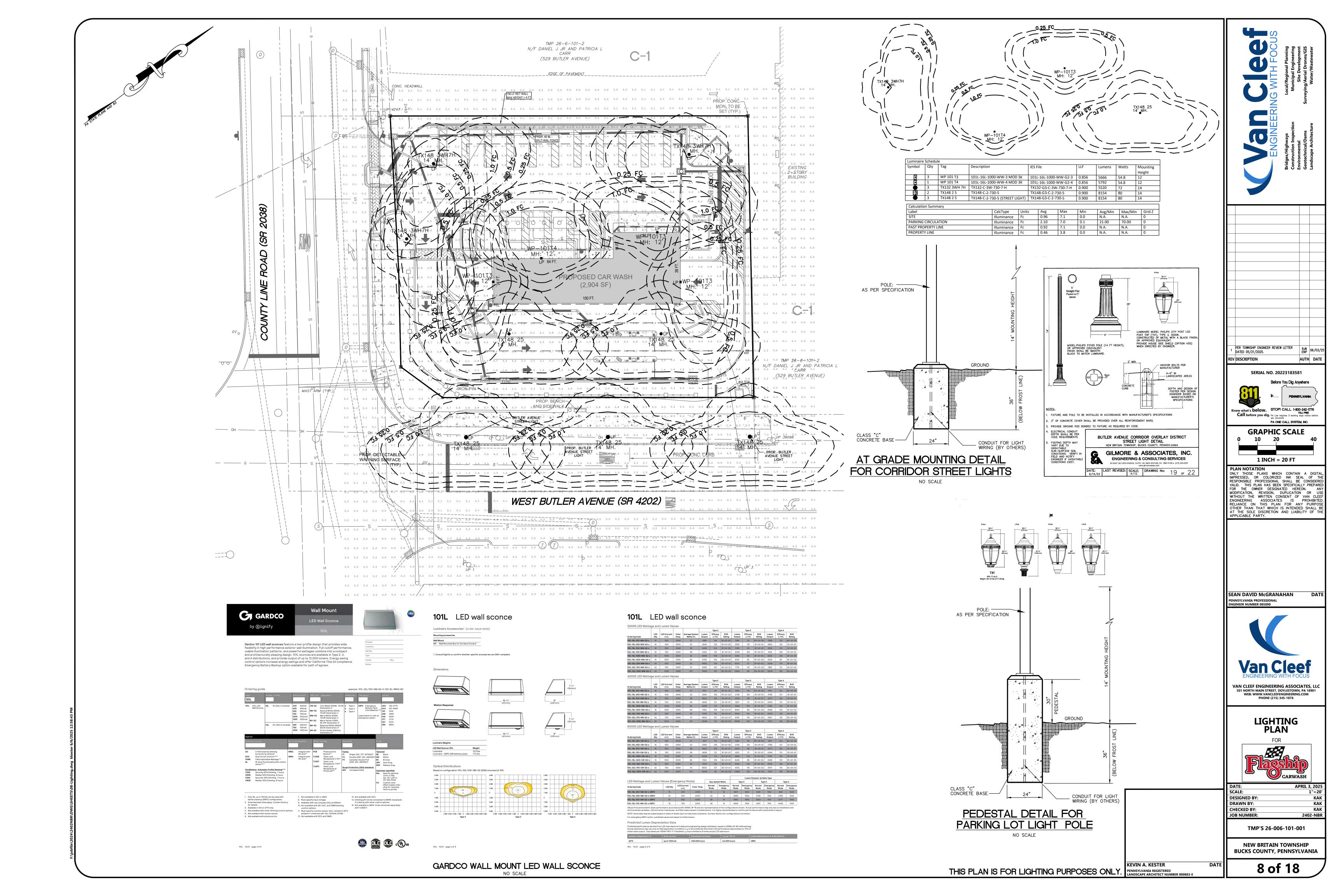
501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM

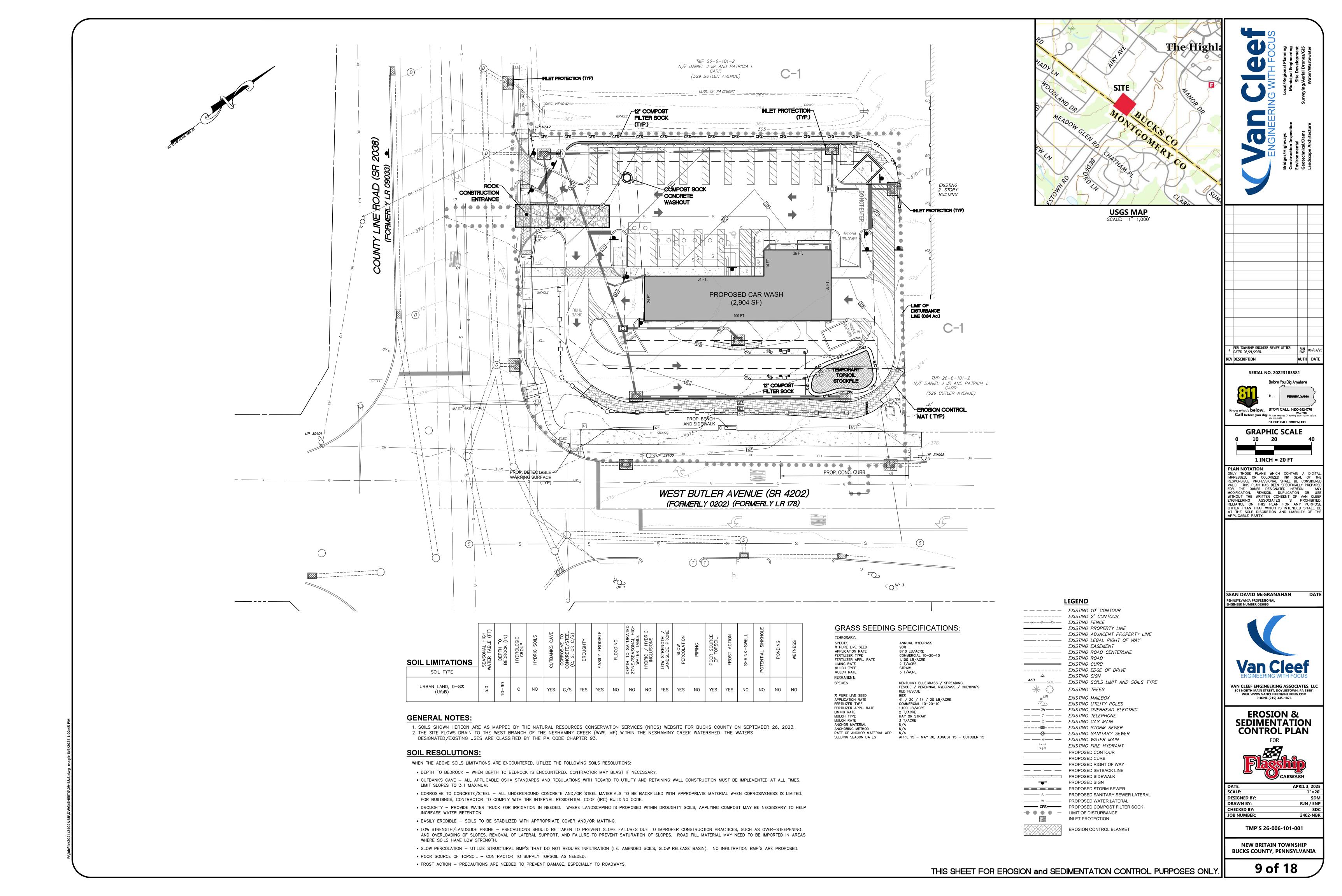
PHONE (215) 345-1876

LANDSCAPE

**PLAN** 

**NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 





# GENERAL EROSION & SEDIMENT CONTROL NOTES 1 ALL FARTH DISTURBANCES INCLUDING CLEARING AND CRUBRING AS

- 1. ALL EARTH DISTURBANCES, INCLUDING CLEARING AND GRUBBING AS WELL AS CUTS AND FILLS SHALL BE DONE IN ACCORDANCE WITH THE APPROVED E&S PLAN. A COPY OF THE APPROVED DRAWINGS (STAMPED, SIGNED AND DATED BY THE REVIEWING AGENCY) MUST BE AVAILABLE AT THE PROJECT SITE AT ALL TIMES. THE REVIEWING AGENCY SHALL BE NOTIFIED OF ANY CHANGES TO THE APPROVED PLAN PRIOR TO IMPLEMENTATION OF THOSE CHANGES. THE REVIEWING AGENCY MAY REQUIRE A WRITTEN SUBMITTAL OF THOSE CHANGES FOR REVIEW AND APPROVAL AT ITS DISCRETION.
- 2. AT LEAST 7 DAYS PRIOR TO STARTING ANY EARTH DISTURBANCE ACTIVITIES, INCLUDING CLEARING AND GRUBBING, THE OWNER AND/OR OPERATOR SHALL INVITE ALL CONTRACTORS, THE LANDOWNER, APPROPRIATE MUNICIPAL OFFICIALS, THE E&S PLAN PREPARER, THE PCSM PLAN PREPARER, THE LICENSED PROFESSIONAL RESPONSIBLE FOR OVERSIGHT OF CRITICAL STAGES OF IMPLEMENTATION OF THE PCSM PLAN, AND A REPRESENTATIVE FROM THE LOCAL CONSERVATION DISTRICT TO AN ON-SITE PRECONSTRUCTION MEETING.
- 3. AT LEAST 3 DAYS PRIOR TO STARTING ANY EARTH DISTURBANCE ACTIVITIES, OR EXPANDING INTO AN AREA PREVIOUSLY UNMARKED, THE PENNSYLVANIA ONE CALL SYSTEM INC. SHALL BE NOTIFIED AT 1-800-242-1776 FOR THE LOCATION OF EXISTING UNDERGROUND UTILITIES.
- 4. ALL EARTH DISTURBANCE ACTIVITIES SHALL PROCEED IN ACCORDANCE WITH THE SEQUENCE PROVIDED ON THE PLAN DRAWINGS. DEVIATION FROM
- THAT SEQUENCE MUST BE APPROVED IN WRITING FROM THE LOCAL CONSERVATION DISTRICT OR BY THE DEPARTMENT PRIOR TO IMPLEMENTATION.

  5. AREAS TO BE FILLED ARE TO BE CLEARED, GRUBBED, AND STRIPPED OF TOPSOIL TO REMOVE TREES, VEGETATION, ROOTS AND OTHER
- OBJECTIONABLE MATERIAL.

  6. CLEARING, GRUBBING, AND TOPSOIL STRIPPING SHALL BE LIMITED TO THOSE AREAS DESCRIBED IN EACH STAGE OF THE CONSTRUCTION SEQUENCE.
  GENERAL SITE CLEARING, GRUBBING AND TOPSOIL STRIPPING MAY NOT COMMENCE IN ANY STAGE OR PHASE OF THE PROJECT UNTIL THE E&S
- 7. AT NO TIME SHALL CONSTRUCTION VEHICLES BE ALLOWED TO ENTER AREAS OUTSIDE THE LIMIT OF DISTURBANCE BOUNDARIES SHOWN ON THE PLAN MAPS. THESE AREAS MUST BE CLEARLY MARKED AND FENCED OFF BEFORE CLEARING AND GRUBBING OPERATIONS BEGIN.

BMPS SPECIFIED BY THE BMP SEQUENCE FOR THAT STAGE OR PHASE HAVE BEEN INSTALLED AND ARE FUNCTIONING AS DESCRIBED IN THIS E&S

- 8. TOPSOIL REQUIRED FOR THE ESTABLISHMENT OF VEGETATION SHALL BE STOCKPILED AT THE LOCATION(S) SHOWN ON THE PLAN MAPS(S) IN THE AMOUNT NECESSARY TO COMPLETE THE FINISH GRADING OF ALL EXPOSED AREAS THAT ARE TO BE STABILIZED BY VEGETATION. EACH STOCKPILE SHALL BE PROTECTED IN THE MANNER SHOWN ON THE PLAN DRAWINGS. STOCKPILE HEIGHTS SHALL NOT EXCEED 35 FEET. STOCKPILE SLOPES SHALL BE 2H:1V OR FLATTER.
- 9. IMMEDIATELY UPON DISCOVERING UNFORESEEN CIRCUMSTANCES POSING THE POTENTIAL FOR ACCELERATED EROSION AND/OR SEDIMENT POLLUTION, THE OPERATOR SHALL IMPLEMENT APPROPRIATE BEST MANAGEMENT PRACTICES TO ELIMINATE POTENTIAL FOR EROSION AND SEDIMENT POLLUTION AND NOTIFY THE LOCAL CONSERVATION DISTRICT AND/OR THE REGIONAL OFFICE OF THE DEPARTMENT.
- 10. ALL BUILDING MATERIAL AND WASTES MUST BE REMOVED FROM THE SITE AND RECYCLED IN ACCORDANCE WITH DEP'S SOLID WASTE REGULATIONS (25 PA CODE 260.1 ET SEQ., 271.1 ET SEQ., AND 287.1 ET SEQ.), AND/OR ANY ADDITIONAL LOCAL, STATE OR FEDERAL REGULATIONS. NO BUILDING MATERIALS (USED OR UNUSED) OR WASTE MATERIALS SHALL BE BURNED, BURIED, DUMPED OR DISCHARGED AT THE SITE.
- 11. ALL OFF-SITE WASTE AND BORROW AREAS MUST HAVE AN E&S PLAN APPROVED BY THE LOCAL CONSERVATION DISTRICT OR THE DEPARTMENT FULLY IMPLEMENTED PRIOR TO BEING ACTIVATED.
- 12. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT ANY MATERIAL BROUGHT ON SITE IS CLEAN FILL. FORM FP-001 MUST BE RETAINED BY THE PROPERTY OWNER FOR ANY FILL MATERIAL AFFECTED BY A SPILL OR RELEASE OF A REGULATED SUBSTANCE BUT QUALIFYING AS CLEAN FILL DUE TO ANALYTICAL TESTING.
- 13. ALL PUMPING OF WATER FROM ANY WORK AREA SHALL BE DONE ACCORDING TO THE PROCEDURE DESCRIBED IN THIS PLAN, OVER UNDISTURBED VEGETATED AREAS.
- 14. UNTIL THE SITE IS STABILIZED, ALL EROSION AND SEDIMENT BMPS SHALL BE MAINTAINED PROPERLY. MAINTENANCE SHALL INCLUDE INSPECTIONS OF ALL EROSION AND SEDIMENT BMPS AFTER EACH RUNOFF EVENT AND ON A WEEKLY BASIS. ALL PREVENTATIVE AND REMEDIAL MAINTENANCE WORK, INCLUDING CLEAN OUT, REPAIR, REPLACEMENT, REGRADING, RESEEDING, REMULCHING AND RENETTING MUST BE PERFORMED IMMEDIATELY. IF THE E&S BMPS FAIL TO PERFORM AS EXPECTED, REPLACEMENT BMPS, OR MODIFICATIONS OF THOSE INSTALLED WILL BE REQUIRED.
- 15. A LOG SHOWING DATES THAT E&S BMPS WERE INSPECTED AS WELL AS ANY DEFICIENCIES FOUND AND THE DATE THEY WERE CORRECTED SHALL BE MAINTAINED ON THE SITE AND BE MADE AVAILABLE TO REGULATORY AGENCY OFFICIALS AT THE TIME OF INSPECTION. WRITTEN DOCUMENTATION OF INSPECTION AND REPLACEMENT OF BMP'S BY CONTRACTOR.
- 16. SEDIMENT TRACKED ONTO ANY PUBLIC ROADWAY OR SIDEWALK SHALL BE IMMEDIATELY RETURNED TO THE CONSTRUCTION SITE AND DISPOSED IN THE MANNER DESCRIBED IN THIS PLAN. IN NO CASE SHALL THE SEDIMENT BE WASHED, SHOVELED, OR SWEPT INTO ANY ROADSIDE DITCH, STORM SEWER, OR SURFACE WATER.
- 17. ALL SEDIMENT REMOVED FROM BMPS SHALL BE DISPOSED OF IN THE MANNER DESCRIBED ON THE PLAN DRAWINGS.
- 18. AREAS WHICH ARE TO BE TOPSOILED SHALL BE SCARIFIED TO A MINIMUM DEPTH OF 8" OR TO THE DEPTH ENCOUNTERED WITHIN THE SITE PRIOR TO PLACEMENT OF TOPSOIL. AREAS TO BE VEGETATED SHALL HAVE A MINIMUM 8" OF TOPSOIL (OR TO THE DEPTH ENCOUNTERED WITHIN THE SITE, WHICHEVER IS GREATER) IN PLACE PRIOR TO SEEDING AND MULCHING.
- 19. ALL FILLS SHALL BE COMPACTED AS REQUIRED TO REDUCE EROSION, SLIPPAGE, SETTLEMENT, SUBSIDENCE OR OTHER RELATED PROBLEMS. FILL INTENDED TO SUPPORT BUILDINGS, STRUCTURES AND CONDUITS, ETC. SHALL BE COMPACTED IN ACCORDANCE WITH LOCAL REQUIREMENTS OR CODES.
- 20. ALL EARTHEN FILLS SHALL BE PLACED IN COMPACTED LAYERS NOT TO EXCEED 9 INCHES IN THICKNESS.
- 21. FILL MATERIALS SHALL BE FREE OF FROZEN PARTICLES, BRUSH, ROOTS, SOD, OR OTHER FOREIGN OR OBJECTIONABLE MATERIALS THAT WOULD INTERFERE WITH OR PREVENT CONSTRUCTION OF SATISFACTORY FILLS.
- 22. FROZEN MATERIALS OR SOFT, MUCKY, OR HIGHLY COMPRESSIBLE MATERIALS SHALL NOT BE INCORPORATED INTO FILLS.
- 23. FILL SHALL NOT BE PLACED ON SATURATED OR FROZEN SURFACES.
- 24. SEEPS OR SPRINGS ENCOUNTERED DURING CONSTRUCTION SHALL BE HANDLED IN ACCORDANCE WITH THE STANDARD AND SPECIFICATION FOR SUBSURFACE DRAIN OR OTHER APPROVED METHOD.
- 25. ALL GRADED AREAS SHALL BE PERMANENTLY STABILIZED IMMEDIATELY UPON REACHING FINISHED GRADE. CUT SLOPES IN COMPETENT BEDROCK AND ROCK FILLS NEED NOT BE VEGETATED. SEEDED AREAS WITHIN 50 FEET OF A SURFACE WATER, OR AS OTHERWISE SHOWN ON THE PLAN DRAWINGS, SHALL BE BLANKETED ACCORDING TO THE STANDARDS OF THIS PLAN.
- 26. IMMEDIATELY AFTER EARTH DISTURBANCE ACTIVITIES CEASE IN ANY AREA OR SUBAREA OF THE PROJECT, THE OPERATOR SHALL STABILIZE ALL DISTURBED AREAS. DURING NON-GERMINATING MONTHS, MULCH OR PROTECTIVE BLANKETING SHALL BE APPLIED AS DESCRIBED IN THE PLAN. AREAS NOT AT FINISHED GRADE, WHICH WILL BE REACTIVATED WITHIN 1 YEAR, MAY BE STABILIZED IN ACCORDANCE WITH THE TEMPORARY STABILIZATION SPECIFICATIONS. THOSE AREAS WHICH WILL NOT BE REACTIVATED WITHIN 1 YEAR SHALL BE STABILIZED IN ACCORDANCE WITH THE PERMANENT STABILIZATION SPECIFICATIONS.
- 27. PERMANENT STABILIZATION IS DEFINED AS A MINIMUM UNIFORM, PERENNIAL 70% VEGETATIVE COVER OR OTHER PERMANENT NON-VEGETATIVE COVER WITH A DENSITY SUFFICIENT TO RESIST ACCELERATED EROSION. CUT AND FILL SLOPES SHALL BE CAPABLE OF RESISTING FAILURE DUE TO SLUMPING, SLIDING, OR OTHER MOVEMENTS.
- 28. E&S BMPS SHALL REMAIN FUNCTIONAL AS SUCH UNTIL ALL AREAS TRIBUTARY TO THEM ARE PERMANENTLY STABILIZED OR UNTIL THEY ARE REPLACED BY ANOTHER BMP APPROVED BY THE LOCAL CONSERVATION DISTRICT OR THE DEPARTMENT.
- 29. UPON COMPLETION OF ALL EARTH DISTURBANCE ACTIVITIES AND PERMANENT STABILIZATION OF ALL DISTURBED AREAS, THE OWNER AND/OR OPERATOR SHALL CONTACT THE LOCAL CONSERVATION DISTRICT FOR AN INSPECTION PRIOR TO REMOVAL/CONVERSION OF THE E&S BMPS.
- 30. AFTER FINAL SITE STABILIZATION HAS BEEN ACHIEVED, TEMPORARY EROSION AND SEDIMENT BMPS MUST BE REMOVED OR CONVERTED TO PERMANENT POST CONSTRUCTION STORMWATER MANAGEMENT BMPS. AREAS DISTURBED DURING REMOVAL OR CONVERSION OF THE BMPS SHALL BE STABILIZED IMMEDIATELY. IN ORDER TO ENSURE RAPID REVEGETATION OF DISTURBED AREAS, SUCH REMOVAL/CONVERSIONS ARE TO BE DONE ONLY DURING THE GERMINATING SEASON.
- 31. FAILURE TO CORRECTLY INSTALL E&S BMPS, FAILURE TO PREVENT SEDIMENT—LADEN RUNOFF FROM LEAVING THE CONSTRUCTION SITE, OR FAILURE TO TAKE IMMEDIATE CORRECTIVE ACTION TO RESOLVE FAILURE OF E&S BMPS MAY RESULT IN ADMINISTRATIVE, CIVIL, AND/OR CRIMINAL PENALTIES BEING INSTITUTED BY THE DEPARTMENT AS DEFINED IN SECTION 602 OF THE PENNSYLVANIA CLEAN STREAMS LAW. THE CLEAN STREAMS LAW PROVIDES FOR UP TO \$10,000 PER DAY IN CIVIL PENALTIES, UP TO \$10,000 IN SUMMARY CRIMINAL PENALTIES, AND UP TO \$25,000 IN MISDEMEANOR CRIMINAL PENALTIES FOR EACH VIOLATION.
- 32. FILL MATERIAL FOR EMBANKMENTS SHALL BE FREE OF ROOTS, OR OTHER WOODY VEGETATION, ORGANIC MATERIAL, LARGE STONES, AND OTHER OBJECTIONABLE MATERIALS.
- 33. STOCKPILE HEIGHTS MUST NOT EXCEED 35 FEET; STOCKPILE SLOPES MUST NOT EXCEED 2.1.
- 34. THE OPERATOR/RESPONSIBLE PERSON (O/RP) ON SITE SHALL ASSURE THAT THE APPROVED EROSION AND SEDIMENT CONTROL PLAN IS PROPERLY AND COMPLETELY IMPLEMENTED.
- 35. THE O/RP SHALL ASSURE THAT AN EROSION AND SEDIMENT CONTROL PLAN HAS BEEN PREPARED, APPROVED BY THE BUCKS COUNTY CONSERVATION DISTRICT AND IS BEING IMPLEMENTED AND MAINTAINED FOR ALL SOIL AND/OR ROCK SPOIL AND BORROW AREAS REGARDLESS OF THEIR LOCATIONS.
- 36. ALL PUMPING OF SEDIMENT-LADEN WATER SHALL BE THROUGH A SEDIMENT CONTROL BMP SUCH AS A PUMPED WATER FILTER BAG DISCHARGING OVER UNDISTURBED AREAS.
- 37. A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN MUST BE AVAILABLE ON THE PROJECT SITE AT ALL TIMES.
- 38. EROSION AND SEDIMENT BMPS MUST BE CONSTRUCTED, STABILIZED AND FUNCTIONAL BEFORE SITE DISTURBANCE BEGINS WITHIN THE TRIBUTARY AREAS OF THOSE BMPS.
- 39. DISTURBED AREAS THAT ARE AT FINISHED GRADE OR WHICH WILL NOT BE RE-DISTURBED WITHIN ONE YEAR MUST BE STABILIZED IN ACCORDANCE WITH PERMANENT VEGETATIVE STABILIZATION SPECIFICATIONS.
- 40. AN AREA SHALL BE CONSIDERED TO HAVE ACHIEVED FINAL STABILIZATION WHEN IT HAS A MINIMUM UNIFORM 70% (PERCENT) VEGETATIVE OR OTHER PERMANENT NON-VEGETATIVE COVER WITH A DENSITY SUFFICIENT TO RESIST ACCELERATED SURFACE EROSION AND SUBSURFACE CHARACTERISTICS SUFFICIENT TO RESIST SLIDING AND OTHER MOVEMENTS.
- 41. SEDIMENT REMOVED FROM BMPS SHALL BE DISPOSED OF ON-SITE IN LANDSCAPED AREAS OUTSIDE OF STEEP SLOPES, WETLANDS, FLOODPLAINS OR DRAINAGE SWALES AND IMMEDIATELY STABILIZED OR PLACED IN SOIL STOCKPILES AND STABILIZED.
- 42. BMPs SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER MEASURABLE STORM EVENTS (I.E. AT LEAST 0.25 INCH).
- 43. TEMPORARY STABILIZATION SHALL BE PROVIDED TO ALL BARE SOIL AREAS THAT WILL REMAIN UNDISTURBED FOR 4 OR MORE DAYS IN THE FORM OF 3 TON/ACRE STRAW MULCH OR EQUIVALENT.

### CONSTRUCTION SEQUENCE

- 1. AT LEAST 7 DAYS PRIOR TO START OF WORK OR EARTH DISTURBANCE ACTIVITIES, A PRE—CONSTRUCTION MEETING SHALL BE HELD INCLUDING THE OWNER, SITE CONTRACTOR(S), TOWNSHIP ENGINEER(S), COUNTY CONSERVATION DISTRICT, AND THE PLAN DESIGNER.
- 2. THE LIMITS OF DISTURBANCE SHALL BE DELINEATED WITH SURVEY STAKES OR SIMILAR, PRIOR TO ANY DISTURBANCE AND SHALL NOT BE DISTURBED DURING SITE CONSTRUCTION EXCEPT FOR TEMPORARY IMPACTS FOR MITIGATION OR RESTORATION PER THE PLAN.
- 3. INSTALL THE ROCK CONSTRUCTION ENTRANCE PER THE PLAN.
- 4. CONSTRUCTION FENCING SHALL BE INSTALLED ALONG THE SOUTH AND EASTERN PROPERTY LINES. PLACE COMPOST FILTER SOCK AND INLET PROTECTION AS DELINEATED ON THE PLAN AND DOWNSTREAM OF ALL EXCAVATED OR TOPSOIL STOCKPILING AREAS TO BE DISTURBED. INSTALL CONCRETE WASHOUT FACILITY PER THE PLAN.
- 5. DEMOLISH EXISTING BUILDING.
- 6. STRIP TOPSOIL WITHIN AREAS OF PROPOSED EARTHWORK AND STOCKPILE PER THE PLAN. CESSATION OF GRADING ACTIVITIES FOR FOUR (4) DAYS OR LONGER REQUIRES TEMPORARY SEEDING.
- 7. BEGIN CONSTRUCTION OF PROPOSED RETAINING WALL, STORM SEWERS, SEWER/GAS/WATER LATERALS, BUILDING FEATURES, DRIVEWAY AND PARKING AREAS, FLAGSHIP CARWASH SIGNAGE, SIDEWALK AND ADA FEATURES AND ANY OTHER ASSOCIATED FEATURES.
  7.1. CONSTRUCTION OF THE SANITARY SEWER LATERAL WILL REQUIRE TEMPORARY REMOVAL OF THE ROCK CONSTRUCTION ENTRANCE. UPON COMPLETION OF THE SANITARY SEWER LATERAL CONNECTION WORK, THE ROCK CONSTRUCTION ENTRANCE SHALL BE REINSTALLED IN THE

LOCATION DEPICTED ON THE PLANS. CARE SHALL BE TAKEN AS TO NOT TRACK SEDIMENT ONTO COUNTY LINE ROAD DURING THE

- CONSTRUCTION OF THE SANITARY SEWER LATERAL CONNECTION.

  8. ALL 3:1 SLOPES SHALL BE STABILIZED AND LINED WITH TEMPORARY EROSION CONTROL MATTING.
- 9. FINISH GRADE AND SPREAD TOPSOIL. SEED AND MULCH EACH AREA OF DISTURBANCE IMMEDIATELY AFTER CONSTRUCTION IS COMPLETED. AS DISTURBED AREAS WITHIN A PROJECT APPROACH FINAL GRADE, PREPARATIONS SHOULD BE MADE FOR SEEDING AND MULCHING TO BEGIN (I.E. ANTICIPATE COMPLETION DATE AND SCHEDULE SEEDING). PRIOR TO PLACING TOPSOIL, SUBSOIL SHALL BE SCARIFIED. PLANT LANDSCAPING AS DEPICTED ON THE LANDSCAPE AND LIGHTING PLAN.
- 10. REMOVE TEMPORARY CONTROL MEASURES AFTER UNIFORM EROSION RESISTANT PERENNIAL VEGETATION HAS BEEN ESTABLISHED, MINIMUM OF UNIFORM COVERAGE OR A DENSITY OF 70% ACROSS THE DISTURBED AREA, TO THE POINT WHERE THE SURFACE SOIL IS CAPABLE OF RESISTING EROSION DURING RUNOFF EVENTS AND STABILIZATION OF THE SITE IS COMPLETE TO THE SATISFACTION OF THE COUNTY CONSERVATION DISTRICT. AREAS DISTURBED DURING THE REMOVAL OF THE CONTROLS MUST BE STABILIZED. THE COUNTY CONSERVATION DISTRICT AND TOWNSHIP SHALL BE NOTIFIED PRIOR TO REMOVAL OF ANY EROSION CONTROLS.

### PROJECT CONSTRUCTION WASTES

- 1. TREES CHIPPED AND REMOVED OFFSITE FOR MULCH.
- 2. BUILDING MATERIALS SHALL BE REMOVED AND DISPOSED OF AT PROPER RECYCLING FACILITY AS PER NOTE #6 OF THE GENERAL EROSION & SEDIMENT CONTROL NOTES.
- 3. ROCK WASTE ALL ROCKS THAT ARE SUITABLE FOR LANDSCAPING USES SHALL BE USED ON SITE OR AT ANOTHER FACILITY.
- 4. SOIL WASTE ALL EXCAVATED SOIL SHALL BE USED ON SITE, AS FILL MATERIAL WHERE POSSIBLE. ANY EXCESS MATERIAL NOT USED ON SITE MAY ONLY BE DISPOSED AT A FACILITY WITH A VALID EARTH DISTURBANCE AND NPDES PERMIT APPROVED AND WERE ALL BMPS ARE PROPERLY INSTALLED. ALL SEDIMENT REMOVED DURING MAINTENANCE AND CLEANING OF EROSION AND SEDIMENTATION CONTROLS DEVICES WILL BE INCORPORATED INTO THE FINE GRADING UPSTREAM FROM A SEDIMENT CONTROL DEVICE. EXCESS MATERIAL THAT IS REMOVED FROM EROSION AND SEDIMENTATION CONTROL MEASURES AND CANNOT BE INCORPORATED INTO THE FINE GRADING STABILIZATION SHALL BE REMOVED FROM THE SITE AND MAY ONLY BE DISPOSED AT A FACILITY WITH A VALID EARTH DISTURBANCE AND NPDES PERMIT APPROVAL AND WERE ALL BMPS ARE PROPERLY INSTALLED.
- 5. ACCUMULATED SEDIMENT FROM COMPOST FILTER SOCKS TO BE RETURNED TO SITE UPSTREAM OF ALL PROPOSED BMP'S.

### MAINTENANCE OF FACILITIES

1. SILT FENCE/FILTER SOCK SHOULD BE INSPECTED AND MAINTAINED ON A DAILY BASIS.

2. UNTIL THE SITE IS STABILIZED, ALL EROSION AND SEDIMENT CONTROL DEVICES MUST BE MAINTAINED PROPERLY. MAINTENANCE MUST INCLUDE INSPECTIONS OF ALL EROSION AND SEDIMENT CONTROLS AFTER EACH RUNOFF EVENT AND ON A WEEKLY BASIS. ALL PREVENTATIVE AND REMEDIAL MAINTENANCE WORK, INCLUDING CLEAN OUT, REPAIR, REPLACEMENT, REGRADING, RESEEDING, REMULCHING, AND RENETTING MUST BE PERFORMED IMMEDIATELY. IF E&S CONTROL BMPS FAIL TO PERFORM AS EXPECTED, REPLACEMENT BMPS OR MODIFICATIONS OF INSTALLED MEASURES WILL BE

3. CONTRACTOR SHALL MAINTAIN AND MAKE AVAILABLE TO THE COUNTY CONSERVATION DISTRICT COMPLETE, WRITTEN INSPECTION LOGS OF THE ABOVE NOTED INSPECTION AND MAINTENANCE.

- 4. SEEDING, MULCHING AND FERTILIZING SHALL BE IN ACCORDANCE WITH THE SEEDING AND MULCHING SCHEDULE.
- 5. THE CONTRACTOR SHALL HAVE AVAILABLE WATER TRUCKS OR OTHER MEANS OF CONTROLLING EXCESSIVE DUST AND AIRBORNE DEBRIS.
- 6. ALL AREAS OF CONCENTRATED SURFACE DRAINAGE SHALL BE SEEDED AND MULCHED, AND PROTECTED WITH TEMPORARY TURF REINFORCEMENT MAT: NORTH AMERICAN GREEN #D575 (OR EQUAL). IF AREAS ARE TO BE SODDED, TURF REINFORCEMENT IS NOT REQUIRED.
- 7. SEEDED AREAS THAT HAVE WASHED AWAY SHALL BE FILLED AND GRADED AS NECESSARY AND THEN RESEEDED. A BURLAP OR STRAW COVER WILL BE APPLIED TO RETAIN THE SEED UNTIL IT HAS A CHANCE TO ROOT PROPERLY.

8. THE ABOVE PROCEDURE SHALL BE REPEATED AFTER EACH SIZABLE STORM UNTIL NO MORE SIGNS OF EROSION ARE EVIDENT. AT MONTHLY INTERVALS THEREAFTER, INSPECTIONS AND NECESSARY CLEANING WILL BE DONE. TRASH THAT IS REMOVED FROM ANY OF THE CONTROL DEVICES SHALL BE DISPOSED OF AT AN APPROVED DISPOSAL AREA. SILT THAT HAS ACCUMULATED SHALL BE REMOVED AND ALLOWED TO DRY AND USED AS FILL WHEREVER REQUIRED ON THE SITE.

9. IMMEDIATELY UPON DISCOVERING UNFORESEEN CIRCUMSTANCES POSING THE POTENTIAL FOR ACCELERATED EROSION AND/OR SEDIMENT POLLUTION, THE CONTRACTOR SHALL IMPLEMENT APPROPRIATE MEASURES TO ADDRESS SAID CIRCUMSTANCES.

# FILL MATERIALS

IF THE SITE WILL NEED TO HAVE FILL IMPORTED FROM AN OFF SITE LOCATION, THE RESPONSIBILITY FOR PERFORMING ENVIRONMENTAL DUE DILIGENCE AND THE DETERMINATION OF CLEAN FILL WILL RESIDE WITH THE OPERATOR.

CLEAN FILL: IS DEFINED AS UNCONTAMINATED, NON-WATER SOLUBLE, ON-DECOMPOSABLE, INERT, SOLID MATERIAL. THE TERM INCLUDES SOIL, ROCK, STONE, DREDGED MATERIAL, USED ASPHALT, AND BRICK, BLOCK OR CONCRETE FROM CONSTRUCTION AND DEMOLITION ACTIVITIES HAT IS SEPARATE FROM OTHER WASTE AND IS RECOGNIZABLE AS SUCH. THE TERM DOES NOT INCLUDE MATERIALS PLACED IN OR ON THE WATERS OF THE COMMONWEALTH UNLESS OTHERWISE AUTHORIZED. (THE ERM "USED ASPHALT" DOES NOT INCLUDE MILLED ASPHALT OR ASPHALT THAT HAS BEEN PROCESSED FOR RE-USE.)

ENVIRONMENTAL DUE DILIGENCE: INVESTIGATIVE TECHNIQUES, INCLUDING, BUT NOT LIMITED TO, VISUAL PROPERTY INSPECTIONS, ELECTRONIC DATA BASE SEARCHES, REVIEW OF PROPERTY OWNERSHIP, REVIEW OF PROPERTY USE HISTORY, SANBORN MAPS, ENVIRONMENTAL QUESTIONNAIRES, TRANSACTION SCREENS, ANALYTICAL TESTING, ENVIRONMENTAL ASSESSMENTS OR AUDITS. ANALYTICAL TESTING IS NOT A REQUIRED PART OF DUE DILIGENCE UNLESS VISUAL INSPECTION AND/OR REVIEW OF THE PAST LAND USE OF THE PROPERTY INDICATES THAT THE FILL MAY HAVE BEEN SUBJECTED TO A SPILL OR RELEASE OF REGULATED SUBSTANCE. IF THE FILL MAY HAVE BEEN AFFECTED BY A SPILL OR RELEASE OF A REGULATED SUBSTANCE, IT MUST BE TESTED TO DETERMINE IF IT QUALIFIES AS CLEAN FILL. TESTING SHOULD BE PERFORMED IN ACCORDANCE WITH APPENDIX A OF THE DEPARTMENT'S POLICY "MANAGEMENT OF FILL".

# FILL MATERIAL THAT DOES NOT QUALIFY AS CLEAN FILL IS REGULATED FILL. REGULATED FILL IS WASTE AND MUST BE MANAGED IN ACCORDANCE WITH THE DEPARTMENT'S MUNICIPAL OR RESIDUAL WASTE REGULATIONS BASED ON 25 PA. CODE CHAPTERS 287 RESIDUAL WASTE MANAGEMENT OR 271 MUNICIPAL WASTE MANAGEMENT, WHICHEVER IS APPLICABLE.

# E&S PLAN PLANNING & DESIGN NOTES

- 1. THE CONTRACTOR SHALL MINIMIZE THE EXTENT AND DURATION OF THE EARTH DISTURBANCE BY LIMITING THE CLEARING, GRUBBING, AND TOPSOIL STRIPPING TO AREAS IN EACH PHASE OF THE CONSTRUCTION SEQUENCE. GENERAL SITE CLEARING, GRUBBING AND TOPSOIL STRIPPING MAY NOT COMMENCE IN ANY STAGE OR PHASE OF THE PROJECT UNTIL THE E&S BMP'S SPECIFIED BY THE CONSTRUCTION SEQUENCE FOR THAT PHASE HAVE BEEN INSTALLED AND ARE FUNCTIONING AS DESCRIBED IN THE E&S PLAN. IMMEDIATELY AFTER EARTH DISTURBANCE ACTIVITIES CEASE, THE CONTRACTOR SHALL STABILIZE AND AREAS DISTURBED BY ANY ACTIVITIES.
- 2. TO MAXIMIZE THE PROTECTION OF EXISTING DRAINAGE FEATURES AND VEGETATION, E&S BMP'S MUST BE CONSTRUCTED, STABILIZED, AND FUNCTIONAL BEFORE SITE DISTURBANCE BEGINS WITHIN THE TRIBUTARY AREAS TO THOSE BMP'S. E&S BMP'S SHALL REMAIN FUNCTIONAL UNTIL ALL AREAS TRIBUTARY TO THEM ARE PERMANENTLY STABILIZED. AT NO TIME WILL CONSTRUCTION VEHICLES BE ALLOWED TO ENTER AREAS OUTSIDE THE LIMIT OF DISTURBANCE BOUNDARY. SEDIMENT TRACKED ONTO ANY PUBLIC ROADWAYS SHALL BE RETURNED TO THE CONSTRUCTION SITE BY THE END OF EACH WORK DAY. IN NO CASE SHALL THE SEDIMENT BE WASHED, SHOVELED, OR SWEPT INTO ANY ROADSIDE DITCH, STORM SEWER, OR SURFACE WATER. ALL GRADED AREAS SHALL BE PERMANENTLY STABILIZED IMMEDIATELY UPON REACHING FINISHED GRADE.
- 3. TO MINIMIZE SOIL COMPACTION, NO LAND CLEARING AND/OR GRADING SHALL BE PERFORMED OUTSIDE OF LIMIT OF DISTURBANCE FENCING.
  PROPER CARE SHALL BE TAKEN WITHIN UNDERGROUND INFILTRATION TRENCH TO MINIMIZE SOIL COMPACTION.
- 4. THE FOLLOWING MEASURES OR CONTROLS UTILIZED TO MINIMIZE GENERATION OF INCREASED STORMWATER RUNOFF: A ROCK CONSTRUCTION ENTRANCE, COMPOST FILTER SOCKS AROUND THE DISTURBANCE, A CONCRETE WASHOUT, EROSION CONTROL BLANKET, AND TEMPORARY/PERMANENT STABILIZATION OF ALL LOT FEATURES.
- 5. THERE ARE NO PROPOSED INFILTRATION BMP'S OUTSIDE PROPOSED GRADING AREAS.
- 6. THERE ARE NO EXISTING/PROPOSED RIPARIAN FOREST BUFFERS WITHIN THE LIMITS OF DISTURBANCE THEREFORE NO ASSOCIATED REQUIREMENTS ARE SHOWN ON THE PLANS.
- 7. THIS PROJECT IS NOT LOCATED IN A SPECIAL PROTECTION (HQ/EV) WATERSHED, THEREFORE NO ANTIDEGRADATION ANALYSIS IS REQUIRED.
- 8. THERE ARE NO NATURALLY OCCURRING GEOLOGIC FORMATIONS (KARST SOILS) THAT MAY CAUSE POLLUTION WITHIN THE PROJECT LIMIT OF DISTURBANCE; THEREFORE, MEASURES TO AVOID/MINIMIZE/OR MITIGATE ARE NOT NECESSARY.

# GRASS SEEDING SPECIFICATIONS: TEMPORARY:

ANNUAL RYEGRASS % PURE LIVE SEED APPLICATION RATE 87.0 LB/ACRE FERTILIZER TYPE COMMERCIAL 10-20-10 FERTILIZER APPL, RATE 1,100 LB/ACRE LIMING RATE 2 T/ACRE MULCH TYPE STRAW MULCH RATE 3 T/ACRE PERMANENT: KENTUCKY BLUEGRASS / SPREADING SPECIES FESCUE / PERENNIAL RYEGRASS / CHEWING'S RED FESCUE % PURE LIVE SEE APPLICATION RATE 41 / 20 / 14 / 20 LB/ACRE FERTILIZER TYPE COMMERCIAL 10-20-10 FERTILIZER APPL, RATE 1,100 LB/ACRE LIMING RATE MULCH TYPE HAY OR STRAW MULCH RATE 3 T/ACRE ANCHOR MATERIAL ANCHORING METHOD RATE OF ANCHOR MATERIAL APPL. N/A APRIL 15 - MAY 30, AUGUST 15 - OCTOBER 15 SEEDING SEASON DATES

THIS SHEET FOR EROSION and SEDIMENTATION CONTROL PURPOSES ONLY.



	DESCRIPTION	AUTH	
1	PER TOWNSHIP ENGINEER REVIEW LETTER DATED 05/21/2025.	RJN ENP	0
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SERIAL NO. 20223183581



PLAN NOTATION

ONLY THOSE PLANS WHICH CONTAIN A DIGITA
IMPRESSED, OR COLORIZED INK SEAL OF THE
RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERE
YALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARE
FOR THE OWNER DESIGNATED HEREON. AN
MODIFICATION, REVISION, DUPLICATION OR US
WITHOUT THE WRITTEN CONSENT OF VAN CLEE
ENGINEERING ASSOCIATES IS PROHIBITE
RELIANCE ON THIS PLAN FOR ANY PURPOS

THER THAN THAT WHICH IS INTENDED SHALL

THE SOLE DISCRETION AND LIABILITY OF

APPLICABLE PARTY.

SEAN DAVID McGRANAHAN
PENNSYLVANIA PROFESSIONAL

NEER NUMBER 085090



VAN CLEEF ENGINEERING ASSOCIATES, LLC 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEFFENGINEERING.COM PHONE (215) 345-1876



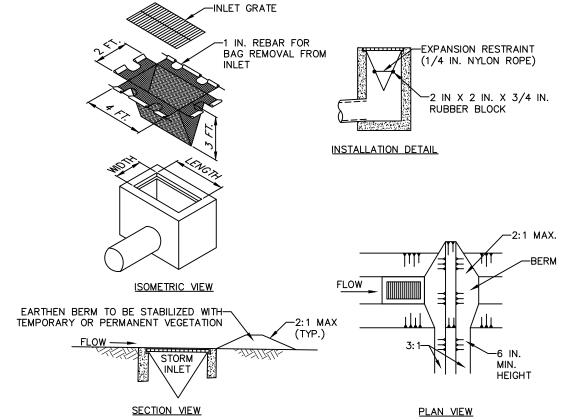
DATE: APRIL 3, 2025
SCALE: NO SCALE
DESIGNED BY: SDM
DRAWN BY: RJN / ENP
CHECKED BY: SDC

TMP'S 26-006-101-001

2402-NBF

B NUMBER

NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA



NOTES:

MAXIMUM DRAINAGE AREA = 1/2 ACRE.

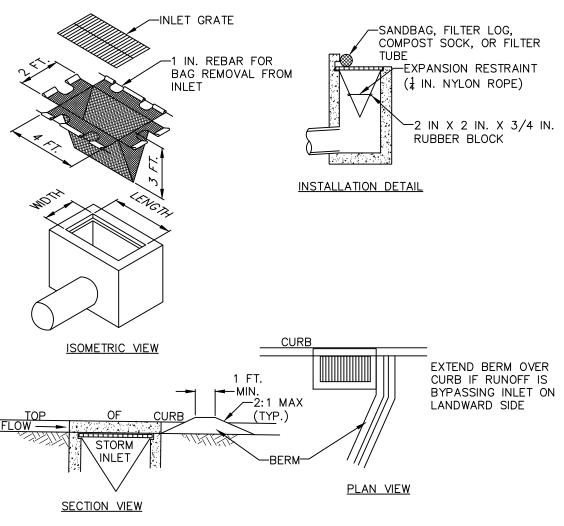
INLET PROTECTION SHALL NOT BE REQUIRED FOR INLET TRIBUTARY TO SEDIMENT BASIN OR TRAP. BERMS SHALL BE REQUIRED FOR ALL INSTALLATIONS. ROLLED EARTHEN BERM IN ROADWAY SHALL BE MAINTAINED UNTIL ROADWAY IS STONED. ROAD SUBBASE BERM ON ROADWAY SHALL BE MAINTAINED UNTIL ROADWAY IS PAVED. EARTHEN BERM IN CHANNEL SHALL BE MAINTAINED UNTIL PERMANENT STABILIZATION IS COMPLETED OR REMAIN PERMANENTLY.

AT A MINIMUM, THE FABRIC SHALL HAVE A MINIMUM GRAB TENSILE STRENGTH OF 120 LBS., A MINIMUM BURST STRENGTH OF 200 PSI, AND A MINIMUM TRAPEZOIDAL TEAR STRENGTH OF 50 LBS. FILTER BAGS SHALL BE CAPABLE OF TRAPPING ALL PARTICLES NOT PASSING A NO. 40 SIEVE.

INLET FILTER BAGS SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EACH RUNOFF EVENT. BAGS SHALL BE EMPTIED AND RINSED OR REPLACED WHEN HALF FULL OR WHEN FLOW CAPACITY HAS BEEN REDUCED SO AS TO CAUSE FLOODING OR BYPASSING OF THE INLET. DAMAGED OR CLOGGED BAGS SHALL BE REPLACED. A SUPPLY SHALL BE MAINTAINED ON SITE FOR REPLACEMENT OF BAGS. ALL NEEDED REPAIRS SHALL BE INITIATED IMMEDIATELY AFTER THE INSPECTION. DISPOSE ACCUMULATED SEDIMENT AS WELL AS ALL USED BAGS ACCORDING TO THE PLAN NOTES.

DO NOT USE ON MAJOR PAVED ROADWAYS WHERE PONDING MAY CAUSE TRAFFIC HAZARDS.

### FILTER BAG INLET PROTECTION - TYPE M INLET (NOT TO SCALE)



MAXIMUM DRAINAGE AREA = 1/2 ACRE.

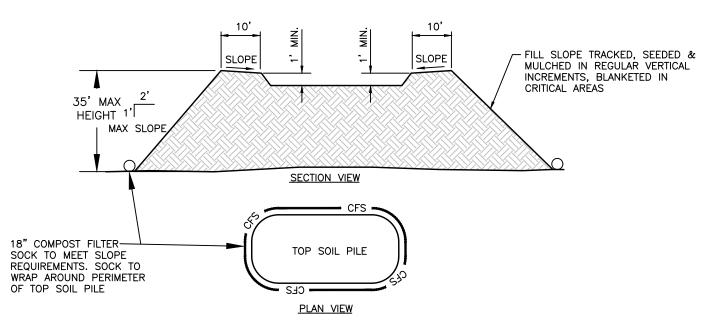
INLET PROTECTION SHALL NOT BE REQUIRED FOR INLET TRIBUTARY TO SEDIMENT BASIN OR TRAP. BERMS SHALL BE REQUIRED FOR ALL INSTALLATIONS.

ROLLED EARTHEN BERM SHALL BE MAINTAINED UNTIL ROADWAY IS STONED. ROAD SUBBASE BERM SHALL BE MAINTAINED UNTIL ROADWAY IS PAVED. SIX INCH MINIMUM HEIGHT ASPHALT BERM SHALL BE MAINTAINED UNTIL ROADWAY SURFACE RECEIVES FINAL COAT. AT A MINIMUM, THE FABRIC SHALL HAVE A MINIMUM GRAB TENSILE STRENGTH OF 120 LBS, A MINIMUM BURST STRENGTH OF 200 PSI, AND A MINIMUM TRAPEZOIDAL TEAR STRENGTH OF 50 LBS. FILTER BAGS SHALL BE CAPABLE OF TRAPPING ALL PARTICLES NOT PASSING A NO. 40

INLET FILTER BAGS SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EACH RUNOFF EVENT. BAGS SHALL BE EMPTIED AND RINSED OR REPLACED WHEN HALF FULL OR WHEN FLOW CAPACITY HAS BEEN REDUCED SO AS TO CAUSE FLOODING OR BYPASSING OF THE INLET. DAMAGED OR CLOGGED BAGS SHALL BE REPLACED. A SUPPLY SHALL BE MAINTAINED ON SITE FOR REPLACEMENT OF BAGS. ALL NEEDED REPAIRS SHALL BE INITIATED IMMEDIATELY AFTER THE INSPECTION. DISPOSE OF ACCUMULATED SEDIMENT AS WELL AS ALL USED BAGS ACCORDING TO THE PLAN NOTES.

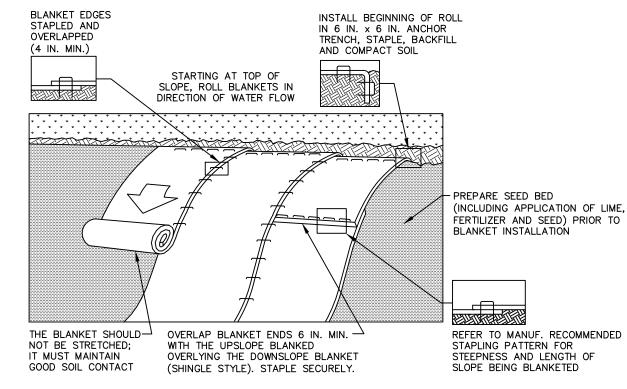
DO NOT USE ON MAJOR PAVED ROADWAYS WHERE PONDING MAY CAUSE TRAFFIC HAZARDS.

### FILTER BAG INLET PROTECTION - TYPE C INLET (NOT TO SCALE)



1. TEMPORARY BERMS TO BE PLACED, MAINTAINED, AND ADJUSTED CONTINUOUSLY UNTIL 90% VEGETATIVE GROWTH IS ESTABLISHED ON THE EXTERIOR SLOPES WITH PERMANENT STORM DRAINAGE FACILITIES FUNCTIONING. 2. BERMS MUST OUTLET TO TEMPORARY SLOPE PIPES, PERMANENT SLOPE PIPES, TEMPORARY CHANNELS, OR PERMANENT CHANNELS. TOPSOIL STOCKPILE AREA

(NOT TO SCALE)

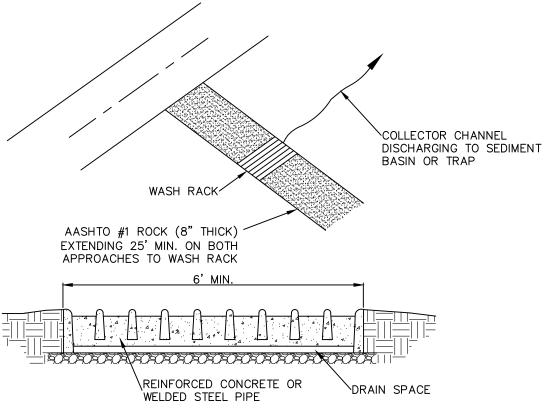


SEED AND SOIL AMENDMENTS SHALL BE APPLIED ACCORDING TO THE RATES IN THE PLAN DRAWINGS PRIOR TO PROVIDE ANCHOR TRENCH AT TOE OF SLOPE IN SIMILAR FASHION AS AT TOP OF SLOPE. SLOPE SURFACE SHALL BE FREE OF ROCKS, CLODS, STICKS, AND GRASS. BLANKET SHALL HAVE GOOD CONTINUOUS CONTACT WITH UNDERLYING SOIL THROUGHOUT ENTIRE LENGTH. LAY

THE BLANKET SHALL BE STAPLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. BLANKETED AREAS SHALL BE INSPECTED WEEKLY AND AFTER EACH RUNOFF EVENT UNTIL PERENNIAL VEGETATION IS ESTABLISHED TO A MINIMUM UNIFORM 70% COVERAGE THROUGHOUT THE BLANKETED AREA. DAMAGED OR DISPLACED BLANKETS SHALL BE RESTORED OR REPLACED WITHIN 4 CALENDAR DAYS.

BLANKET LOOSELY AND STAKE OR STAPLE TO MAINTAIN DIRECT CONTACT WITH SOIL. DO NOT STRETCH

# EROSION CONTROL BLANKET INSTALLATION DETAIL



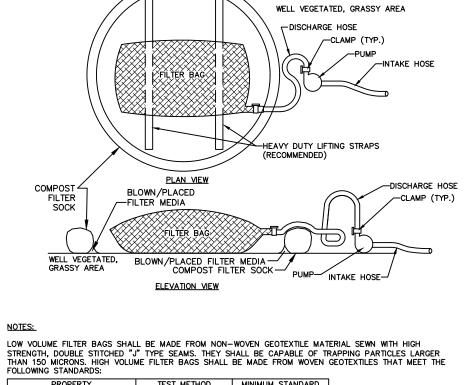
WASH RACK SHALL BE 20 FEET (MIN.) WIDE OR TOTAL WIDTH OF ACCESS. WASH RACK SHALL BE DESIGNED AND CONSTRUCTED TO ACCOMMODATE ANTICIPATED CONSTRUCTION VEHICULAR TRAFFIC.

A WATER SUPPLY SHALL BE MADE AVAILABLE TO WASH THE WHEELS OF ALL VEHICLES EXITING

MAINTENANCE: ROCK CONSTRUCTION ENTRANCE THICKNESS SHALL BE CONSTANTLY MAINTAINED TO THE SPECIFIED DIMENSIONS BY ADDING ROCK. A STOCKPILE OF ROCK MATERIAL SHALL BE MAINTAINED ON SITE FOR THIS PURPOSE, DRAIN SPACE UNDER WASH RACK SHALL BE KEPT OPEN AT ALL TIMES, DAMAGE TO THE WASH RACK SHALL BE REPAIRED PRIOR TO FURTHER USE OF THE RACK. ALL SEDIMENT DEPOSITED ON ROADWAYS SHALL BE REMOVED AND RETURNED TO THE CONSTRUCTION SITE IMMEDIATELY. WASHING THE ROADWAY OR SWEEPING THE DEPOSITS INTO ROADWAY DITCHES, SEWERS, CULVERTS, OR OTHER DRAINAGE COURSES IS

# ROCK CONSTRUCTION ACCESS WITH WASH RACK

(NOT TO SCALE)

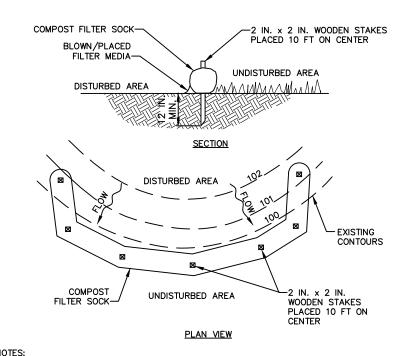


LOW VOLUME FILTER BAGS SHALL BE MADE FROM NON-WOVEN GEOTEXTILE MATERIAL SEWN WITH HIGH

PROPERTY	TEST METHOD	MINIMUM STANDARD
AVG. WIDE WIDTH STRENGTH	ASTM D-4884	60 LB/IN
GRAB TENSILE	ASTM D-4632	205 LB
PUNCTURE	ASTM D-4833	110 LB
MULLEN BURST	ASTM D-3786	350 PSI
UV RESISTANCE	ASTM D-4355	70%
AOS % RETAINED	ASTM D-4751	80 SIEVE

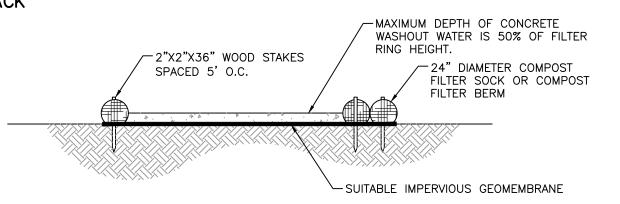
A SUITABLE MEANS OF ACCESSING THE BAG WITH MACHINERY REQUIRED FOR DISPOSAL PURPOSES SHALL BE PROVIDED. FILTER BAGS SHALL BE REPLACED WHEN THEY BECOME 1/2 FULL OF SEDIMENT. SPARE BAGS SHALL BE KEPT AVAILABLE FOR REPLACEMENT OF THOSE THAT HAVE FAILED OR ARE FILLED. BAGS SHALL BE PLACED ON STRAPS TO FACILITATE REMOVAL UNLESS BAGS COME WITH LIFTING STRAPS ALREADY ATTACHED. BAGS SHALL BE LOCATED IN WELL—VEGETATED (GRASSY) AREA, AND DISCHARGE ONTO STABLE, EROSION RESISTANT AREAS. WHERE THIS IS NOT POSSIBLE, A GEOTEXTILE UNDERLAYMENT AND FLOW PATH SHALL BE PROVIDED. BAGS MAY BE PLACED ON FILTER STONE TO INCREASE DISCHARGE CAPACITY. BAGS SHALL NOT BE PLACED ON SLOPES GREATER THAN 5%. FOR SLOPES EXCEEDING 5%, CLEAN ROCK OR OTHER NON-ERODIBLE AND NON-POLLUTING MATERIAL MAY BE PLACED UNDER THE BAG TO REDUCE SLOPE STEEPNESS. NO DOWNSLOPE SEDIMENT BARRIER IS REQUIRED FOR MOST INSTALLATIONS. COMPOST BERM OR COMPOST FILTER SOCK SHALL BE INSTALLED BELOW BAGS LOCATED IN HQ OR EV WATERSHEDS, WITHIN 50 FEET OF ANY RECEIVING SURFACE WATER OR WHERE GRASSY AREA IS NOT AVAILABLE. THE PUMP DISCHARGE HOSE SHALL BE INSERTED INTO THE BAGS IN THE MANNER SPECIFIED BY THE MANUFACTURER AND SECURELY CLAMPED. A PIECE OF PVC PIPE IS RECOMMENDED FOR THIS PURPOSE. THE PUMPING RATE SHALL BE NO GREATER THAN 750 GPM OR 1/2 THE MAXIMUM SPECIFIED BY THE MANUFACTURER, WHICHEVER IS LESS. PUMP INTAKES SHALL BE FLOATING AND SCREENED. FILTER BAGS SHALL BE INSPECTED DAILY. IF ANY PROBLEM IS DETECTED, PUMPING SHALL CEASE IMMEDIATELY AND NOT RESUME UNTIL THE PROBLEM IS CORRECTED.

### PUMPED WATER FILTER BAG (NOT TO SCALE)



SOCK FABRIC SHALL MEET STANDARDS OF TABLE 4.1 OF THE PA DEP EROSION CONTROL COMPOST FILTER SOCK SHALL BE PLACED AT EXISTING LEVEL GRADE. BOTH ENDS OF THE BARRIER SHALL BE EXTENDED AT LEAST 8 FEET UP SLOPE AT 45 DEGREES TO THE MAIN BARRIER ALIGNMENT. MAXIMUM SLOPE LENGTH ABOVE ANY BARRIER SHALL NOT EXCEED THAT SPECIFIED FOR THE SIZE OF THE SOCK AND THE SLOPE OF ITS TRIBUTARY AREA. TRAFFIC SHALL NOT BE PERMITTED TO CROSS COMPOST FILTER SOCKS. ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN IT REACHES 1/2 THE ABOVE GROUND HEIGHT OF THE BARRIER AND DISPOSED IN THE MANNER DESCRIBED ELSEWHERE IN THE PLAN. COMPOST FILTER SOCKS SHALL BE INSPECTED WEEKLY AND AFTER EACH RUNOFF EVENT. DAMAGED SOCKS SHALL BE REPAIRED ACCORDING TO MANUFACTURER'S SPECIFICATIONS OR REPLACED WITHIN 24 HOURS OF INSPECTION. BIODEGRADABLE COMPOST FILTER SOCKS SHALL BE REPLACED AFTER 6 MONTHS; PHOTODEGRADABLE SOCKS AFTER 1 YEAR. POLYPROPYLENE SOCKS SHALL BE REPLACED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS. UPON STABILIZATION OF THE AREA TRIBUTARY TO THE SOCK, STAKES SHALL BE REMOVED. THE SOCK MAY BE LEFT IN PLACE AND VEGETATED OR REMOVED. IN THE LATTER CASE, THE MESH SHALL BE CUT OPEN AND THE MULCH SPREAD AS A SOIL SUPPLEMENT.

### COMPOST FILTER SOCK (NOT TO SCALE)



SECTION A-A ~2"X2"X36" WOOD STAKES - DIRECT CONCRETE WASHOUT SPACED 5' O.C. WATER INTO FILTER RING -24" DIAMETER COMPOST FILTER SOCK OR COMPOST FILTER BERM, 4' MIN. OVERLAP ON UPSLOPE SIDE OF FILTER RING

1. INSTALL ON FLAT GRADE (2% MAXIMUM SLOPE) FOR OPTIMAL PERFORMANCE 2. 18" DIAMETER FILTER SOCK MAY BE STACKED ONTO DOUBLE 24" DIAMETER FILTER SOCKS IN PYRAMIDAL CONFIGURATION FOR ADDED HEIGHT 3. ALL CONCRETE WASHOUT FACILITIES SHOULD BE INSPECTED DAILY. COMPOST SOCK MUST REMAIN IN CONTINUOUS CONTACT WITH THE GEOMEMBRANE AT ALL LOCATIONS. DAMAGED OR LEAKING WASHOUTS SHOULD BE DEACTIVATED AND REPAIRED OR

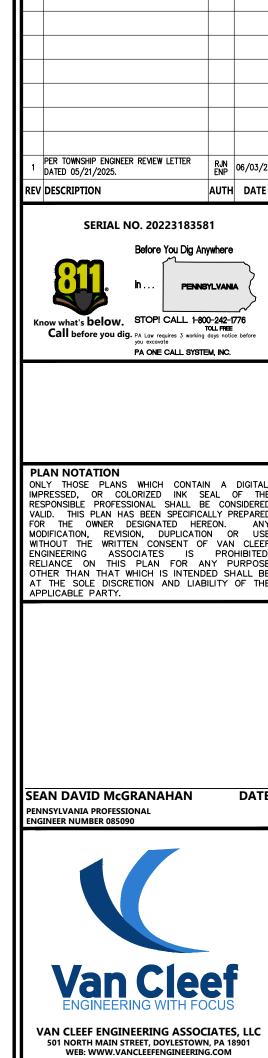
COMPOST SOCK CONCRETE WASHOUT

4. ACCUMULATED MATERIALS TO BE REMOVED WHEN THEY REACH 75% CAPACITY.

REPLACED IMMEDIATELY.

(NOT TO SCALE)

THIS SHEET FOR EROSION and SEDIMENTATION CONTROL PURPOSES ONLY



PHONE (215) 345-1876

**EROSION &** 

**SEDIMENTATION** 

**CONTROL DETAILS** 

TMP'S 26-006-101-001

**NEW BRITAIN TOWNSHIP** 

**BUCKS COUNTY, PENNSYLVANIA** 

11 of 18

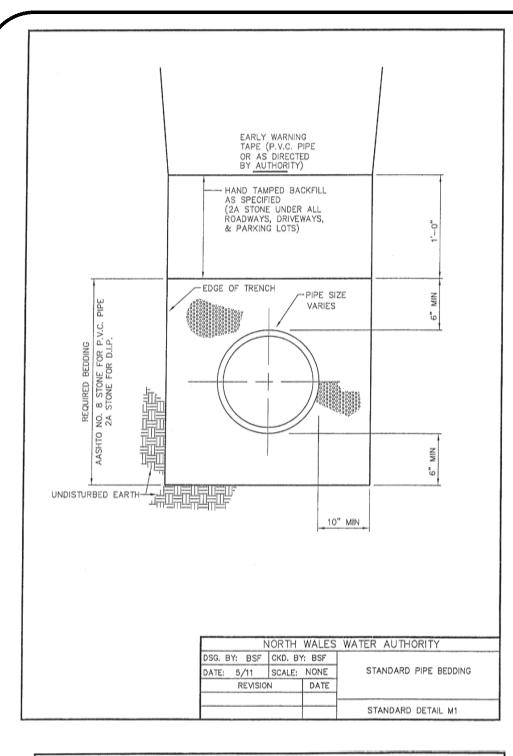
RJN / ENP

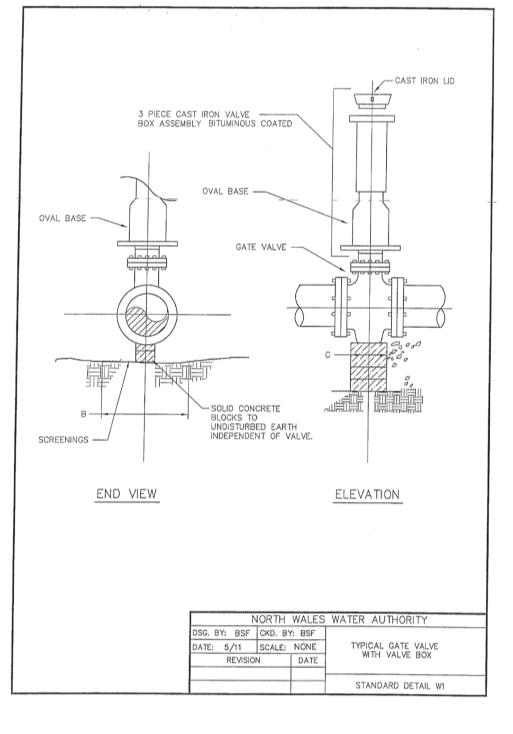
DESIGNED BY

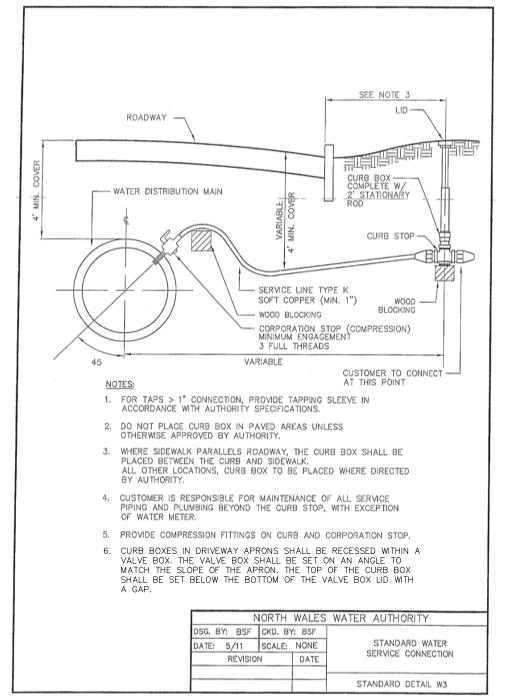
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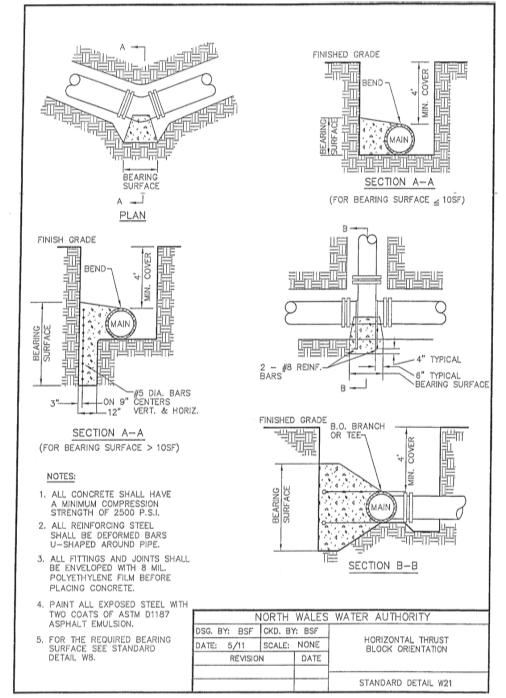
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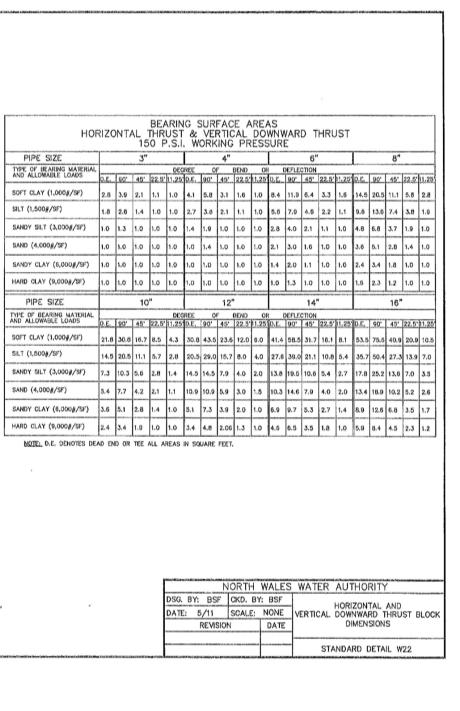
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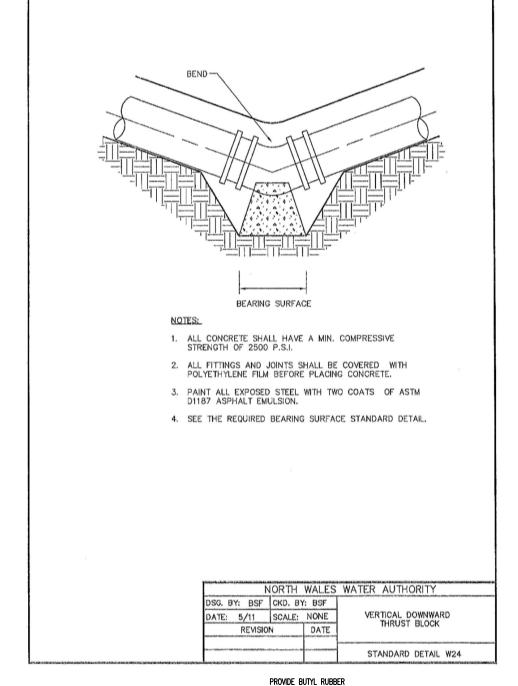


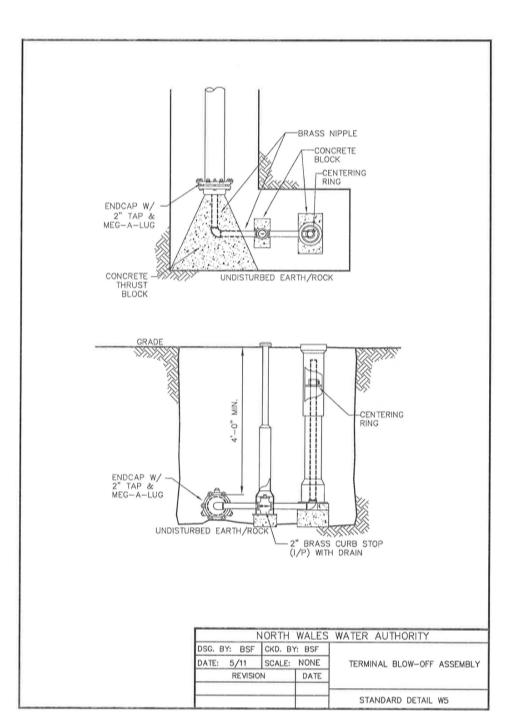


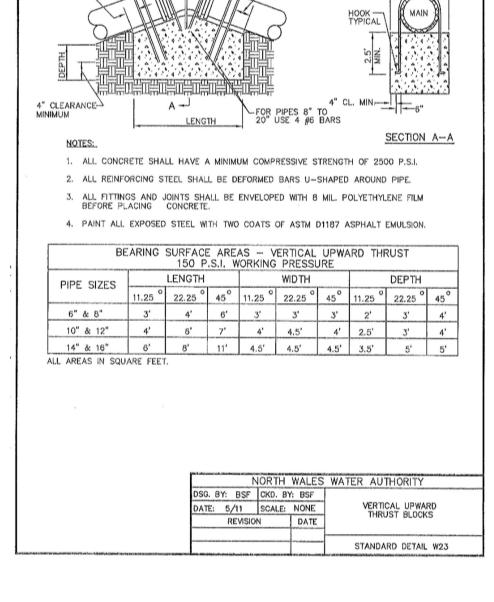


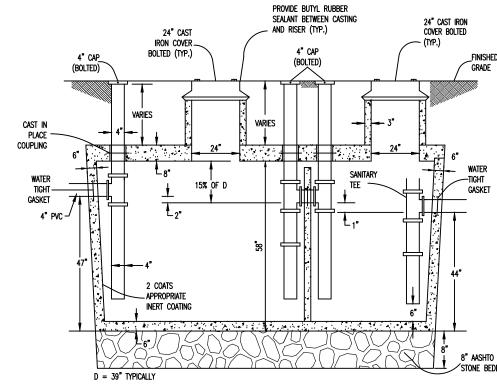


FOR 24" AND LARGER PIPES USE 7 #6 BARS AS SHOWN









ALL INLET AND OUTLET PIPES SHALL BE INSTALLED NO MORE THAN 6" FROM THE BOTTOM OF THE GREASE TRAP. TANK TAPERS TOP TO BOTTOM AND IS TRAPEZOIDAL IN CROSS SECTION.

TANK IS 5000 PSI CONCRETE-STEEL REINFORCED (@ 28 DAYS) CONCRETE CONFORMS TO ACI 318-16-4.5.1 AND 318-16-4.5.2, ASTM A615 AND A185

-DIMENSION: 151" INTERIOR/ 163" EXTERIOR LENGTH x 72" INTERIOR/ 84" EXTERIOR WIDTH

- 1) WHEN LOCATED IN DRIVEWAYS OR PAVED AREAS, GREASE TRAP TO BE DESIGNED FOR APPROPRIATE LOAD BEARING CONDITIONS. GREASE TRAP SHALL BE CAPABLE OF WITHSTANDING HS-20 LOADING.

  2) ALL PIPE PENETRATIONS SHALL BE WATERTIGHT.

  3) COPPASE TRAP SHALL BE PROVIDED WITH CASTICHT MANHOLE COVERS IN ACCORDANCE WITH TOWNISHIP STANDARD.
- 2) ALL PIPE PENEURATIONS SHALL BE WATERTIGHT.

  3) GREASE TRAP SHALL BE PROVIDED WITH GAS—TIGHT MANHOLE COVERS, IN ACCORDANCE WITH TOWNSHIP STANDARD SPECIFICATIONS.

  4) PRECAST CONCRETE SHALL HAVE A MINIMUM 28—DAY COMPRESSIVE STRENGTH 5000 PSI.
- JOINT SHALL BE SEALED WITH BUTYL RUBBER SEALANT (KENT SEAL #2 OR APPROVED EQUIVALENT) AND THE EXTERIOR OF THE JOINT SHALL BE SEALED WITH NON-SHRINK GROUT IN CONFORMANCE WITH THE TOWNSHIP STANDARD GREASE TRAP DETAIL.

  7) TANK SHALL BE TESTED FOR WATER TIGHTNESS BY FILLING FOR 24 HRS. TO SOAK, THEN TOPPED OFF, AND THEN

6) SPECIFIC SEALANT DETAIL AT CONCRETE RISER TO CONCRETE VAULT INTERFACE SHALL BE WATERTIGHT. AT A MINIMUM, THE

EXTERIOR CONCRETE SURFACES BELOW GRADE SHALL HAVE 2 COATS OF COAL TAR EPOXY.

10) INLET AND OUTLET EQUIPPED WITH PIPE SEALS.

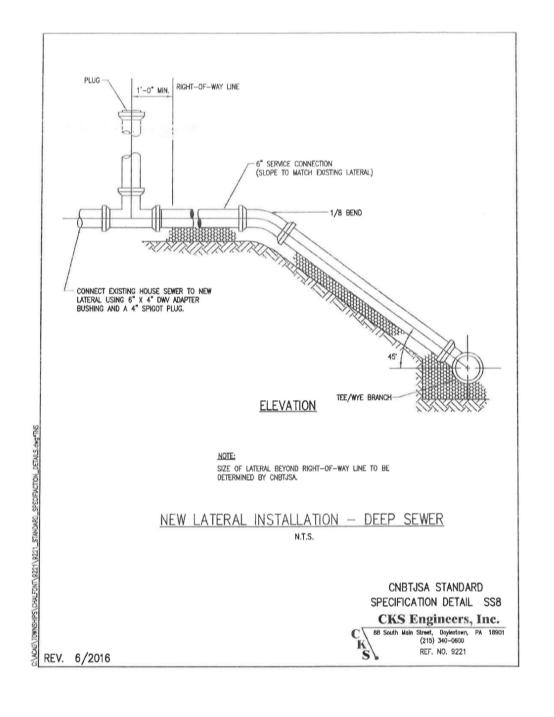
WATCHED FOR 24 HRS. NO DROP IN WATER IS ALLOWED.

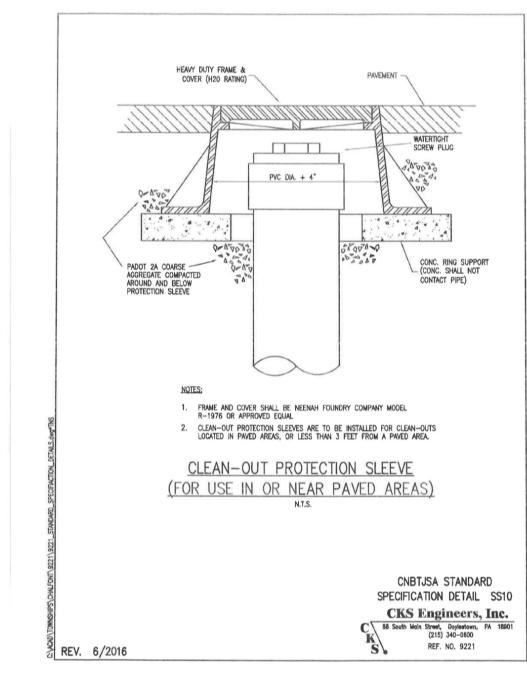
8) CAST IRON SHALL BE BOLTED TO CONCRETE WITH MASTIC TAPE (KENT SEAL OR APPROVED EQUIVALENT) SEALANT.

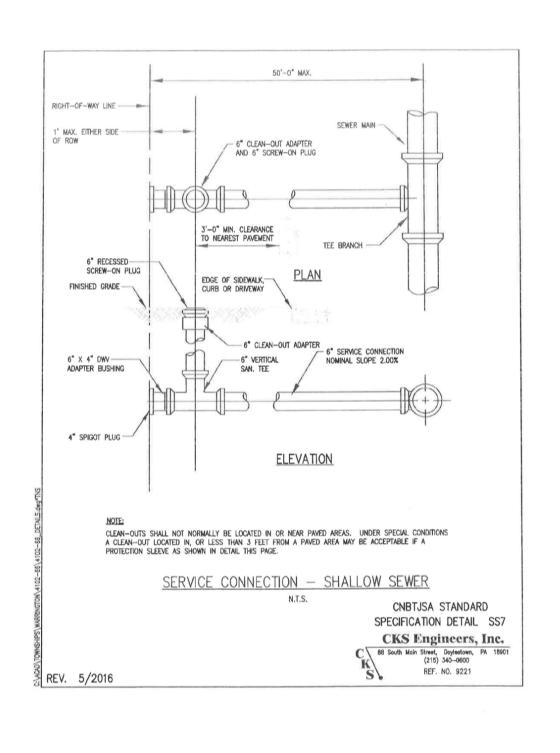
9) MAXIMUM EARTH COVER=5.0', HS-20 LOADING.

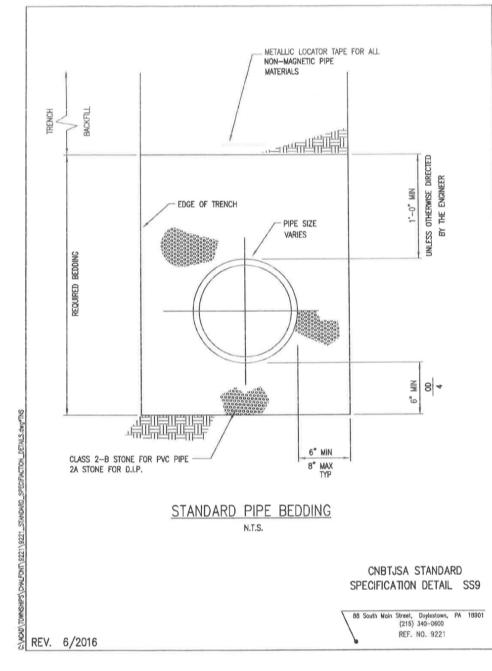
GREASE TRAP (1,500 GALLON - 2 COMPARTMENT)

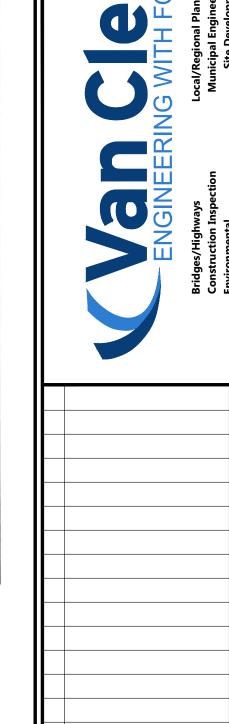
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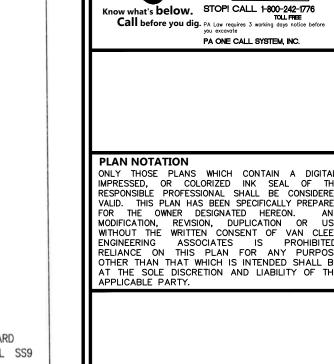












PER TOWNSHIP ENGINEER REVIEW LETTER

**SERIAL NO. 20223183581** 

Before You Dig Anywhere

PENNSYLVANIA

AUTH DATE

DATED 05/21/2025.

REV DESCRIPTION





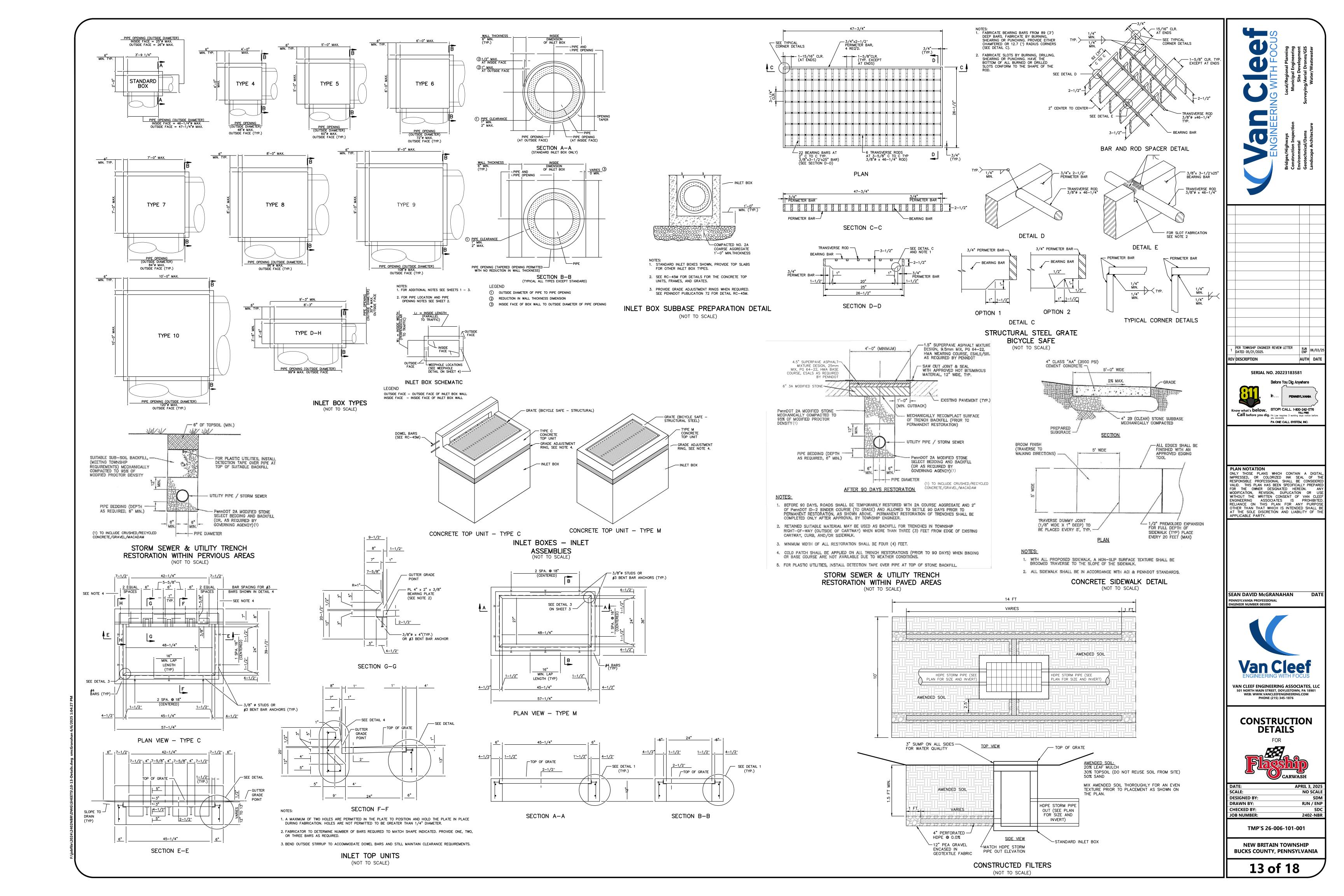
PHONE (215) 345-1876

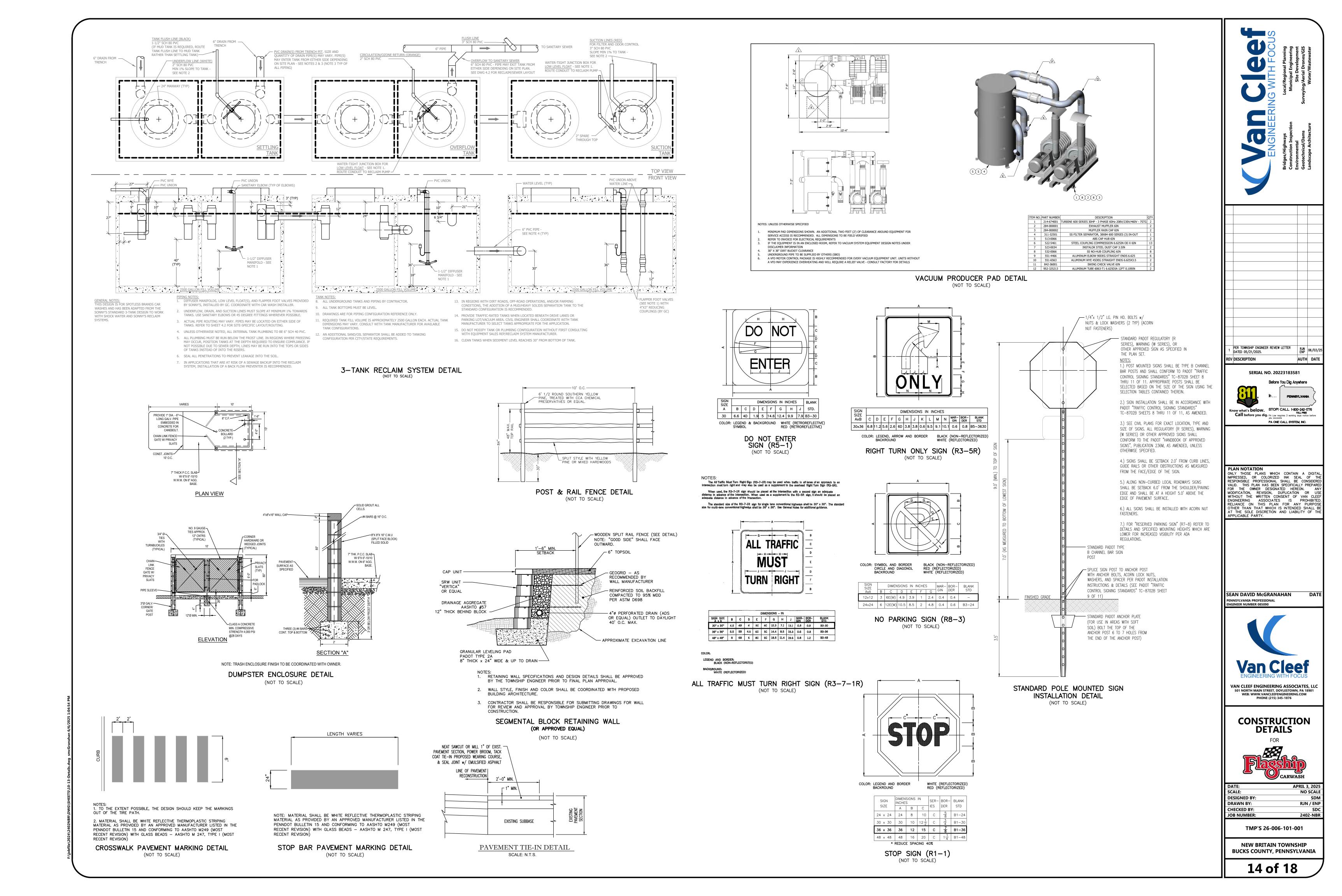


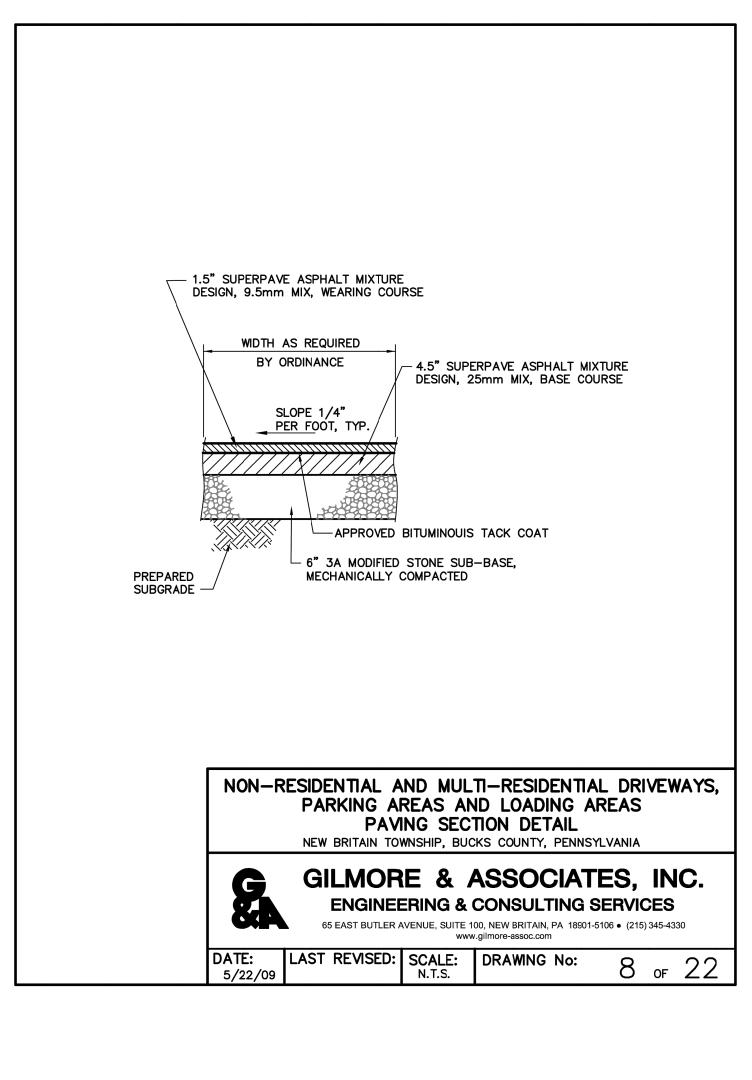
	CARWASH
DATE:	APRIL 3, 2025
SCALE:	NO SCALE
DESIGNED BY:	SDM
DRAWN BY:	RJN / ENP
CHECKED BY:	SDC
JOB NUMBER:	2402-NBR

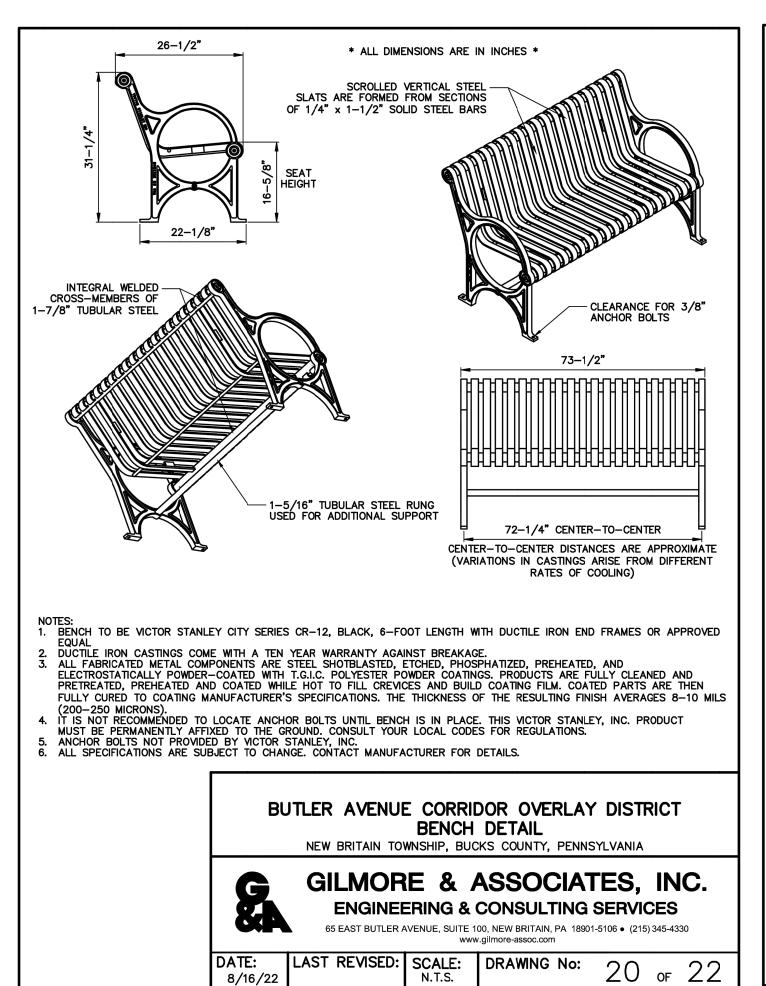
TMP'S 26-006-101-001

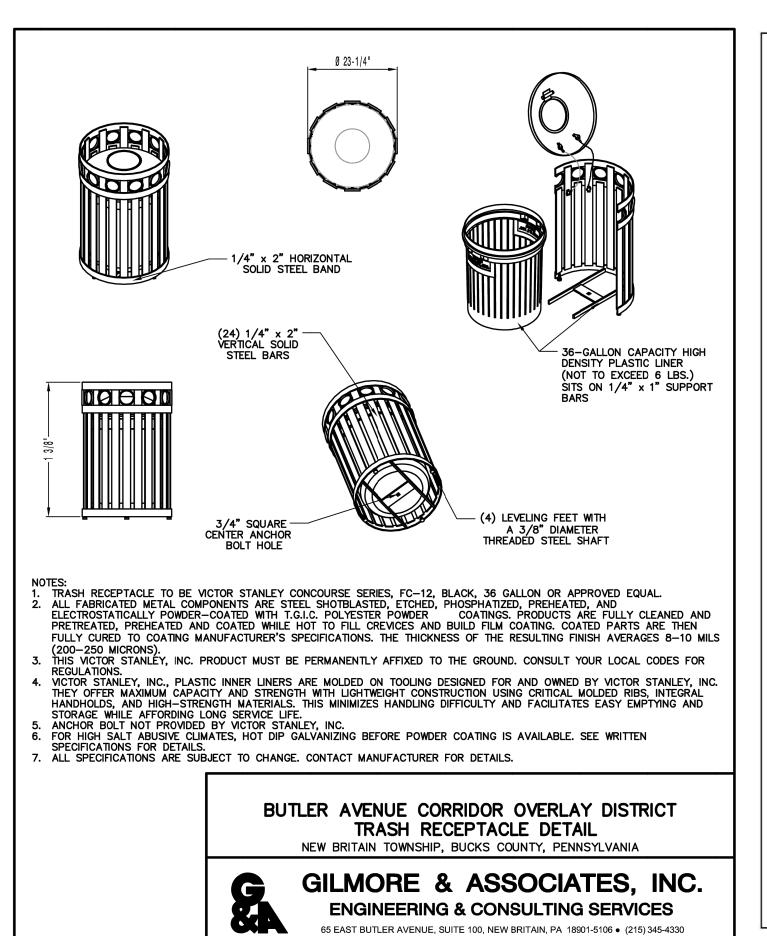
NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA







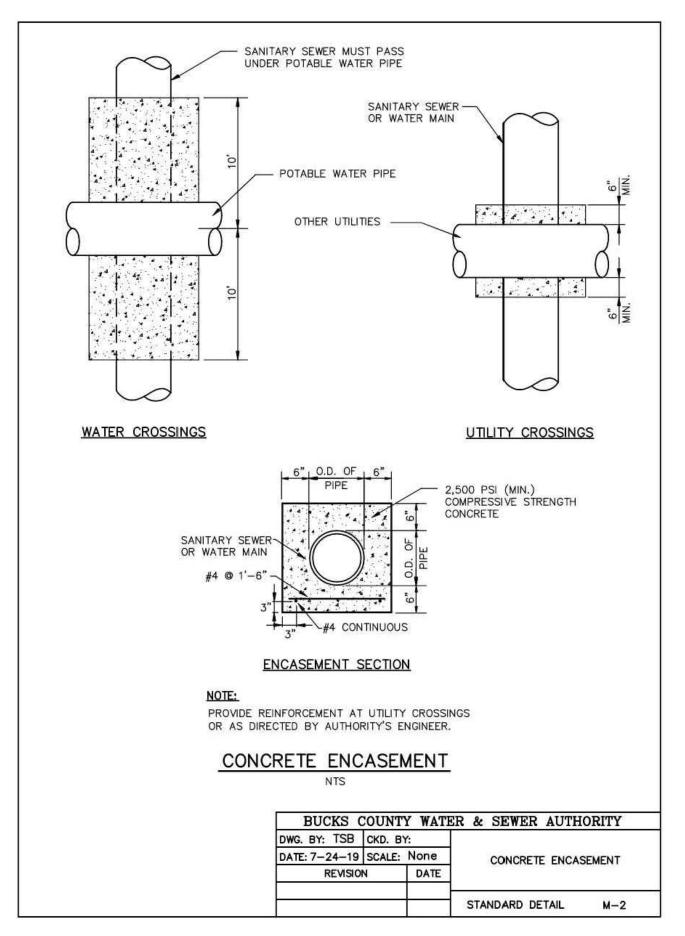


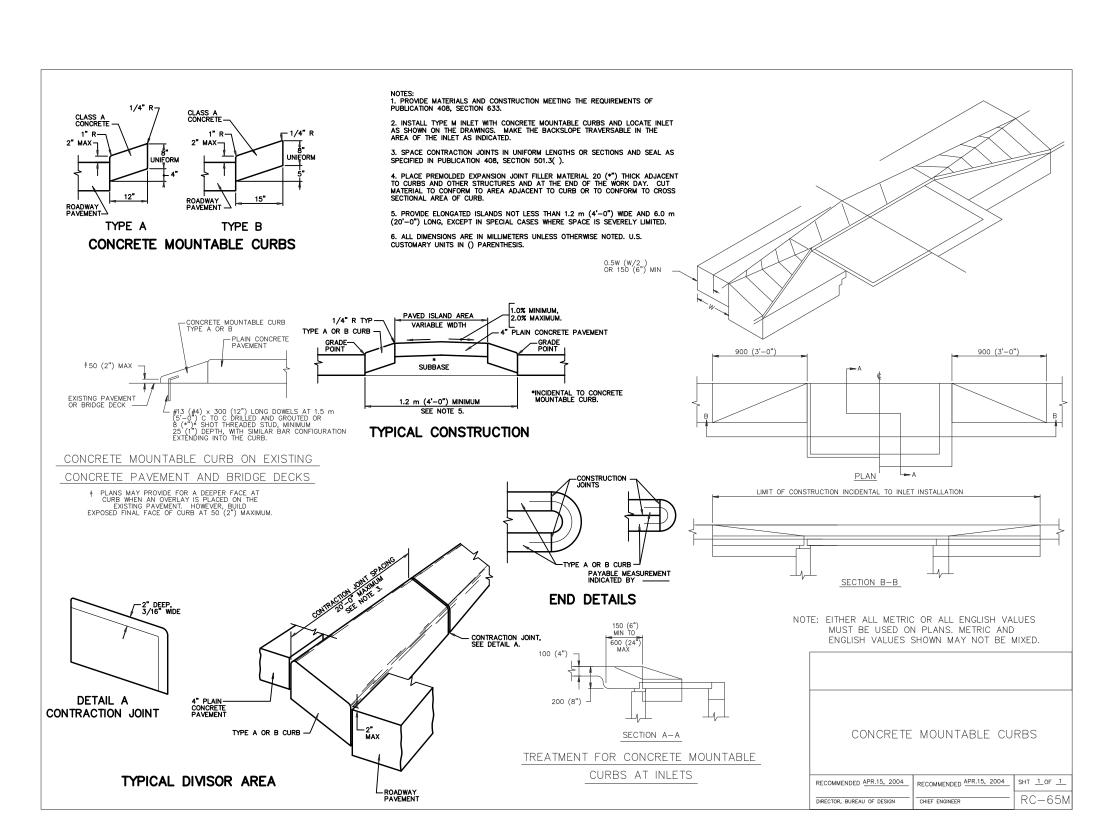


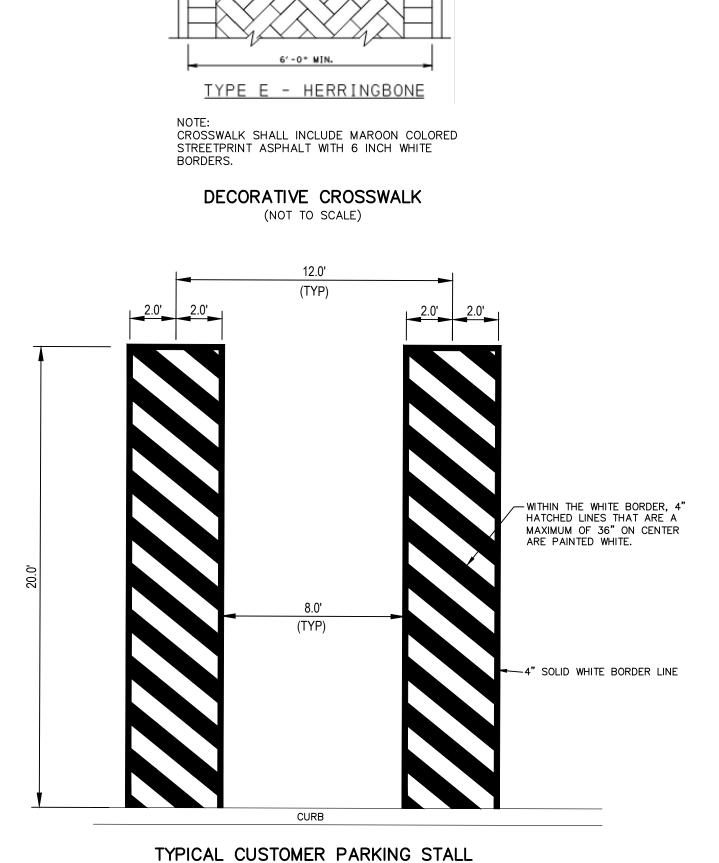
LAST REVISED: | SCALE: | DRAWING No:

8/16/22

21 of 22

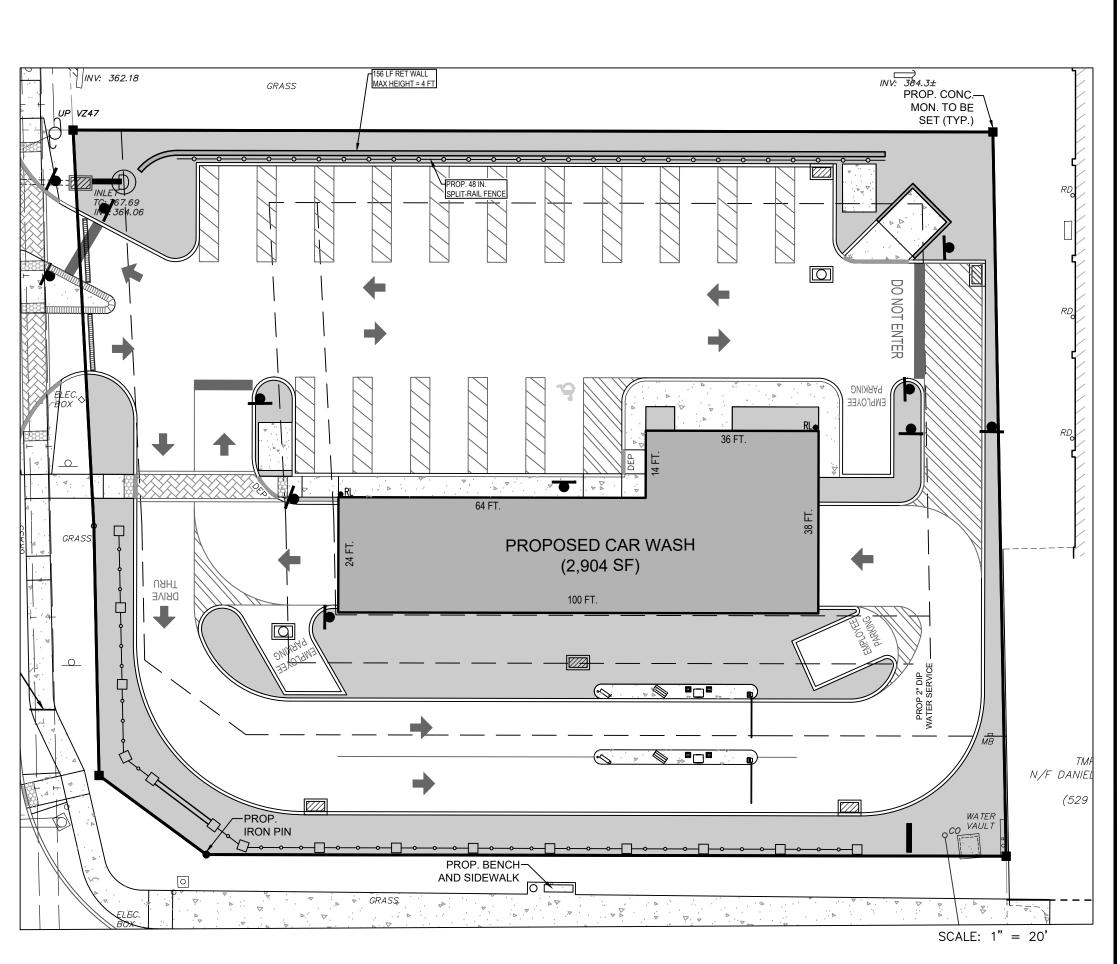






(NOT TO SCALE)

BORDER (REQUIRED) EACH SIDE.



15% MIN. SITE AREA TO BE DESIGNED AS GREEN SPACE:

0.65 ACRES X 43,560 SF = 28,314 SF X 0.15 = 4,247 SF GREEN SPACE REQUIRED. GREEN SPACE AROUND BENCH, GATEWAY MONUMENT SIGN, AND TRASH RECEPTACLE OPEN GREEN AREA = 7,112 SF PROVIDED.



1	PER TOWNSHIP ENGINEER DATED 05/21/2025.	REVIEW	LETTER	rjn Enp	06/03
REV	DESCRIPTION			AUTH	DAT
	SERIAL N		2318358 You Dig An		
	<b>811</b> .	ln	PENNS		^
Kn	ow what's below. Call before you dig.		quires 3 working		

PLAN NOTATION

ONLY THOSE PLANS WHICH CONTAIN A DIGITAL IMPRESSED, OR COLORIZED INK SEAL OF THE RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERED VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPAREL FOR THE OWNER DESIGNATED HEREON. ANY MODIFICATION, REVISION, DUPLICATION OR USE WITHOUT THE WRITTEN CONSENT OF VAN CLEEF ENGINEERING ASSOCIATES IS PROHIBITED RELIANCE ON THIS PLAN FOR ANY PURPOSE OTHER THAN THAT WHICH IS INTENDED SHALL BE AT THE SOLE DISCRETION AND LIABILITY OF THE APPLICABLE PARTY.

PA ONE CALL SYSTEM, INC.

SEAN DAVID McGRANAHAN I
PENNSYLVANIA PROFESSIONAL
ENGINEER NUMBER 085090



ENGINEERING WITH FOCUS

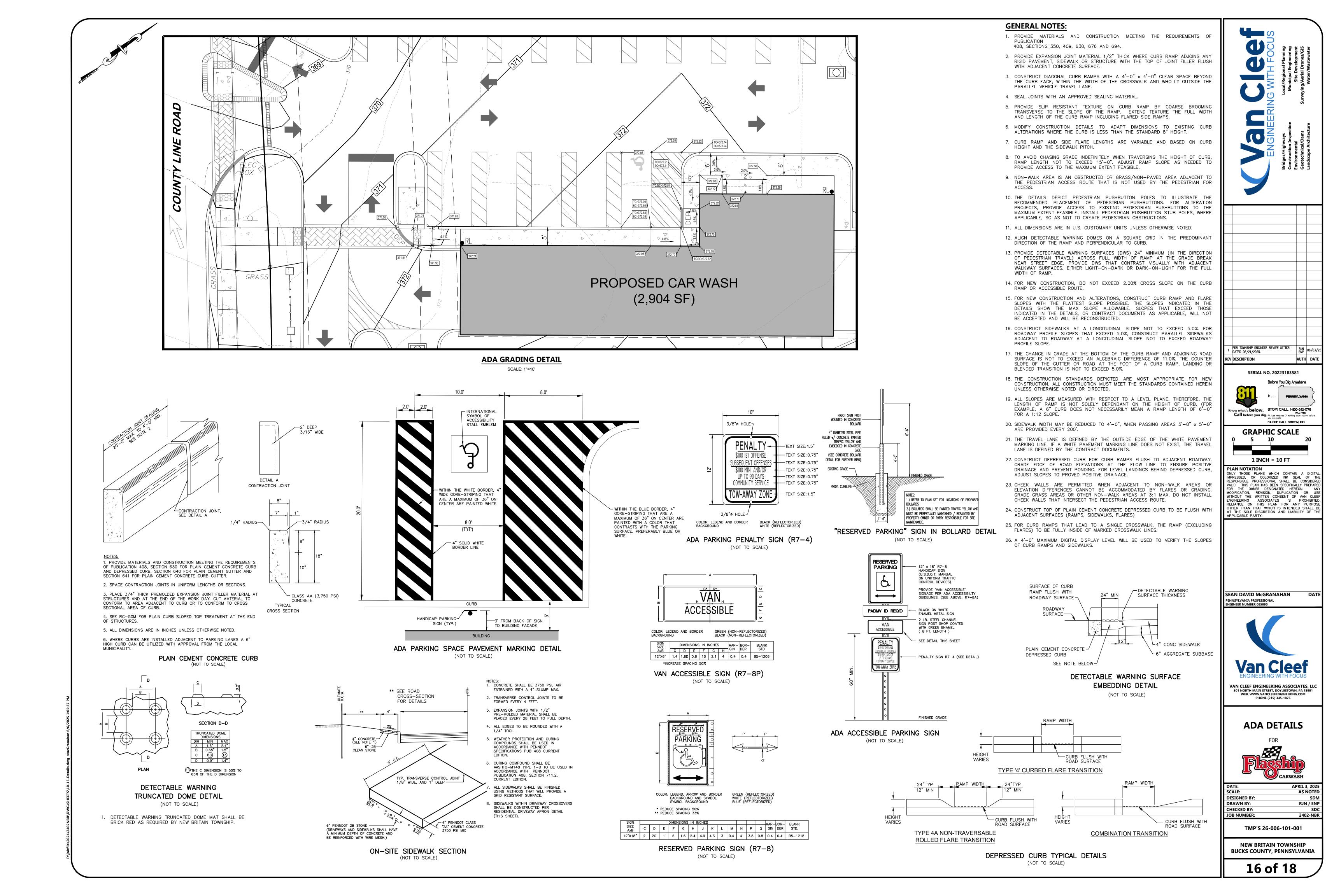
AN CLEEF ENGINEERING ASSOCIATES, LL
501 NORTH MAIN STREET, DOYLESTOWN, PA 18901
WEB: WWW.VANCLEEFENGINEERING.COM
PHONE (215) 345-1876

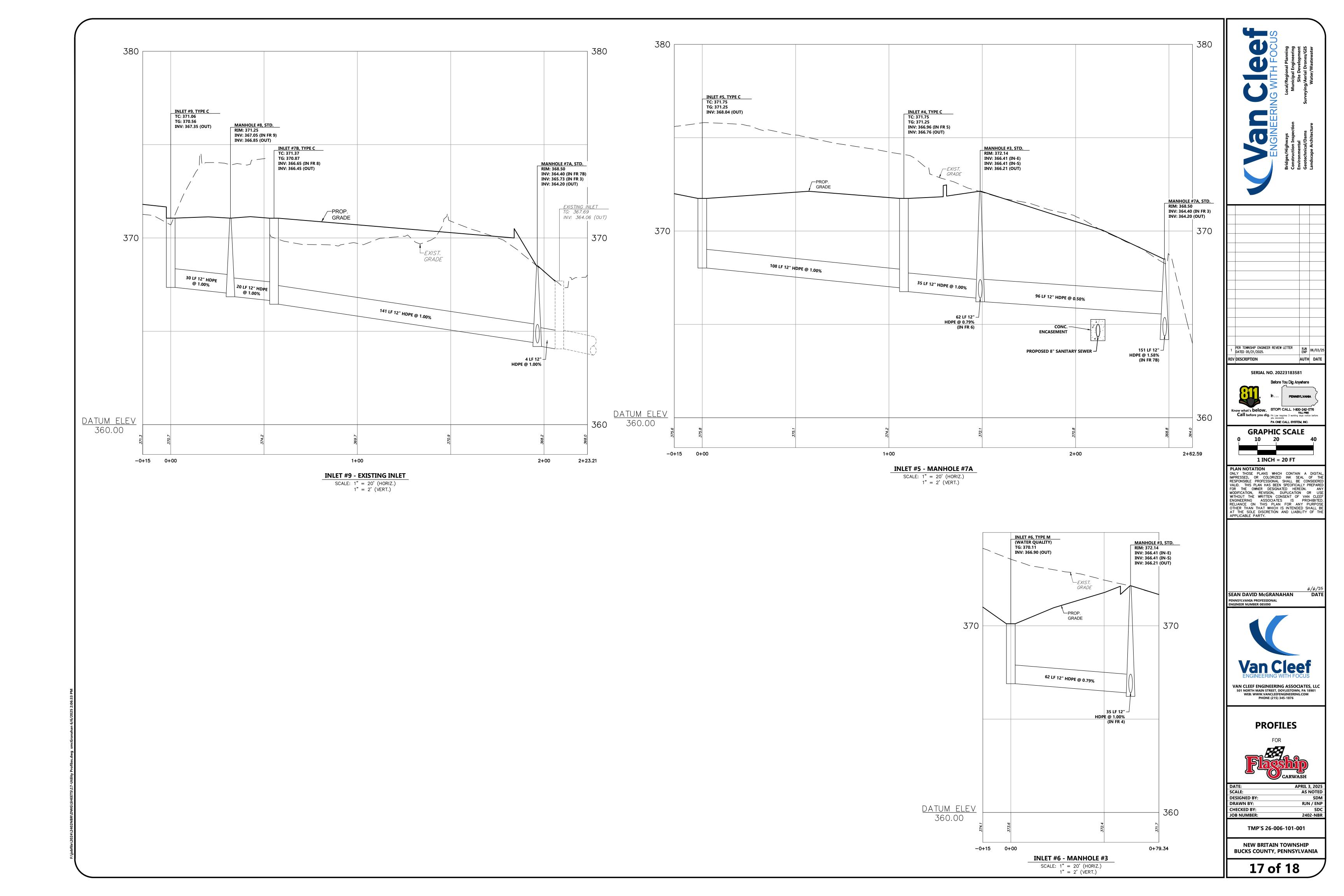


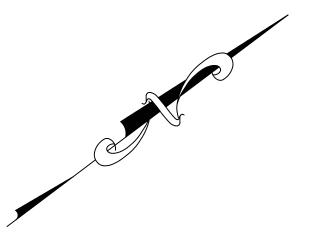


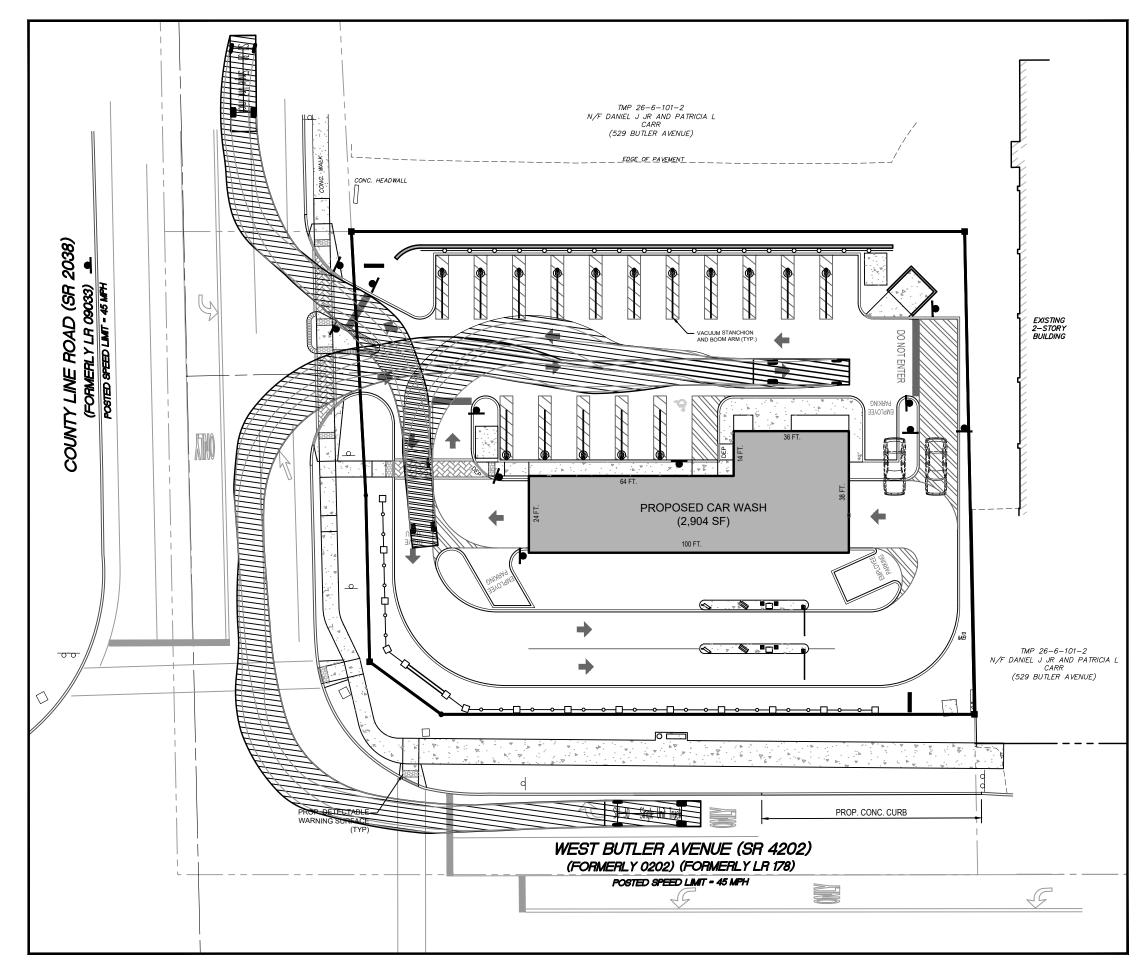
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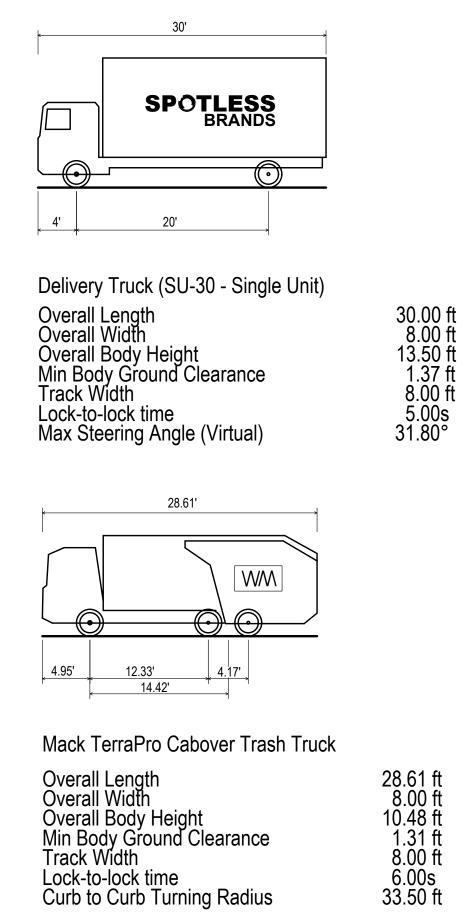
NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA

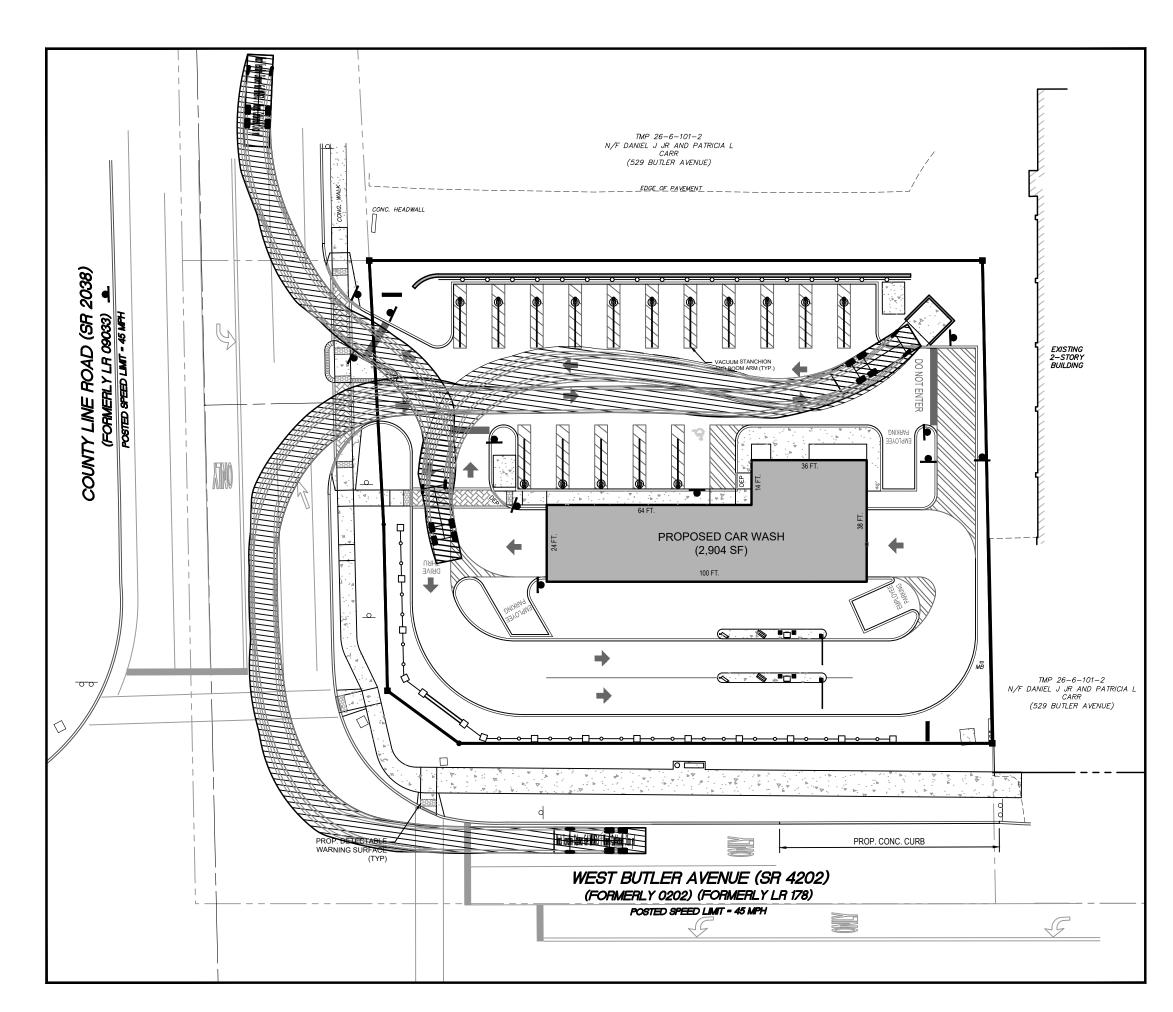






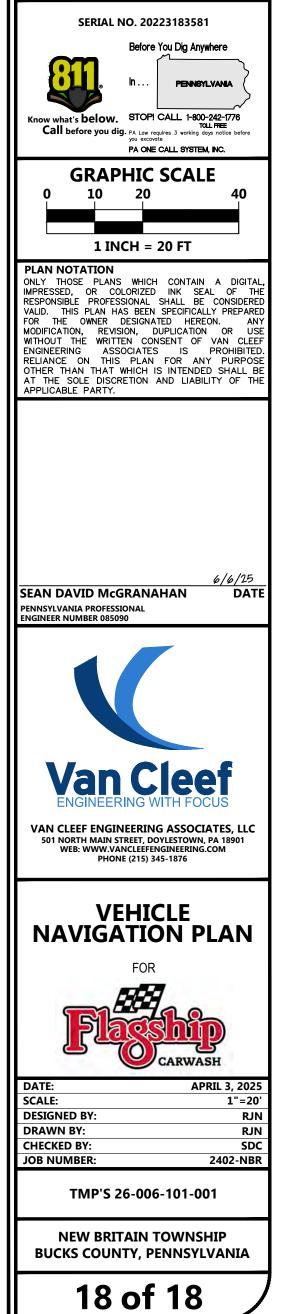






TRASH TRUCK (MACK TERRAPRO CABOVER)

SCALE: 1"=20'



PER TOWNSHIP ENGINEER REVIEW LETTER DATED 05/21/2025.

AUTH DATE

REV DESCRIPTION

DELIVERY TRUCK (SU-30)

# POST CONSTRUCTION STORMWATER CONVEYANCE CALCULATIONS

for

FLAGSHIP CARWASH VCEA No. 24-02-NBR

Situated in:

New Britain Township Bucks County, Pennsylvania

> April 03, 2025 Revised June 03, 2025

> > Prepared by:

Van Cleef Engineering Associates 501 North Main Street Doylestown, PA 18901

Sean David McGranahan, P.E. P.E. No. 085090



# **Post Construction Stormwater Management Flagship Carwash (VCEA No. 24-02-NBR)**

## **Applicant:**

Spotless Brands 545 West Butler Avenue Chalfont, PA 18914

## **General Site Description:**

Spotless Brands proposes to redevelop the existing parcel to include a car wash. The parcel is located at 545 West Butler Avenue in New Britain Township, Bucks County. The project will include the construction of a commercial building with, parking facilities, stormwater conveyance facilities, and other associated utilities. Pertinent data characterizing the existing and future site conditions are shown on the accompanying Land Development Plans.

### Pre-Development

The pre-development condition drains to an existing inlet located at the site entrance that directs traffic onto County Line Road. The site currently consists of a commercial building (no longer in use) parking lot and other amenities. The site has been in this condition for the past thirty years as indicated in aerial photographs available via the Penn Pilot website (www.pennpilot.psu.edu) and Google images.

# Post-Development

The post-development condition of the site maintains the same point of interest and relative drainage patterns. There is a decrease in the total impervious surface coverage with a subsequent decrease in runoff rates and volume. Therefore, no stormwater management storage facility has been designed.

### **Analysis of Impact:**

The pre-development condition of the development site consists of one (1) point of interest that flows offsite and ultimately converges downstream into the West Branch of the Neshaminy Creek. Stormwater flows from the southeast corner of the site to the northwest corner of the site to an existing inlet. Stormwater is then conveyed to the West Branch of the Neshaminy Creek (WWF, MF).

### Volume Control (Section 26-123)

The proposed condition for this site reduces the impervious coverage on site, therefore, these requirements are satisfied.

### Stormwater Peak Rate Control and Management Districts (Section 26-124)

The proposed condition for this site reduces the impervious coverage on site, therefore, these requirements are satisfied.

### **Storm Drainage System:**

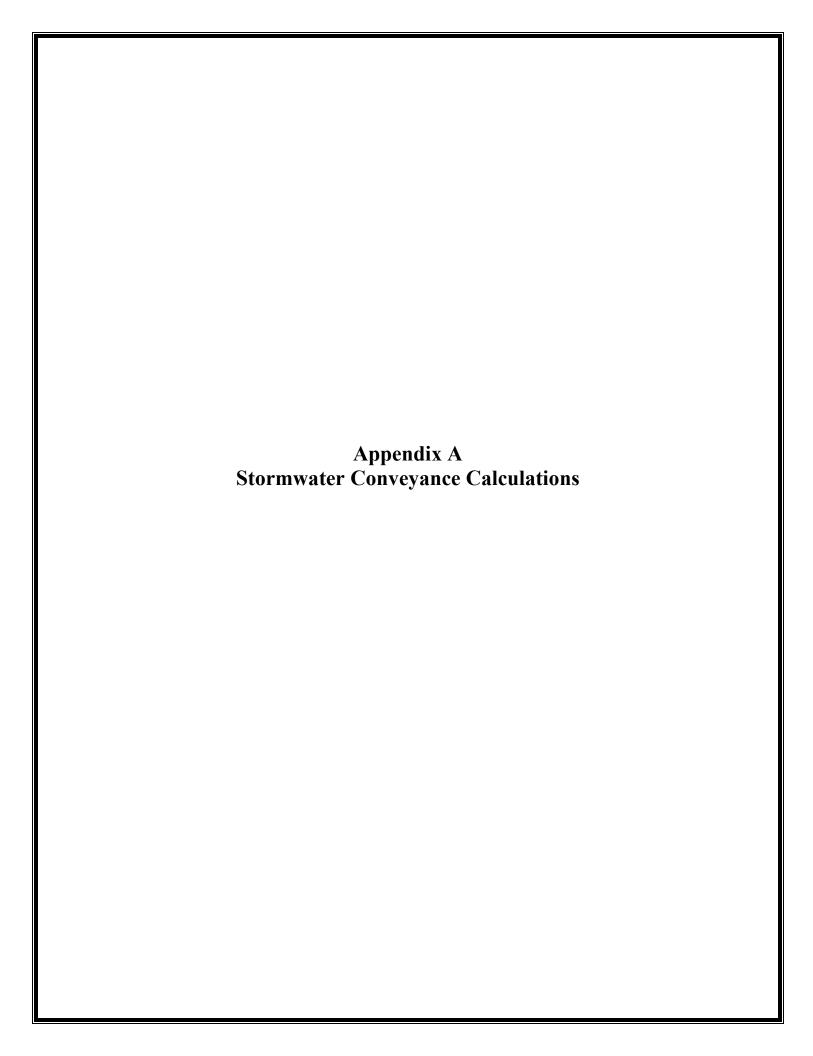
The storm drainage system has been designed to intercept runoff and convey stormwater to the existing inlet located along County Line Road. Hydraflow Storm Sewers Extension, version 10.4, has been utilized for designing the storm conveyance system. Conveyance design precipitation amounts are based on NOAA Altlas 14 precipitation data for the 100 year storm event. The Rational method was used in sizing all proposed storm sewers. The calculations and drainage areas have been provided on Drainage Area Plans included with this Report.

### **Soils Information:**

Soils data for the site was obtained from a Soil Survey of Bucks County, Pennsylvania issued September 26, 2023. Soils types found on the property are as follows:

UfuB – Urban Land, 0 to 8 percent slopes

There are no naturally occurring geologic formations (Karst Soils) that may cause pollution within the project limits of disturbance. The potential soil conditions that may cause pollution are listed in the soil limitations and resolutions in this report. The instructions for proper handling and/or disposal of all materials have been addressed within the Soils Resolution notes. Although there is no potential for stream pollution, inlet filter bags and silt socks have been provided downstream of earth disturbance to capture sediment runoff.





Project:	Spotless Carwash	By: SDM	Date:	3/14/2025
Location:	New Britain Township	Checked:	Date:	

# **RUNOFF C CALCULATIONS**

		NUNUE	F C CALC	<u>JLATIONS</u>	_	
			AREAS (SF)			
	LAND USE	Impervious	Pervious			
	HSG	С	С		TOTAL AREA	WEIGHTED
INLET#	RUNOFF C	0.99	0.25		(SF)	RUNOFF C
EX-I	INL-1	0	254		254	0.25
EX-II	NL-1B	6,463	1,310		7,773	0.87
EX-II	NL-1C	4,557	525		5,082	0.91
EX-II	VL-1D	3,615	0		3,615	0.99
TI	D-1	5,924	355		6,280	0.95
TI	D-2	2,569	449		3,018	0.88
IN	L-4	2,686	2,235		4,921	0.65
IN	L-5	2,549	2,135		4,684	0.65
IN	L-6	3,004	1,779		4,783	0.71
INI	L-7B	1,656	119		1,775	0.94
IN	L-9	3,687	552		4,239	0.89

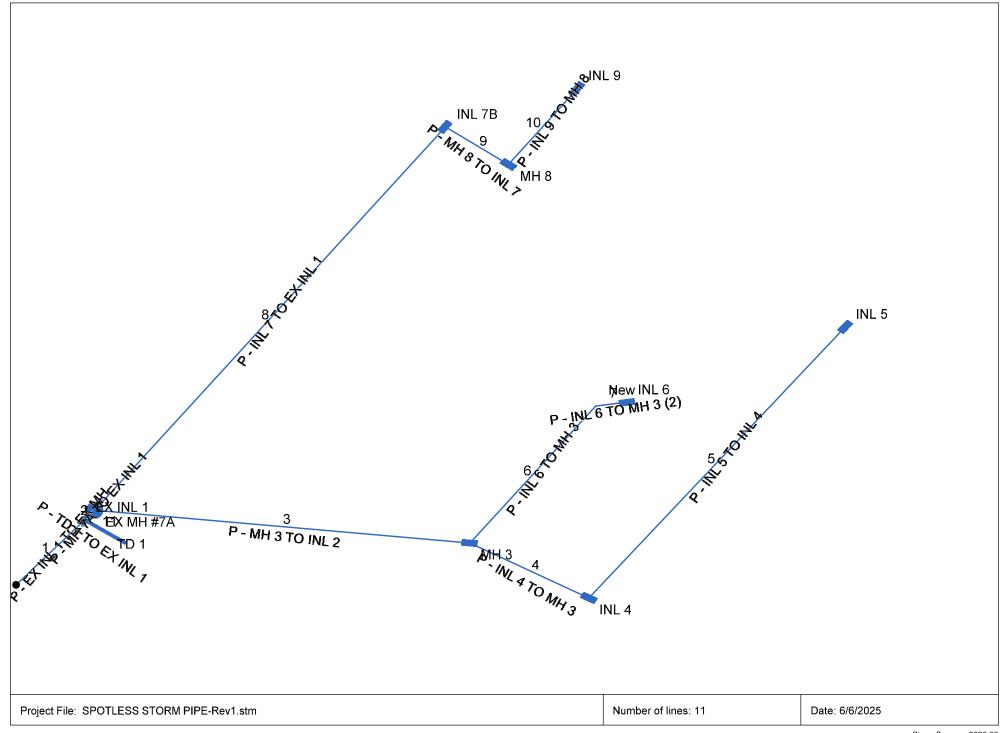


Project:	Spotless Carwash	By: SDM	<i>Date:</i> 3/:	14/2025
Location:	New Britain Township	Checked:	Date:	

# **RUNOFF C CALCULATIONS**

·		NONOI	T C CALC	JEA HONS	-	
			AREAS (AC)			
	LAND USE	Impervious	Pervious			
	HSG	С	С		TOTAL AREA	WEIGHTED
INLET#	RUNOFF C	0.99	0.25		(AC)	RUNOFF C
EX-I	INL-1	0.000	0.006		0.006	0.25
EX-II	NL-1B	0.148	0.030		0.178	0.87
EX-II	NL-1C	0.105	0.012		0.117	0.91
EX-II	NL-1D	0.083	0.000		0.083	0.99
TI	D-1	0.136	0.008		0.144	0.95
TI	D-2	0.059	0.010		0.069	0.88
IN	IL-4	0.062	0.051		0.113	0.65
IN	IL-5	0.059	0.049		0.108	0.65
IN	IL-6	0.069	0.041		0.110	0.71
INI	L-7B	0.038	0.003		0.041	0.94
IN	IL-9	0.085	0.013		0.097	0.89

# Hydraflow Storm Sewers Extension for Autodesk® Civil 3D® Plan



# **Storm Sewer Inventory Report**

ne		Align	ment			Flow	Data					Physical	Data				Line ID
<b>D.</b>	Dnstr Line No.	Length	Defl angle (deg)	Junc Type	Known Q (cfs)	Drng Area (ac)	Runoff Coeff (C)	Inlet Time (min)	Invert EI Dn (ft)	Line Slope (%)	Invert EI Up (ft)	Line Size (in)	Line Shape	N Value (n)	J-Loss Coeff (K)	Inlet/ Rim El (ft)	
1	End	27.000	-49.831	Grate	0.00	0.01	0.25	5.0	363.77	1.00	364.04	12	Cir	0.013	1.50	367.69	P - EX INL 1 TO EX
2	1	4.000	0.000	мн	0.00	0.00	0.00	0.0	364.16	1.00	364.20	12	Cir	0.013	0.86	368.50	P - MH 7A TO EX IN
3	2	96.000	56.058	мн	0.00	0.00	0.00	0.0	365.73	0.50	366.21	12	Cir	0.013	0.89	371.20	P - MH 3 TO INL 2
4	3	35.000	23.715	Curb	0.00	0.11	0.65	5.0	366.41	1.00	366.76	12	Cir	0.013	1.49	371.25	P - INL 4 TO MH 3
5	4	108.000	-82.837	Curb	0.00	0.11	0.65	5.0	366.96	1.00	368.04	12	Cir	0.013	1.00	371.25	P - INL 5 TO INL 4
6	3	54.000	-59.857	None	0.00	0.00	0.00	0.0	366.41	0.80	366.84	12	Cir	0.013	0.75	371.20	P - INL 6 TO MH 3
7	6	8.000	45.000	Grate	0.00	0.11	0.71	5.0	366.84	0.75	366.90	12	Cir	0.013	1.00	371.20	P - INL 6 TO MH 3
3	2	151.000	-3.910	Curb	0.00	0.04	0.94	5.0	364.40	1.35	366.44	12	Cir	0.013	1.50	370.87	P - INL 7 TO EX IN
)	8	20.000	90.480	мн	0.00	0.00	0.00	0.0	366.65	1.00	366.85	12	Cir	0.013	1.00	371.25	P - MH 8 TO INL 7
0	9	30.000	-90.679	Curb	0.00	0.10	0.89	5.0	367.05	1.00	367.35	12	Cir	0.013	1.00	370.56	P - INL 9 TO MH 8
1	1	7.000	85.016	Grate	0.00	0.14	0.95	5.0	365.86	2.86	366.06	12	Cir	0.013	0.50	367.89	P - TD 1 TO EX INL

# **Structure Report**

Project File: SPOTLESS STORM PIPE-Rev1.stm

Struct	Structure ID	Junction	Rim		Structure			Line Ou	t	Line In			
No.		Туре	Elev (ft)	Shape	Length (ft)	Width (ft)	Size (in)	Shape	Invert (ft)	Size (in)	Shape	Invert (ft)	
1	EX INL 1	Grate	367.69	Rect	4.00	2.00	12	Cir	364.04	12 12	Cir Cir	364.16 365.86	
2	EX MH #7A	Manhole	368.50	Cir	4.00	2.00	12	Cir	364.20	12 12	Cir Cir	365.73 364.40	
3	MH 3	Manhole	371.20	Rect	4.00	2.00	12	Cir	366.21	12 12	Cir Cir	366.41 366.41	
4	INL 4	Curb-Horiz	371.25	Rect	4.00	2.00	12	Cir	366.76	12	Cir	366.96	
5	INL 5	Curb-Horiz	371.25	Rect	4.00	2.00	12	Cir	368.04				
6	New	None	371.20	n/a	n/a	n/a	12	Cir	366.84	12	Cir	366.84	
7	INL 6	Grate	371.20	Rect	4.00	2.00	12	Cir	366.90				
8	INL 7B	Curb-Horiz	370.87	Rect	4.00	2.00	12	Cir	366.44	12	Cir	366.65	
9	MH 8	Manhole	371.25	Rect	4.00	2.00	12	Cir	366.85	12	Cir	367.05	
10	INL 9	Curb-Horiz	370.56	Rect	4.00	2.00	12	Cir	367.35				
11	TD 1	Grate	367.89	Rect	12.00	1.00	12	Cir	366.06	12	Cir	366.85	

Number of Structures: 11 Run Date: 6/6/2025

# **Storm Sewer Summary Report**

₋ine No.	Line ID	Flow rate (cfs)	Line Size (in)	Line shape	Line length (ft)	Invert EL Dn (ft)	Invert EL Up (ft)	Line Slope (%)	HGL Down (ft)	HGL Up (ft)	Minor loss (ft)	HGL Junct (ft)	Dns Line No.	Junction Type
1	P - EX INL 1 TO EX MH	3.73	12	Cir	27.000	363.77	364.04	1.000	364.61	364.92	0.61	365.53	End	Grate
2	P - MH 7A TO EX INL 1	2.36	12	Cir	4.000	364.16	364.20	1.000	365.53*	365.54*	0.12	365.67	1	Manhole
3	P - MH 3 TO INL 2	1.55	12	Cir	96.000	365.73	366.21	0.500	366.30	366.78	0.16	366.94	2	Manhole
4	P - INL 4 TO MH 3	1.02	12	Cir	35.000	366.41	366.76	1.000	366.94	367.18	n/a	367.18 j	3	Curb-Horiz
5	P - INL 5 TO INL 4	0.56	12	Cir	108.000	366.96	368.04	1.000	367.23	368.35	n/a	368.35	4	Curb-Horiz
6	P - INL 6 TO MH 3	0.62	12	Cir	54.000	366.41	366.84	0.796	366.94	367.17	n/a	367.17 j	3	None
7	P - INL 6 TO MH 3 (2)	0.62	12	Cir	8.000	366.84	366.90	0.750	367.17	367.23	n/a	367.23	6	Grate
8	P - INL 7 TO EX INL 1	0.95	12	Cir	151.000	364.40	366.44	1.351	365.67	366.85	n/a	366.85 j	2	Curb-Horiz
9	P - MH 8 TO INL 7	0.67	12	Cir	20.000	366.65	366.85	1.000	366.94	367.19	0.13	367.19	8	Manhole
10	P - INL 9 TO MH 8	0.69	12	Cir	30.000	367.05	367.35	1.000	367.35	367.70	0.13	367.70	9	Curb-Horiz
11	P - TD 1 TO EX INL 1	1.56	12	Cir	7.000	365.86	366.06	2.857	366.21	366.59	0.11	366.59	1	Grate

Project File: SPOTLESS STORM PIPE-Rev1.stm

Number of lines: 11

Run Date: 6/6/2025

NOTES: Return period = 100 Yrs.; \*Surcharged (HGL above crown).; j - Line contains hyd. jump.

# **Storm Sewer Tabulation**

Statio	n	Len	Drng A	rea	Rnoff	Area x	С	Тс		Rain	Total		Vel	Pipe		Invert Ele	ev	HGL Ele	v	Grnd / Ri	m Elev	Line ID
Line	То		Incr	Total	coeff	Incr	Total	Inlet	Syst	(1)	flow	full		Size	Slope	Dn	Up	Dn	Up	Dn	Up	
	Line	(ft)	(ac)	(ac)	(C)			(min)	(min)	(in/hr)	(cfs)	(cfs)	(ft/s)	(in)	(%)	(ft)	(ft)	(ft)	(ft)	(ft)	(ft)	
1	End	27.000		0.69	0.25	0.00	0.54	5.0	8.7	6.9	3.73	3.56	5.21	12	1.00	363.77	364.04	364.61	364.92	367.82	367.69	P - EX INL 1 TO E
2	1		0.00	0.47	0.00	0.00	0.34	0.0	8.7	6.9	2.36	3.56	3.01	12	1.00	364.16	364.20	365.53	365.54	367.69	368.50	P - MH 7A TO EX
3	2	96.000		0.33	0.00	0.00	0.22	0.0	7.9	7.1	1.55	2.52	3.37	12	0.50	365.73	366.21	366.30	366.78	368.50	371.20	P - MH 3 TO INL
4	3	35.000	0.11	0.22	0.65	0.07	0.14	5.0	7.5	7.2	1.02	3.56	2.82	12	1.00	366.41	366.76	366.94	367.18	371.20	371.25	P - INL 4 TO MH
5	4	108.000	0.11	0.11	0.65	0.07	0.07	5.0	5.0	8.0	0.56	3.56	3.00	12	1.00	366.96	368.04	367.23	368.35	371.25	371.25	P - INL 5 TO INL
6	3	54.000	0.00	0.11	0.00	0.00	0.08	0.0	5.2	7.9	0.62	3.18	2.13	12	0.80	366.41	366.84	366.94	367.17	371.20	371.20	P - INL 6 TO MH
7	6	8.000	0.11	0.11	0.71	0.08	0.08	5.0	5.0	8.0	0.62	3.08	2.79	12	0.75	366.84	366.90	367.17	367.23	371.20	371.20	P - INL 6 TO MH
8	2	151.000	0.04	0.14	0.94	0.04	0.12	5.0	6.0	7.7	0.95	4.14	2.18	12	1.35	364.40	366.44	365.67	366.85	368.50	370.87	P - INL 7 TO EX I
9	8	20.000	0.00	0.10	0.00	0.00	0.09	0.0	5.6	7.8	0.67	3.56	3.16	12	1.00	366.65	366.85	366.94	367.19	370.87	371.25	P - MH 8 TO INL
10	9	30.000	0.10	0.10	0.89	0.09	0.09	5.0	5.0	8.0	0.69	3.56	3.18	12	1.00	367.05	367.35	367.35	367.70	371.25	370.56	P - INL 9 TO MH
11	1	7.000	0.14	0.21	0.95	0.14	0.20	5.0	5.2	7.9	1.56	6.02	5.07	12	2.86	365.86	366.06	366.21	366.59	367.69	367.89	P - TD 1 TO EX I

Number of lines: 11

NOTES:Intensity = 46.21 / (Inlet time + 9.30) ^ 0.66; Return period =Yrs. 100; c = cir e = ellip b = box

Project File: SPOTLESS STORM PIPE-Rev1.stm

Run Date: 6/6/2025

# **Inlet Report**

Line	Inlet ID	Q =	Q	Q	Q	Junc	Curb li	nlet	Gra	ite Inlet				G	utter					Inlet		Вур
No		CIA (cfs)	carry (cfs)	capt (cfs)	Byp (cfs)	Туре	Ht (in)	L (ft)	Area (sqft)	L (ft)	W (ft)	So (ft/ft)	W (ft)	Sw (ft/ft)	Sx (ft/ft)	n		Spread (ft)		Spread (ft)	Depr (in)	Line No
1	EX INL 1	0.02	0.00	0.02	0.00	Grate	0.0	0.00	0.00	4.00	2.00	0.010	2.00	0.050	0.020	0.013	0.04	0.87	0.00	0.00	0.0	Off
2	EX MH #7A	0.00	0.00	0.00	0.00	мн	0.0	0.00	0.00	0.00	0.00	Sag	2.00	0.050	0.020	0.013	0.00	0.00	0.00	0.00	0.0	1
3	MH 3	0.00	0.00	0.00	0.00	мн	0.0	0.00	0.00	0.00	0.00	Sag	2.00	0.050	0.020	0.013	0.00	0.00	0.00	0.00	0.0	Off
4	INL 4	0.57	0.00	0.57	0.00	Curb	4.0	4.00	0.00	0.00	0.00	Sag	2.00	0.005	0.040	0.013	0.13	5.03	0.13	5.03	0.0	Off
5	INL 5	0.56	0.00	0.56	0.00	Curb	4.0	4.00	0.00	0.00	0.00	Sag	2.00	0.005	0.040	0.013	0.13	4.99	0.13	4.99	0.0	4
6	New	0.00	0.00	0.00	0.00	None	0.0	0.00	0.00	0.00	0.00	Sag	2.00	0.050	0.020	0.013	0.00	0.00	0.00	0.00	0.0	3
7	INL 6	0.62	0.00	0.62	0.00	Grate	0.0	0.00	8.00	4.00	2.00	Sag	2.00	0.050	0.020	0.013	0.14	3.88	0.14	3.88	0.0	6
8	INL 7B	0.30	0.00	0.30	0.00	Curb	4.0	4.00	0.00	0.00	0.00	Sag	2.00	0.050	0.020	0.013	0.13	3.33	0.13	3.33	0.0	2
9	MH 8	0.00	0.00	0.00	0.00	мн	0.0	0.00	0.00	0.00	0.00	Sag	2.00	0.050	0.020	0.013	0.00	0.00	0.00	0.00	0.0	8
10	INL 9	0.69	0.00	0.69	0.00	Curb	4.0	4.00	0.00	0.00	0.00	Sag	2.00	0.050	0.020	0.013	0.18	5.79	0.18	5.79	0.0	9
11	TD 1	1.09	0.00	1.09	0.00	Grate	0.0	0.00	12.00	12.00	1.00	Sag	2.00	0.050	0.020	0.013	0.11	2.64	0.11	2.64	0.0	1

Project File: SPOTLESS STORM PIPE-Rev1.stm Number of lines: 11 Run Date: 6/6/2025

NOTES: Inlet N-Values = 0.016; Intensity = 46.21 / (Inlet time + 9.30) ^ 0.66; Return period = 100 Yrs.; \* Indicates Known Q added. All curb inlets are Horiz throat.

# **FL-DOT Report**

No Li		of struc	Value											1				1			l I
		1	1			C1 = 0. $C2 = 0.$		of conc	of Flow	(1)	CA	Q Tatal	elev	Ele	v of Crown		Span	Pipe	Full	Flow	Frequency: 100 yrs
						C3 = 0.			in sect			Total Flow		Ele	v of Invert						Proj: SPOTLESS STORM
				(ft)	l .	Sub- Total (ac)	Sum CA	(min)	(min)	(in/hr)		Q (cfs)	(ft)	Up (ft)	Down (ft)	Fall (ft)	Size (in)	Slope (%)	Vel (ft/s)	Cap (cfs)	Line description
1 E	End	Grate	0.013	27.000	0.00 0.00 0.00	0.00 0.00 0.00	0.00 0.00 0.00	8.70	0.09	6.86	0.54	0.00 3.73	367.69	364.92 365.04 364.04	364.61 364.77 363.77	0.31 0.27	12 12 Cir	1.13 1.00	5.21 4.53	3.73 3.56	P - EX INL 1 TO EX MH
2	1	мн	0.013	4.000	0.00 0.00 0.00	0.00 0.00 0.00	0.00 0.00 0.00	8.68	0.02	6.87	0.34	0.00 2.36	368.50	365.54 365.20 364.20	365.53 365.16 364.16	0.02	12 12 Cir	0.44 1.00	3.01 4.53	2.36 3.56	P - MH 7A TO EX INL 1
3 2	2	мн	0.013	96.000	0.00 0.00 0.00	0.00 0.00 0.00	0.00 0.00 0.00	7.94	0.74	7.06	0.22	0.00 1.55	371.20	366.78 367.21 366.21	366.30 366.73 365.73	0.48 0.48	12 12 Cir	0.50 0.50	3.37 3.21	1.55 2.52	P - MH 3 TO INL 2
4	3	Curb	0.013	35.000	0.00 0.00 0.00	0.00 0.00 0.00	0.00 0.00 0.00	7.52	0.42	7.18	0.14	0.00 1.02	371.25	367.18 367.76 366.76	366.94 367.41 366.41	0.25 0.35	12 12 Cir	0.71 1.00	2.82 4.53	1.02 3.56	P - INL 4 TO MH 3
5	4	Curb	0.013	108.00	0.00 0.00 0.00	0.00 0.00 0.00	0.00 0.00 0.00	5.00	2.52	7.99	0.07	0.00 0.56	371.25	368.35 369.04 368.04	367.23 367.96 366.96	1.12 1.08	12 12 Cir	1.04 1.00	3.00 4.53	0.56 3.56	P - INL 5 TO INL 4
6	3	None	0.013	54.000	0.00 0.00 0.00	0.00 0.00 0.00	0.00 0.00 0.00	5.17	1.14	7.93	0.08	0.00 0.62	371.20	367.17 367.84 366.84	366.94 367.41 366.41	0.23 0.43	12 12 Cir	0.43 0.80	2.13 4.05	0.62 3.18	P - INL 6 TO MH 3
7 6	6	Grate	0.013	8.000	0.00 0.00 0.00	0.00 0.00 0.00	0.00 0.00 0.00	5.00	0.17	7.99	0.08	0.00 0.62	371.20	367.23 367.90 366.90	367.17 367.84 366.84	0.06	12 12 Cir	0.76 0.75	2.79 3.93	0.62 3.08	P - INL 6 TO MH 3 (2)
8 2	2	Curb	0.013	151.00	0.00 0.00 0.00	0.00 0.00 0.00	0.00 0.00 0.00	5.95	2.02	7.66	0.12	0.00 0.95	370.87	366.85 367.44 366.44	365.67 365.40 364.40	1.18 2.04	12 12 Cir	0.78 1.35	2.18 5.27	0.95 4.14	P - INL 7 TO EX INL 1
9 8	8	МН	0.013	20.000	0.00 0.00 0.00	0.00 0.00 0.00	0.00 0.00 0.00	5.57	0.38	7.79	0.09	0.00 0.67	371.25	367.19 367.85 366.85	366.94 367.65 366.65	0.25	12 12 Cir	1.24 1.00	3.16 4.53	0.67 3.56	P - MH 8 TO INL 7
10	9	Curb	0.013	30.000	0.00 0.00 0.00	0.00 0.00 0.00	0.00 0.00 0.00	5.00	0.57	7.99	0.09	0.00 0.69	370.56	367.70 368.35 367.35	367.35 368.05 367.05	0.35	12 12 Cir	1.16 1.00		0.69 3.56	P - INL 9 TO MH 8

NOTES: Intensity = 46.21 / (Inlet time + 9.30) ^ 0.66 (in/hr); Time of flow in section is based on full flow.

Project File: SPOTLESS STORM PIPE-Rev1.stm

# **FL-DOT Report**

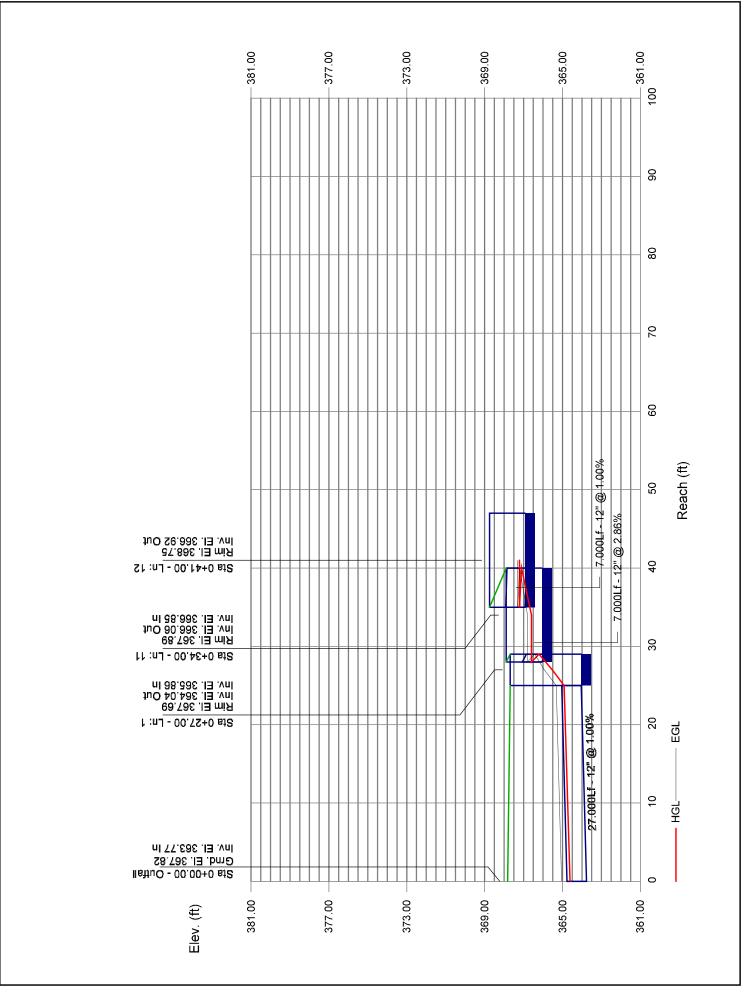
Line No	To Line	Type of	n - Value	Len	Draina	ge Area	ı	Time	Time	Inten	Total CA	Add	Inlet elev	Elev	of HGL		Rise	HGL	ADD		Date: 6/6/2025
NO	Line	struc	value			C1 = 0. C2 = 0.		of conc	of Flow in	(1)	CA	Q Total	elev	Elev	of Crown		Span	Pipe	Full l	low	Frequency: 100 yrs
						C3 = 0.			sect			Flow		Elev	of Invert						Proj: SPOTLESS STORM
					Incre- ment	Sub- Total	Sum CA					Q		Up	Down	Fall	Size	Slope	Vel	Сар	
				(ft)	(ac)	(ac)		(min)	(min)	(in/hr)		(cfs)	(ft)	(ft)	(ft)	(ft)	(in)	(%)	(ft/s)	(cfs)	Line description
11	1	Grate	0.013		0.00 0.00 0.00	0.00 0.00 0.00	0.00 0.00 0.00	5.19	0.06	7.92	0.20	0.00	367.89	366.59 367.06 366.06	366.21 366.86 365.86	0.38	12 12 Cir		5.07 7.66	1.56	P - TD 1 TO EX INL 1

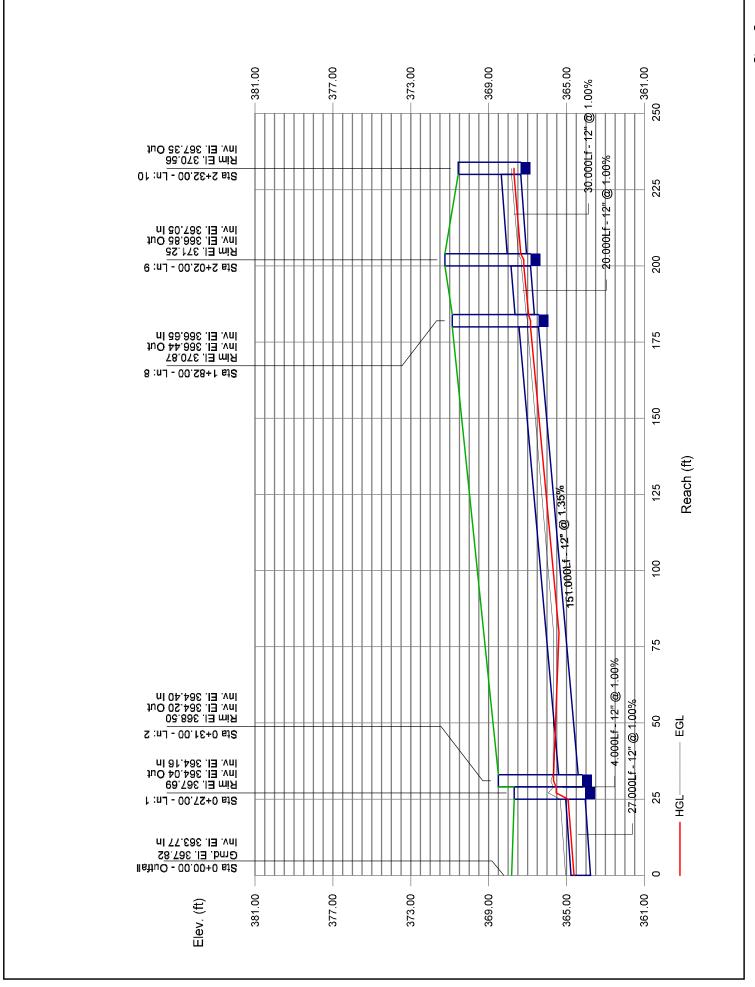
NOTES: Intensity = 46.21 / (Inlet time + 9.30) ^ 0.66 (in/hr); Time of flow in section is based on full flow.

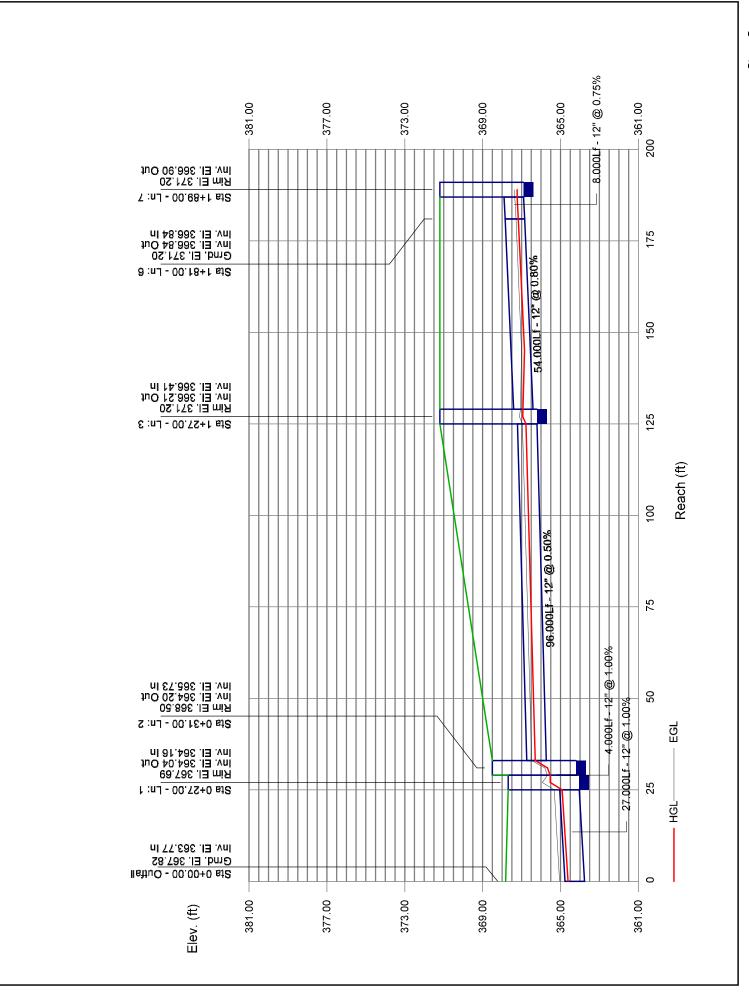
Project File: SPOTLESS STORM PIPE-Rev1.stm

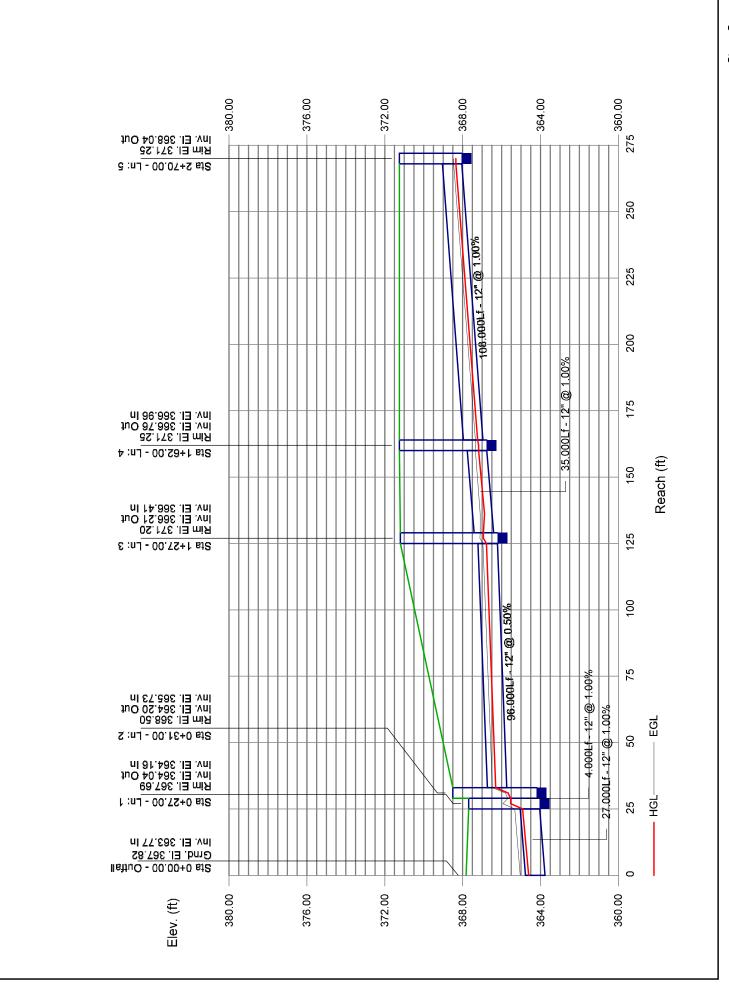
# **Storm Sewer Inlet Time Tabulation**

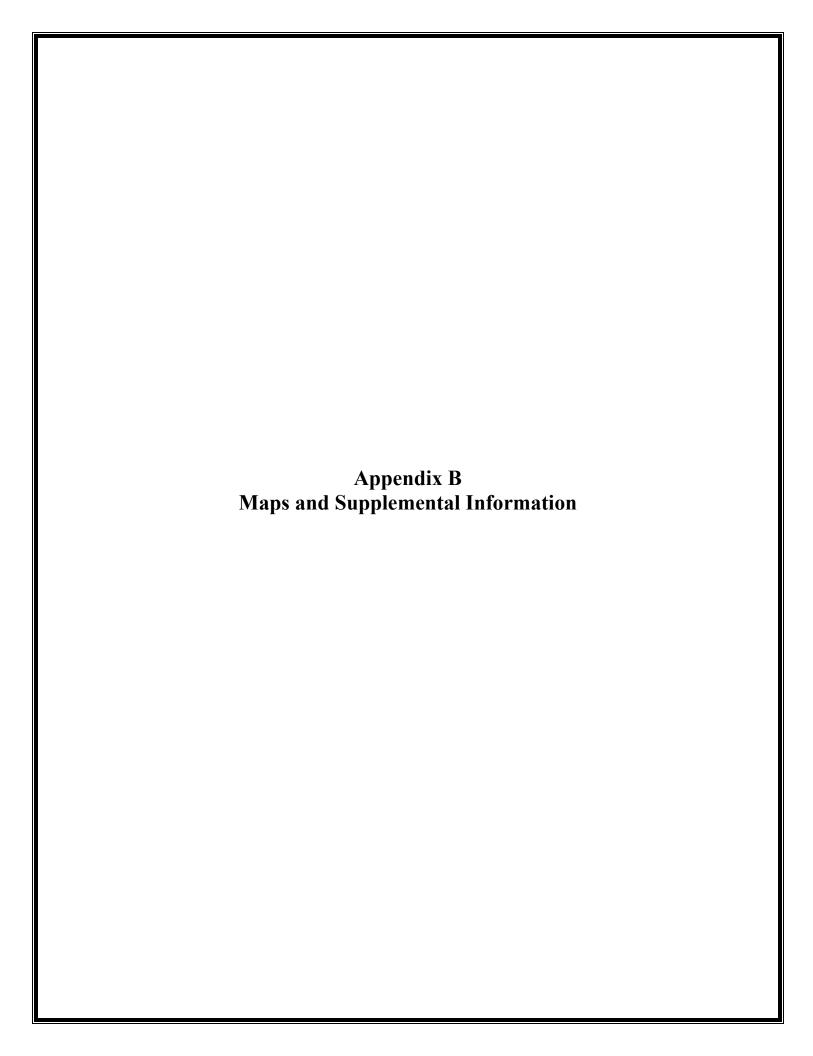
Line	Line ID	Тс		She	et Flow	,		Sha	allow Co	ncentrat	ed Flow				Cha	annel Flo	w			Total
No.			n- Value	flow Length (ft)	2-yr 24h P (in)	Slope	Travel Time (min)	flow Length (ft)	Water Slope (%)	Surf Descr	Ave Vel (ft/s)	Travel Time (min)	X-sec Area (sqft)	Wetted Perim (ft)	Chan Slope (%)	n- Value	Vel	flow Length (ft)	Travel Time (min)	Travel Time (min)
1	P - EX INL 1 TO E	User																		5.00
2	P - MH 7A TO EX	User																		0.00
3	P - MH 3 TO INL	User																		0.00
4	P - INL 4 TO MH	User																		5.00
5	P - INL 5 TO INL	User																		5.00
6	P - INL 6 TO MH	User																		0.00
7	P - INL 6 TO MH	User																		5.00
8	P - INL 7 TO EX I	User																		5.00
9	P - MH 8 TO INL	User																		0.00
10	P - INL 9 TO MH	User																		5.00
11	P - TD 1 TO EX I	User																		5.00
Projec	t File: SPOTLESS \$	STORM PI	│ PE-Rev1.	stm	l N	lin. Tc us	ed for inte	ensity calcu	 ulations =	5 min		1	Number of	lines: 11			Date:	6/6/2025		













### NOAA Atlas 14, Volume 2, Version 3 Location name: Chalfont, Pennsylvania, USA\* Latitude: 40.2661°, Longitude: -75.2283° Elevation: 375 ft\*\*

\* source: ESRI Maps \*\* source: USGS



### POINT PRECIPITATION FREQUENCY ESTIMATES

G.M. Bonnin, D. Martin, B. Lin, T. Parzybok, M.Yekta, and D. Riley NOAA, National Weather Service, Silver Spring, Maryland

PF tabular | PF graphical | Maps & aerials

### PF tabular

PDS-b	ased poir	nt precipit	ation freq	uency est	timates w	ith 90% co	onfidence	intervals	(in inches	s/hour) <sup>1</sup>
	-					ce interval (y			•	<u>,                                      </u>
Duration	1	2	5	10	25	50	100	200	500	1000
5-min	<b>4.06</b> (3.71-4.43)	<b>4.82</b> (4.42-5.28)	<b>5.68</b> (5.17-6.19)	<b>6.28</b> (5.72-6.86)	<b>7.01</b> (6.35-7.64)	<b>7.51</b> (6.77-8.21)	<b>7.99</b> (7.19-8.75)	<b>8.42</b> (7.54-9.24)	<b>8.94</b> (7.92-9.83)	<b>9.31</b> (8.20-10.3)
10-min	<b>3.24</b> (2.96-3.54)	<b>3.86</b> (3.53-4.22)	<b>4.54</b> (4.15-4.96)	<b>5.02</b> (4.58-5.48)	<b>5.59</b> (5.06-6.10)	<b>5.98</b> (5.39-6.53)	<b>6.35</b> (5.71-6.95)	<b>6.68</b> (5.97-7.32)	<b>7.07</b> (6.26-7.77)	<b>7.33</b> (6.46-8.08)
15-min	<b>2.70</b> (2.47-2.95)	<b>3.23</b> (2.96-3.54)	<b>3.83</b> (3.50-4.18)	<b>4.23</b> (3.86-4.62)	<b>4.72</b> (4.28-5.15)	<b>5.05</b> (4.55-5.52)	<b>5.36</b> (4.81-5.86)	<b>5.62</b> (5.02-6.16)	<b>5.93</b> (5.26-6.52)	<b>6.13</b> (5.40-6.76)
30-min	<b>1.85</b> (1.69-2.02)	<b>2.23</b> (2.04-2.44)	<b>2.72</b> (2.48-2.97)	<b>3.07</b> (2.80-3.35)	<b>3.50</b> (3.17-3.81)	<b>3.80</b> (3.43-4.15)	<b>4.10</b> (3.69-4.48)	<b>4.37</b> (3.91-4.79)	<b>4.72</b> (4.18-5.19)	<b>4.96</b> (4.37-5.48)
60-min	<b>1.15</b> (1.06-1.26)	<b>1.40</b> (1.28-1.53)	<b>1.74</b> (1.59-1.90)	<b>2.00</b> (1.82-2.18)	<b>2.33</b> (2.11-2.54)	<b>2.58</b> (2.32-2.81)	<b>2.82</b> (2.54-3.09)	<b>3.07</b> (2.74-3.36)	<b>3.38</b> (3.00-3.72)	<b>3.62</b> (3.19-4.00)
2-hr	<b>0.686</b> (0.625-0.753)	<b>0.833</b> (0.760-0.914)	<b>1.04</b> (0.948-1.14)	<b>1.20</b> (1.09-1.32)	<b>1.42</b> (1.28-1.55)	<b>1.59</b> (1.42-1.74)	<b>1.76</b> (1.57-1.93)	<b>1.94</b> (1.71-2.12)	<b>2.17</b> (1.90-2.39)	<b>2.35</b> (2.04-2.59)
3-hr	<b>0.500</b> (0.453-0.552)	<b>0.606</b> (0.550-0.669)	<b>0.760</b> (0.689-0.838)	<b>0.879</b> (0.794-0.968)	<b>1.04</b> (0.933-1.14)	<b>1.16</b> (1.04-1.28)	<b>1.30</b> (1.15-1.42)	<b>1.43</b> (1.25-1.57)	<b>1.60</b> (1.39-1.77)	<b>1.74</b> (1.50-1.93)
6-hr	<b>0.313</b> (0.286-0.347)	<b>0.379</b> (0.345-0.419)	<b>0.473</b> (0.430-0.523)	<b>0.550</b> (0.498-0.606)	<b>0.658</b> (0.591-0.725)	<b>0.746</b> (0.665-0.820)	<b>0.839</b> (0.741-0.922)	<b>0.937</b> (0.819-1.03)	<b>1.07</b> (0.924-1.18)	<b>1.18</b> (1.00-1.31)
12-hr	<b>0.189</b> (0.173-0.210)	<b>0.229</b> (0.209-0.254)	<b>0.288</b> (0.262-0.319)	<b>0.337</b> (0.305-0.373)	<b>0.409</b> (0.366-0.450)	<b>0.469</b> (0.417-0.517)	<b>0.535</b> (0.470-0.589)	<b>0.606</b> (0.525-0.668)	<b>0.710</b> (0.603-0.786)	<b>0.797</b> (0.666-0.883)
24-hr	<b>0.112</b> (0.103-0.122)	<b>0.135</b> (0.124-0.148)	<b>0.170</b> (0.156-0.185)	<b>0.199</b> (0.182-0.216)	<b>0.241</b> (0.219-0.261)	<b>0.276</b> (0.250-0.299)	<b>0.314</b> (0.283-0.340)	<b>0.356</b> (0.317-0.385)	<b>0.416</b> (0.367-0.450)	<b>0.466</b> (0.406-0.505)
2-day	<b>0.065</b> (0.059-0.071)	<b>0.078</b> (0.071-0.086)	<b>0.098</b> (0.089-0.108)	<b>0.115</b> (0.104-0.126)	<b>0.138</b> (0.125-0.152)	<b>0.158</b> (0.142-0.173)	<b>0.178</b> (0.159-0.195)	<b>0.200</b> (0.178-0.220)	<b>0.232</b> (0.204-0.254)	<b>0.258</b> (0.224-0.283)
3-day	<b>0.045</b> (0.041-0.050)	<b>0.055</b> (0.050-0.060)	<b>0.069</b> (0.063-0.075)	<b>0.080</b> (0.073-0.088)	<b>0.096</b> (0.087-0.105)	<b>0.109</b> (0.099-0.119)	<b>0.123</b> (0.111-0.135)	<b>0.138</b> (0.124-0.151)	<b>0.160</b> (0.141-0.174)	<b>0.177</b> (0.155-0.194)
4-day	<b>0.036</b> (0.033-0.039)	<b>0.043</b> (0.040-0.047)	<b>0.054</b> (0.049-0.059)	<b>0.063</b> (0.057-0.068)	<b>0.075</b> (0.068-0.082)	<b>0.085</b> (0.077-0.093)	<b>0.096</b> (0.087-0.104)	<b>0.107</b> (0.097-0.117)	<b>0.123</b> (0.110-0.134)	<b>0.136</b> (0.121-0.149)
7-day	<b>0.024</b> (0.022-0.026)	<b>0.028</b> (0.026-0.031)	<b>0.035</b> (0.033-0.038)	<b>0.041</b> (0.038-0.044)	<b>0.049</b> (0.045-0.053)	<b>0.055</b> (0.051-0.060)	<b>0.062</b> (0.057-0.067)	<b>0.069</b> (0.063-0.075)	<b>0.080</b> (0.072-0.087)	<b>0.088</b> (0.079-0.096)
10-day	<b>0.019</b> (0.017-0.020)	<b>0.022</b> (0.021-0.024)	<b>0.027</b> (0.025-0.030)	<b>0.031</b> (0.029-0.034)	<b>0.037</b> (0.034-0.040)	<b>0.041</b> (0.038-0.045)	<b>0.046</b> (0.042-0.050)	<b>0.051</b> (0.046-0.055)	<b>0.057</b> (0.052-0.062)	<b>0.063</b> (0.057-0.068)
20-day	<b>0.012</b> (0.012-0.013)	<b>0.015</b> (0.014-0.016)	<b>0.018</b> (0.017-0.019)	<b>0.020</b> (0.019-0.022)	<b>0.023</b> (0.022-0.025)	<b>0.026</b> (0.024-0.028)	<b>0.028</b> (0.026-0.030)	<b>0.031</b> (0.028-0.033)	<b>0.034</b> (0.031-0.036)	<b>0.036</b> (0.033-0.039)
30-day	<b>0.010</b> (0.010-0.011)	<b>0.012</b> (0.011-0.013)	<b>0.014</b> (0.013-0.015)	<b>0.016</b> (0.015-0.017)	<b>0.018</b> (0.017-0.019)	<b>0.020</b> (0.018-0.021)	<b>0.021</b> (0.020-0.022)	<b>0.023</b> (0.021-0.024)	<b>0.025</b> (0.023-0.026)	<b>0.026</b> (0.024-0.028)
45-day	<b>0.009</b> (0.008-0.009)	<b>0.010</b> (0.010-0.011)	<b>0.012</b> (0.011-0.012)	<b>0.013</b> (0.012-0.014)	<b>0.014</b> (0.014-0.015)	<b>0.016</b> (0.015-0.016)	<b>0.017</b> (0.016-0.018)	<b>0.018</b> (0.017-0.019)	<b>0.019</b> (0.018-0.020)	<b>0.020</b> (0.018-0.021)
60-day	<b>0.008</b> (0.007-0.008)	<b>0.009</b> (0.009-0.010)	<b>0.010</b> (0.010-0.011)	<b>0.011</b> (0.011-0.012)	<b>0.013</b> (0.012-0.013)	<b>0.014</b> (0.013-0.014)	<b>0.014</b> (0.014-0.015)	<b>0.015</b> (0.014-0.016)	<b>0.016</b> (0.015-0.017)	<b>0.017</b> (0.016-0.018)

<sup>&</sup>lt;sup>1</sup> Precipitation frequency (PF) estimates in this table are based on frequency analysis of partial duration series (PDS).

Numbers in parenthesis are PF estimates at lower and upper bounds of the 90% confidence interval. The probability that precipitation frequency estimates (for a given duration and average recurrence interval) will be greater than the upper bound (or less than the lower bound) is 5%. Estimates at upper bounds are not checked against probable maximum precipitation (PMP) estimates and may be higher than currently valid PMP values.

Please refer to NOAA Atlas 14 document for more information.

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# PF graphical



NRCS Natural

Natural Resources Conservation Service A product of the National Cooperative Soil Survey, a joint effort of the United States Department of Agriculture and other Federal agencies, State agencies including the Agricultural Experiment Stations, and local participants

# Custom Soil Resource Report for Bucks County, Pennsylvania



# **Preface**

Soil surveys contain information that affects land use planning in survey areas. They highlight soil limitations that affect various land uses and provide information about the properties of the soils in the survey areas. Soil surveys are designed for many different users, including farmers, ranchers, foresters, agronomists, urban planners, community officials, engineers, developers, builders, and home buyers. Also, conservationists, teachers, students, and specialists in recreation, waste disposal, and pollution control can use the surveys to help them understand, protect, or enhance the environment.

Various land use regulations of Federal, State, and local governments may impose special restrictions on land use or land treatment. Soil surveys identify soil properties that are used in making various land use or land treatment decisions. The information is intended to help the land users identify and reduce the effects of soil limitations on various land uses. The landowner or user is responsible for identifying and complying with existing laws and regulations.

Although soil survey information can be used for general farm, local, and wider area planning, onsite investigation is needed to supplement this information in some cases. Examples include soil quality assessments (http://www.nrcs.usda.gov/wps/portal/nrcs/main/soils/health/) and certain conservation and engineering applications. For more detailed information, contact your local USDA Service Center (https://offices.sc.egov.usda.gov/locator/app?agency=nrcs) or your NRCS State Soil Scientist (http://www.nrcs.usda.gov/wps/portal/nrcs/detail/soils/contactus/?cid=nrcs142p2 053951).

Great differences in soil properties can occur within short distances. Some soils are seasonally wet or subject to flooding. Some are too unstable to be used as a foundation for buildings or roads. Clayey or wet soils are poorly suited to use as septic tank absorption fields. A high water table makes a soil poorly suited to basements or underground installations.

The National Cooperative Soil Survey is a joint effort of the United States Department of Agriculture and other Federal agencies, State agencies including the Agricultural Experiment Stations, and local agencies. The Natural Resources Conservation Service (NRCS) has leadership for the Federal part of the National Cooperative Soil Survey.

Information about soils is updated periodically. Updated information is available through the NRCS Web Soil Survey, the site for official soil survey information.

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# **How Soil Surveys Are Made**

Soil surveys are made to provide information about the soils and miscellaneous areas in a specific area. They include a description of the soils and miscellaneous areas and their location on the landscape and tables that show soil properties and limitations affecting various uses. Soil scientists observed the steepness, length, and shape of the slopes; the general pattern of drainage; the kinds of crops and native plants; and the kinds of bedrock. They observed and described many soil profiles. A soil profile is the sequence of natural layers, or horizons, in a soil. The profile extends from the surface down into the unconsolidated material in which the soil formed or from the surface down to bedrock. The unconsolidated material is devoid of roots and other living organisms and has not been changed by other biological activity.

Currently, soils are mapped according to the boundaries of major land resource areas (MLRAs). MLRAs are geographically associated land resource units that share common characteristics related to physiography, geology, climate, water resources, soils, biological resources, and land uses (USDA, 2006). Soil survey areas typically consist of parts of one or more MLRA.

The soils and miscellaneous areas in a survey area occur in an orderly pattern that is related to the geology, landforms, relief, climate, and natural vegetation of the area. Each kind of soil and miscellaneous area is associated with a particular kind of landform or with a segment of the landform. By observing the soils and miscellaneous areas in the survey area and relating their position to specific segments of the landform, a soil scientist develops a concept, or model, of how they were formed. Thus, during mapping, this model enables the soil scientist to predict with a considerable degree of accuracy the kind of soil or miscellaneous area at a specific location on the landscape.

Commonly, individual soils on the landscape merge into one another as their characteristics gradually change. To construct an accurate soil map, however, soil scientists must determine the boundaries between the soils. They can observe only a limited number of soil profiles. Nevertheless, these observations, supplemented by an understanding of the soil-vegetation-landscape relationship, are sufficient to verify predictions of the kinds of soil in an area and to determine the boundaries.

Soil scientists recorded the characteristics of the soil profiles that they studied. They noted soil color, texture, size and shape of soil aggregates, kind and amount of rock fragments, distribution of plant roots, reaction, and other features that enable them to identify soils. After describing the soils in the survey area and determining their properties, the soil scientists assigned the soils to taxonomic classes (units). Taxonomic classes are concepts. Each taxonomic class has a set of soil characteristics with precisely defined limits. The classes are used as a basis for comparison to classify soils systematically. Soil taxonomy, the system of taxonomic classification used in the United States, is based mainly on the kind and character of soil properties and the arrangement of horizons within the profile. After the soil

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scientists classified and named the soils in the survey area, they compared the individual soils with similar soils in the same taxonomic class in other areas so that they could confirm data and assemble additional data based on experience and research.

The objective of soil mapping is not to delineate pure map unit components; the objective is to separate the landscape into landforms or landform segments that have similar use and management requirements. Each map unit is defined by a unique combination of soil components and/or miscellaneous areas in predictable proportions. Some components may be highly contrasting to the other components of the map unit. The presence of minor components in a map unit in no way diminishes the usefulness or accuracy of the data. The delineation of such landforms and landform segments on the map provides sufficient information for the development of resource plans. If intensive use of small areas is planned, onsite investigation is needed to define and locate the soils and miscellaneous areas.

Soil scientists make many field observations in the process of producing a soil map. The frequency of observation is dependent upon several factors, including scale of mapping, intensity of mapping, design of map units, complexity of the landscape, and experience of the soil scientist. Observations are made to test and refine the soil-landscape model and predictions and to verify the classification of the soils at specific locations. Once the soil-landscape model is refined, a significantly smaller number of measurements of individual soil properties are made and recorded. These measurements may include field measurements, such as those for color, depth to bedrock, and texture, and laboratory measurements, such as those for content of sand, silt, clay, salt, and other components. Properties of each soil typically vary from one point to another across the landscape.

Observations for map unit components are aggregated to develop ranges of characteristics for the components. The aggregated values are presented. Direct measurements do not exist for every property presented for every map unit component. Values for some properties are estimated from combinations of other properties.

While a soil survey is in progress, samples of some of the soils in the area generally are collected for laboratory analyses and for engineering tests. Soil scientists interpret the data from these analyses and tests as well as the field-observed characteristics and the soil properties to determine the expected behavior of the soils under different uses. Interpretations for all of the soils are field tested through observation of the soils in different uses and under different levels of management. Some interpretations are modified to fit local conditions, and some new interpretations are developed to meet local needs. Data are assembled from other sources, such as research information, production records, and field experience of specialists. For example, data on crop yields under defined levels of management are assembled from farm records and from field or plot experiments on the same kinds of soil.

Predictions about soil behavior are based not only on soil properties but also on such variables as climate and biological activity. Soil conditions are predictable over long periods of time, but they are not predictable from year to year. For example, soil scientists can predict with a fairly high degree of accuracy that a given soil will have a high water table within certain depths in most years, but they cannot predict that a high water table will always be at a specific level in the soil on a specific date.

After soil scientists located and identified the significant natural bodies of soil in the survey area, they drew the boundaries of these bodies on aerial photographs and

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identified each as a specific map unit. Aerial photographs show trees, buildings, fields, roads, and rivers, all of which help in locating boundaries accurately.

# Soil Map

The soil map section includes the soil map for the defined area of interest, a list of soil map units on the map and extent of each map unit, and cartographic symbols displayed on the map. Also presented are various metadata about data used to produce the map, and a description of each soil map unit.



#### MAP LEGEND

#### Area of Interest (AOI)

Area of Interest (AOI)

#### Soils

Soil Map Unit Polygons



Soil Map Unit Lines



Soil Map Unit Points

#### Special Point Features

Blowout



Clay Spot

Closed Depression

Gravel Pit

Gravelly Spot

Landfill
A Lava Flow

Mine or Quarry

Miscellaneous Water

Perennial Water

Rock Outcrop

+ Saline Spot

Sandy Spot

Severely Eroded Spot

Sinkhole

Slide or Slip

Sodic Spot

#### \_\_\_\_\_

۵

Spoil Area Stony Spot



Very Stony Spot



Wet Spot Other



Special Line Features

#### Water Features

~

Streams and Canals

#### Transportation

+++ Rails

Interstate Highways

.

US Routes
Major Roads

~

Local Roads

#### **Background**

1

Aerial Photography

#### MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:24.000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service

Web Soil Survey URL:

Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Bucks County, Pennsylvania Survey Area Data: Version 20, Sep 4, 2023

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Jun 3, 2022—Jul 20, 2022

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

## Map Unit Legend

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
UfuB	Urban land, 0 to 8 percent slopes	1.3	100.0%
Totals for Area of Interest		1.3	100.0%

# **Map Unit Descriptions**

The map units delineated on the detailed soil maps in a soil survey represent the soils or miscellaneous areas in the survey area. The map unit descriptions, along with the maps, can be used to determine the composition and properties of a unit.

A map unit delineation on a soil map represents an area dominated by one or more major kinds of soil or miscellaneous areas. A map unit is identified and named according to the taxonomic classification of the dominant soils. Within a taxonomic class there are precisely defined limits for the properties of the soils. On the landscape, however, the soils are natural phenomena, and they have the characteristic variability of all natural phenomena. Thus, the range of some observed properties may extend beyond the limits defined for a taxonomic class. Areas of soils of a single taxonomic class rarely, if ever, can be mapped without including areas of other taxonomic classes. Consequently, every map unit is made up of the soils or miscellaneous areas for which it is named and some minor components that belong to taxonomic classes other than those of the major soils.

Most minor soils have properties similar to those of the dominant soil or soils in the map unit, and thus they do not affect use and management. These are called noncontrasting, or similar, components. They may or may not be mentioned in a particular map unit description. Other minor components, however, have properties and behavioral characteristics divergent enough to affect use or to require different management. These are called contrasting, or dissimilar, components. They generally are in small areas and could not be mapped separately because of the scale used. Some small areas of strongly contrasting soils or miscellaneous areas are identified by a special symbol on the maps. If included in the database for a given area, the contrasting minor components are identified in the map unit descriptions along with some characteristics of each. A few areas of minor components may not have been observed, and consequently they are not mentioned in the descriptions, especially where the pattern was so complex that it was impractical to make enough observations to identify all the soils and miscellaneous areas on the landscape.

The presence of minor components in a map unit in no way diminishes the usefulness or accuracy of the data. The objective of mapping is not to delineate pure taxonomic classes but rather to separate the landscape into landforms or landform segments that have similar use and management requirements. The delineation of such segments on the map provides sufficient information for the development of resource plans. If intensive use of small areas is planned, however, onsite investigation is needed to define and locate the soils and miscellaneous areas.

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An identifying symbol precedes the map unit name in the map unit descriptions. Each description includes general facts about the unit and gives important soil properties and qualities.

Soils that have profiles that are almost alike make up a *soil series*. Except for differences in texture of the surface layer, all the soils of a series have major horizons that are similar in composition, thickness, and arrangement.

Soils of one series can differ in texture of the surface layer, slope, stoniness, salinity, degree of erosion, and other characteristics that affect their use. On the basis of such differences, a soil series is divided into *soil phases*. Most of the areas shown on the detailed soil maps are phases of soil series. The name of a soil phase commonly indicates a feature that affects use or management. For example, Alpha silt loam, 0 to 2 percent slopes, is a phase of the Alpha series.

Some map units are made up of two or more major soils or miscellaneous areas. These map units are complexes, associations, or undifferentiated groups.

A *complex* consists of two or more soils or miscellaneous areas in such an intricate pattern or in such small areas that they cannot be shown separately on the maps. The pattern and proportion of the soils or miscellaneous areas are somewhat similar in all areas. Alpha-Beta complex, 0 to 6 percent slopes, is an example.

An *association* is made up of two or more geographically associated soils or miscellaneous areas that are shown as one unit on the maps. Because of present or anticipated uses of the map units in the survey area, it was not considered practical or necessary to map the soils or miscellaneous areas separately. The pattern and relative proportion of the soils or miscellaneous areas are somewhat similar. Alpha-Beta association, 0 to 2 percent slopes, is an example.

An *undifferentiated group* is made up of two or more soils or miscellaneous areas that could be mapped individually but are mapped as one unit because similar interpretations can be made for use and management. The pattern and proportion of the soils or miscellaneous areas in a mapped area are not uniform. An area can be made up of only one of the major soils or miscellaneous areas, or it can be made up of all of them. Alpha and Beta soils, 0 to 2 percent slopes, is an example.

Some surveys include *miscellaneous areas*. Such areas have little or no soil material and support little or no vegetation. Rock outcrop is an example.

#### **Bucks County, Pennsylvania**

#### UfuB—Urban land, 0 to 8 percent slopes

#### **Map Unit Setting**

National map unit symbol: 17sq Elevation: 800 to 1,500 feet

Mean annual precipitation: 36 to 46 inches Mean annual air temperature: 41 to 62 degrees F

Frost-free period: 130 to 170 days

Farmland classification: Not prime farmland

#### **Map Unit Composition**

Urban land: 90 percent

Minor components: 10 percent

Estimates are based on observations, descriptions, and transects of the mapunit.

#### **Description of Urban Land**

#### Setting

Parent material: Pavement, buildings and other artifically covered areas human transported material

#### Interpretive groups

Land capability classification (irrigated): None specified

Land capability classification (nonirrigated): 8s

Hydric soil rating: No

#### **Minor Components**

#### Udorthents, unstable fill

Percent of map unit: 10 percent Down-slope shape: Linear Across-slope shape: Linear Hydric soil rating: No

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United States Department of Agriculture, Natural Resources Conservation Service. National range and pasture handbook. http://www.nrcs.usda.gov/wps/portal/nrcs/detail/national/landuse/rangepasture/?cid=stelprdb1043084

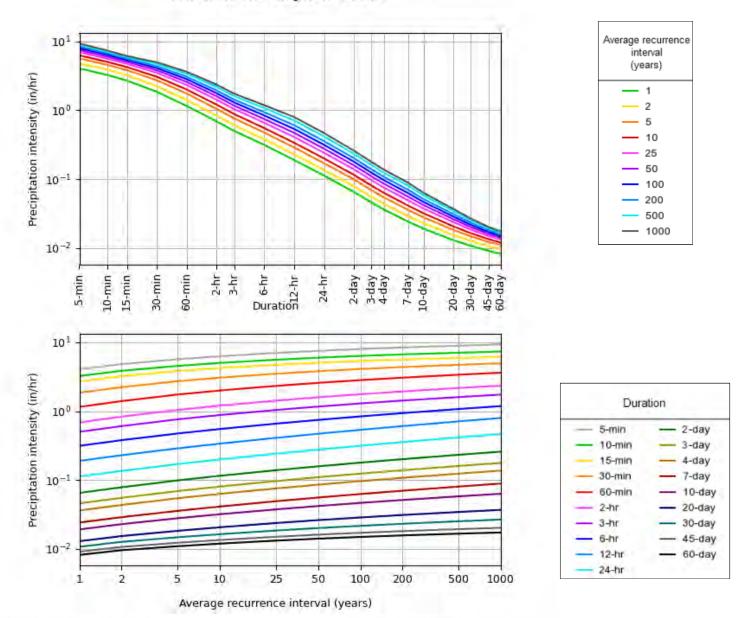
#### Custom Soil Resource Report

United States Department of Agriculture, Natural Resources Conservation Service. National soil survey handbook, title 430-VI. http://www.nrcs.usda.gov/wps/portal/nrcs/detail/soils/scientists/?cid=nrcs142p2\_054242

United States Department of Agriculture, Natural Resources Conservation Service. 2006. Land resource regions and major land resource areas of the United States, the Caribbean, and the Pacific Basin. U.S. Department of Agriculture Handbook 296. http://www.nrcs.usda.gov/wps/portal/nrcs/detail/national/soils/?cid=nrcs142p2\_053624

United States Department of Agriculture, Soil Conservation Service. 1961. Land capability classification. U.S. Department of Agriculture Handbook 210. http://www.nrcs.usda.gov/Internet/FSE\_DOCUMENTS/nrcs142p2\_052290.pdf

#### PDS-based intensity-duration-frequency (IDF) curves Latitude: 40.2661°, Longitude: -75.2283°



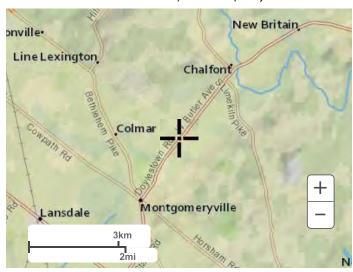
NOAA Atlas 14, Volume 2, Version 3

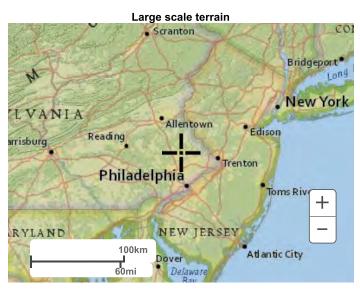
Created (GMT): Wed Sep 27 20:25:01 2023

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#### Maps & aerials

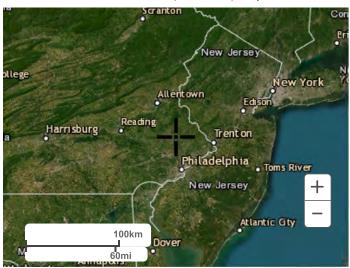
Small scale terrain







Large scale aerial



Back to Top

US Department of Commerce
National Oceanic and Atmospheric Administration
National Weather Service
National Water Center
1325 East West Highway
Silver Spring, MD 20910
Questions?: HDSC.Questions@noaa.gov

**Disclaimer** 



**Date:** 11/26/2024

**Subject:** Highway Occupancy Permit Application No. 357565, Cycle No.1 - Returned For

Revisions

**To:** Spotless Brands

950 Herndon Parkway

Suite 400

Herdon, VA 20170

**From:** PennDOT Engineering District 6-0

7000 Geerdes Boulevard King of Prussia, PA 19406

#### Dear Applicant,

PennDOT has reviewed your application for completeness, consistency and compliance with applicable Department Regulations. This review has identified issues that must be addressed in order for our review to continue.

The Department's review comments are attached.

Once the comments have been addressed, please resubmit the application and associated material for further review.

Upon resubmission, the applicant's engineer should put together a letter that describes how each comment has been addressed and where each can be found. This will help expedite the review. For guidance on HOP applications refer to 67 PA Code, Chapter 441, Chapter 459 and PennDOT Publication 282, "Highway Occupancy Permit Guidelines". Additional comments may follow upon review of the resubmitted application.

As an alternative to contacting the individual indicated below, the District is offering applicants an opportunity to attend HOP Collaboration Workshop Meetings (held weekly) for clarification on any of the following comments. To sign up, please open the HOP Collaboration Workshop link located in the Links section on the EPS Applications Portal and follow the instructions.

If you have any questions regarding this matter, you may contact Erik Schmidt, PE, PTOE, at (215) 592-4200.



#### **Response Comments**

**Date:** 11/26/2024

**Application Number:** 357565, Cycle No.1

#### **Form Letter Notes**

- (1) \* Upon resubmission, the applicant's engineer must provide a letter that describes how each comment has been addressed and where each can be found in the plan set. A copy of these comments and any previously submitted plans should also be provided. Further, if the applicants engineer has made any design changes independent of PennDOT reviewer comments, they must include a section in the response-to-comments letter that describes the additional design changes.
  - \* Additional comments may follow upon review of the resubmitted application. If you have any questions about the technical aspects of this review, please contact the Department's representative, Erik Schmidt, PE, PTOE of McCormick Taylor, Inc. at ECSchmidt@mccormicktaylor.com. Please reference the HOP Application number in all correspondence.
  - \* For guidance on Highway Occupancy Permit applications, refer to PA Code Title 67, Chapter 441, Chapter 459 and PennDOT Publication 282. This will help expedite the review.

#### General

- (1) PLEASE SUBMIT A CHECK FOR \$50.00 MADE PAYABLE TO PENNDOT-ATTN: MARY ELLEN CULHANE, PERMITS SUPERVISOR, 7000 GEERDES BLVD. KING OF PRUSSIA, PA. 19406. PLEASE INCLUDE THE APPLICATION NUMBER ON THE CHECK FOR OUR REFERENCE.
- (2) Comments for this review are only associated with the plans and documents attached for this cycle. Additional comments may follow upon review of the updated plans and documents of the resubmitted application.
- (3) The Department requires that a cost estimate be entered into EPS before HOP issuance. This cost estimate must include all work proposed within the Departments Right-of-Way, including signal equipment, all roadway work, pavement markings, and signage, plus a 15% contingency to be entered into the Fee information & Estimate Construction Cost section in the Application setup box. Further, this section requires the cost estimate to be broken down into site frontage costs and off-site improvements costs. Please enter this information in the manner required. This cost estimate will be the basis for the value of the Letter of Credit the Department will require as security prior to the start of actual construction.

- (4) The Department requires written evidence, prior to issuing a permit, that New Britain Township has had the opportunity to comment. Provide written evidence (e.g., municipal engineering review, council or planning commission meeting minutes, executed TE-160, etc.), which is less than one year old, to satisfy this requirement. Also, provide County Planning Commission Review Letters if available. PennDOT will consider any comments or recommendations from the municipal and county reviews before approving the access permit. In addition, please provide the email address(es) for the appropriate individual(s) at the municipality (i.e., Township Manager, Borough Manager, Mayor, etc.) and at the County Planning Commission on the Application Information screen so that a copy of the Department's review letter can be forwarded. (PA Code, Title 67, Chapter 441.3(j)).
- (5) Submit HOP Plans for review.

#### **Application**

(1) Consistent with current Department Policy, applicants for Highway Occupancy Permits must apply for an EPS Business Partner ID (BPID). The EPS BPID is to be used in the establishment of a billing account for the invoicing of inspection costs. After an EPS BPID is obtained and activated by the applicant's system administrator, a user ID will then need to be created to ensure that the EPS BPID is integrated into EPS and searchable through the "looking glass" feature. Once this has been established, please provide the following information in the Applicant Contact Info area under "Applicant Team":

#### BPID;

Contact information (name/title/address/phone/email) for a general contact person who typically deals with the Highway Occupancy Permit application process;

Contact information (name/title/address/phone/email) for a billing contact person who typically deals with the Highway Occupancy Permit invoicing process.

For information on obtaining an EPS BPID, you may visit:

https://www.epermittingsyst.penndot.gov/EPS/home/manageBPRegistration.jsp (follow the instructions in the PINK area) or contact the ECMS Help Desk at 717.783.8330. Having an ECMS BPID does not guarantee the establishment of an EPS BPID as they are not reciprocal to one another.

(2) PennDOT Form M-950AA, Applicant's Authorization for Agent to Apply for Highway Occupancy

Permit, must be completed and submitted with all Highway Occupancy Permit applications.

#### Transportation Impact Study/Transportation Impact Assessment

(1) The Traffic Services Division has reviewed the submitted Transportation Impact Assessment (TIA) prepared by Albert Federico Consulting, LLC, dated October 29, 2024, and has no further comments. This is based on the presented land use, trip generation, design horizon year, and proposed mitigation. Any modifications to these parameters will require a revision of the TIS and additional review by the Department. This will be a condition of the permit.

#### Access Configuration/Profile- Driveways/Local Roads

(1) Truck turning templates must be provided for the largest vehicle anticipated to access the site (all movements) to ensure that the driveways can safely accommodate the design vehicle. Additionally, the templates must show how these vehicles will circulate within the site. For each site access, if the largest permitted vehicle type to utilize that access is smaller than a WB-67, then a note must be included on the plans, turning templates for that vehicle size provided, appropriate signage provided, and that vehicle size will become a condition of the permit and be recorded.

#### **Sight Distance- Driveways/Local Roads**

(1) As per Title 67, Chapter 441.8(h)(2)(iv), the Safe Stopping Sight Distance is the absolute minimum acceptable sight distance for driveways. Sight distance values obtained from the use of reduction factors which fall below the Safe Stopping Sight Distance will not be accepted by the Department. It is the designers responsibility to ensure that this minimum requirement is satisfied. Provide full documentation of sight distance measurements and calculations at the proposed site driveways. Ensure that the available sight distance measurement is documented and indicate the roadway grade used in calculations.

#### **ADA Compliance**

(1) Ensure that an accessible pedestrian path is provided between the frontage sidewalk and the proposed building. ADA compliance within the limits of work must be evaluated in the final design.

#### Utilities

(1) Department records indicate that PennDOT Fiber Optic facilities are present within the proposed limits of work and may be impacted by the anticipated improvements. If fiber optic facilities are affected, the applicant is responsible for the relocation of the fiber, testing of the fiber communications both before and after the relocation of the fiber indicated on the approved plans,

and the associated costs. Any relocation must be performed by a communications contractor that is acceptable to the District Traffic Unit. The Highway Occupancy Permit will not be issued until the fiber optic impact has been reviewed and the relocation scheme approved by the Department.



ENGINEERING & CONSULTING SERVICES

June 17, 2025

File No. 220705201

Doreen Curtin, Zoning Officer New Britain Township 207 Park Avenue Chalfont, PA 18914

Reference: 545 W. Butler Avenue (Spotless Carwash) – Preliminary/ Final Plan Review 2

TMP #26-006-101-001

Dear Doreen:

Pursuant to your request, Gilmore & Associates, Inc. has reviewed the revised Preliminary Land Development Plan for the above-referenced project. Accordingly, we offer the following comments for consideration:

#### I. Submission

- A. Preliminary/Final Land Development Plans for Flagship Carwash, as prepared by Van Cleef Engineering Associates, LLC, consisting of sixteen (16) sheets, dated April 3, 2025, last revised June 3, 3025.
- B. Post Construction Stormwater Conveyance Calculation for Flagship Carwash, as prepared by Van Cleef Engineering Associates, LLC, dated April 3, 2025, last revised June 3, 2025.
- C. Supplemental Traffic data received June 12, 2025 from Albert Federico Consulting, LLCWaiver Request letter for Flagship Car Wash, as prepared by Van Cleef Engineering Associates, LLC, dated April 3, 2025, revised June 6, 2025.

#### II. Reference Documentation

A. Zoning Hearing Board Decision of Spotless Brands, LLC, dated March 4, 2025.

#### III. General Description

The 0.65-acre parcel is located at the intersection of West Butler Avenue (S.R. 4202) and County Line Road (S.R. 2038) within New Britain Township's C-1 Commercial Zoning District and Butler Corridor Overlay District. The site previously contained Wawa and is now vacant with a 24-space parking lot and full access from both West Butler Avenue and County Line Road. The site is currently at maximum 75% permitted impervious area for a lot in the C-1 Zoning District.

The Applicant, Flagship Pennsylvania Propco LLC, proposes to remove the existing building and construct a 2,904-sf drive-through car wash, a J19 Use, which is permitted by-right in the C-1 Zoning District. The plan shows a drive-through, 16-space parking area with 15 vacuums and 3 additional employee parking spaces. A double lane is proposed for queuing and a bypass lane provided as requested by the Township. The existing access from County Line Road is proposed to be modified to a right-in/ right-out access while the West Butler Avenue access is proposed to be eliminated. The plan includes a possible gateway sign along with Butler Corridor Overlay features. The project is exempt from stormwater management due to the amount of impervious surface area not being increased.

#### IV. Review Comments

#### A. Subdivision and Land Development Ordinance Waivers

The following waivers have been formally requested by the Applicant:

- 1. §22-403 From providing separate preliminary and final submissions.
- 2. §22-706.1.D & §22-708.6.I From the requirement that all curbs have a minimum 5-foot radius, which we support.
- 3. §22-708.2.F From the requirement to provide 6-foot-wide sidewalk adjacent to parking stalls. The sidewalk along the building has a width of 4.33 feet. We have no objection to this waiver request conditioned upon the Applicant providing wheel stops for these spaces to prevent overhang.
- 4. §22-711.4.C From the requirement that the top and bottom of slopes be a minimum of five (5) feet from the property line. The proposed grading extends to the northeastern and northwestern property lines. We have no objection to this waiver.
- 5. §22-712.5.E From the requirement that storm sewers be reinforced concrete pipe with a minimum diameter of 15 inches. The plan proposes 12-inch HDPE pipe within the site. We have no objection to this waiver as the submitted calculations indicate that the 12-inch storm pipe is adequate to convey the anticipated flows.
- 6. §22-722.4.C.(1)(a)&(b) and (2) From the Butler Avenue Corridor Overlay District 25-foot setback requirement for parking areas and interior drives from Butler Avenue and 10-foot setback from other street lines. The closest drive-through lane is 8 feet from the Butler Avenue legal right-of-way, where the existing parking lot is 20 feet, and 8 feet from County Line Road. We have no objection to this waiver.
- 7. §22-722.4.C.(7)(b) From the requirement that all street trees be planted in the planting strip between the curbline and the sidewalk. The plan proposes to install the street trees between the sidewalk and the Butler Avenue Corridor Overlay District buffer fence as requested by the Township. We have no objection to this waiver.
- 8. §22-722.4.D.(6) From the requirement to provide bicycle parking, which we support.

#### B. Subdivision and Land Development Ordinance

We offer the following comments with respect to the current New Britain Township Subdivision and Land Development Ordinance:

- 1. §22-401.8 The Township shall confirm that the applicant sent out the project and Planning Commission Meeting notification to properties within 1,000 feet of the proposed development.
- 2. §22-502.1 A review of the plan standards and technical information will be provided directly to the Design Engineer under separate cover.
- 3. §22-704 The Existing Features/ Demolition Plan notes that a portion of the existing driveway pavement for TMP 26-6-101-2 that extends onto the subject tract is to be removed. In addition, the business sign for this adjacent property appears to be on the subject property as well. The Applicant should coordinate with the adjacent owner on the removal or relocation of these features, or an easement should be provided for these features to remain.
- 4. §22-705 All plans that require access to a street or highway under the jurisdiction of PennDOT shall require a Highway Occupancy Permit. Any documentation submitted as part of the HOP process shall be simultaneously submitted to the municipality for review purposes.
- 5. §22-710.2.D. The water supply for fire protection shall be approved by the Fire Marshall. We note that the domestic and fire services are required as the building is required to be sprinklered.

- 6. §22-711.4 A retaining wall is proposed along the northwestern property line with an exposed wall height over 4 feet at the middle. The following comments should be addressed:
  - a. The Site Plan shows the proposed fence 1.5' from the face of curb. The proposed split rail fence does not provide a sufficient barrier at the edge of the retaining wall. We recommend a guiderail, concrete wheel stops, or other edge protection be considered to protect vehicles.
  - b. The retaining wall has a maximum height of 4 feet and building code requires fall protection for an elevation change exceeding 30 inches. Appropriate fall protection shall be provided.
  - c. Retaining wall specifications and design details shall be approved by the Township Engineer prior to final plan approval. The wall design should consider curb cross-section and paving sections if they overlap the geogrid, and other features such as vacuums to verify that the wall can be constructed as designed. Similarly, the proposed storm pipe between Manhole MH#7A and Inlet INL#7B is directly behind the retaining wall and may conflict with the wall reinforcement. We recommend the design engineer consider realigning the pipe connections to avoid this conflict.
- 7. §22-714.7 Medium activity nonresidential parking areas shall have a minimum footcandle coverage of 0.4 and an 4:1 avg:min ratio. The Calculation Summary on the Lighting Plan notes a minimum footcandle coverage of 0.1 and an avg:min ratio of 21 for the parking circulation area. In addition, two parking spaces on the northwestern side of the parking lot appear to have minimal coverage. The lighting should be revised to meet the minimum and uniformity requirements.
- 8. §§22-715.2.C.(1) & G(2) The amount of land dedicated for park and recreation areas for nonresidential subdivisions shall be 2,500 square feet per 4,000 square feet of building area unless a fee-in-lieu of land is offered. Based on a proposed building area of 2,904 square feet at a rate of \$2,500 per 4,000 s.f., a fee of \$1,815.00 is required.
- 9. §22-720 A PADEP Sewage Facilities Planning Module shall be submitted for review.
- 10. §22-722.4.B.(2)&(4) The subject property is within the Township's Butler Avenue Corridor Overlay District (COD) and shall conform to this section as required by Zoning §27-2118. The proposed building materials shall be stone, brick, slate, etc. We recommend the preliminary renderings for the building, gateway sign, retaining wall, buffer fence pillars, lighting, landscaping, fencing, etc. be presented to the Supervisors and Planning Commission for discussion.
- 11. §22-905.1.A The Applicant is responsible for any other required approvals, permits, etc. (i.e., BCPC, PennDOT, BCCD, Fire Marshal, NWWA, CNBJSA, etc.) as applicable.

#### C. Traffic Comments

- 1. <u>§27-2501</u> Though PennDOT has previously reviewed the Traffic Impact Analysis: (TIA) and has no additional comments, we request the collision data analysis be updated to include a summary of the crash data and indicate any accident clusters that should reviewed for corrective measures at the study area intersections.
- 2. The Applicant should determine if the signal equipment on the northeast corner of County Line Road and West Butler Avenue needs to be modified due to the relocation and modifications to the sidewalk. The Applicant will need to revise the Traffic Signal Permit plan to incorporate the modifications to any signal equipment and sidewalk modifications.
- 3. The plans show the curb ramps at the intersection of County Line Road and West Butler Avenue are being replaced. The pedestrian push button poles should be shown on the plans.
- 4. At the intersection of County Line Road and West Butler Avenue, we recommend the signal backplates be updated to louvered backplates with reflective yellow tape.
- 5. There may be conflicts between customers maneuvering the site and trash pickup based on the truck turning movements, therefore, we recommend trash pickup hours be limited to outside of the hours of operation.

#### D. Stormwater Management Ordinance

- 1. §26-121.8.C We recommend green infrastructure and/or low impact development practices be considered and documented for the site to offset the impacts of the impervious on the PennDOT MS4 storm sewer system and downslope properties receiving stormwater from the site.
- 2. §26-132.2.E, -164.1, & -165.2 Inspection, operations, and maintenance responsibilities for the proposed storm sewer facilities shall be provided on the plans for the stormwater conveyance systems and the "Constructed Filter" at Inlet INL#6. The Applicant shall sign an Operation and Maintenance (O&M) Agreement prepared by the Township covering all privately owned storm sewer facilities.
- 3. §26-151.A A Storm Sewer Maintenance Fee applies to all existing and proposed storm sewer located in public rights-of-way within the Township at a rate of \$2.50 per linear foot of existing/ proposed roadway. Based on 362 feet of frontage, a fee of \$905.00 is required.

If you have any questions regarding the above, please contact this office.

Sincerely,

Janene Marchand, P.E.

Gilmore & Associates, Inc.

Sanura Manchamal

Township Engineers & Assistant Township Zoning Officer

JM/tw

CC: Daniel Fox, Township Manager
Alexandra Mullin, Assistant to the Manager
Randy Teschner, Fire Marshall
Scott Holbert, Esq. Township Solicitor
David M. Shafkowitz, Esq., Shafkowitz Law Group, P.C.
Spotless Brands, LLC (<a href="mailto:ilombardo@spotlessbrands.com">ilombardo@spotlessbrands.com</a>), Applicant
L. Scott Mill, R.L.A., Van Cleef Engineering
Robert Bender, Director, NWWA
John Larson, Director, CNBTJSA

Craig D. Kennard, P.E., E.V.P., Gilmore & Associates, Inc.

# PRELIMINARY / FINAL LAND DEVELOPMENT PLANS

**FOR** 

# FLAGSHIP CARWASH

PREPARED FOR

# SPOTLESS BRANDS

SITUATED IN

# NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA

# **OWNER OF RECORD:**

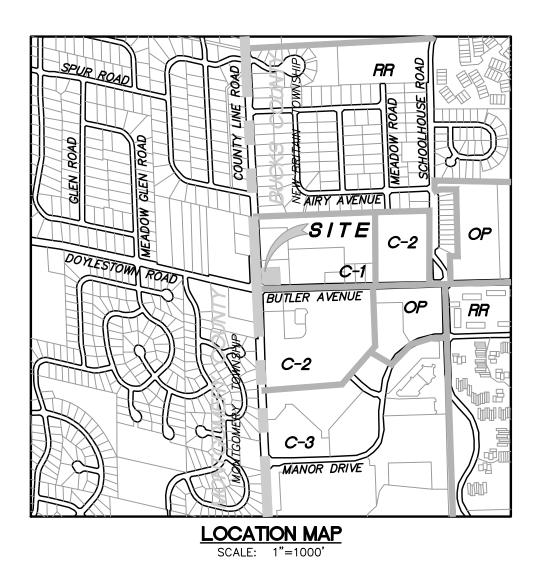
FLAGSHIP PENNSYLVANIA PROPCO, LLC

SITE ADDRESS: 545 WEST BUTLER AVENUE CHALFONT, PA 18914

PARID 26-006-101-001 INSTRUMENT NUMBER 2025004671

**SITE AREA:** 

28,414 SQ. FT. OR 0.652 ACRES



1 INCH = 1000 FT

**GRAPHIC SCALE** 

DATE: APRIL 3, 2025 REVISED: JUNE 3, 2025



VAN CLEEF ENGINEERING ASSOCIATES, LLC 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM PHONE (215) 345-1876

# **INDEX OF SHEETS**

# SHEET No. TITLE 1 COVER SHEET

SITE PLAN (SHEET 1 OF 1 TO BE RECORDED)

EXISTING FEATURES PLAN

4 AERIAL PLAN

5 GRADING PLAN6 UTILITY PLAN

7 LANDSCAPE PLAN

8 LIGHTING PLAN9 EROSION AND SEDIMENTATION CONTROL PLAN

10 CONSTRUCTION DETAILS

11 UTILITY DETAILS

12 EROSION AND SEDIMENTATION CONTROL NOTES

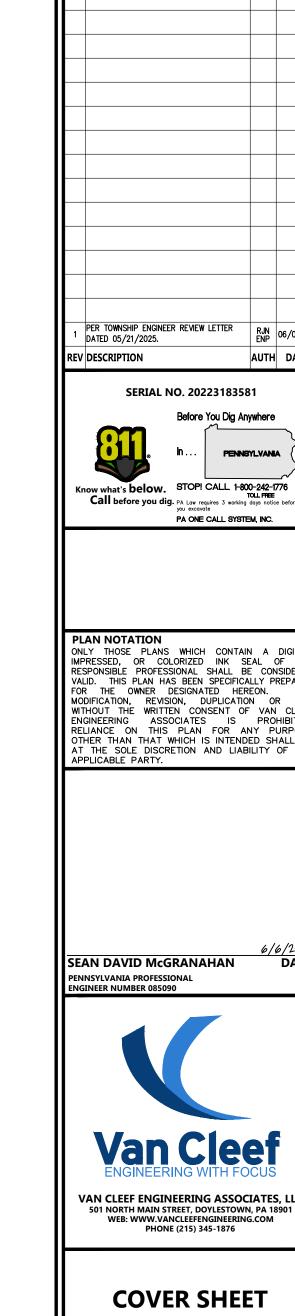
13 EROSION AND SEDIMENTATION CONTROL DETAILS

**VEHICLE NAVIGATION PLAN** 

14 CONSTRUCTION DETAILS

15 CONSTRUCTION DETAILS
16 ADA DETAILS

17 UTILITY PROFILES



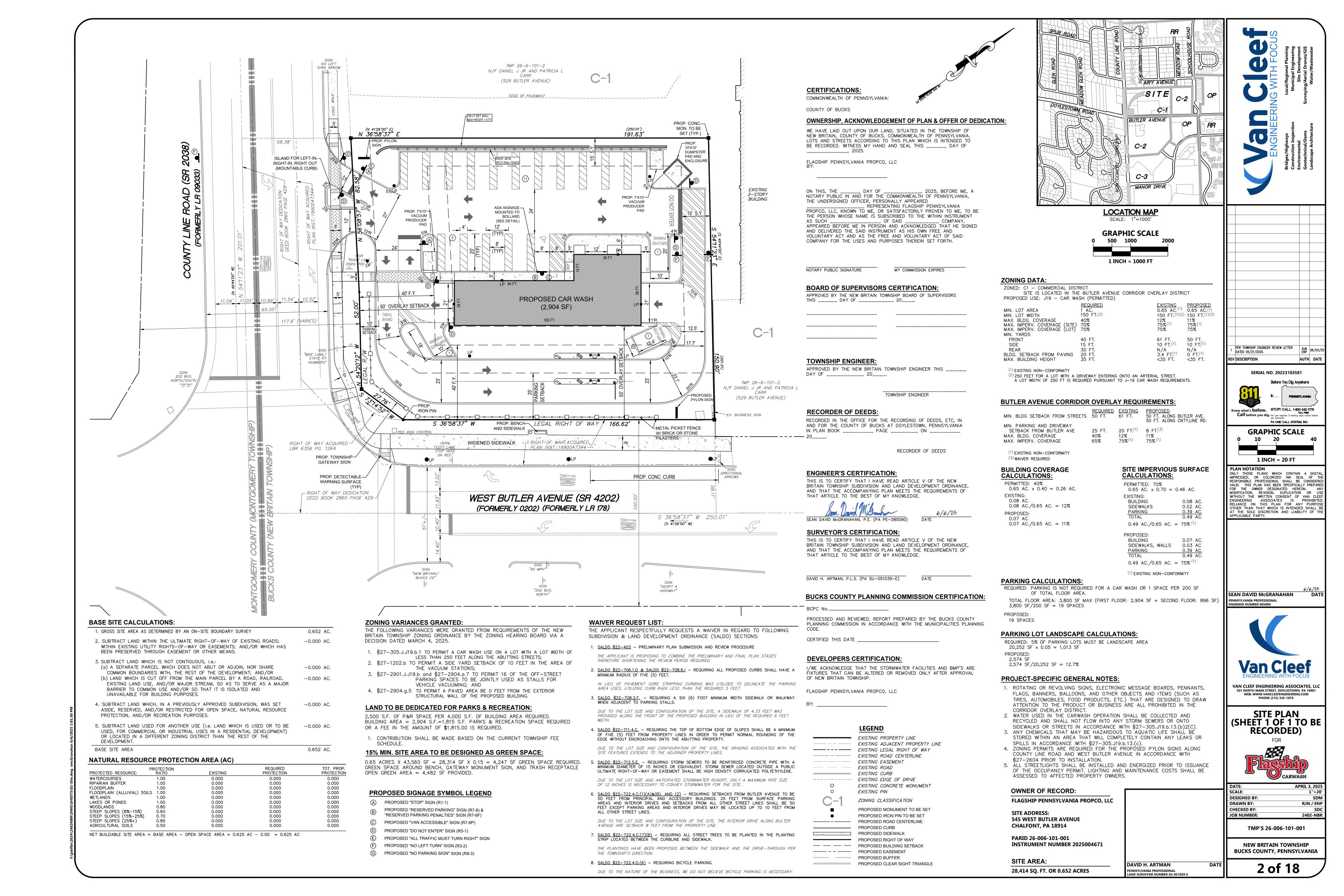
RJN / ENF

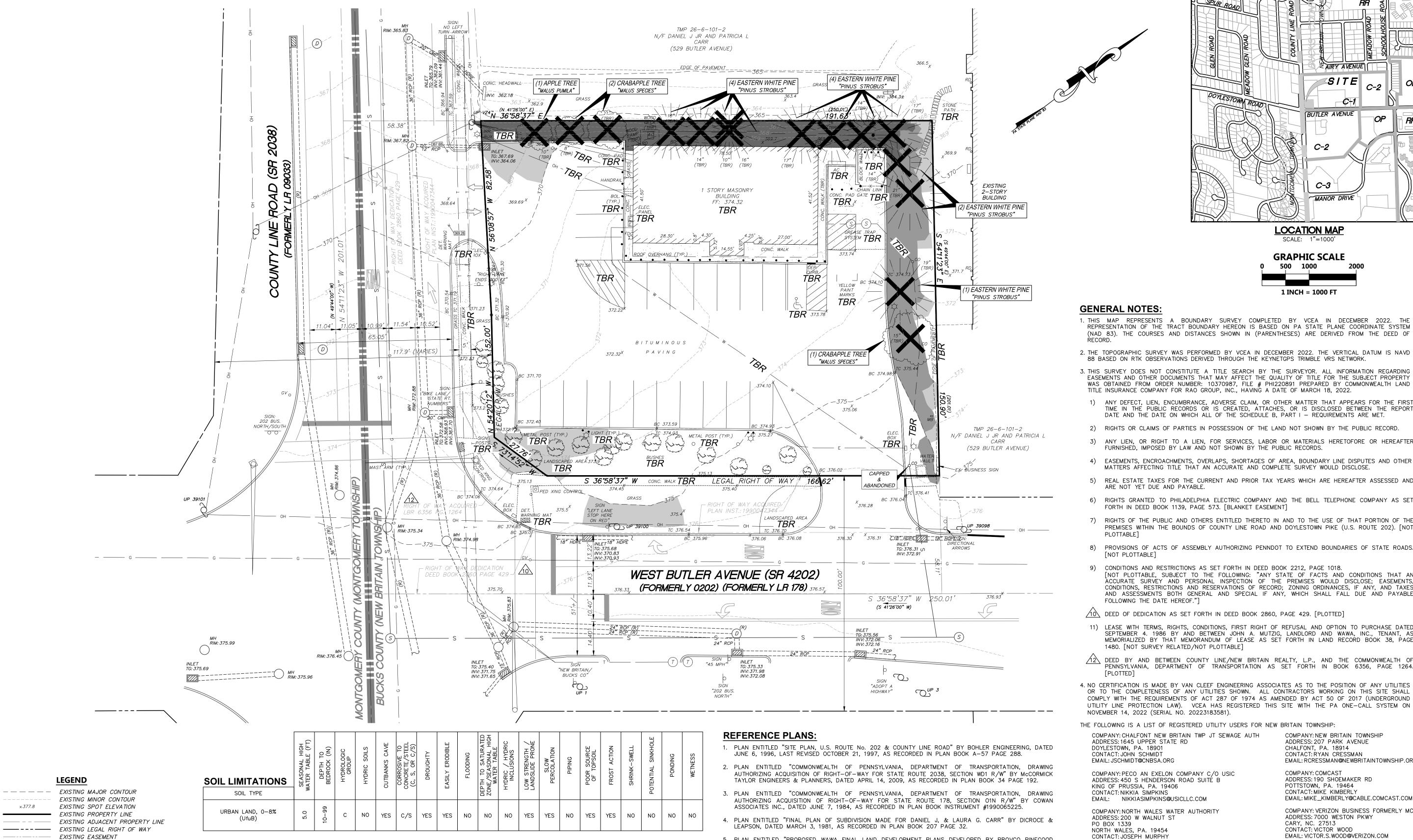
**HECKED BY** 

TMP'S 26-006-101-001

**NEW BRITAIN TOWNSHIP** 

**BUCKS COUNTY, PENNSYLVANIA** 





# **GENERAL NOTES:**

\_\_\_\_\_

====== EXISTING CURB

---- EXISTING EDGE OF DRIVE

EXISTING ROAD CENTERLINE

EXISTING SOILS LIMIT AND SOILS TYPE

EXISTING ROAD

EXISTING SIGN

EXISTING TREES

— — OH — EXISTING OVERHEAD ELECTRIC

— — T — EXISTING TELEPHONE

==== EXISTING STORM SEWER

— — W — EXISTING WATER MAIN

EXISTING SANITARY SEWER

— — G — EXISTING GAS MAIN

EXISTING UTILITY POLES

EXISTING WATER VALVE

EXISTING SLOPE 8 TO 15 PERCENT

EXISTING SLOPE 15 TO 25 PERCENT

EXISTING SLOPE 25< PERCENT

EXISTING ITEM TO BE REMOVED

EXISTING TREE TO BE REMOVED

1. SOILS SHOWN HEREON ARE AS MAPPED BY THE NATURAL RESOURCES CONSERVATION SERVICES (NRCS) WEBSITE FOR BUCKS COUNTY ON SEPTEMBER 26, 2023. 2. THE SITE FLOWS DRAIN TO THE WEST BRANCH OF THE NESHAMINY CREEK (WWF, MF) WITHIN THE NESHAMINY CREEK WATERSHED. THE WATERS DESIGNATED/EXISTING USES ARE CLASSIFIED BY THE PA CODE CHAPTER 93.

# SOIL RESOLUTIONS:

WHEN THE ABOVE SOILS LIMITATIONS ARE ENCOUNTERED, UTILIZE THE FOLLOWING SOILS RESOLUTIONS:

- DEPTH TO BEDROCK WHEN DEPTH TO BEDROCK IS ENCOUNTERED, CONTRACTOR MAY BLAST IF NECESSARY.
- CUTBANKS CAVE ALL APPLICABLE OSHA STANDARDS AND REGULATIONS WITH REGARD TO UTILITY AND RETAINING WALL CONSTRUCTION MUST BE IMPLEMENTED AT ALL TIMES. LIMIT SLOPES TO 3:1 MAXIMUM. • CORROSIVE TO CONCRETE/STEEL - ALL UNDERGROUND CONCRETE AND/OR STEEL MATERIALS TO BE BACKFILLED WITH APPROPRIATE MATERIAL WHEN CORROSIVENESS IS LIMITED.
- FOR BUILDINGS, CONTRACTOR TO COMPLY WITH THE INTERNAL RESIDENTIAL CODE (IRC) BUILDING CODE. • DROUGHTY - PROVIDE WATER TRUCK FOR IRRIGATION IN NEEDED. WHERE LANDSCAPING IS PROPOSED WITHIN DROUGHTY SOILS, APPLYING COMPOST MAY BE NECESSARY TO HELP
- EASILY ERODIBLE SOILS TO BE STABILIZED WITH APPROPRIATE COVER AND/OR MATTING.
- LOW STRENGTH/LANDSLIDE PRONE PRECAUTIONS SHOULD BE TAKEN TO PREVENT SLOPE FAILURES DUE TO IMPROPER CONSTRUCTION PRACTICES, SUCH AS OVER-STEEPENING AND OVERLOADING OF SLOPES, REMOVAL OF LATERAL SUPPORT, AND FAILURE TO PREVENT SATURATION OF SLOPES. ROAD FILL MATERIAL MAY NEED TO BE IMPORTED IN AREAS WHERE SOILS HAVE LOW STRENGTH.
- SLOW PERCOLATION UTILIZE STRUCTURAL BMP'S THAT DO NOT REQUIRE INFILTRATION (I.E. AMENDED SOILS, SLOW RELEASE BASIN). NO INFILTRATION BMP'S ARE PROPOSED. • POOR SOURCE OF TOPSOIL - CONTRACTOR TO SUPPLY TOPSOIL AS NEEDED.
- FROST ACTION PRECAUTIONS ARE NEEDED TO PREVENT DAMAGE, ESPECIALLY TO ROADWAYS.

- 5. PLAN ENTITLED "PROPOSED WAWA FINAL LAND DEVELOPMENT PLANS DEVELOPED BY PROVCO PINEGOOD CHALFONT, LLC" BY BOHLER ENGINEERING, DATED MAY 18, 2018, LAST REVISED NOVEMBER 1, 2019, AS RECORDED IN PLAN BOOK INSTRUMENT # 2020034823.
- 6. PLAN ENTITLED "SHOWALTER SUBDIVISION MINOR SUBDIVISION PLAN" BY SHOWALTER & ASSOCIATES, DATED MARCH 26, 1984, LAST REVISED APRIL 10, 1990, AS RECORDED IN PLAN BOOK 256 PAGE 82.

# SUMMARY OF ADVERSE FEE-SIMPLE CONVEYANCES:

50,244 S.F. DEED INSTRUMENT NO. 2022054655 -12,566 S.F. DEED BK, 2860 PG, 429 - 8,050 S.F. PLAN INSTRUMENT NO. 1990047344 - 1,214 S.F. DEED LRB 6356 PG. 1264

28,414 S.F. CURRENT SURVEY

ZATRY AVENUE *≤SITE* BUTLER AVENUE RR C-3 **LOCATION MAP** 

**GRAPHIC SCALE** 

500 1000 1 INCH = 1000 FT

#### **GENERAL NOTES:**

1. THIS MAP REPRESENTS A BOUNDARY SURVEY COMPLETED BY VCEA IN DECEMBER 2022. THE REPRESENTATION OF THE TRACT BOUNDARY HEREON IS BASED ON PA STATE PLANE COORDINATE SYSTEM (NAD 83). THE COURSES AND DISTANCES SHOWN IN (PARENTHESES) ARE DERIVED FROM THE DEED OF

88 BASED ON RTK OBSERVATIONS DERIVED THROUGH THE KEYNETGPS TRIMBLE VRS NETWORK. 3. THIS SURVEY DOES NOT CONSTITUTE A TITLE SEARCH BY THE SURVEYOR. ALL INFORMATION REGARDING

- EASEMENTS AND OTHER DOCUMENTS THAT MAY AFFECT THE QUALITY OF TITLE FOR THE SUBJECT PROPERTY WAS OBTAINED FROM ORDER NUMBER: 10370987, FILE # PHI220891 PREPARED BY COMMONWEALTH LAND TITLE INSURANCE COMPANY FOR RAO GROUP, INC., HAVING A DATE OF MARCH 18, 2022. 1) ANY DEFECT, LIEN, ENCUMBRANCE, ADVERSE CLAIM, OR OTHER MATTER THAT APPEARS FOR THE FIRST
- TIME IN THE PUBLIC RECORDS OR IS CREATED, ATTACHES, OR IS DISCLOSED BETWEEN THE REPORT DATE AND THE DATE ON WHICH ALL OF THE SCHEDULE B, PART I - REQUIREMENTS ARE MET.
- 3) ANY LIEN, OR RIGHT TO A LIEN, FOR SERVICES, LABOR OR MATERIALS HERETOFORE OR HEREAFTER
- FURNISHED, IMPOSED BY LAW AND NOT SHOWN BY THE PUBLIC RECORDS.
- 4) EASEMENTS, ENCROACHMENTS, OVERLAPS, SHORTAGES OF AREA, BOUNDARY LINE DISPUTES AND OTHER MATTERS AFFECTING TITLE THAT AN ACCURATE AND COMPLETE SURVEY WOULD DISCLOSE.
- 5) REAL ESTATE TAXES FOR THE CURRENT AND PRIOR TAX YEARS WHICH ARE HEREAFTER ASSESSED AND ARE NOT YET DUE AND PAYABLE.
- 6) RIGHTS GRANTED TO PHILADELPHIA ELECTRIC COMPANY AND THE BELL TELEPHONE COMPANY AS SET FORTH IN DEED BOOK 1139, PAGE 573. [BLANKET EASEMENT]
- 7) RIGHTS OF THE PUBLIC AND OTHERS ENTITLED THERETO IN AND TO THE USE OF THAT PORTION OF THE PREMISES WITHIN THE BOUNDS OF COUNTY LINE ROAD AND DOYLESTOWN PIKE (U.S. ROUTE 202). [NOT
- 8) PROVISIONS OF ACTS OF ASSEMBLY AUTHORIZING PENNDOT TO EXTEND BOUNDARIES OF STATE ROADS. [NOT PLOTTABLE]
- 9) CONDITIONS AND RESTRICTIONS AS SET FORTH IN DEED BOOK 2212, PAGE 1018. [NOT PLOTTABLE, SUBJECT TO THE FOLLOWING: "ANY STATE OF FACTS AND CONDITIONS THAT AN ACCURATE SURVEY AND PERSONAL INSPECTION OF THE PREMISES WOULD DISCLOSE; EASEMENTS, CONDITIONS, RESTRICTIONS AND RESERVATIONS OF RECORD; ZONING ORDINANCES, IF ANY, AND TAXES AND ASSESSMENTS BOTH GENERAL AND SPECIAL IF ANY, WHICH SHALL FALL DUE AND PAYABLE FOLLOWING THE DATE HEREOF."]
- 10 DEED OF DEDICATION AS SET FORTH IN DEED BOOK 2860, PAGE 429. [PLOTTED]
- 11) LEASE WITH TERMS, RIGHTS, CONDITIONS, FIRST RIGHT OF REFUSAL AND OPTION TO PURCHASE DATED SEPTEMBER 4. 1986 BY AND BETWEEN JOHN A. MUTZIG, LANDLORD AND WAWA, INC., TENANT, AS MEMORIALIZED BY THAT MEMORANDUM OF LEASE AS SET FORTH IN LAND RECORD BOOK 38, PAGE 1480. [NOT SURVEY RELATED/NOT PLOTTABLE]
- DEED BY AND BETWEEN COUNTY LINE/NEW BRITAIN REALTY, L.P., AND THE COMMONWEALTH OF PENNSYLVANIA, DEPARTMENT OF TRANSPORTATION AS SET FORTH IN BOOK 6356, PAGE 1264.
- 4. NO CERTIFICATION IS MADE BY VAN CLEEF ENGINEERING ASSOCIATES AS TO THE POSITION OF ANY UTILITIES OR TO THE COMPLETENESS OF ANY UTILITIES SHOWN. ALL CONTRACTORS WORKING ON THIS SITE SHALL COMPLY WITH THE REQUIREMENTS OF ACT 287 OF 1974 AS AMENDED BY ACT 50 OF 2017 (UNDERGROUND UTILITY LINE PROTECTION LAW). VCEA HAS REGISTERED THIS SITE WITH THE PA ONE-CALL SYSTEM ON
- THE FOLLOWING IS A LIST OF REGISTERED UTILITY USERS FOR NEW BRITAIN TOWNSHIP:

COMPANY: CHALFONT NEW BRITAIN TWP JT SEWAGE AUTH ADDRESS: 1645 UPPER STATE RD DOYLESTOWN, PA. 18901 CONTACT: JOHN SCHMIDT

COMPANY: PECO AN EXELON COMPANY C/O USIC ADDRESS: 450 S HENDERSON ROAD SUITE B KING OF PRUSSIA, PA. 19406 CONTACT: NIKKIA SIMPKINS

EMAIL: NIKKIASIMPKINS@USICLLC.COM COMPANY: NORTH WALES WATER AUTHORITY ADDRESS: 200 W WALNUT ST PO BOX 1339

NORTH WALES, PA. 19454 CONTACT: JOSEPH MURPHY EMAIL: JMURPHY@NWWATER.COM

- 5. THIS SURVEY DOES NOT CERTIFY AS TO THE PRESENCE OR ABSENCE OF UNDERGROUND IMPROVEMENTS OR
- 6. THIS SURVEY DOES NOT CERTIFY AS TO THE PRESENCE OR ABSENCE OF WETLANDS OR WATERS.
- 7. THE PARCEL SHOWN HEREON IS NOT LOCATED IN A SPECIAL FLOOD HAZARD AREA (SFHA), PER FLOOD INSURANCE RATE MAPS (FIRM) PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA), BEING PANEL 288 OF 532 MAP NUMBER 42017C0288K, EFFECTIVE DATE MARCH 21, 2017.

OWNER OF RECORD:

**545 WEST BUTLER AVENUE** CHALFONT, PA 18914

PARID 26-006-101-001 **INSTRUMENT NUMBER 2025004671** 

COMPANY: NEW BRITAIN TOWNSHIP

EMAIL: RCRESSMAN@NEWBRITAINTOWNSHIP.ORG

EMAIL: MIKE\_KIMBERLY@CABLE.COMCAST.COM

COMPANY: VERIZON BUSINESS FORMERLY MCI

ADDRESS: 207 PARK AVENUE

ADDRESS: 190 SHOEMAKER RD

ADDRESS: 7000 WESTON PKWY

EMAIL: VICTOR.S.WOOD@VERIZON.COM

CHALFONT, PA. 18914 CONTACT: RYAN CRESSMAN

POTTSTOWN, PA. 19464

CONTACT: MIKE KIMBERLY

COMPANY: COMCAST

CARY, NC. 27513

CONTACT: VICTOR WOOD

**NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 

RAWN BY

HECKED BY **B NUMBER** 

3 of 18

TMP'S 26-006-101-001

PER TOWNSHIP ENGINEER REVIEW LETTER

**SERIAL NO. 20223183581** 

now what's below. STOPI CALL 1-800-242-1776

**GRAPHIC SCALE** 

1 INCH = 20 FT

NLY THOSE PLANS WHICH CONTAIN A DIG MPRESSED, OR COLORIZED INK SEAL OF T RESPONSIBLE PROFESSIONAL SHALL BE CONSIDER

VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPART FOR THE OWNER DESIGNATED HEREON. AN

MODIFICATION, REVISION, DUPLICATION OR U
WITHOUT THE WRITTEN CONSENT OF VAN CLE
ENGINEERING ASSOCIATES IS PROHIBITE
RELIANCE ON THIS PLAN FOR ANY PURPO

THER THAN THAT WHICH IS INTENDED SHALL

AT THE SOLE DISCRETION AND LIABILITY OF APPLICABLE PARTY.

EAN DAVID McGRANAHAN NSYLVANIA PROFESSIONAL

VAN CLEEF ENGINEERING ASSOCIATES, LLC

501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM

**EXISTING FEATURES** 

**DEMOLITION PLAN** 

RJN / ENP

PHONE (215) 345-1876

NEER NUMBER 085090

Call before you dig. PA Law requires 3 working days notice

0 10 20

PLAN NOTATION

Before You Dig Anywhere

PA ONE CALL SYSTEM, INC.

AUTH DATE

DATED 05/21/2025.

EV DESCRIPTION





SITE AREA:

28,414 SQ. FT. OR 0.652 ACRES OAVID H. ARTMAN



ENGINEERING WITH FOCUS
Bridges/Highways
Construction Inspection
Environmental
Geotechnical/Dams
Surveying/Aerial Drones/GIS

PER TOWNSHIP ENGINEER REVIEW LETTER DATED 05/21/2025. RJN 06/0
DESCRIPTION AUTH DA

SERIAL NO. 20223183581



elow. STOPI CALL 1-800-242-1776
e you dig. PA Law requires 3 working days notice before you excevate
PA ONE CALL SYSTEM, INC.

**GRAPHIC SCALE**25 50 10

1 INCH = 50 FT

PLAN NOTATION

ONLY THOSE PLANS WHICH CONTAIN A DIGITAL, IMPRESSED, OR COLORIZED INK SEAL OF THE RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERED VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARED FOR THE OWNER DESIGNATED HEREON. ANY MODIFICATION, REVISION, DUPLICATION OR USE WITHOUT THE WRITTEN CONSENT OF VAN CLEEF ENGINEERING ASSOCIATES IS PROHIBITED. RELIANCE ON THIS PLAN FOR ANY PURPOSE OTHER THAN THAT WHICH IS INTENDED SHALL BE AT THE SOLE DISCRETION AND LIABILITY OF THE APPLICABLE PARTY.

6/6/10

SEAN DAVID McGRANAHAN DATENSYLVANIA PROFESSIONAL ENGINEER NUMBER 085090



VAN CLEEF ENGINEERING ASSOCIATES, LLC
501 NORTH MAIN STREET, DOYLESTOWN, PA 18901
WEB: WWW.VANCLEEFENGINEERING.COM
PHONE (215) 345-1876

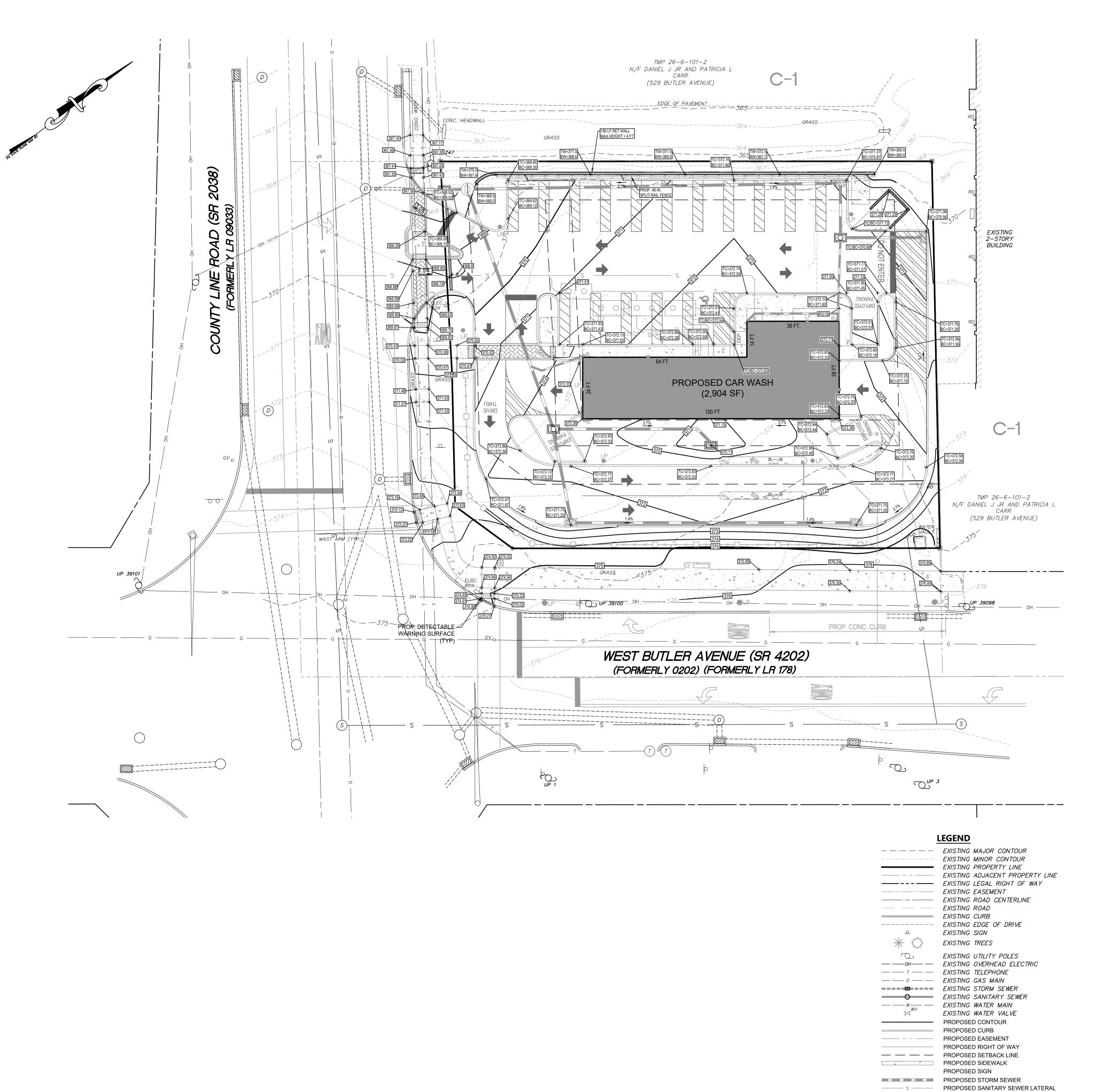
AERIAL PLAN



DATE: APRIL 3, 2025
SCALE: 1"=50'
DESIGNED BY: SDM
DRAWN BY: RJN / ENP
CHECKED BY: SDC
JOB NUMBER: 2402-NBR

TMP'S 26-006-101-001

NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA



#### **GRADING GENERAL NOTES:**

- 1. VERTICAL DATUM IS NAVD 1988 AND ESTABLISHED BY OBSERVATIONS REFERENCED TO THE KEYSTONE VRS NETWORK (KEYNETGPS).
- 2. THE MINIMUM SLOPE IN GRASSED AREAS SHALL NOT BE LESS THAN 2.0% AND THE MINIMUM IN PAVED AREAS SHOULD NOT BE LESS THAN 1.0%.
- 3. PAVEMENT SHALL BE SAW CUT IN STRAIGHT LINES TO THE FULL DEPTH OF THE EXISTING PAVEMENT. ALL DEBRIS FROM REMOVAL OPERATIONS SHALL BE REMOVED FROM THE SITE AT THE TIME OF EXCAVATION. STOCKPILING OF DEBRIS WILL NOT BE PERMITTED.
- 4. IN CASE OF DISCREPANCIES BETWEEN PLANS, THE SITE / RECORD PLAN WILL SUPERSEDE IN ALL CASES. THE ENGINEER OF RECORD MUST BE IMMEDIATELY NOTIFIED IN WRITING OF ANY CONFLICTS.
- 5. THE CONTRACTOR SHALL BE REQUIRED TO SECURE ALL NECESSARY PERMITS (INCLUDING DEP, ETC.) FOR ALL OFF—SITE HAUL AND/OR BORROW SITES. CONTRACTOR SHALL SUPPLY A COPY OF APPROVALS TO DESIGN ENGINEER AND OWNER PRIOR TO INITIATING WORK.
- 6. EXISTING INLETS AND STORM SEWER INDICATED AS FILLED W/DEBRIS SHALL BE CLEANED AND FLUSHED. NEW INLETS AND PIPES SHALL BE CHECKED FOR SILT/DEBRIS AFTER CONSTRUCTION AND FLUSHED/CLEANED IF NECESSARY.
- 7. DEPTH OF EXISTING UTILITIES IN PORTIONS OF THE SITE ARE UNKNOWN. WHERE EXISTING UTILITIES ARE TO REMAIN AND ARE FOUND TO HAVE INADEQUATE GROUND COVER AFTER FINAL PROPOSED GRADES HAVE BEEN ESTABLISHED, THE DESIGN ENGINEER SHALL BE CONTACTED IMMEDIATELY AND PRIOR TO FURTHER CONSTRUCTION ACTIVITIES IN THE AREA OF SAID CONFLICT.
- 8. ALL DESIGNERS AND CONTRACTORS UTILIZING THIS PLAN AND THE INFORMATION CONTAINED THEREON ARE CAUTIONED TO COMPLY WITH THE REQUIREMENTS OF PENNSYLVANIA ACT 287. LOCATION OF EXISTING AND PROPOSED UNDERGROUND UTILITIES AND FACILITIES SHOWN ON THE DRAWINGS HAVE BEEN DEVELOPED FROM INFORMATION MADE AVAILABLE. COMPLETENESS AND ACCURACY OF LOCATION AND DEPTH OF UTILITIES AND FACILITIES CANNOT BE GUARANTEED. THE CONTRACTOR IS TO VERIFY THE DEPTH AND LOCATION OF ALL UTILITIES AND FACILITIES BEFORE THE START OF WORK. UTILIZE HAND EXCAVATION AS REQUIRED. WORK IS ALSO TO BE DONE IN ACCORDANCE WITH THE STANDARDS OF THE UTILITY COMPANIES WHOSE FACILITIES ARE IN THE PROXIMITY OF THE WORK. OTHER UTILITIES MAY BE REQUIREMENTS OF PENNSYLVANIA ACT 38 (1991), THE CONTRACTOR SHALL CONTACT THE PENNSYLVANIA ONE CALL SYSTEM AT 1-800-242-1776, AT LEAST 3 DAYS PRIOR TO EXCAVATION.
- 9. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE FEDERAL, STATE AND LOCAL CODES, AND ALL REGULATIONS APPURTENANT TO THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970. ALL WORK SHALL BE DONE IN ACCORDANCE WITH PROJECT SPECIFICATIONS INCLUDING CURRENT APPLICABLE STANDARDS AND REQUIREMENTS. WHERE ANY STANDARDS SEEM IN CONFLICT WITH THESE DRAWINGS, NOTIFY THE DESIGN ENGINEER AND CONSTRUCTION MANAGER FOR DIRECTION PRIOR TO PROCEEDING WITH WORK.
- 10. CONTRACTORS SHALL HAVE ALL REQUIRED SUBMITTAL APPROVALS PRIOR TO BEGINNING WORK OR ORDERING
- 11. CONTRACTORS SHALL VERIFY ALL DIMENSIONS, INVERTS, ELEVATIONS, AND EXISTING CONDITIONS PRIOR TO PROCEEDING WITH THE WORK OR PROCUREMENT OF MATERIALS. VARIATIONS BETWEEN DRAWINGS AND ACTUAL FIELD CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER AND RESOLVED PRIOR TO PROCEEDING WITH THE WORK.
- 12. CONTRACTOR SHALL BE RESPONSIBLE FOR SAFETY, PROCEDURES, MEANS AND METHODS, SEQUENCING, AND COORDINATION.
- 13. ALL WORK SHALL BE PERFORMED BY QUALIFIED, EXPERIENCED PERSONNEL.
- 14. CONTRACTOR SHALL NOTIFY THE OWNER OF PREEXISTING CONDITIONS OF DETERIORATION IN AREAS OF WORK THAT ARE UNCOVERED OR EXPOSED DURING THE WORK.
- 15. FIELD CHANGES REQUIRE PRIOR DESIGN ENGINEERING REVIEW AND WRITTEN CONFIRMATION.
- 16. CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR TO ANY SIDEWALKS, LAWN, TREES, PAVING, AND OTHER IMPROVEMENTS DISTURBED OR DAMAGED BY DEMOLITION ACTIVITIES PROPOSED HEREIN.
- 17. CONTRACTOR SHALL PROVIDE PROPER TEMPORARY BRACING AND SHORING OF ALL CONSTRUCTION TO REMAIN OR DEMOLITION WORK IN PROGRESS.
- 18. CONTRACTOR SHALL PROVIDE LAYOUT, LINE AND GRADE UNLESS OTHERWISE NOTED.
- 19. CONTRACTOR SHALL NOTIFY THE OWNER OF ANY DISCREPANCIES WITHIN THE DRAWINGS, SPECIFICATIONS, CODES OR STANDARDS FOR CORRECTIVE ACTION PRIOR TO START OF WORK.
- 20. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE AND PROTECTION OF VEHICULAR AND PEDESTRIAN TRAFFIC. ALL TRAFFIC CONTROL MEASURES SHALL BE IN ACCORDANCE WITH LOCAL, PENNDOT & OSHA REGULATIONS.
- 21. CONTRACTOR SHALL PROVIDE PROTECTION FOR EXISTING UTILITIES UNLESS OTHERWISE NOTED. THE CONTRACTOR SHALL IMMEDIATELY REPAIR ANY UTILITY LINE INTERRUPTION AT NO ADDITIONAL CONTRACT COST. THE CONTRACTOR SHALL PROVIDE ADEQUATE PROTECTION AND SUPPORT FOR ALL UTILITIES EXPOSED DURING THE WORK TO INSURE AGAINST DAMAGE AT NO ADDITIONAL COST.
- 22. CONTRACTOR SHALL PROVIDE TEMPORARY DEWATERING OF EXCAVATIONS THROUGHOUT THE DURATION OF CONTRACT AT NO ADDITIONAL COST.
- 23. EXISTING UTILITIES SHOWN ARE BASED ON AVAILABLE DATA. DUE TO THE POTENTIAL LACK OF COMPLETE OR ACCURATE DATA REGARDING EXISTING ONSITE AND OFFSITE UTILITIES, THE CONTRACTOR SHALL ASSESSAVAILABLE DATA, SHALL REQUEST UTILITY COMPANY MARKOUTS, SHALL COORDINATE WITH AFFECTED UTILITY COMPANIES, AND SHALL DIG TEST PITS AT ALL PROPOSED UTILITY CROSSING LOCATIONS SO THAT ELEVATIONS CAN BE TAKEN TO ASSESS POTENTIAL CONFLICTING PIPES/UTILITIES PRIOR TO ANY PROPOSED UTILITY CONSTRUCTION. IF UNEXPECTED UTILITY LOCATIONS OR ELEVATIONS OR PIPE CONFLICTS ARE ENCOUNTERED DURING CONSTRUCTION, OR IF TEST PITS REVEAL POTENTIAL CONFLICT, DESIGN ENGINEER SHALL BE NOTIFIED SO THAT ELEVATIONS AND LOCATIONS (WHERE NECESSARY) OF AFFECTED UTILITIES CAN BE OBTAINED TO FACILITATE NECESSARY DESIGN ADJUSTMENTS.
- 24. REMOVAL OF EXISTING UTILITIES SHALL BE COORDINATED WITH THE APPROPRIATE UTILITY COMPANIES.
- 25. REFER TO THE SITE / RECORD PLAN FOR ADDITIONAL NOTES.
- 26. ALL SIDEWALKS, CROSSWALK, TRAILS, ENTRANCES, AND RAMPS TO BE BUILT IN ACCORDANCE WITH ADA STANDARDS. MAXIMUM LONGITUDINAL SLOPE TO BE 5%. MAXIMUM CROSS SLOPE TO BE 2%. IT IS RECOMMENDED TO CONSTRUCT CROSS SLOPES AT 1.5% AND LONGITUDINAL SLOPES AT 4.9% TO ALLOW FOR CONSTRUCTION TOLERANCE.
- 27. REFER TO ADA CURB RAMP CONSTRUCTION DETAILS AND ADA ACCESSIBILITY PLAN FOR ADDITIONAL INFORMATION SPECIFIC TO CURB RAMP AND ACCESSIBLE ROUTE GRADING.
- 28. AFTER FINAL GRADING THERE SHALL BE A MINIMUM OF EIGHT INCHES OF TOPSOIL ON THE ENTIRE SITE OTHER THAN THAT PORTION OF THE SITE WHERE THERE ARE BUILDINGS OR OTHER IMPERVIOUS SURFACE COVERAGE. THE DEVELOPER IS NOT REQUIRED TO IMPORT TOPSOIL TO THE SITE IF AN EIGHT—INCH FINAL DEPTH CANNOT BE ACHIEVED. IN CASES WHERE THE ORIGINAL SITE HAS LESS THAN EIGHT INCHES OF TOPSOIL AT THE START OF GRADING ACTIVITIES, THE TOTAL AMOUNT AVAILABLE WILL BECOME THE MINIMUM TOPSOIL REQUIREMENT. NO TOPSOIL WILL BE ALLOWED TO BE RELEASED FROM THE SITE.
- 29. ALL AREAS SHALL BE PROPERLY GRADED TO ENSURE DRAINAGE FLOW AWAY FROM THE PROPOSED BUILDING WITHOUT PONDING OR OBSTRUCTION.
- 30. DURING GRADING OPERATIONS, NECESSARY MEASURES FOR DUST CONTROL SHALL BE EXERCISED.
- 31. THE CHALFONT NEW BRITAIN JOINT SEWER AUTHORITY IS TO BE CONTACTED ONE WEEK PRIOR TO SANITARY SEWER CONSTRUCTION, AND 72 HOURS PRIOR TO EXCAVATING NEAR, AND CONNECTING INTO ANY EXISTING SANITARY SEWER.
- 32. ALL SANITARY SEWER CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THE NORTH WALES WATER AUTHORITY SPECIFICATIONS.
- 33. PUBLIC WATER AND SEWER CURRENTLY SERVE THE EXISTING BUILDING. THE PROPOSED WATER SERVICE
- SHALL CONNECT TO THE EXISTING WATER VAULT.
- 34. PROPOSED HANDICAP PARKING SPACES SHALL BE PROVIDED WITH POST-MOUNTED SIGNS.
- 35. CONTRACTOR TO CONNECT NEW BUILDING UTILITY CONNECTIONS TO EXISTING WATER AND SEWER.36. CONTRACTOR SHALL VERIFY COMPLIANCE WITH ADA STANDARDS WITH PROJECT ARCHITECT FOR RAMP AREAS
- PROPOSED ADJACENT TO DOORWAYS.
- 37. ALL UTILITY SERVICES TO THE PROPOSED BUILDING SHALL BE INSTALLED UNDERGROUND.

38. TOPSOIL MAY NOT BE REMOVED FROM THE SITE WITHOUT APPROVAL FROM THE TOWNSHIP.

39. FINAL DESIGN OF PROPOSED WATER AND SEWER FACILITIES ARE SUBJECT TO THE REVIEW AND APPROVAL BY THE SERVICING WATER AND SEWER AUTHORITY.

# RETAINING WALL NOTES:

PROPOSED WATER LATERAL FINISHED FLOOR ELEVATION

PROPOSED SPOT ELEVATION

507.30

- 1. RETAINING WALL SPECIFICATIONS AND DESIGN DETAILS SHALL BE APPROVED BY THE TOWNSHIP ENGINEER PRIOR TO FINAL PLAN APPROVAL.
- 2. WALL STYLE, FINISH AND COLOR SHALL BE COORDINATED WITH PROPOSED BUILDING ARCHITECTURE.



PER TOWNSHIP ENGINEER REVIEW LETTER RJN 06/03/

SERIAL NO. 20223183581



DATED 05/21/2025.

EV DESCRIPTION

GRAPHIC SCALE

PA ONE CALL SYSTEM, INC.

10 20

1 INCH = 20 FT

PLAN NOTATION

ONLY THOSE PLANS WHICH CONTAIN A DIGI

IMPRESSED, OR COLORIZED INK SEAL OF THE RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERED VALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARED FOR THE OWNER DESIGNATED HEREON. ANY MODIFICATION, REVISION, DUPLICATION OR USE WITHOUT THE WRITTEN CONSENT OF VAN CLEEF ENGINEERING ASSOCIATES IS PROHIBITED. RELIANCE ON THIS PLAN FOR ANY PURPOSE OTHER THAN THAT WHICH IS INTENDED SHALL BE AT THE SOLE DISCRETION AND LIABILITY OF THE APPLICABLE PARTY.

EAN DAVID McGRANAHAN DENNSYLVANIA PROFESSIONAL



VAN CLEEF ENGINEERING ASSOCIATES, LLC
501 NORTH MAIN STREET, DOYLESTOWN, PA 18901
WEB: WWW.VANCLEEFENGINEERING.COM

PHONE (215) 345-1876

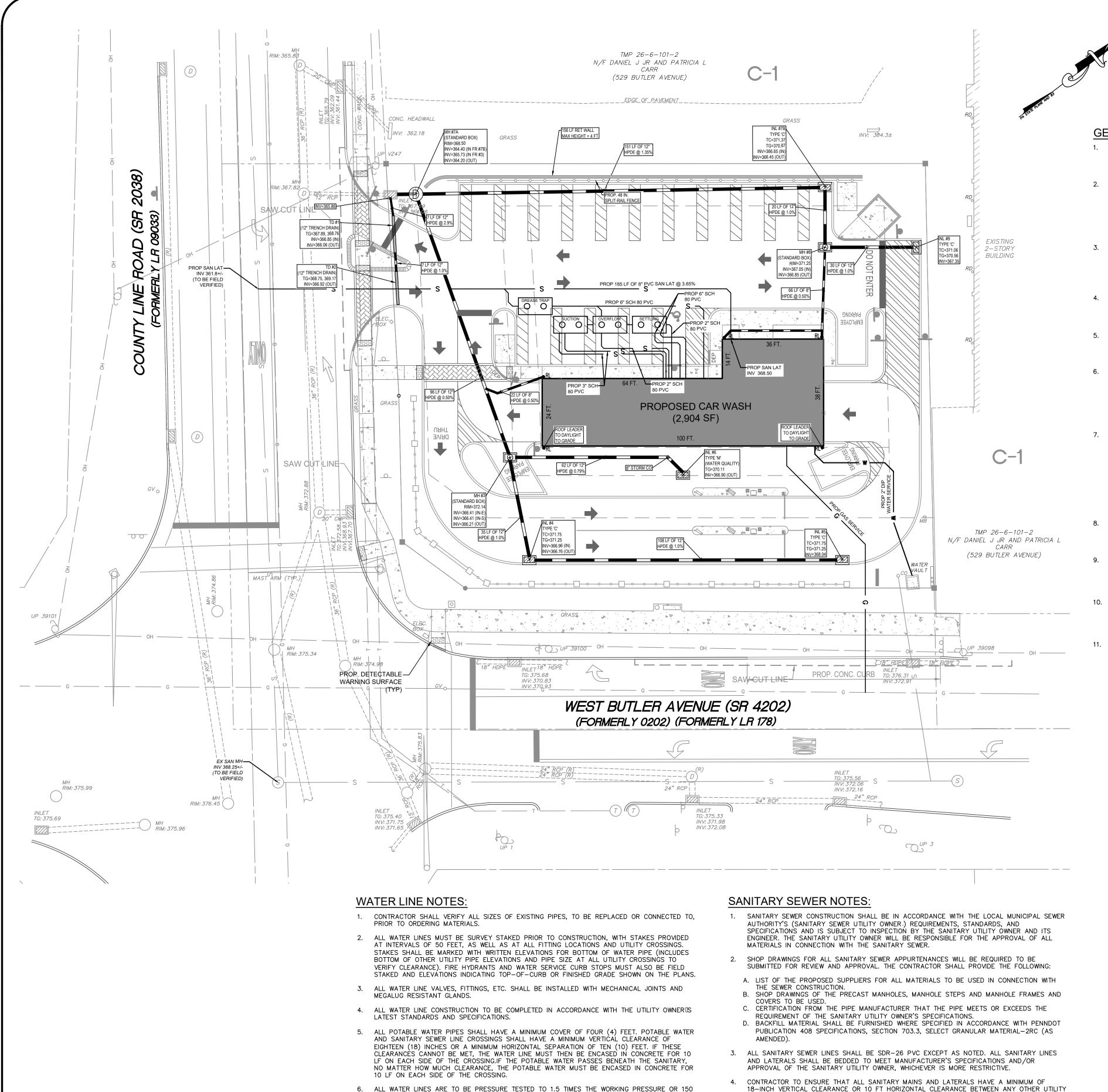
GRADING PLAN



DATE: APRIL 3, 2025
SCALE: 1"=20"
DESIGNED BY: SDM
DRAWN BY: RJN / ENP
CHECKED BY: SDC
JOB NUMBER: 2402-NBR

TMP'S 26-006-101-001

NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA



P.S.I., WHICHEVER IS GREATER, IN SECTIONS BETWEEN VALVES. THE TEST PRESSURE SHALL BE

MAINTAINED FOR A PERIOD NOT LESS THAN 2 HOURS. SHOULD ANY PRESSURE TEST DISCLOSE

AN INABILITY TO HOLD THE TEST PRESSURE, THE CONTRACTOR SHALL LOCATE AND CORRECT

7. THE INSTALLATION OR REPAIR OF ANY UNDERGROUND FACILITIES OR PIPING WHICH CONNECTS TO

OR FURNISHES WATER FOR THE FIRE PROTECTION SPRINKLER SYSTEM SHALL BE PERFORMED

ONLY BY A LICENSED UTILITY CONTRACTOR, FIRE PROTECTION SPRINKLER CONTRACTOR, OR

LICENSED PLUMBER. A COPY OF THE LICENSE OR CERTIFICATE OF COMPETENCY SHALL BE

ALL STORM CONVEYANCE PIPE SHALL BE SMOOTH LINED DOUBLE WALLED HIGH DENSITY

POLYETHYLENE PIPE (HDPE), UNLESS OTHERWISE SPECIFIED. ANY ROOF LEADERS SHALL BE

3. LANDSCAPING, FENCES AND STRUCTURES SHALL BE PLACED A MINIMUM OF 3 FEET AWAY FROM

IF A CONFLICT ARISES DURING THE INSTALLATION OF ANY PART OF THE STORM SEWER SYSTEM

DEFECTS AND RETEST TO THE SATISFACTION OF THE UTILITY OWNER

PROVIDED TO THE INSPECTOR AT THE FINAL INSPECTION.

THE ENGINEER IS TO BE NOTIFIED IMMEDIATELY IN WRITING.

STORM SEWERS OUTSIDE OF THE RIGHT OF WAY OF STREETS.

STORM SEWER NOTES:

#### GENERAL UTILITIES NOTES:

- ALL NEW UTILITIES/SERVICES, INCLUDING ELECTRIC, TELEPHONE, CABLE TV, ETC. ARE TO BE INSTALLED UNDERGROUND. ALL NEW UTILITIES/SERVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE UTILITY/SERVICE PROVIDER INSTALLATION SPECIFICATIONS
- THE LOCATION OF ALL EXISTING AND PROPOSED SERVICES ARE APPROXIMATE. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING WITH LOCAL UTILITY COMPANIES TO LOCATE ALL PUBLIC OR PRIVATE UTILITIES WHICH LIE IN OR ADJACENT TO THE CONSTRUCTION SITE PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION, REMOVAL OR EXCAVATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING, AT HIS EXPENSE, ALL EXISTING UTILITIES DAMAGED DURING CONSTRUCTION. SANITARY SEWER AND ALL OTHER UTILITY SERVICE CONNECTION POINT CONFLICTS SHALL BE REPORTED IMMEDIATELY IN WRITING TO THE ENGINEER.
- 3. ALL UTILITY EXTENSIONS FROM THE BUILDING BY OTHER TRADES SHALL BE CONNECTED TO THE CONTINUATION UTILITY LINES ON THE SITE. THE GENERAL CONTRACTOR SHALL MAKE THE CONNECTIONS AT A POINT 50 FROM THE BUILDING WALL WHERE OTHER TRADES HAVE ENDED THEIR WORK. THE GC SHALL COORDINATE SCHEDULING OF THESE CONNECTIONS WITH OTHER TRADES TO ASSURE THE SYSTEM IS COMPLETE WHEN FLOW WILL BEGIN IN THE LINES.
- 4. CONSTRUCTION SHALL COMMENCE BEGINNING AT THE LOWEST INVERT (POINT OF CONNECTION) AND PROGRESS UP GRADIENT. PROPOSED INTERFACE POINTS (CROSSINGS) WITH EXISTING UNDERGROUND UTILITIES SHALL BE FIELD VERIFIED BY TEST PIT PRIOR TO COMMENCEMENT OF CONSTRUCTION.
- 5. DEFINE AND LOCATE VERTICALLY AND HORIZONTALLY ALL ACTIVE UTILITY AND/OR SERVICE SYSTEMS THAT ARE NOT TO REMAIN. THE CONTRACTOR IS RESPONSIBLE TO PROTECT AND MAINTAIN ALL ACTIVE SYSTEMS THAT ARE NOT BEING REMOVED/RELOCATED DURING SITE ACTIVITY.
- 6. THE CONTRACTOR SHALL FAMILIARIZE THEMSELVES WITH THE APPLICABLE UTILITY SERVICE PROVIDER REQUIREMENTS AND IS RESPONSIBLE FOR COMPLIANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS, CODES AND REGULATIONS.THE CONTRACTOR IS ALSO RESPONSIBLE FOR ALL COORDINATION REGARDING UTILITY WORK AS IDENTIFIED OR REQUIRED FOR PROJECT. THE CONTRACTOR SHALL PROVIDE THE OWNER WRITTEN NOTIFICATIONS THAT THE EXISTING UTILITIES AND SERVICES HAVE BEEN TERMINATED AND ABANDONED IN ACCORDANCE WITH JURISDICTION AND UTILITY COMPANY
- 7. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATION OF SITE PLAN DOCUMENTS AND ARCHITECTURAL DESIGN FOR EXACT BUILDING UTILITY CONNECTION LOCATIONS, DOOR ACCESS, AND EXTERIOR GRADING. (THE UTILITY SERVICE SIZES ARE TO BE DETERMINED BY ARCHITECT.) THE CONTRACTOR SHALL COORDINATE INSTALLATION OF UTILITIES/SERVICES WITH THE INDIVIDUAL COMPANIES TO AVOID CONFLICTS AND ENSURE PRÓPER DEPTHS ARE ACHIEVED. IN ADDITION TO MEETING THE UTILITY OWNER REQUIREMENTS, THE UTILITY TIE-INS/CONNECTIONS SHALL BE COORDINATED. THIS SHALL BE COMPLETED PRIOR TO CONNECTING TO THE EXISTING UTILITY/SERVICE. WHERE CONFLICTS EXIST WITH THESE PLANS, THE ENGINEER OF RECORD SHALL BE NOTIFIED IN WRITING TO RESOLVE SAID CONFLICTS PRIOR TO
- 8. THE TOPS OF EXISTING MANHOLES, INLET STRUCTURES, AND SANITARY CLEANOUT TOPS SHALL BE ADJUSTED, IF REQUIRED, TO MATCH THE GRADING AND UTILITY PLAN(S). THE SANITARY CLEANOUT TOPS SHALL BE REINSTALLED IN THE SAME MANNÈR AS THE ORIGINAL CLEANOUTS.

CONSTRUCTION.

- CONTRACTOR SHALL CONTACT THE LOCAL (I.E. TOWNSHIP, BOROUGH, ETC.) CODE ENFORCEMENT OFFICE WHEN UNDERGROUND PIPING IS BEING HYDROSTATICALLY TESTED ON SITE. APPLICANTS ARE ALSO REMINDED THAT FLUSHING OF THE UNDERGROUND PIPING SHALL BE WITNESSED BY A TOWNSHIP OFFICIAL PRIOR TO FINAL RISER CONNECTIONS, PER NFPA 13.
- 10. UNDERGROUND UTILITY MARKOUTS BY THE UTILITY COMPANIES WERE ORDERED AND THE PHYSICAL LOCATION OF THESE MARKOUTS ARE SHOWN HEREON, HOWEVER, NO CERTIFICATION IS MADE BY VCEA AS TO THE ACTUAL UNDERGROUND POSITION OF ANY UTILITIES OR TO THE COMPLETENESS OR ACCURACY OF THE UTILITY MARKOUTS
- 11. IT IS IMPERATIVE THAT UTILITY COMPANIES ARE NOTIFIED PRIOR TO ANY EXCAVATION AND/OR CONSTRUCTION. CALL 1-800-242-1776 TO ORDER UTILITY MARKOUTS BY OTHERS.

# ---- EXISTING MAJOR CONTOUR

**LEGEND** 

EXISTING MINOR CONTOUR EXISTING PROPERTY LINE ----- EXISTING ADJACENT PROPERTY LINE ---- EXISTING LEGAL RIGHT OF WAY ---- EXISTING EASEMENT ------ EXISTING ROAD CENTERLINE ---- EXISTING ROAD EXISTING CURB ---- EXISTING EDGE OF DRIVE EXISTING SIGN

EXISTING TREES EXISTING UTILITY POLES — — OH — EXISTING OVERHEAD ELECTRIC — — T — EXISTING TELEPHONE

CROSSING. OTHERWISE CONTRACTOR SHALL ENCASE MAIN OR LATERAL IN CONCRETE WITH A

MINIMUM OF 10 LF ON EACH SIDE OF THE CROSSING. CLEANOUTS ARE TO BE PLACED AT

INSTALLED WITH WATERTIGHT FRAMES AND COVERS. ANY MANHOLE TO BE INSTALLED OR

6. CONTRACTOR SHALL VERIFY ALL SIZES, MATERIALS, AND CONDITION OF EXISTING PIPES, TO BE

8. ALL PROPOSED SANITARY SEWER FACILITIES WILL BE OWNED AND PERPETUALLY MAINTAINED BY

REQUIREMENTS WITH RESPECT TO THE SANITARY SEWERAGE FACILITIES SERVED BY THE LOCAL

MUNICIPAL SEWER AUTHORITY'S (SANITARY SEWER UTILITY OWNER) SHALL BE IN ACCORDANCE

WITH THE AUTHORITY'S STANDARD SPECIFICATIONS FOR CONSTRUCTION OF SANITARY SEWERS,

7. ALL EXISTING MANHOLES SHALL BE TESTED PER SANITARY UTILITY OWNER STANDARDS TO

9. ALL MATERIALS USED, AND DETAILS OF CONSTRUCTION, INSTALLATION PROCEDURES. AND

ADJUSTED OUTSIDE OF PAVED AREAS SHALL BE INSTALLED A MINIMUM OF 6" ABOVE FINISH

5. ALL SANITARY SEWER MANHOLES PROPOSED TO BE INSTALLED OR ADJUSTED SHALL BE

MAXIMUM INTERVALS OF 100-FT AND AT ALL BENDS.

ASSURE NO LEAKS EXIST WITHIN THE STRUCTURES.

THE PROPERTY OWNER IN PERPETUITY.

REPLACED OR CONNECTED TO. PRIOR TO ORDERING MATERIALS.

FORCE MAINS, PUMPING STATIONS AND WASTEWATER TREATMENT.

— — G — EXISTING GAS MAIN ===== EXISTING STORM SEWER EXISTING SANITARY SEWER — — w — EXISTING WATER MAIN EXISTING WATER VALVE PROPOSED CONTOUR PROPOSED CURB ----- PROPOSED EASEMENT PROPOSED RIGHT OF WAY — — PROPOSED SETBACK LINE PROPOSED SIDEWALK PROPOSED SIGN PROPOSED STORM SEWER ----- S ------ PROPOSED SANITARY SEWER LATERAL PROPOSED WATER LATERAL

- THE CONTRACTOR, DURING THE PERFORMANCE OF ALL WORK ASSOCIATED WITH THE CONSTRUCTION OF THE PROJECT, IS RESPONSIBLE FOR COMPLIANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS, CODES AND REGULATIONS.
- 13. ANY DISCREPANCIES FOUND BETWEEN THE DRAWINGS AND SITE CONDITIONS OR ANY INCONSISTENCIES OR AMBIGUITIES IN DRAWINGS SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER, IN WRITING, WHO SHALL PROMPTLY ADDRESS SUCH INCONSISTENCIES OR AMBIGUITIES. WORK DONE BY THE CONTRACTOR AFTER HIS DISCOVERY OF SUCH DISCREPANCIES, INCONSISTENCIES OR AMBIGUITIES SHALL BE DONE AT THE CONTRACTOR'S RISK.
- 14. THE CHALFONT NEW BRITAIN JOINT SEWER AUTHORITY IS TO BE CONTACTED ONE WEEK PRIOR TO SANITARY SEWER CONSTRUCTION, AND 72 HOURS PRIOR TO EXCAVATING NEAR, AND CONNECTING INTO ANY EXISTING SANITARY SEWER.
- 15. ALL SANITARY SEWER CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THE CHALFONT NEW BRITAIN JOINT SEWER AUTHORITY (CNBSA)
- a.) THE MINIMUM ALLOWABLE LATERAL SLOPE SHALL BE 0.02 FT/FT. b.) MAGNETIC - TYPE LOCATOR TAPE SHALL BE LAID DIRECTLY OVER THE TOP OF PVC PIPE, TWO FEET (2') BELOW GRADE. c.) CONTRACTOR SHALL MAKE TESTS AS DIRECTED BY THE ENGINEER TO ASCERTAIN IF THE PIPE IS PROPERLY ALIGNED AND THE JOINTS ARE LIGHT. ENGINEER MUST WITNESS ALL TESTS OR THE CONTRACTOR WILL NEED TO RETEST IN THE PRESENCE OF THE ENGINEER AT THE SOLE COST OF THE CONTRACTOR.
- CONTRACTOR TO CONNECT NEW BUILDING UTILITY CONNECTIONS TO EXISTING WATER AND SEWER.
- 17. FINAL DESIGN OF PROPOSED WATER AND SEWER FACILITIES ARE SUBJECT TO THE REVIEW AND APPROVAL BY THE SERVICING WATER AND SEWER AUTHORITY. ALL WATER MAIN CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH NORTH WALES WATER AUTHORITY SPECIFICATIONS.
- 18. LATERALS ARE TO BE LOCATED AS SHOWN ON THE PLAN.
- WATER SERVICE IS TO BE LOCATED A MIN. OF 1.5 FEET ABOVE THE SANITARY LATERAL WITH A MINIMUM COVER OF 48".
- NO GROUNDING OF ELECTRICAL SERVICE TO COPPER WATER SERVICE IS PERMITTED. CABLE AND TELECOMM UTILITIES TO FOLLOW ELECTRIC FACILITIES ALIGNMENTS,
- 18" VERTICAL CLEARANCE OR 10 FEET OF HORIZONTAL CLEARANCE SHALL BE PROVIDED BETWEEN THE SANITARY SEWER AND ALL OTHER UTILITIES. OTHERWISE, CONCRETE ENCASEMENT OF THE SANITARY SEWER SHALL BE PROVIDED.
- 22. ALL DOWNSPOUT LOCATIONS TO BE OUTFITTED WITH A OVERFLOW / CLEANOUT. OVERFLOWS SHALL HAVE A SPLASHBLOCK INSTALLED AT THEIR OUTFALL LOCATION.
- 23. PROPOSED ROOF DRAIN CONNECTIONS SHOWN SCHEMATICALLY, FINAL DESIGN TO BE PROVIDED BY ARCHITECT AT TIME OF BUILDING PERMITS.
- 24. MATERIALS SHOWN HEREON, SUCH AS ADS N-12 PIPE, OR APPROVED EQUAL SHALL BE UTILIZED.
- 25. REFER TO ARCHITECTURAL, MECHANICAL, ELECTRICAL AND PLUMBING PLANS FOR ADDITIONAL COORDINATION W/ IN 5' OF BUILDINGS.
- PUBLIC SEWER SERVICE WILL BE PROVIDED BY BCSWA, AND CONSTRUCTION OF ALL SANITARY SEWER IMPROVEMENTS SHALL COMPLY WITH AUTHORITY STANDARDS.
- 27. ALL BURIED GRAVITY SANITARY SEWER MAINS AND LATERALS SHALL BE CLASS 50 DUCTILE IRON PIPE OR PVC SDR-26 OR SDR-35.
- THRUST BLOCKS SHALL BE PROVIDED AT ALL BENDS AND TEES.
- 29. GENERAL CONTRACTOR (GC) TO COORDINATE LIGHT POLE LOCATIONS WITH TRADES.

# **GENERAL CONSTRUCTION NOTES:**

CONTACT: JOSEPH MURPHY
EMAIL: JMURPHY@NWWATER.COM

1. NO CERTIFICATION IS MADE BY VAN CLEEF ENGINEERING ASSOCIATES AS TO THE POSITION OF ANY UTILITIES OR TO THE COMPLETENESS OF ANY UTILITIES SHOWN. ALL CONTRACTORS WORKING ON THIS SITE SHALL COMPLY WITH THE REQUIREMENTS OF ACT 287 OF 1974 AS AMENDED BY ACT 50 OF 2017 (UNDERGROUND UTILITY LINE PROTECTION LAW). VCEA HAS REGISTERED THIS SITE WITH THE PA ONE-CALL SYSTEM ON NOVEMBER 14, 2022 (SERIAL NO. 20223183581).

THE FOLLOWING IS A LIST OF REGISTERED UTILITY USERS FOR NEW BRITAIN

COMPANY: CHALFONT NEW BRITAIN TWP JT SEWAGE AUTH ADDRESS: 1645 UPPER STATE RD DOYLESTOWN, PA. 18901 CONTACT: JOHN SCHMIDT CHALFONT, PA. 18914 EMAIL: RCRESSMAN@NEWBRITAINTOWNSHIP.ORG EMAIL: JSCHMIDT@CNBSA.ORG COMPANY: COMCAST COMPANY: PECO AN EXELON COMPANY C/O USIC ADDRESS: 190 SHOEMAKER RE KING OF PRUSSIA, PA. 19406 CONTACT: MIKE KIMBERLY EMAIL: MIKE\_KIMBERLY@CABLE.COMCAST.COM EMAIL: NIKKIASIMPKINS@USICLLC.COM COMPANY: VERIZON BUSINESS FORMERLY MCI COMPANY: NORTH WALES WATER AUTHORITY ADDRESS: 7000 WESTON PKWY ADDRESS: 200 W WALNUT ST PO BOX 1339 NORTH WALES, PA. 19454 CONTACT: VICTOR WOOD

EMAIL: VICTOR.S.WOOD@VERIZON.COM

- 2. IT IS IMPERATIVE THAT UTILITY COMPANIES ARE NOTIFIED PRIOR TO ANY EXCAVATION AND/OR CONSTRUCTION. CALL 1-800-242-1776 TO ORDER UTILITY MARKOUTS BY OTHERS.
- 3. THE LOCATIONS OF EXISTING UTILITIES AS SHOWN ARE APPROXIMATE ONLY. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL PUBLIC OR PRIVATE UTILITIES WHICH LIE IN OR ADJACENT TO THE CONSTRUCTION SITE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING, AT HIS EXPENSE, ALL EXISTING UTILITIES DAMAGED DURING CONSTRUCTION.
- 4. THE CONTRACTOR, DURING THE PERFORMANCE OF ALL WORK ASSOCIATED WITH THE CONSTRUCTION OF THE PROJECT, IS RESPONSIBLE FOR COMPLIANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS, CODES AND REGULATIONS.
- 5. ANY DISCREPANCIES FOUND BETWEEN THE DRAWINGS AND SITE CONDITIONS OR ANY INCONSISTENCIES OR AMBIGUITIES IN DRAWINGS SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER, IN WRITING, WHO SHALL PROMPTLY ADDRESS SUCH INCONSISTENCIES OR AMBIGUITIES. WORK DONE BY THE CONTRACTOR AFTER HIS DISCOVERY OF SUCH DISCREPANCIES, INCONSISTENCIES OR AMBIGUITIES SHALL BE DONE AT THE CONTRACTOR'S RISK.
- 6. ALL MATERIAL USED FOR FILL OR BACKFILL SHALL BE FREE OF WOOD, ROOTS, ROCKS, BOULDERS OR ANY OTHER NON-COMPACTIBLE SOIL TYPE MATERIAL. UNSATISFACTORY MATERIALS ALSO INCLUDE MAN-MADE FILLS AND REFUSE DEBRIS DERIVED FROM ANY SOURCE.
- 7. ALL EXCAVATIONS, INCLUDING TRENCHES, SHALL BE KEPT DRY TO PROTECT THEIR
- 8. MATERIALS USED TO CONSTRUCT EMBANKMENTS FOR ANY PURPOSE, BACKFILL AROUND DRAINAGE STRUCTURES OR IN UTILITY TRENCHES OR ANY OTHER DEPRESSIONS REQUIRING FILL OR BACKFILL SHALL BE COMPACTED TO AT LEAST 95% OF MAXIMUM DENSITY AS DETERMINED BY THE STANDARD PROCTOR TEST AS SET OUT IN ASTM STANDARD D-698 UNLESS SPECIFIED ELSEWHERE WITHIN THE CONSTRUCTION PLANS OR IF SPECIFIED DIFFERENTLY BY THE TOWNSHIP ENGINEER. CONTRACTOR TO VERIFY COMPACTION REQUIREMENTS WITH TOWNSHIP ENGINEER'S OFFICE PRIOR TO EXCAVATION.
- 9. ALL CONSTRUCTION DEBRIS INCLUDING EXCESS EXCAVATED MATERIAL, SCRAP WOOD, BRICKS, BLOCKS, ETC. SHALL BE DISPOSED OF BY THE CONTRACTOR IN ACCORDANCE WITH FEDERAL, STATE AND LOCAL REQUIREMENTS.
- 10. ELEVATIONS, DIMENSIONS, AND THE LOCATIONS OF LINEAR FEATURES SHALL BE FIELD VERIFIED BY THE CONTRACTOR.
- 11. MINIMUM SLOPE OF ALL GRASS AREAS SHALL NOT BE LESS THAN 2%, AND FOR PAVED AREAS, NOT LESS THAN 1%.

**NEW BRITAIN TOWNSHIP** 

**BUCKS COUNTY, PENNSYLVANIA** 

TMP'S 26-006-101-001

RAWN BY

HECKED BY

**B NUMBER** 

RJN / ENP

PER TOWNSHIP ENGINEER REVIEW LETTER

**SERIAL NO. 20223183581** 

now what's below. STOP! CALL 1-800-242-1776

**GRAPHIC SCALE** 

1 INCH = 20 FT

NLY THOSE PLANS WHICH CONTAIN A DIGI MPRESSED, OR COLORIZED INK SEAL OF TH RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERE

ALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARE OR THE OWNER DESIGNATED HEREON. AN

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NGINEERING ASSOCIATES IS PROHIBITE RELIANCE ON THIS PLAN FOR ANY PURPO

THER THAN THAT WHICH IS INTENDED SHALL

AT THE SOLE DISCRETION AND LIABILITY OF APPLICABLE PARTY.

EAN DAVID McGRANAHAN

VAN CLEEF ENGINEERING ASSOCIATES, LLC

501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM

PHONE (215) 345-1876

UTILITY PLAN

NSYLVANIA PROFESSIONA

NEER NUMBER 085090

Call before you dig. PA Law requires 3 working days notice

10 20

PLAN NOTATION

Before You Dig Anywhere

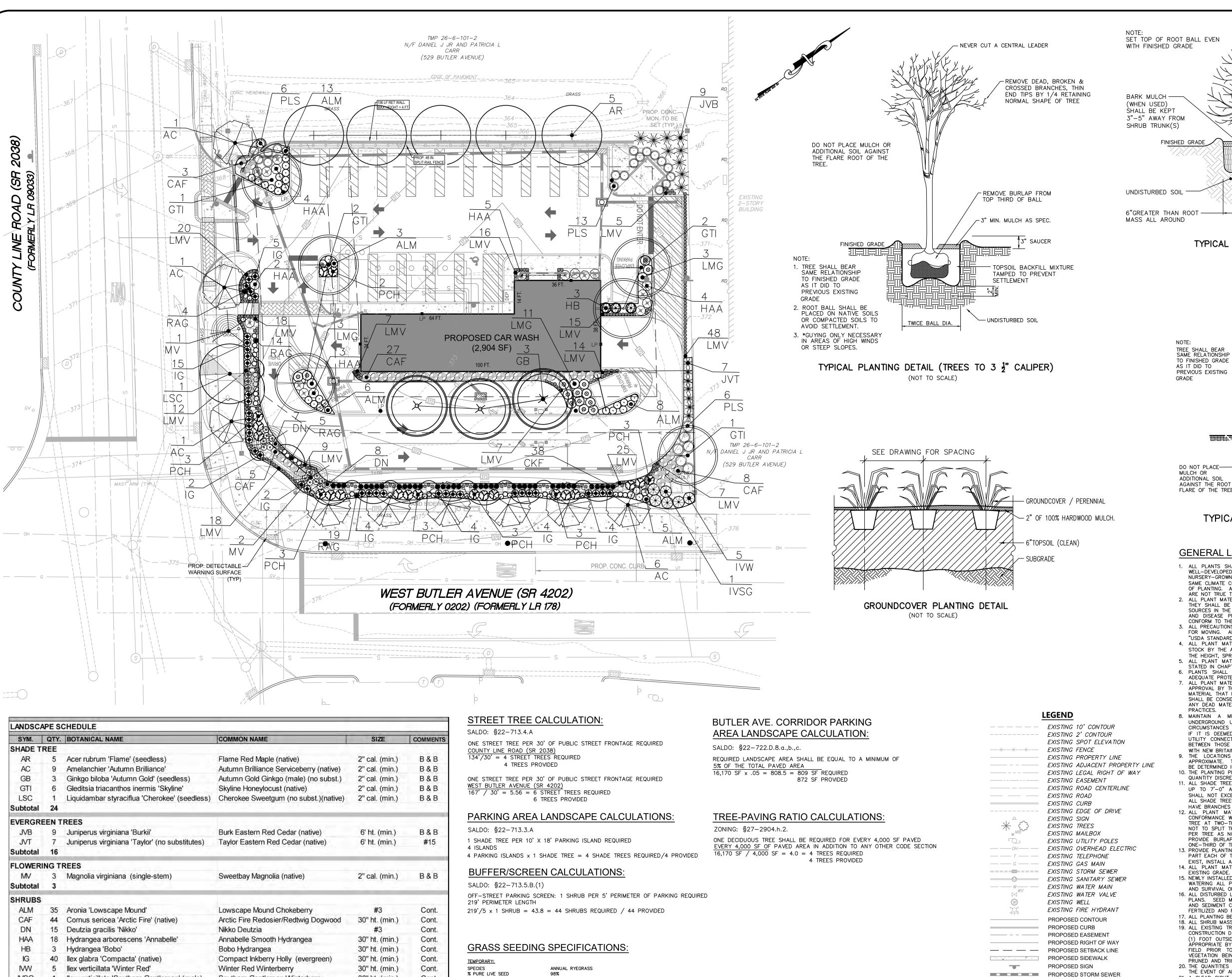
PA ONE CALL SYSTEM, INC.

PENNSYLVAN

AUTH DATE

DATED 05/21/2025.

EV DESCRIPTION



87.0 LB/ACRE

1,100 LB/ACRE

2 T/ACRE

3 T/ACRE

RED FESCUE

1,100 LB/ACRE

HAY OR STRAW

3 T/ACRE

STRAW

COMMERCIAL 10-20-10

KENTUCKY BLUEGRASS / CREEPING

41 / 20 / 14 / 20 LB/ACRE

COMMERCIAL 10-20-10

FESCUE / PERENNIAL RYEGRASS / CHEWING'S

APRIL 15 - MAY 30, AUGUST 15 - OCTOBER 15

**IVSG** 

LMG

PCH

PLS

RAG

Subtotal 224

**PERENNIALS** 

Subtotal 302

TOTAL 569

llex verticillata 'Southern Gentleman' (male)

17 Lonicera pliata 'Moss Green'

38 Calamagrostis 'Karl Foerster'

221 Liriope muscari 'Variegata'

20 Prunus laurocerasus 'Chestnut Hill'

26 Prunus laurocerasus 'Schipkaensis'

43 Rudbeckia fulgida 'American Gold Rush'

Southern Gentleman Winterberry

Schip / Skip Laurel (evergreen)

Karl Foerster Feather Reed Grass

Amer. Gold Rush Black-Eyed Susan

Variegated Liriope (evergreen)

Moss Green Prostrate Honeysuckle

Chestnut Hill Cherry Laurel (evergreen)

30" ht. (min.)

30" ht. (min.)

30" ht. (min.)

2 Gal.

1 Gal.

1 Gal.

#2

Cont.

Cont.

Cont.

Cont.

24" o.c.

18" o.c.

24" o.c.

APPLICATION RATE

FERTILIZER APPL. RATE

FERTILIZER TYPE

LIMING RATE

MULCH TYPE

MULCH RATE

PERMANENT:

% PURE LIVE SEED

FERTILIZER TYPE

ANCHOR MATERIAL

ANCHORING METHOD

RATE OF ANCHOR MATERIAL APPL. N/A

LIMING RATE

MULCH TYPE MULCH RATE

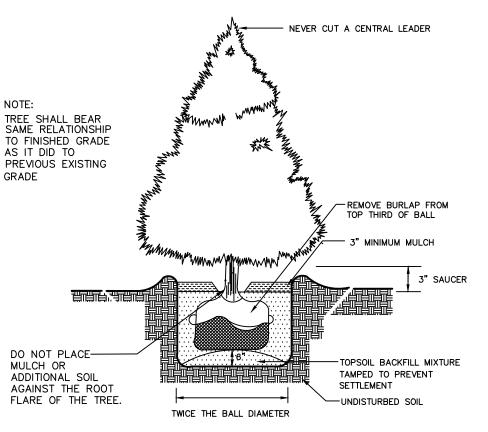
APPLICATION RATE

FERTILIZER APPL. RATE

SPECIES

SET TOP OF ROOT BALL EVEN REMOVE CONTAINER AND MAKE 2 SHALLOW CUTS THE LENGTH OF THE ROOT MASS OR LOOSEN ROOTS. 3" MULCH - SHREDDED BARK MULCH 3" MIN. RIM OF WATER SAUCER -SOIL BACKFILL MIXTURE: SPECIFIED MIX OR 1/3 TOPSOIL, 1/3 PEAT MOSS, 1/3 SAND - SCARIFY SUBGRADE

TYPICAL SHRUB PLANTING DETAIL



TYPICAL EVERGREEN PLANTING DETAIL

## **GENERAL LANDSCAPING NOTES**

- ALL PLANTS SHALL BE TYPICAL OF THEIR SPECIES OR VARIETY; AND SHALL HAVE NORMAL WELL-DEVELOPED BRANCHES AND VIGOROUS FIBROUS ROOT SYSTEMS. ALL PLANTS SHALL B NURSERY-GROWN UNLESS OTHERWISE STATED; THEY SHALL HAVE BEEN GROWING UNDER TH SAME CLIMATE CONDITIONS AS THE MUNICIPALITY FOR AT LEAST TWO (2) YEARS PRIOR TO DATE OF PLANTING. ALL PLANTS WHICH ARE FOUND UNSUITABLE IN GROWTH OR CONDITION OR WHICH ARE NOT TRUE TO NAME SHALL BE REMOVED AND REPLACED WITH ACCEPTABLE PLANTS. 2. ALL PLANT MATERIAL SHALL BE TWICE TRANSPLANTED, NURSERY-GROWN OF SPECIMEN QUALITY. THEY SHALL BE OF SYMMETRICAL GROWTH OR TYPICAL OF THE VARIETY AND SUPPLIED FROM SOURCES IN THE SAME HARDINESS ZONE AS THE DEVELOPMENT IS LOCATED AND FREE OF INSECT AND DISEASE PROBLEMS OR OBJECTIONABLE DISFIGUREMENTS. ALL PLANT MATERIAL SHALL CONFORM TO THE STANDARDS OF THE AMERICAN ASSOCIATION OF NURSERYMEN.
- 3. ALL PRECAUTIONS CUSTOMARY IN GOOD TRADE PRACTICE SHALL BE TAKEN IN PREPARING PLANTS FOR MOVING. ALL BALLED AND BURLAPPED PLANTS SHALL BE DUG TO MEET OR EXCEED THE "USDA STANDARDS FOR NURSERY STOCK". 4. ALL PLANT MATERIAL SHALL MEET THE STANDARDS OF AMERICAN STANDARD FOR NURSERY STOCK BY THE AMERICAN ASSOCIATION OF NURSERYMEN (1994), OR MOST RECENT EDITION, AND THE HEIGHT, SPREAD AND/OR CALIPER FOR TREES AND SHRUBS.
- 5. ALL PLANT MATERIAL SHALL BE INSTALLED IN ACCORDANCE WITH THE PLANTING PRACTICES STATED IN CHAPTER 3 OF TREE MAINTENANCE BY P. P. PIRONE, FIFTH OR MOST RECENT EDITION. 6. PLANTS SHALL BE PACKED, TRANSPORTED AND HANDLED WITH UTMOST CARE TO INSURE ADEQUATE PROTECTION AGAINST INJURY. 7. ALL PLANT MATERIALS SHALL BE QUARANTEED FOR A PERIOD OF 18 MONTHS FOLLOWING FINAL APPROVAL BY THE TOWNSHIP, EXCEPT WHERE OTHERWISE NOTED IN THIS SECTION. ANY PLANT MATERIAL THAT HAS 25% OR GREATER DEAD BRANCHES SHALL BE CONSIDERED DEAD. A TREE SHALL BE CONSIDERED DEAD WHEN THE MAIN LEADER HAS DIED OR 25% OF THE CROWN IS DEAD. ANY DEAD MATERIAL SHALL BE REPLACED AND INSTALLED ACCORDING TO APPROVED PLANTING
- 8. MAINTAIN A MINIMUM FIFTEEN (15) FOOT SEPARATION DISTANCE BETWEEN TREES AND UNDERGROUND UTILITIES. THIS MINIMUM MAY BE REDUCED TO TEN (10) FEET IN CERTAIN CIRCUMSTANCES WITH APPROVAL OF THE TOWNSHIP ENGINEER. HOWEVER, DURING CONSTRUCTION, IF IT IS DEEMED NECESSARY TO FIELD ADJUST THE LOCATIONS OF ANY PLANTINGS AND/OR UTILITY CONNECTIONS DUE TO THE NEED TO PROVIDE THE MINIMUM TEN FOOT SEPARATION BETWEEN THOSE PLAN ELEMENTS, THIS ADJUSTMENT SHOULD BE UNDERTAKEN IN CONSULTATION WITH NEW BRITAIN TOWNSHIP STAFF/PROFESSIONALS.
- 9. THE LOCATIONS OF ALL PLANT MATERIAL INDICATED ON THE LANDSCAPE PLANS ARE APPROXIMATE. THE FINAL LOCATION OF ALL PLANT MATERIAL AND PLANTING BED LINES SHALL BE DETERMINED IN THE FIELD UNDER THE DIRECTION OF THE LANDSCAPE ARCHITECT. 10. THE PLANTING PLAN SHALL TAKE PRECEDENCE OVER THE PLANT SCHEDULE SHOULD ANY PLANT 11. ALL SHADE TREES PLANTED NEAR PEDESTRIAN OR VEHICULAR ACCESS SHOULD BE CLEAR TRUNK UP TO 7'-0" ABOVE GRADE. ALL SHRUBBERY MATERIAL LOCATED WITHIN SIGHT TRIANGLES SHALL NOT EXCEED A MATURE HEIGHT OF 3' ABOVE THE ELEVATION OF THE ADJACENT CURB. ALL SHADE TREES PLANTED OR EXISTING IN SIGHT TRIANGLES SHALL BE PRUNED SO AS NOT TO HAVE BRANCHES BELOW 7'-0".
- 12. ALL PLANT MATERIAL SHALL BE PROPERLY GUYED, STAKED, WRAPPED AND PLANTED IN CONFORMANCE WITH THE TYPICAL PLANTING DETAILS. GUY WIRES SHALL BE ATTACHED TO THE TREE AT TWO—THIRDS THE HEIGHT OF THE TREE AND SHOULD BE LOCATED AT POINTS SO AS NOT TO SPLIT THE TRUNKS OF MULTI-STEMMED TREES. PROVIDE TWO TO THREE TREE STAKES PER TREE AS NOTED ON THE PLANS. INSTALL ALL PLANT MATERIAL ON UNDISTURBED GRADE. PROVIDE BURLAP WRAPPING WITH A 50% OVERLAP. CUT AND REMOVE BURLAP FROM TOP ONE-THIRD OF THE ROOT BALL
- 13. PROVIDE PLANTING PITS AS INDICATED ON PLANTING DETAILS. BACKFILL PLANTING PITS WITH ONE PART EACH OF TOPSOIL, PEAT MOSS AND PARENT MATERIAL. IF WET OR CLAY SOIL CONDITIONS EXIST, INSTALL A 4" PERFORATED DRAIN. RUN TO GRADE OR CATCH BASIN 14. ALL PLANT MATERIAL SHALL BEAR THE SAME RELATION TO FINISHED GRADE AS IT DID TO
- 15. NEWLY INSTALLED PLANT MATERIAL SHALL BE WATERED AT THE TIME OF INSTALLATION. REGULAR WATERING ALL PLANT MATERIAL SHALL BE PROVIDED TO ENSURE THE ESTABLISHMENT, GROWTH AND SURVIVAL OF ALL PLANTS. 16. ALL DISTURBED LAWN AREAS SHALL BE STABILIZED WITH SEED AS INDICATED ON THE LANDSCAPE
- PLANS. SEED MIXTURE SHALL BE AS LISTED IN THE SEEDING SCHEDULE ON THE SOIL EROSION AND SEDIMENT CONTROL NARRATIVE. ALL DISTURBED LAWN AREAS SHALL BE TOPSOILED, LIMED, FERTILIZED AND FINE GRADED PRIOR TO LAWN INSTALLATION. 17. ALL PLANTING BEDS SHALL RECEIVE MINIMUM 3" OF SHREDDED HARDWOOD BARK.
  18. ALL SHRUB MASSES SHALL BE PLANTED IN CONTINUOUS MULCHED BEDS.
- 19. ALL EXISTING TREES AND SHRUBS TO BE PRESERVED ON SITE SHALL BE PROTECTED AGAINST CONSTRUCTION DAMAGE BY SNOW FENCING. ALL FENCING SHALL BE PLACED A MINIMUM OF ONE (1) FOOT OUTSIDE THE INDIVIDUAL TREE DRIPLINE, OR AT THE SPECIFIC LOCATION DETERMINED APPROPRIATE BY THE LANDSCAPE ARCHITECT. ALL TREES TO REMAIN SHALL BE IDENTIFIED IN THE FIELD PRIOR TO COMMENCEMENT OF CONSTRUCTION, GRADING OR CLEARING. ALL EXISTING VEGETATION BEING PRESERVED AND LOCATED AT THE EDGE OF THE NEW TREELINE SHALL BE PRUNED AND TRIMMED TO REMOVE DEAD OR DAMAGED BRANCHES. 20. THE QUANTITIES SHOWN ON THE PLAN SHALL HAVE PRECEDENCE OVER THE PLANT SCHEDULE IN THE EVENT OF ANY QUANTITY DISCREPANCIES.
- 21. A CLEAR-SIGHT TRIANGLE OF TEN FEET (10') MEASURED FROM THE POINT OF INTERSECTION OF THE STREET LINE AND THE EDGE OF THE ACCESSWAY SHALL BE MAINTAINED, WITHIN WHICH VEGETATION AND OTHER VISUAL OBSTRUCTIONS SHALL BE LIMITED TO A HEIGHT OF NOT MORE

PROPOSED WATER LATERAL THAN TWO FEET (2') ABOVE THE STREET GRADE. PROPOSED LIGHT PROPOSED LANDSCAPING

PROPOSED SANITARY SEWER LATERAL

HECKED BY

PENNSYLVANIA REGISTERED

THIS PLAN IS FOR LANDSCAPE PURPOSES ONLY.

7 of 18

DESIGNED BY

OB NUMBER

RAWN BY

PER TOWNSHIP ENGINEER REVIEW LETTER

**SERIAL NO. 20223183581** 

now what's below. STOPI CALL 1-800-242-1770

**GRAPHIC SCALE** 

1 INCH = 20 FT

ONLY THOSE PLANS WHICH CONTAIN A DIGIT MPRESSED, OR COLORIZED INK SEAL OF TH RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERE

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EAN DAVID McGRANAHAN

VAN CLEEF ENGINEERING ASSOCIATES, LLC

501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM

PHONE (215) 345-1876

LANDSCAPE

**PLAN** 

TMP'S 26-006-101-001

**NEW BRITAIN TOWNSHIP** 

**BUCKS COUNTY, PENNSYLVANIA** 

INSYLVANIA PROFESSIONAL

NEER NUMBER 085090

Call before you dig. PA Law requires 3 working days noti

10 20

PLAN NOTATION

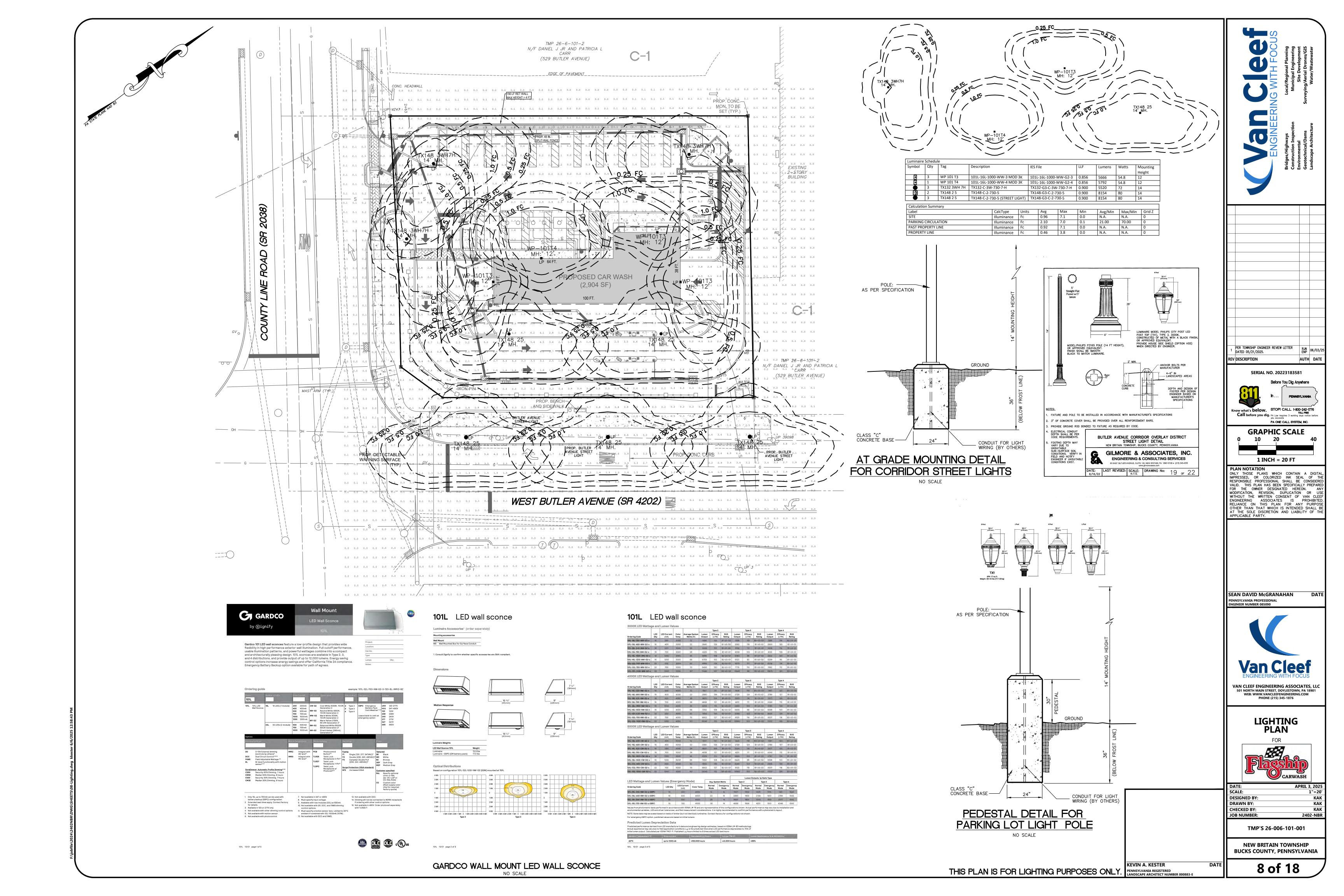
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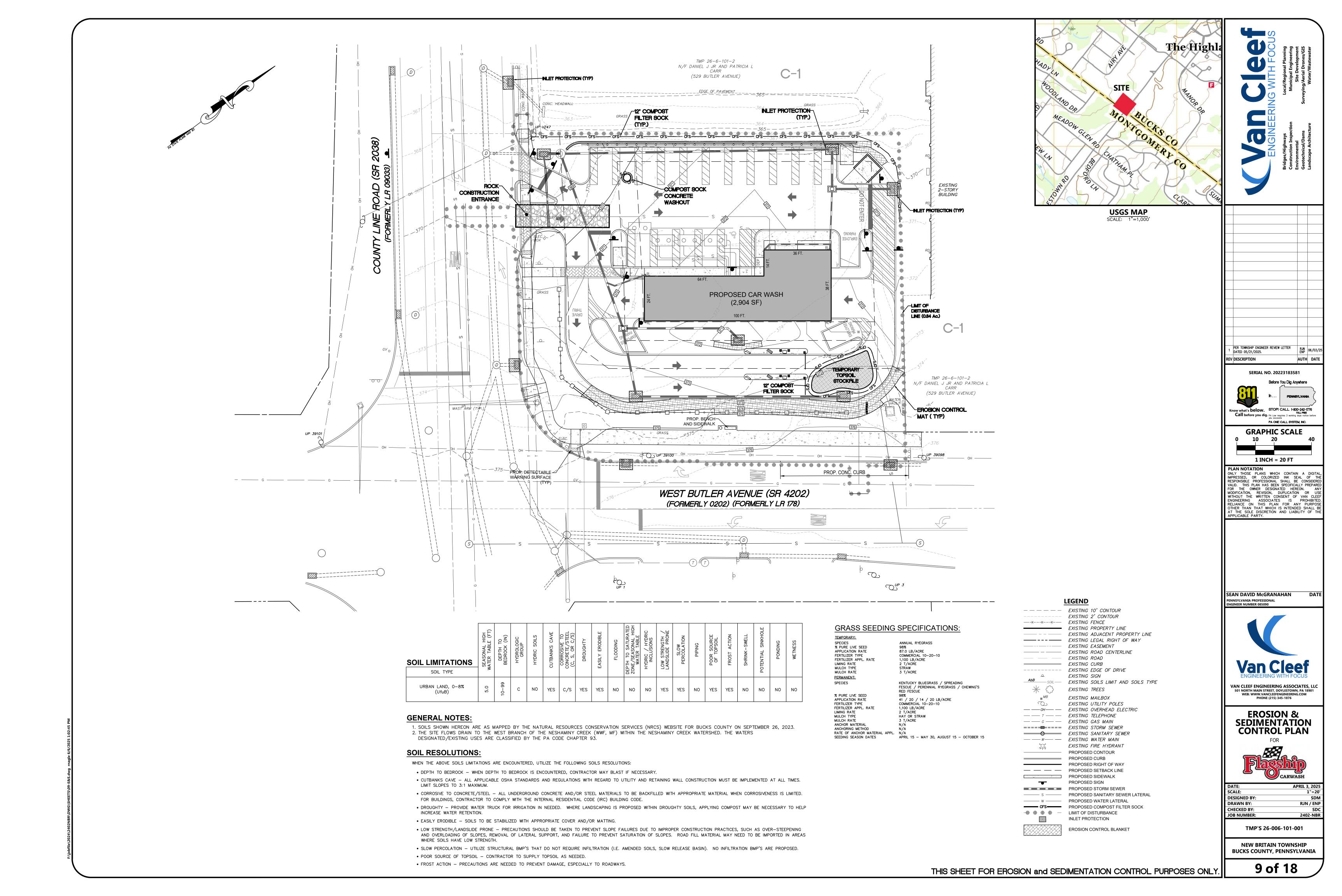
PA ONE CALL SYSTEM, INC.

AUTH DATE

DATED 05/21/2025.

V DESCRIPTION





# GENERAL EROSION & SEDIMENT CONTROL NOTES

- 1. ALL EARTH DISTURBANCES. INCLUDING CLEARING AND GRUBBING AS WELL AS CUTS AND FILLS SHALL BE DONE IN ACCORDANCE WITH THE APPROVED E&S PLAN. A COPY OF THE APPROVED DRAWINGS (STAMPED, SIGNED AND DATED BY THE REVIEWING AGENCY) MUST BE AVAILABLE AT THE PROJECT SITE AT ALL TIMES. THE REVIEWING AGENCY SHALL BE NOTIFIED OF ANY CHANGES TO THE APPROVED PLAN PRIOR TO IMPLEMENTATION OF THOSE CHANGES. THE REVIEWING AGENCY MAY REQUIRE A WRITTEN SUBMITTAL OF THOSE CHANGES FOR REVIEW AND APPROVAL AT ITS DISCRETION.
- 2. AT LEAST 7 DAYS PRIOR TO STARTING ANY EARTH DISTURBANCE ACTIVITIES, INCLUDING CLEARING AND GRUBBING, THE OWNER AND/OR OPERATOR SHALL INVITE ALL CONTRACTORS, THE LANDOWNER, APPROPRIATE MUNICIPAL OFFICIALS, THE E&S PLAN PREPARER, THE PCSM PLAN PREPARER, THE LICENSED PROFESSIONAL RESPONSIBLE FOR OVERSIGHT OF CRITICAL STAGES OF IMPLEMENTATION OF THE PCSM PLAN, AND A REPRESENTATIVE FROM THE LOCAL CONSERVATION DISTRICT TO AN ON-SITE PRECONSTRUCTION MEETING.
- 3. AT LEAST 3 DAYS PRIOR TO STARTING ANY EARTH DISTURBANCE ACTIVITIES, OR EXPANDING INTO AN AREA PREVIOUSLY UNMARKED, THE PENNSYLVANIA ONE CALL SYSTEM INC. SHALL BE NOTIFIED AT 1-800-242-1776 FOR THE LOCATION OF EXISTING UNDERGROUND UTILITIES.
- 4. ALL EARTH DISTURBANCE ACTIVITIES SHALL PROCEED IN ACCORDANCE WITH THE SEQUENCE PROVIDED ON THE PLAN DRAWINGS. DEVIATION FROM THAT SEQUENCE MUST BE APPROVED IN WRITING FROM THE LOCAL CONSERVATION DISTRICT OR BY THE DEPARTMENT PRIOR TO IMPLEMENTATION.
- 5. AREAS TO BE FILLED ARE TO BE CLEARED, GRUBBED, AND STRIPPED OF TOPSOIL TO REMOVE TREES, VEGETATION, ROOTS AND OTHER OBJECTIONABLE MATERIAL.
- 6. CLEARING, GRUBBING, AND TOPSOIL STRIPPING SHALL BE LIMITED TO THOSE AREAS DESCRIBED IN EACH STAGE OF THE CONSTRUCTION SEQUENCE. GENERAL SITE CLEARING, GRUBBING AND TOPSOIL STRIPPING MAY NOT COMMENCE IN ANY STAGE OR PHASE OF THE PROJECT UNTIL THE E&S BMPS SPECIFIED BY THE BMP SEQUENCE FOR THAT STAGE OR PHASE HAVE BEEN INSTALLED AND ARE FUNCTIONING AS DESCRIBED IN THIS E&S
- 7. AT NO TIME SHALL CONSTRUCTION VEHICLES BE ALLOWED TO ENTER AREAS OUTSIDE THE LIMIT OF DISTURBANCE BOUNDARIES SHOWN ON THE PLAN MAPS. THESE AREAS MUST BE CLEARLY MARKED AND FENCED OFF BEFORE CLEARING AND GRUBBING OPERATIONS BEGIN.
- 8. TOPSOIL REQUIRED FOR THE ESTABLISHMENT OF VEGETATION SHALL BE STOCKPILED AT THE LOCATION(S) SHOWN ON THE PLAN MAPS(S) IN THE AMOUNT NECESSARY TO COMPLETE THE FINISH GRADING OF ALL EXPOSED AREAS THAT ARE TO BE STABILIZED BY VEGETATION. EACH STOCKPILE SHALL BE PROTECTED IN THE MANNER SHOWN ON THE PLAN DRAWINGS. STOCKPILE HEIGHTS SHALL NOT EXCEED 35 FEET. STOCKPILE SLOPES
- 9. IMMEDIATELY UPON DISCOVERING UNFORESEEN CIRCUMSTANCES POSING THE POTENTIAL FOR ACCELERATED EROSION AND/OR SEDIMENT POLLUTION, THE OPERATOR SHALL IMPLEMENT APPROPRIATE BEST MANAGEMENT PRACTICES TO ELIMINATE POTENTIAL FOR EROSION AND SEDIMENT POLLUTION AND NOTIFY THE LOCAL CONSERVATION DISTRICT AND/OR THE REGIONAL OFFICE OF THE DEPARTMENT.
- 10. ALL BUILDING MATERIAL AND WASTES MUST BE REMOVED FROM THE SITE AND RECYCLED IN ACCORDANCE WITH DEP'S SOLID WASTE REGULATIONS (25 PA CODE 260.1 ET SEQ., 271.1 ET SEQ., AND 287.1 ET SEQ.), AND/OR ANY ADDITIONAL LOCAL, STATE OR FEDERAL REGULATIONS. NO BUILDING MATERIALS (USED OR UNUSED) OR WASTE MATERIALS SHALL BE BURNED, BURIED, DUMPED OR DISCHARGED AT THE SITE.
- 11. ALL OFF-SITE WASTE AND BORROW AREAS MUST HAVE AN E&S PLAN APPROVED BY THE LOCAL CONSERVATION DISTRICT OR THE DEPARTMENT FULLY IMPLEMENTED PRIOR TO BEING ACTIVATED.
- 12. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT ANY MATERIAL BROUGHT ON SITE IS CLEAN FILL, FORM FP-001 MUST BE RETAINED BY THE PROPERTY OWNER FOR ANY FILL MATERIAL AFFECTED BY A SPILL OR RELEASE OF A REGULATED SUBSTANCE BUT QUALIFYING AS CLEAN FILL DUE TO ANALYTICAL TESTING.
- 13. ALL PUMPING OF WATER FROM ANY WORK AREA SHALL BE DONE ACCORDING TO THE PROCEDURE DESCRIBED IN THIS PLAN, OVER UNDISTURBED VEGETATED AREAS.
- 14. UNTIL THE SITE IS STABILIZED, ALL EROSION AND SEDIMENT BMPS SHALL BE MAINTAINED PROPERLY. MAINTENANCE SHALL INCLUDE INSPECTIONS OF ALL EROSION AND SEDIMENT BMPS AFTER EACH RUNOFF EVENT AND ON A WEEKLY BASIS. ALL PREVENTATIVE AND REMEDIAL MAINTENANCE WORK, INCLUDING CLEAN OUT, REPAIR, REPLACEMENT, REGRADING, RESEEDING, REMULCHING AND RENETTING MUST BE PERFORMED IMMEDIATELY. IF THE E&S BMPS FAIL TO PERFORM AS EXPECTED, REPLACEMENT BMPS, OR MODIFICATIONS OF THOSE INSTALLED WILL BE REQUIRED.
- 15. A LOG SHOWING DATES THAT E&S BMPS WERE INSPECTED AS WELL AS ANY DEFICIENCIES FOUND AND THE DATE THEY WERE CORRECTED SHALL BE MAINTAINED ON THE SITE AND BE MADE AVAILABLE TO REGULATORY AGENCY OFFICIALS AT THE TIME OF INSPECTION. WRITTEN DOCUMENTATION OF INSPECTION AND REPLACEMENT OF BMP's BY CONTRACTOR.
- 16. SEDIMENT TRACKED ONTO ANY PUBLIC ROADWAY OR SIDEWALK SHALL BE IMMEDIATELY RETURNED TO THE CONSTRUCTION SITE AND DISPOSED IN THE MANNER DESCRIBED IN THIS PLAN. IN NO CASE SHALL THE SEDIMENT BE WASHED, SHOVELED, OR SWEPT INTO ANY ROADSIDE DITCH, STORM SEWER, OR SURFACE WATER.
- 17. ALL SEDIMENT REMOVED FROM BMPS SHALL BE DISPOSED OF IN THE MANNER DESCRIBED ON THE PLAN DRAWINGS.
- 18. AREAS WHICH ARE TO BE TOPSOILED SHALL BE SCARIFIED TO A MINIMUM DEPTH OF 8" OR TO THE DEPTH ENCOUNTERED WITHIN THE SITE PRIOR TO PLACEMENT OF TOPSOIL. AREAS TO BE VEGETATED SHALL HAVE A MINIMUM 8" OF TOPSOIL (OR TO THE DEPTH ENCOUNTERED WITHIN THE SITE, WHICHEVER IS GREATER) IN PLACE PRIOR TO SEEDING AND MULCHING.
- 19. ALL FILLS SHALL BE COMPACTED AS REQUIRED TO REDUCE EROSION, SLIPPAGE, SETTLEMENT, SUBSIDENCE OR OTHER RELATED PROBLEMS. FILL INTENDED TO SUPPORT BUILDINGS, STRUCTURES AND CONDUITS, ETC. SHALL BE COMPACTED IN ACCORDANCE WITH LOCAL REQUIREMENTS OR
- 20. ALL EARTHEN FILLS SHALL BE PLACED IN COMPACTED LAYERS NOT TO EXCEED 9 INCHES IN THICKNESS.
- 21. FILL MATERIALS SHALL BE FREE OF FROZEN PARTICLES, BRUSH, ROOTS, SOD, OR OTHER FOREIGN OR OBJECTIONABLE MATERIALS THAT WOULD INTERFERE WITH OR PREVENT CONSTRUCTION OF SATISFACTORY FILLS.
- 22. FROZEN MATERIALS OR SOFT, MUCKY, OR HIGHLY COMPRESSIBLE MATERIALS SHALL NOT BE INCORPORATED INTO FILLS.
- 23. FILL SHALL NOT BE PLACED ON SATURATED OR FROZEN SURFACES.
- 24. SEEPS OR SPRINGS ENCOUNTERED DURING CONSTRUCTION SHALL BE HANDLED IN ACCORDANCE WITH THE STANDARD AND SPECIFICATION FOR SUBSURFACE DRAIN OR OTHER APPROVED METHOD.
- 25. ALL GRADED AREAS SHALL BE PERMANENTLY STABILIZED IMMEDIATELY UPON REACHING FINISHED GRADE. CUT SLOPES IN COMPETENT BEDROCK AND ROCK FILLS NEED NOT BE VEGETATED. SEEDED AREAS WITHIN 50 FEET OF A SURFACE WATER, OR AS OTHERWISE SHOWN ON THE PLAN DRAWINGS. SHALL BE BLANKETED ACCORDING TO THE STANDARDS OF THIS PLAN.
- 26. IMMEDIATELY AFTER EARTH DISTURBANCE ACTIVITIES CEASE IN ANY AREA OR SUBAREA OF THE PROJECT. THE OPERATOR SHALL STABILIZE ALL DISTURBED AREAS. DURING NON-GERMINATING MONTHS, MULCH OR PROTECTIVE BLANKETING SHALL BE APPLIED AS DESCRIBED IN THE PLAN. AREAS NOT AT FINISHED GRADE. WHICH WILL BE REACTIVATED WITHIN 1 YEAR. MAY BE STABILIZED IN ACCORDANCE WITH THE TEMPORARY STABILIZATION SPECIFICATIONS. THOSE AREAS WHICH WILL NOT BE REACTIVATED WITHIN 1 YEAR SHALL BE STABILIZED IN ACCORDANCE WITH THE PERMANENT STABILIZATION SPECIFICATIONS.
- 27. PERMANENT STABILIZATION IS DEFINED AS A MINIMUM UNIFORM, PERENNIAL 70% VEGETATIVE COVER OR OTHER PERMANENT NON-VEGETATIVE COVER WITH A DENSITY SUFFICIENT TO RESIST ACCELERATED EROSION. CUT AND FILL SLOPES SHALL BE CAPABLE OF RESISTING FAILURE DUE TO SLUMPING, SLIDING, OR OTHER MOVEMENTS.
- 28. E&S BMPS SHALL REMAIN FUNCTIONAL AS SUCH UNTIL ALL AREAS TRIBUTARY TO THEM ARE PERMANENTLY STABILIZED OR UNTIL THEY ARE REPLACED BY ANOTHER BMP APPROVED BY THE LOCAL CONSERVATION DISTRICT OR THE DEPARTMENT.
- 29. UPON COMPLETION OF ALL EARTH DISTURBANCE ACTIVITIES AND PERMANENT STABILIZATION OF ALL DISTURBED AREAS. THE OWNER AND/OR OPERATOR SHALL CONTACT THE LOCAL CONSERVATION DISTRICT FOR AN INSPECTION PRIOR TO REMOVAL/CONVERSION OF THE E&S BMPS.
- 30. AFTER FINAL SITE STABILIZATION HAS BEEN ACHIEVED, TEMPORARY EROSION AND SEDIMENT BMPS MUST BE REMOVED OR CONVERTED TO PERMANENT POST CONSTRUCTION STORMWATER MANAGEMENT BMPS. AREAS DISTURBED DURING REMOVAL OR CONVERSION OF THE BMPS SHALL BE STABILIZED IMMEDIATELY. IN ORDER TO ENSURE RAPID REVEGETATION OF DISTURBED AREAS, SUCH REMOVAL/CONVERSIONS ARE TO BE DONE ONLY DURING THE GERMINATING SEASON.
- 31. FAILURE TO CORRECTLY INSTALL E&S BMPS, FAILURE TO PREVENT SEDIMENT-LADEN RUNOFF FROM LEAVING THE CONSTRUCTION SITE, OR FAILURE TO TAKE IMMEDIATE CORRECTIVE ACTION TO RESOLVE FAILURE OF E&S BMPS MAY RESULT IN ADMINISTRATIVE, CIVIL, AND/OR CRIMINAL PENALTIES BEING INSTITUTED BY THE DEPARTMENT AS DEFINED IN SECTION 602 OF THE PENNSYLVANIA CLEAN STREAMS LAW. THE CLEAN STREAMS LAW PROVIDES FOR UP TO \$10,000 PER DAY IN CIVIL PENALTIES, UP TO \$10,000 IN SUMMARY CRIMINAL PENALTIES, AND UP TO \$25,000 IN MISDEMEANOR CRIMINAL PENALTIES FOR EACH VIOLATION.
- 32. FILL MATERIAL FOR EMBANKMENTS SHALL BE FREE OF ROOTS, OR OTHER WOODY VEGETATION, ORGANIC MATERIAL, LARGE STONES, AND OTHER OBJECTIONABLE MATERIALS.
- 33. STOCKPILE HEIGHTS MUST NOT EXCEED 35 FEET; STOCKPILE SLOPES MUST NOT EXCEED 2.1.
- 34. THE OPERATOR/RESPONSIBLE PERSON (O/RP) ON SITE SHALL ASSURE THAT THE APPROVED EROSION AND SEDIMENT CONTROL PLAN IS PROPERLY AND COMPLETELY IMPLEMENTED
- 35. THE O/RP SHALL ASSURE THAT AN EROSION AND SEDIMENT CONTROL PLAN HAS BEEN PREPARED, APPROVED BY THE BUCKS COUNTY CONSERVATION DISTRICT AND IS BEING IMPLEMENTED AND MAINTAINED FOR ALL SOIL AND/OR ROCK SPOIL AND BORROW AREAS REGARDLESS OF
- 36. ALL PUMPING OF SEDIMENT-LADEN WATER SHALL BE THROUGH A SEDIMENT CONTROL BMP SUCH AS A PUMPED WATER FILTER BAG DISCHARGING
- 37. A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN MUST BE AVAILABLE ON THE PROJECT SITE AT ALL TIMES.
- 38. EROSION AND SEDIMENT BMPS MUST BE CONSTRUCTED, STABILIZED AND FUNCTIONAL BEFORE SITE DISTURBANCE BEGINS WITHIN THE TRIBUTARY AREAS OF THOSE BMPS.
- 39. DISTURBED AREAS THAT ARE AT FINISHED GRADE OR WHICH WILL NOT BE RE-DISTURBED WITHIN ONE YEAR MUST BE STABILIZED IN ACCORDANCE WITH PERMANENT VEGETATIVE STABILIZATION SPECIFICATIONS.
- 40. AN AREA SHALL BE CONSIDERED TO HAVE ACHIEVED FINAL STABILIZATION WHEN IT HAS A MINIMUM UNIFORM 70% (PERCENT) VEGETATIVE OR OTHER PERMANENT NON-VEGETATIVE COVER WITH A DENSITY SUFFICIENT TO RESIST ACCELERATED SURFACE EROSION AND SUBSURFACE CHARACTERISTICS SUFFICIENT TO RESIST SLIDING AND OTHER MOVEMENTS.
- 41. SEDIMENT REMOVED FROM BMPS SHALL BE DISPOSED OF ON-SITE IN LANDSCAPED AREAS OUTSIDE OF STEEP SLOPES, WETLANDS, FLOODPLAINS OR DRAINAGE SWALES AND IMMEDIATELY STABILIZED OR PLACED IN SOIL STOCKPILES AND STABILIZED.
- 42. BMPs SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER MEASURABLE STORM EVENTS (I.E. AT LEAST 0.25 INCH).
- 43. TEMPORARY STABILIZATION SHALL BE PROVIDED TO ALL BARE SOIL AREAS THAT WILL REMAIN UNDISTURBED FOR 4 OR MORE DAYS IN THE FORM OF 3 TON/ACRE STRAW MULCH OR EQUIVALENT.

#### CONSTRUCTION SEQUENCE

- 1. AT LEAST 7 DAYS PRIOR TO START OF WORK OR EARTH DISTURBANCE ACTIVITIES, A PRE-CONSTRUCTION MEETING SHALL BE HELD INCLUDING THE OWNER, SITE CONTRACTOR(S), TOWNSHIP ENGINEER(S), COUNTY CONSERVATION DISTRICT, AND THE PLAN DESIGNER.
- 2. THE LIMITS OF DISTURBANCE SHALL BE DELINEATED WITH SURVEY STAKES OR SIMILAR, PRIOR TO ANY DISTURBANCE AND SHALL NOT BE DISTURBED DURING SITE CONSTRUCTION EXCEPT FOR TEMPORARY IMPACTS FOR MITIGATION OR RESTORATION PER THE PLAN.
- 3. INSTALL THE ROCK CONSTRUCTION ENTRANCE PER THE PLAN.
- 4. CONSTRUCTION FENCING SHALL BE INSTALLED ALONG THE SOUTH AND EASTERN PROPERTY LINES. PLACE COMPOST FILTER SOCK AND INLET PROTECTION AS DELINEATED ON THE PLAN AND DOWNSTREAM OF ALL EXCAVATED OR TOPSOIL STOCKPILING AREAS TO BE DISTURBED. INSTALL CONCRETE WASHOUT FACILITY PER THE PLAN.
- 5. DEMOLISH EXISTING BUILDING.
- 6. STRIP TOPSOIL WITHIN AREAS OF PROPOSED EARTHWORK AND STOCKPILE PER THE PLAN. CESSATION OF GRADING ACTIVITIES FOR FOUR (4) DAYS OR LONGER REQUIRES TEMPORARY SEEDING.
- 7. BEGIN CONSTRUCTION OF PROPOSED RETAINING WALL, STORM SEWERS, SEWER/GAS/WATER LATERALS, BUILDING FEATURES, DRIVEWAY AND PARKING AREAS, FLAGSHIP CARWASH SIGNAGE, SIDEWALK AND ADA FEATURES AND ANY OTHER ASSOCIATED FEATURES. 7.1. CONSTRUCTION OF THE SANITARY SEWER LATERAL WILL REQUIRE TEMPORARY REMOVAL OF THE ROCK CONSTRUCTION ENTRANCE. UPON COMPLETION OF THE SANITARY SEWER LATERAL CONNECTION WORK, THE ROCK CONSTRUCTION ENTRANCE SHALL BE REINSTALLED IN THE LOCATION DEPICTED ON THE PLANS. CARE SHALL BE TAKEN AS TO NOT TRACK SEDIMENT ONTO COUNTY LINE ROAD DURING THE
- 8. ALL 3:1 SLOPES SHALL BE STABILIZED AND LINED WITH TEMPORARY EROSION CONTROL MATTING.

CONSTRUCTION OF THE SANITARY SEWER LATERAL CONNECTION.

- 9. FINISH GRADE AND SPREAD TOPSOIL. SEED AND MULCH EACH AREA OF DISTURBANCE IMMEDIATELY AFTER CONSTRUCTION IS COMPLETED. AS DISTURBED AREAS WITHIN A PROJECT APPROACH FINAL GRADE, PREPARATIONS SHOULD BE MADE FOR SEEDING AND MULCHING TO BEGIN (I.E. ANTICIPATE COMPLETION DATE AND SCHEDULE SEEDING). PRIOR TO PLACING TOPSOIL, SUBSOIL SHALL BE SCARIFIED. PLANT LANDSCAPING AS DEPICTED ON THE LANDSCAPE AND LIGHTING PLAN.
- 10. REMOVE TEMPORARY CONTROL MEASURES AFTER UNIFORM EROSION RESISTANT PERENNIAL VEGETATION HAS BEEN ESTABLISHED, MINIMUM OF UNIFORM COVERAGE OR A DENSITY OF 70% ACROSS THE DISTURBED AREA. TO THE POINT WHERE THE SURFACE SOIL IS CAPABLE OF RESISTING EROSION DURING RUNOFF EVENTS AND STABILIZATION OF THE SITE IS COMPLETE TO THE SATISFACTION OF THE COUNTY CONSERVATION DISTRICT AREAS DISTURBED DURING THE REMOVAL OF THE CONTROLS MUST BE STABILIZED. THE COUNTY CONSERVATION DISTRICT AND TOWNSHIP SHALL BE NOTIFIED PRIOR TO REMOVAL OF ANY EROSION CONTROLS.

#### PROJECT CONSTRUCTION WASTES

- 1. TREES CHIPPED AND REMOVED OFFSITE FOR MULCH.
- 2. BUILDING MATERIALS SHALL BE REMOVED AND DISPOSED OF AT PROPER RECYCLING FACILITY AS PER NOTE #6 OF THE GENERAL EROSION & SEDIMENT CONTROL NOTES.
- 3. ROCK WASTE ALL ROCKS THAT ARE SUITABLE FOR LANDSCAPING USES SHALL BE USED ON SITE OR AT ANOTHER FACILITY.
- 4. SOIL WASTE ALL EXCAVATED SOIL SHALL BE USED ON SITE, AS FILL MATERIAL WHERE POSSIBLE. ANY EXCESS MATERIAL NOT USED ON SITE MAY ONLY BE DISPOSED AT A FACILITY WITH A VALID EARTH DISTURBANCE AND NPDES PERMIT APPROVED AND WERE ALL BMPS ARE PROPERLY INSTALLED. ALL SEDIMENT REMOVED DURING MAINTENANCE AND CLEANING OF EROSION AND SEDIMENTATION CONTROLS DEVICES WILL BE INCORPORATED INTO THE FINE GRADING UPSTREAM FROM A SEDIMENT CONTROL DEVICE. EXCESS MATERIAL THAT IS REMOVED FROM EROSION AND SEDIMENTATION CONTROL MEASURES AND CANNOT BE INCORPORATED INTO THE FINE GRADING STABILIZATION SHALL BE REMOVED FROM THE SITE AND MAY ONLY BE DISPOSED AT A FACILITY WITH A VALID EARTH DISTURBANCE AND NPDES PERMIT APPROVAL AND WERE ALL BMPS ARE PROPERLY INSTALLED.
- 5. ACCUMULATED SEDIMENT FROM COMPOST FILTER SOCKS TO BE RETURNED TO SITE UPSTREAM OF ALL PROPOSED BMP'S.

### MAINTENANCE OF FACILITIES

1. SILT FENCE/FILTER SOCK SHOULD BE INSPECTED AND MAINTAINED ON A DAILY BASIS.

2. UNTIL THE SITE IS STABILIZED, ALL EROSION AND SEDIMENT CONTROL DEVICES MUST BE MAINTAINED PROPERLY. MAINTENANCE MUST INCLUDE INSPECTIONS OF ALL EROSION AND SEDIMENT CONTROLS AFTER EACH RUNOFF EVENT AND ON A WEEKLY BASIS. ALL PREVENTATIVE AND REMEDIAL MAINTENANCE WORK, INCLUDING CLEAN OUT, REPAIR, REPLACEMENT, REGRADING, RESEEDING, REMULCHING, AND RENETTING MUST BE PERFORMED IMMEDIATELY. IF E&S CONTROL BMPS FAIL TO PERFORM AS EXPECTED, REPLACEMENT BMPS OR MODIFICATIONS OF INSTALLED MEASURES WILL BE

3. CONTRACTOR SHALL MAINTAIN AND MAKE AVAILABLE TO THE COUNTY CONSERVATION DISTRICT COMPLETE, WRITTEN INSPECTION LOGS OF THE ABOVE NOTED INSPECTION AND MAINTENANCE.

- 4. SEEDING, MULCHING AND FERTILIZING SHALL BE IN ACCORDANCE WITH THE SEEDING AND MULCHING SCHEDULE.
- 5. THE CONTRACTOR SHALL HAVE AVAILABLE WATER TRUCKS OR OTHER MEANS OF CONTROLLING EXCESSIVE DUST AND AIRBORNE DEBRIS.
- 6. ALL AREAS OF CONCENTRATED SURFACE DRAINAGE SHALL BE SEEDED AND MULCHED, AND PROTECTED WITH TEMPORARY TURF REINFORCEMENT MAT: NORTH AMERICAN GREEN #D575 (OR EQUAL). IF AREAS ARE TO BE SODDED, TURF REINFORCEMENT IS NOT REQUIRED.
- 7. SEEDED AREAS THAT HAVE WASHED AWAY SHALL BE FILLED AND GRADED AS NECESSARY AND THEN RESEEDED. A BURLAP OR STRAW COVER WILL BE APPLIED TO RETAIN THE SEED UNTIL IT HAS A CHANCE TO ROOT PROPERLY.
- 8. THE ABOVE PROCEDURE SHALL BE REPEATED AFTER EACH SIZABLE STORM UNTIL NO MORE SIGNS OF EROSION ARE EVIDENT. AT MONTHLY INTERVALS THEREAFTER, INSPECTIONS AND NECESSARY CLEANING WILL BE DONE. TRASH THAT IS REMOVED FROM ANY OF THE CONTROL DEVICES SHALL BE DISPOSED OF AT AN APPROVED DISPOSAL AREA. SILT THAT HAS ACCUMULATED SHALL BE REMOVED AND ALLOWED TO DRY AND USED AS FILL WHEREVER REQUIRED ON THE SITE.
- 9. IMMEDIATELY UPON DISCOVERING UNFORESEEN CIRCUMSTANCES POSING THE POTENTIAL FOR ACCELERATED EROSION AND/OR SEDIMENT POLLUTION.

# FILL MATERIALS

IF THE SITE WILL NEED TO HAVE FILL IMPORTED FROM AN OFF SITE LOCATION, THE RESPONSIBILITY FOR PERFORMING ENVIRONMENTAL DUE DILIGENCE AND THE DETERMINATION OF CLEAN FILL WILL RESIDE WITH THE OPERATOR.

<u>LEAN FILL:</u> IS DEFINED AS UNCONTAMINATED, NON-WATER SOLUBLE, ON-DECOMPOSABLE, INERT, SOLID MATERIAL. THE TERM INCLUDES SOIL, ROCK, NE, DREDGED MATERIAL, USED ASPHALT, AND BRICK, BLOCK OR CONCRETE FROM CONSTRUCTION AND DEMOLITION ACTIVITIES HAT IS SEPARATE FROM OTHER WASTE AND IS RECOGNIZABLE AS SUCH. THE TERM DOES NOT INCLUDE MATERIALS PLACED IN OR ON THE WATERS OF THE COMMONWEALTH UNLESS OTHERWISE AUTHORIZED. (THE ERM "USED ASPHALT" DOES NOT INCLUDE MILLED ASPHALT OR ASPHALT THAT HAS BEEN PROCESSED FOR RE-USE.)

ENVIRONMENTAL DUE DILIGENCE: INVESTIGATIVE TECHNIQUES, INCLUDING, BUT NOT LIMITED TO, VISUAL PROPERTY INSPECTIONS, ELECTRONIC DATA BASE SEARCHES, REVIEW OF PROPERTY OWNERSHIP, REVIEW OF PROPERTY USE HISTORY, SANBORN MAPS, ENVIRONMENTAL QUESTIONNAIRES, TRANSACTION SCREENS, ANALYTICAL TESTING, ENVIRONMENTAL ASSESSMENTS OR AUDITS. ANALYTICAL TESTING IS NOT A REQUIRED PART OF DUE DILIGENCE UNLESS VISUAL INSPECTION AND/OR REVIEW OF THE PAST LAND USE OF THE PROPERTY INDICATES THAT THE FILL MAY HAVE BEEN SUBJECTED TO A SPILL OR RELEASE OF REGULATED SUBSTANCE. IF THE FILL MAY HAVE BEEN AFFECTED BY A SPILL OR RELEASE OF A REGULATED SUBSTANCE, IT MUST BE TESTED TO DETERMINE IF IT QUALIFIES AS CLEAN FILL. TESTING SHOULD BE PERFORMED IN ACCORDANCE WITH APPENDIX A OF THE DEPARTMENT'S POLICY "MANAGEMENT OF FILL"

FILL MATERIAL THAT DOES NOT QUALIFY AS CLEAN FILL IS REGULATED FILL. REGULATED FILL IS WASTE AND MUST BE MANAGED IN ACCORDANCE WITH THE DEPARTMENT'S MUNICIPAL OR RESIDUAL WASTE REGULATIONS BASED ON 25 PA. CODE CHAPTERS 287 RESIDUAL WASTE MANAGEMENT OR 271 MUNICIPAL WASTE MANAGEMENT, WHICHEVER IS APPLICABLE.

# E&S PLAN PLANNING & DESIGN NOTES

- 1. THE CONTRACTOR SHALL MINIMIZE THE EXTENT AND DURATION OF THE EARTH DISTURBANCE BY LIMITING THE CLEARING, GRUBBING, AND TOPSOIL STRIPPING TO AREAS IN EACH PHASE OF THE CONSTRUCTION SEQUENCE. GENERAL SITE CLEARING, GRUBBING AND TOPSOIL STRIPPING MAY NOT COMMENCE IN ANY STAGE OR PHASE OF THE PROJECT UNTIL THE E&S BMP'S SPECIFIED BY THE CONSTRUCTION SEQUENCE FOR THAT PHASE HAVE BEEN INSTALLED AND ARE FUNCTIONING AS DESCRIBED IN THE E&S PLAN. IMMEDIATELY AFTER EARTH DISTURBANCE ACTIVITIES CEASE, THE CONTRACTOR SHALL STABILIZE AND AREAS DISTURBED BY ANY ACTIVITIES.
- 2. TO MAXIMIZE THE PROTECTION OF EXISTING DRAINAGE FEATURES AND VEGETATION, E&S BMP'S MUST BE CONSTRUCTED, STABILIZED, AND FUNCTIONAL BEFORE SITE DISTURBANCE BEGINS WITHIN THE TRIBUTARY AREAS TO THOSE BMP'S. E&S BMP'S SHALL REMAIN FUNCTIONAL UNTIL ALL AREAS TRIBUTARY TO THEM ARE PERMANENTLY STABILIZED. AT NO TIME WILL CONSTRUCTION VEHICLES BE ALLOWED TO ENTER AREAS OUTSIDE THE LIMIT OF DISTURBANCE BOUNDARY. SEDIMENT TRACKED ONTO ANY PUBLIC ROADWAYS SHALL BE RETURNED TO THE CONSTRUCTION SITE BY THE END OF EACH WORK DAY. IN NO CASE SHALL THE SEDIMENT BE WASHED, SHOVELED, OR SWEPT INTO ANY ROADSIDE DITCH, STORM SEWER, OR SURFACE WATER. ALL GRADED AREAS SHALL BE PERMANENTLY STABILIZED IMMEDIATELY UPON REACHING FINISHED GRADE.
- 3. TO MINIMIZE SOIL COMPACTION, NO LAND CLEARING AND/OR GRADING SHALL BE PERFORMED OUTSIDE OF LIMIT OF DISTURBANCE FENCING. PROPER CARE SHALL BE TAKEN WITHIN UNDERGROUND INFILTRATION TRENCH TO MINIMIZE SOIL COMPACTION.
- 4. THE FOLLOWING MEASURES OR CONTROLS UTILIZED TO MINIMIZE GENERATION OF INCREASED STORMWATER RUNOFF: A ROCK CONSTRUCTION ENTRANCE, COMPOST FILTER SOCKS AROUND THE DISTURBANCE, A CONCRETE WASHOUT, EROSION CONTROL BLANKET, AND TEMPORARY/PERMANENT STABILIZATION OF ALL LOT FEATURES.
- 5. THERE ARE NO PROPOSED INFILTRATION BMP'S OUTSIDE PROPOSED GRADING AREAS.
- 6. THERE ARE NO EXISTING/PROPOSED RIPARIAN FOREST BUFFERS WITHIN THE LIMITS OF DISTURBANCE THEREFORE NO ASSOCIATED REQUIREMENTS ARE SHOWN ON THE PLANS.
- 7. THIS PROJECT IS NOT LOCATED IN A SPECIAL PROTECTION (HQ/EV) WATERSHED, THEREFORE NO ANTIDEGRADATION ANALYSIS IS REQUIRED.
- 8. THERE ARE NO NATURALLY OCCURRING GEOLOGIC FORMATIONS (KARST SOILS) THAT MAY CAUSE POLLUTION WITHIN THE PROJECT LIMIT OF DISTURBANCE; THEREFORE, MEASURES TO AVOID/MINIMIZE/OR MITIGATE ARE NOT NECESSARY.

#### GRASS SEEDING SPECIFICATIONS: TEMPORARY:

ANNUAL RYEGRASS % PURE LIVE SEED APPLICATION RATE 87.0 LB/ACRE FERTILIZER TYPE COMMERCIAL 10-20-10 FERTILIZER APPL, RATE 1,100 LB/ACRE LIMING RATE 2 T/ACRE MULCH TYPE STRAW MULCH RATE 3 T/ACRE PERMANENT: KENTUCKY BLUEGRASS / SPREADING SPECIES FESCUE / PERENNIAL RYEGRASS / CHEWING'S RED FESCUE % PURE LIVE SEE APPLICATION RATE 41 / 20 / 14 / 20 LB/ACRE FERTILIZER TYPE COMMERCIAL 10-20-10 FERTILIZER APPL, RATE 1,100 LB/ACRE LIMING RATE MULCH TYPE HAY OR STRAW MULCH RATE 3 T/ACRE ANCHOR MATERIAL ANCHORING METHOD RATE OF ANCHOR MATERIAL APPL. N/A APRIL 15 - MAY 30, AUGUST 15 - OCTOBER 15 SEEDING SEASON DATES

THIS SHEET FOR EROSION and SEDIMENTATION CONTROL PURPOSES ONLY



PER TOWNSHIP ENGINEER REVIEW LETTER DATED 05/21/2025. V DESCRIPTION AUTH DAT

**SERIAL NO. 2022318358** 



PLAN NOTATION ONLY THOSE PLANS WHICH CONTAIN A DIGI IMPRESSED, OR COLORIZED INK SEAL OF T RESPONSIBLE PROFESSIONAL SHALL BE CONSIDER ALID. THIS PLAN HAS BEEN SPECIFICALLY PREPARI OR THE OWNER DESIGNATED HEREON. AN MODIFICATION, REVISION, DUPLICATION OR WITHOUT THE WRITTEN CONSENT OF VAN CL NGINEERING ASSOCIATES IS PROHIBITE ELIANCE ON THIS PLAN FOR ANY PURPO

THER THAN THAT WHICH IS INTENDED SHALL

THE SOLE DISCRETION AND LIABILITY OF

APPLICABLE PARTY.

EAN DAVID McGRANAHAN

ISYLVANIA PROFESSIONA



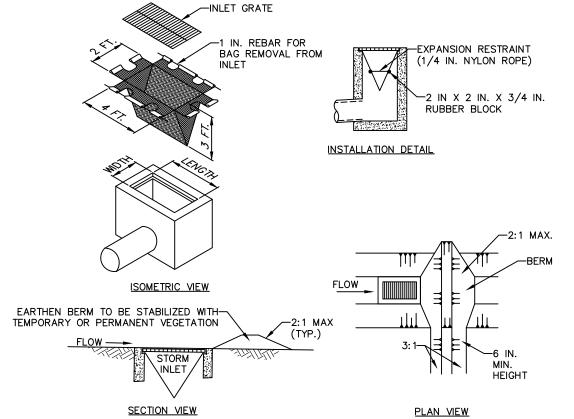
VAN CLEEF ENGINEERING ASSOCIATES, LLC 501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM PHONE (215) 345-1876



DESIGNED E **RJN / ENP** RAWN BY HECKED BY B NUMBER 2402-NBF

TMP'S 26-006-101-001

**NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 



NOTES:

MAXIMUM DRAINAGE AREA = 1/2 ACRE.

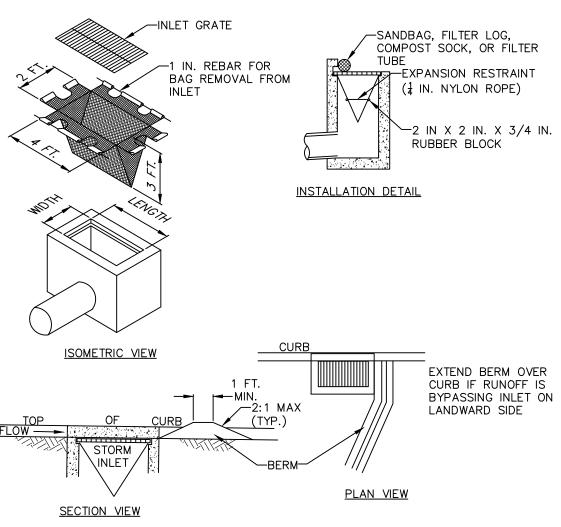
INLET PROTECTION SHALL NOT BE REQUIRED FOR INLET TRIBUTARY TO SEDIMENT BASIN OR TRAP. BERMS SHALL BE REQUIRED FOR ALL INSTALLATIONS. ROLLED EARTHEN BERM IN ROADWAY SHALL BE MAINTAINED UNTIL ROADWAY IS STONED. ROAD SUBBASE BERM ON ROADWAY SHALL BE MAINTAINED UNTIL ROADWAY IS PAVED. EARTHEN BERM IN CHANNEL SHALL BE MAINTAINED UNTIL PERMANENT STABILIZATION IS COMPLETED OR REMAIN PERMANENTLY.

AT A MINIMUM, THE FABRIC SHALL HAVE A MINIMUM GRAB TENSILE STRENGTH OF 120 LBS., A MINIMUM BURST STRENGTH OF 200 PSI, AND A MINIMUM TRAPEZOIDAL TEAR STRENGTH OF 50 LBS. FILTER BAGS SHALL BE CAPABLE OF TRAPPING ALL PARTICLES NOT PASSING A NO. 40 SIEVE.

INLET FILTER BAGS SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EACH RUNOFF EVENT. BAGS SHALL BE EMPTIED AND RINSED OR REPLACED WHEN HALF FULL OR WHEN FLOW CAPACITY HAS BEEN REDUCED SO AS TO CAUSE FLOODING OR BYPASSING OF THE INLET. DAMAGED OR CLOGGED BAGS SHALL BE REPLACED. A SUPPLY SHALL BE MAINTAINED ON SITE FOR REPLACEMENT OF BAGS. ALL NEEDED REPAIRS SHALL BE INITIATED IMMEDIATELY AFTER THE INSPECTION. DISPOSE ACCUMULATED SEDIMENT AS WELL AS ALL USED BAGS ACCORDING TO THE PLAN NOTES.

DO NOT USE ON MAJOR PAVED ROADWAYS WHERE PONDING MAY CAUSE TRAFFIC HAZARDS.

#### FILTER BAG INLET PROTECTION - TYPE M INLET (NOT TO SCALE)



MAXIMUM DRAINAGE AREA = 1/2 ACRE.

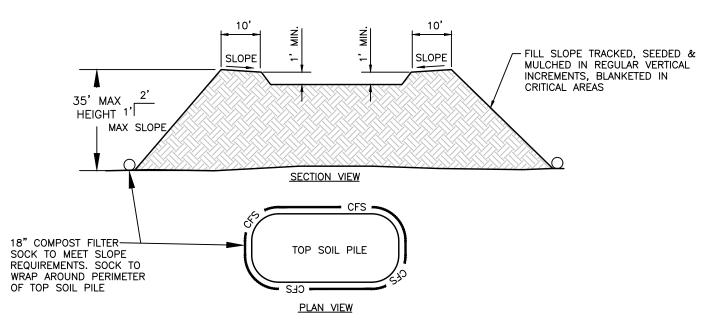
INLET PROTECTION SHALL NOT BE REQUIRED FOR INLET TRIBUTARY TO SEDIMENT BASIN OR TRAP. BERMS SHALL BE REQUIRED FOR ALL INSTALLATIONS.

ROLLED EARTHEN BERM SHALL BE MAINTAINED UNTIL ROADWAY IS STONED. ROAD SUBBASE BERM SHALL BE MAINTAINED UNTIL ROADWAY IS PAVED. SIX INCH MINIMUM HEIGHT ASPHALT BERM SHALL BE MAINTAINED UNTIL ROADWAY SURFACE RECEIVES FINAL COAT. AT A MINIMUM, THE FABRIC SHALL HAVE A MINIMUM GRAB TENSILE STRENGTH OF 120 LBS, A MINIMUM BURST STRENGTH OF 200 PSI, AND A MINIMUM TRAPEZOIDAL TEAR STRENGTH OF 50 LBS. FILTER BAGS SHALL BE CAPABLE OF TRAPPING ALL PARTICLES NOT PASSING A NO. 40

INLET FILTER BAGS SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EACH RUNOFF EVENT. BAGS SHALL BE EMPTIED AND RINSED OR REPLACED WHEN HALF FULL OR WHEN FLOW CAPACITY HAS BEEN REDUCED SO AS TO CAUSE FLOODING OR BYPASSING OF THE INLET. DAMAGED OR CLOGGED BAGS SHALL BE REPLACED. A SUPPLY SHALL BE MAINTAINED ON SITE FOR REPLACEMENT OF BAGS. ALL NEEDED REPAIRS SHALL BE INITIATED IMMEDIATELY AFTER THE INSPECTION. DISPOSE OF ACCUMULATED SEDIMENT AS WELL AS ALL USED BAGS ACCORDING TO THE PLAN NOTES.

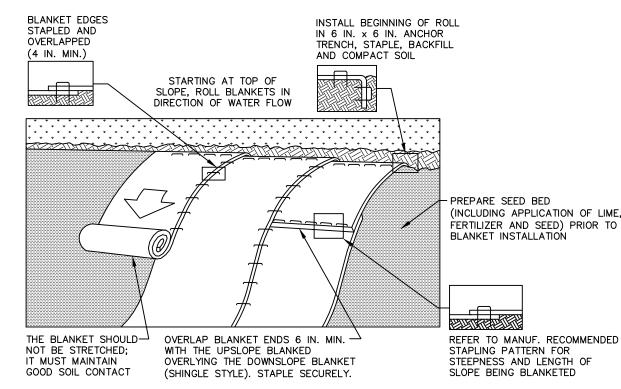
DO NOT USE ON MAJOR PAVED ROADWAYS WHERE PONDING MAY CAUSE TRAFFIC HAZARDS.

#### FILTER BAG INLET PROTECTION - TYPE C INLET (NOT TO SCALE)



1. TEMPORARY BERMS TO BE PLACED, MAINTAINED, AND ADJUSTED CONTINUOUSLY UNTIL 90% VEGETATIVE GROWTH IS ESTABLISHED ON THE EXTERIOR SLOPES WITH PERMANENT STORM DRAINAGE FACILITIES FUNCTIONING. 2. BERMS MUST OUTLET TO TEMPORARY SLOPE PIPES, PERMANENT SLOPE PIPES, TEMPORARY CHANNELS, OR PERMANENT CHANNELS. TOPSOIL STOCKPILE AREA

(NOT TO SCALE)

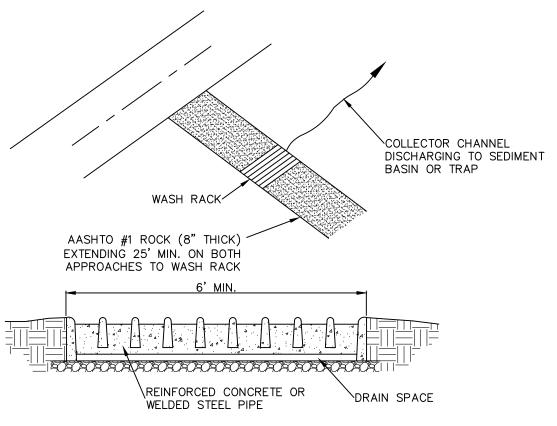


SEED AND SOIL AMENDMENTS SHALL BE APPLIED ACCORDING TO THE RATES IN THE PLAN DRAWINGS PRIOR TO PROVIDE ANCHOR TRENCH AT TOE OF SLOPE IN SIMILAR FASHION AS AT TOP OF SLOPE. SLOPE SURFACE SHALL BE FREE OF ROCKS, CLODS, STICKS, AND GRASS. BLANKET SHALL HAVE GOOD CONTINUOUS CONTACT WITH UNDERLYING SOIL THROUGHOUT ENTIRE LENGTH. LAY

BLANKET LOOSELY AND STAKE OR STAPLE TO MAINTAIN DIRECT CONTACT WITH SOIL. DO NOT STRETCH

THE BLANKET SHALL BE STAPLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. BLANKETED AREAS SHALL BE INSPECTED WEEKLY AND AFTER EACH RUNOFF EVENT UNTIL PERENNIAL VEGETATION IS ESTABLISHED TO A MINIMUM UNIFORM 70% COVERAGE THROUGHOUT THE BLANKETED AREA. DAMAGED OR DISPLACED BLANKETS SHALL BE RESTORED OR REPLACED WITHIN 4 CALENDAR DAYS.

## EROSION CONTROL BLANKET INSTALLATION DETAIL



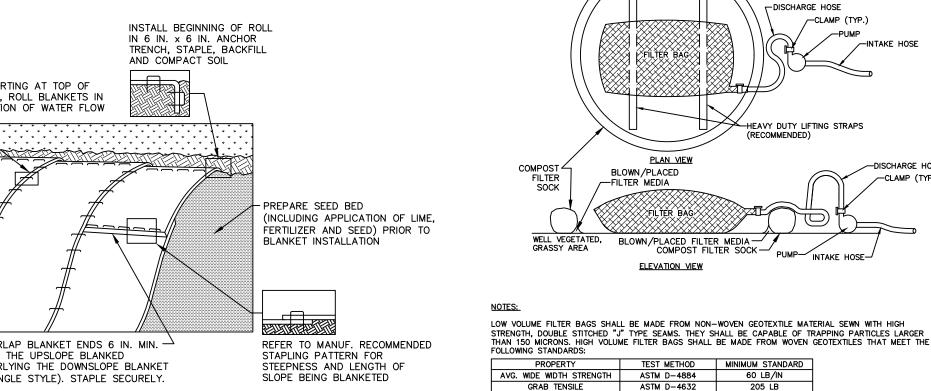
WASH RACK SHALL BE 20 FEET (MIN.) WIDE OR TOTAL WIDTH OF ACCESS. WASH RACK SHALL BE DESIGNED AND CONSTRUCTED TO ACCOMMODATE ANTICIPATED CONSTRUCTION VEHICULAR TRAFFIC.

A WATER SUPPLY SHALL BE MADE AVAILABLE TO WASH THE WHEELS OF ALL VEHICLES EXITING

MAINTENANCE: ROCK CONSTRUCTION ENTRANCE THICKNESS SHALL BE CONSTANTLY MAINTAINED TO THE SPECIFIED DIMENSIONS BY ADDING ROCK. A STOCKPILE OF ROCK MATERIAL SHALL BE MAINTAINED ON SITE FOR THIS PURPOSE, DRAIN SPACE UNDER WASH RACK SHALL BE KEPT OPEN AT ALL TIMES. DAMAGE TO THE WASH RACK SHALL BE REPAIRED PRIOR TO FURTHER USE OF THE RACK. ALL SEDIMENT DEPOSITED ON ROADWAYS SHALL BE REMOVED AND RETURNED TO THE CONSTRUCTION SITE IMMEDIATELY. WASHING THE ROADWAY OR SWEEPING THE DEPOSITS INTO ROADWAY DITCHES, SEWERS, CULVERTS, OR OTHER DRAINAGE COURSES IS

# ROCK CONSTRUCTION ACCESS WITH WASH RACK

(NOT TO SCALE)



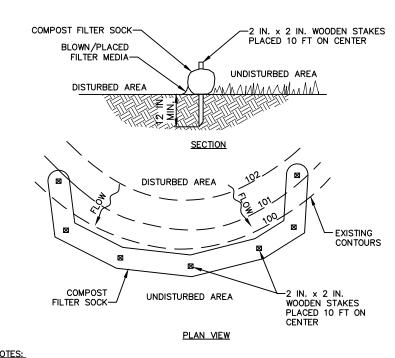
A SUITABLE MEANS OF ACCESSING THE BAG WITH MACHINERY REQUIRED FOR DISPOSAL PURPOSES SHALL BE PROVIDED. FILTER BAGS SHALL BE REPLACED WHEN THEY BECOME 1/2 FULL OF SEDIMENT. SPARE BAGS SHALL BE KEPT AVAILABLE FOR REPLACEMENT OF THOSE THAT HAVE FAILED OR ARE FILLED. BAGS SHALL BE PLACED ON STRAPS TO FACILITATE REMOVAL UNLESS BAGS COME WITH LIFTING STRAPS ALREADY ATTACHED.

BAGS SHALL BE LOCATED IN WELL-VEGETATED (GRASSY) AREA, AND DISCHARGE ONTO STABLE, EROSION RESISTANT AREAS. WHERE THIS IS NOT POSSIBLE, A GEOTEXTILE UNDERLAYMENT AND FLOW PATH SHALL BE PROVIDED. BAGS MAY BE PLACED ON FILTER STONE TO INCREASE DISCHARGE CAPACITY. BAGS SHALL NOT BE PLACED ON SLOPES GREATER THAN 5%. FOR SLOPES EXCEEDING 5%, CLEAN ROCK OR OTHER NON-ERODIBLE AND NON-POLLUTING MATERIAL MAY BE PLACED UNDER THE BAG TO REDUCE SLOPE STEEPNESS. NO DOWNSLOPE SEDIMENT BARRIER IS REQUIRED FOR MOST INSTALLATIONS. COMPOST BERM OR COMPOST FILTER SOCK SHALL BE INSTALLED BELOW BAGS LOCATED IN HQ OR EV WATERSHEDS, WITHIN 50 FEET OF ANY RECEIVING SURFACE WATER OR WHERE GRASSY AREA IS NOT AVAILABLE. THE PUMP DISCHARGE HOSE SHALL BE INSERTED INTO THE BAGS IN THE MANNER SPECIFIED BY THE MANUFACTURER AND SECURELY CLAMPED. A PIECE OF PVC PIPE IS RECOMMENDED FOR THIS PURPOSE. THE PUMPING RATE SHALL BE NO GREATER THAN 750 GPM OR 1/2 THE MAXIMUM SPECIFIED BY THE MANUFACTURER, WHICHEVER IS LESS. PUMP INTAKES SHALL BE FLOATING AND SCREENED. FILTER BAGS SHALL BE INSPECTED DAILY. IF ANY PROBLEM IS DETECTED, PUMPING SHALL CEASE IMMEDIATELY AND NOT RESUME UNTIL THE PROBLEM IS CORRECTED.

WELL VEGETATED, GRASSY AREA

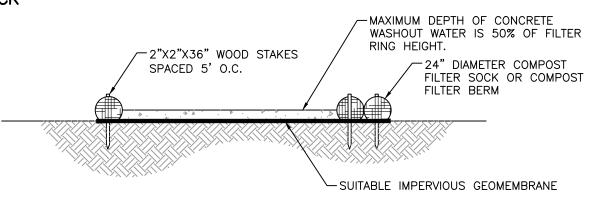
-DISCHARGE HOSE CLAMP (TYP.)

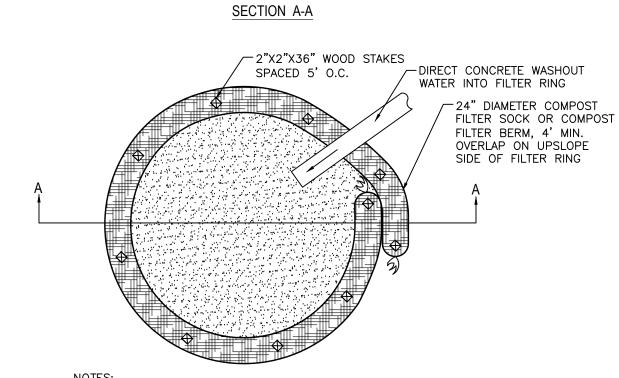
#### PUMPED WATER FILTER BAG (NOT TO SCALE)



SOCK FABRIC SHALL MEET STANDARDS OF TABLE 4.1 OF THE PA DEP EROSION CONTROL COMPOST FILTER SOCK SHALL BE PLACED AT EXISTING LEVEL GRADE. BOTH ENDS OF THE BARRIER SHALL BE EXTENDED AT LEAST 8 FEET UP SLOPE AT 45 DEGREES TO THE MAIN BARRIER ALIGNMENT. MAXIMUM SLOPE LENGTH ABOVE ANY BARRIER SHALL NOT EXCEED THAT SPECIFIED FOR THE SIZE OF THE SOCK AND THE SLOPE OF ITS TRIBUTARY AREA. TRAFFIC SHALL NOT BE PERMITTED TO CROSS COMPOST FILTER SOCKS. ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN IT REACHES 1/2 THE ABOVE GROUND HEIGHT OF THE BARRIER AND DISPOSED IN THE MANNER DESCRIBED ELSEWHERE IN THE PLAN. COMPOST FILTER SOCKS SHALL BE INSPECTED WEEKLY AND AFTER EACH RUNOFF EVENT. DAMAGED SOCKS SHALL BE REPAIRED ACCORDING TO MANUFACTURER'S SPECIFICATIONS OR REPLACED WITHIN 24 HOURS OF INSPECTION. BIODEGRADABLE COMPOST FILTER SOCKS SHALL BE REPLACED AFTER 6 MONTHS; PHOTODEGRADABLE SOCKS AFTER 1 YEAR. POLYPROPYLENE SOCKS SHALL BE REPLACED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS. UPON STABILIZATION OF THE AREA TRIBUTARY TO THE SOCK, STAKES SHALL BE REMOVED. THE SOCK MAY BE LEFT IN PLACE AND VEGETATED OR REMOVED. IN THE LATTER CASE, THE MESH SHALL BE CUT OPEN AND THE MULCH SPREAD AS A SOIL SUPPLEMENT.

#### COMPOST FILTER SOCK (NOT TO SCALE)



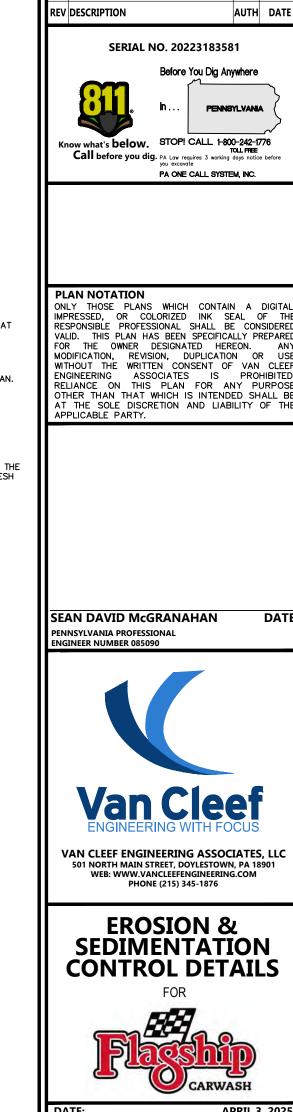


1. INSTALL ON FLAT GRADE (2% MAXIMUM SLOPE) FOR OPTIMAL PERFORMANCE 2. 18" DIAMETER FILTER SOCK MAY BE STACKED ONTO DOUBLE 24" DIAMETER FILTER SOCKS IN PYRAMIDAL CONFIGURATION FOR ADDED HEIGHT 3. ALL CONCRETE WASHOUT FACILITIES SHOULD BE INSPECTED DAILY. COMPOST SOCK MUST REMAIN IN CONTINUOUS CONTACT WITH THE GEOMEMBRANE AT ALL LOCATIONS. DAMAGED OR LEAKING WASHOUTS SHOULD BE DEACTIVATED AND REPAIRED OR REPLACED IMMEDIATELY.

COMPOST SOCK CONCRETE WASHOUT

4. ACCUMULATED MATERIALS TO BE REMOVED WHEN THEY REACH 75% CAPACITY.

(NOT TO SCALE)



DESIGNED BY

HECKED BY

**DB NUMBER** 

TMP'S 26-006-101-001

**NEW BRITAIN TOWNSHIP** 

**BUCKS COUNTY, PENNSYLVANIA** 

11 of 18

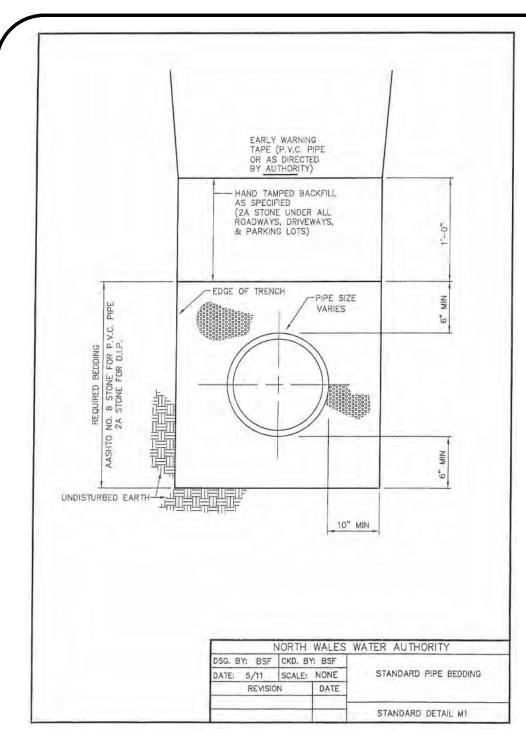
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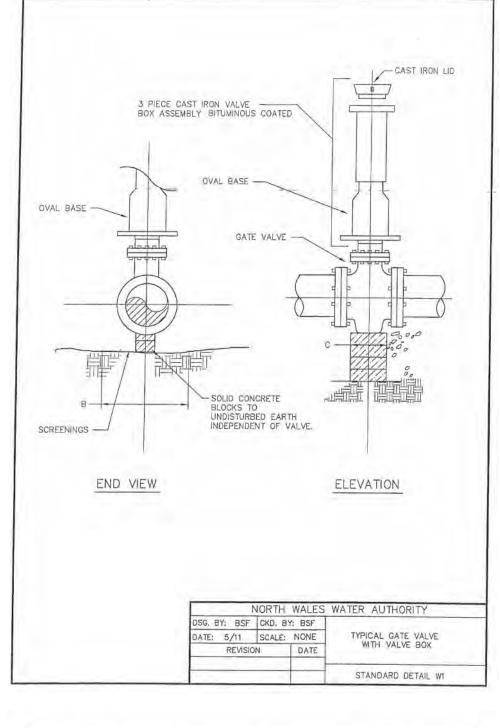
RJN / ENP

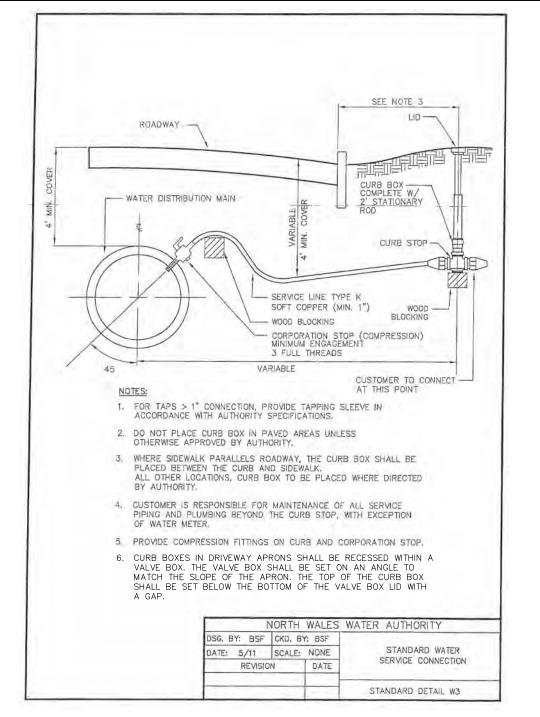
PER TOWNSHIP ENGINEER REVIEW LETTER

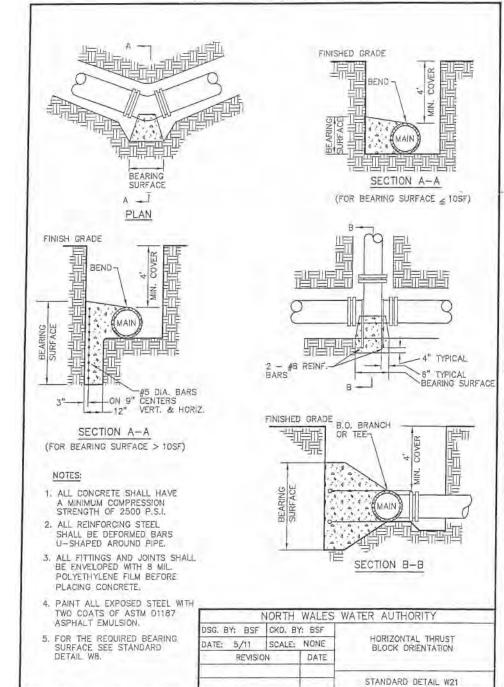
DATED 05/21/2025.

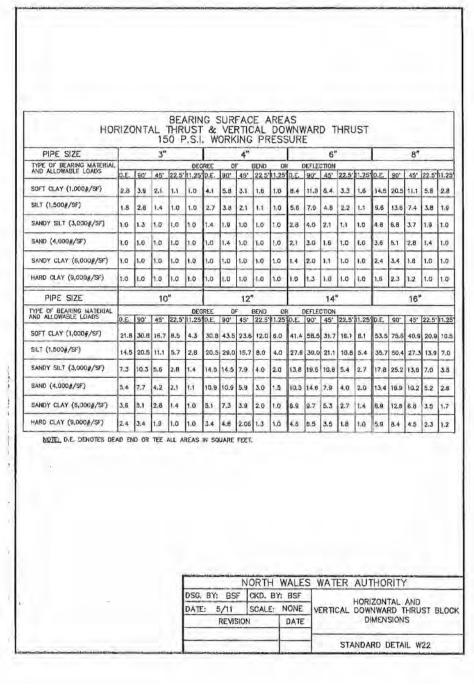
THIS SHEET FOR EROSION and SEDIMENTATION CONTROL PURPOSES ONLY

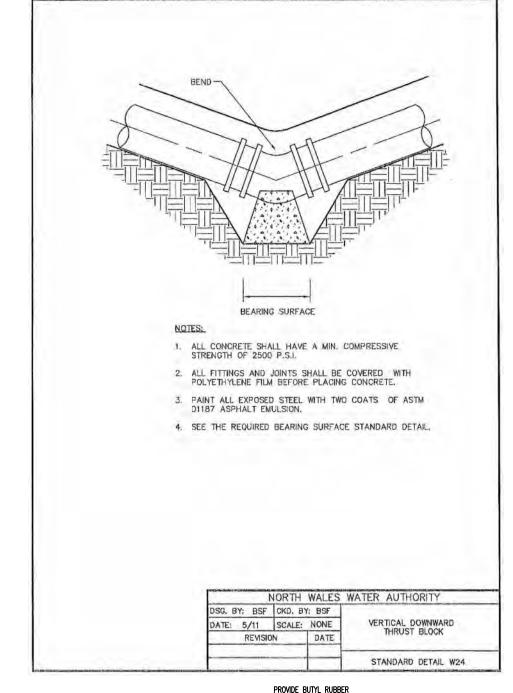


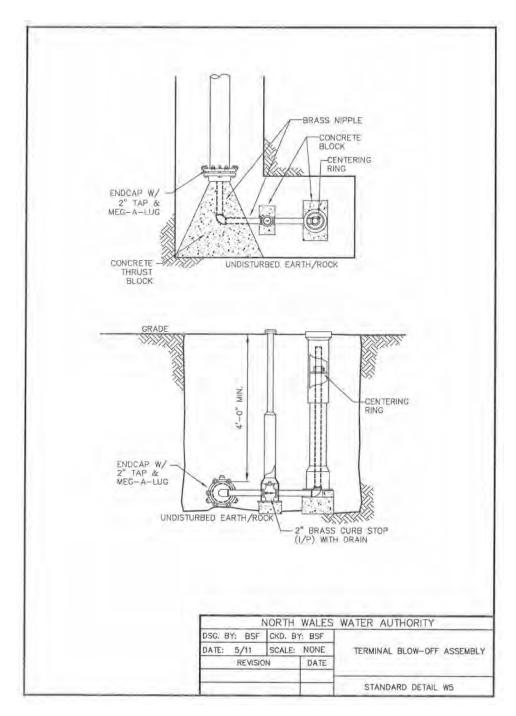


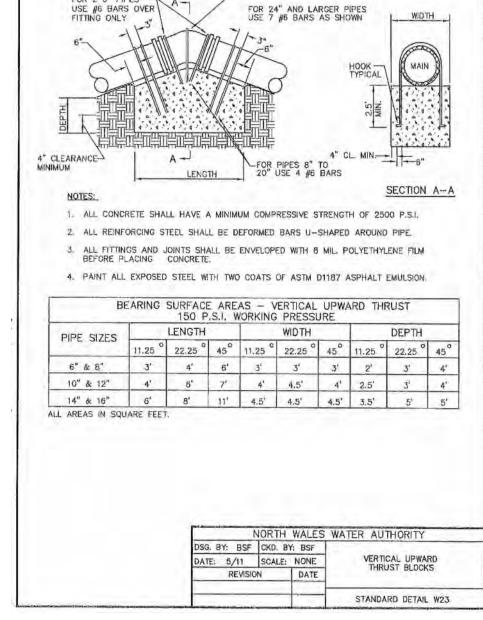




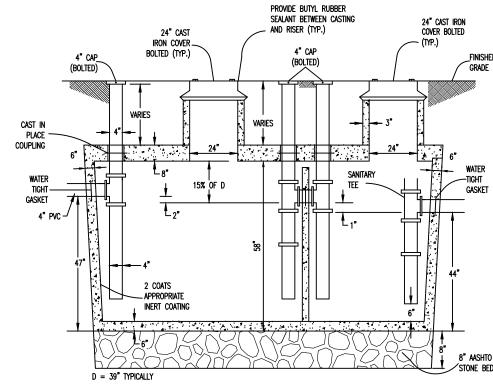








FOR 2 6" PIPES -



ALL INLET AND OUTLET PIPES SHALL BE INSTALLED NO MORE THAN 6" FROM THE BOTTOM OF THE GREASE TRAP. TANK TAPERS TOP TO BOTTOM AND IS TRAPEZOIDAL IN CROSS SECTION. TANK IS 5000 PSI CONCRETE-STEEL REINFORCED (@ 28 DAYS) CONCRETE CONFORMS TO ACI 318-16-4.5.1 AND 318-16-4.5.2, ASTM A615 AND A185 -DIMENSION: 151" INTERIOR/ 163" EXTERIOR LENGTH x 72" INTERIOR/ 84" EXTERIOR WIDTH

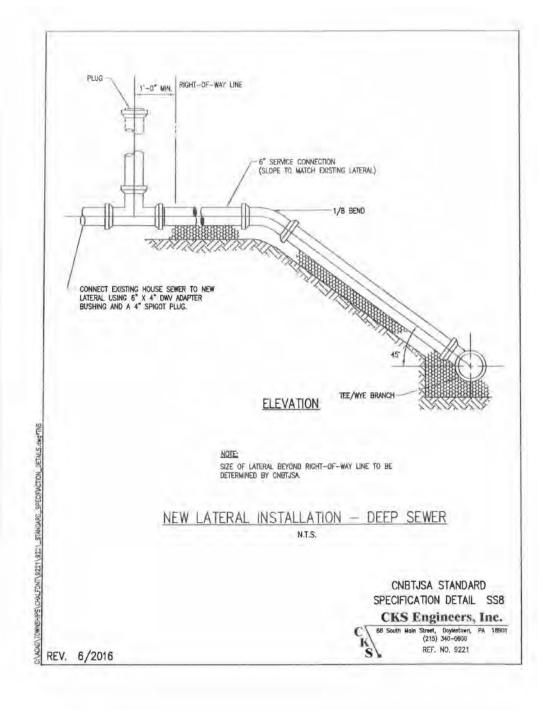
- 1) WHEN LOCATED IN DRIVEWAYS OR PAVED AREAS, GREASE TRAP TO BE DESIGNED FOR APPROPRIATE LOAD BEARING CONDITIONS. GREASE TRAP SHALL BE CAPABLE OF WITHSTANDING HS-20 LOADING. 2) ALL PIPE PENETRATIONS SHALL BE WATERTIGHT. 3) GREASE TRAP SHALL BE PROVIDED WITH GAS-TIGHT MANHOLE COVERS, IN ACCORDANCE WITH TOWNSHIP STANDARD
- SPECIFICATIONS. 4) PRECAST CONCRETE SHALL HAVE A MINIMUM 28-DAY COMPRESSIVE STRENGTH 5000 PSI. EXTERIOR CONCRETE SURFACES BELOW GRADE SHALL HAVE 2 COATS OF COAL TAR EPOXY.
- JOINT SHALL BE SEALED WITH BUTYL RUBBER SEALANT (KENT SEAL #2 OR APPROVED EQUIVALENT) AND THE EXTERIOR OF THE JOINT SHALL BE SEALED WITH NON-SHRINK GROUT IN CONFORMANCE WITH THE TOWNSHIP STANDARD GREASE TRAP

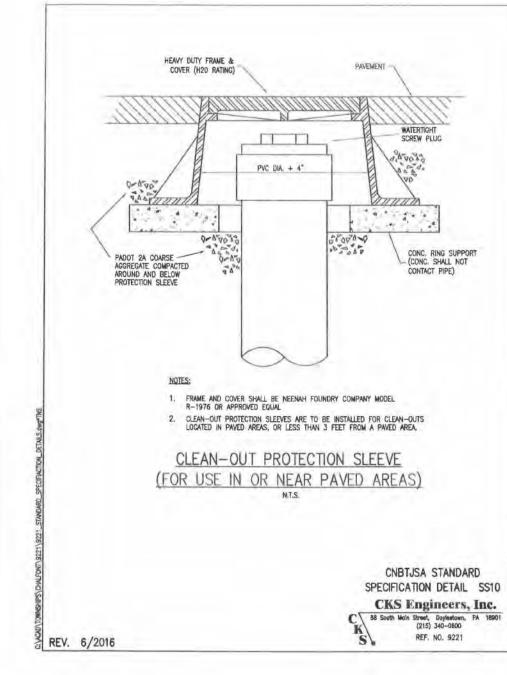
6) SPECIFIC SEALANT DETAIL AT CONCRETE RISER TO CONCRETE VAULT INTERFACE SHALL BE WATERTIGHT. AT A MINIMUM, THE

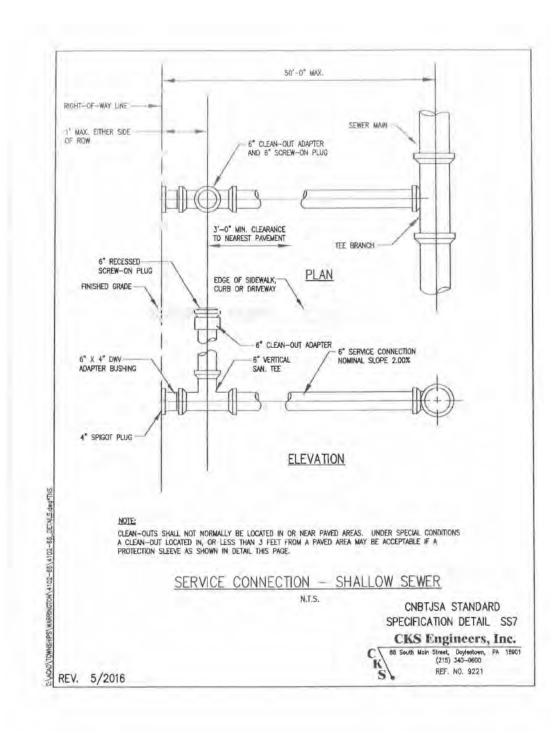
7) TANK SHALL BE TESTED FOR WATER TIGHTNESS BY FILLING FOR 24 HRS. TO SOAK, THEN TOPPED OFF, AND THEN WATCHED FOR 24 HRS. NO DROP IN WATER IS ALLOWED. 8) CAST IRON SHALL BE BOLTED TO CONCRETE WITH MASTIC TAPE (KENT SEAL OR APPROVED EQUIVALENT) SEALANT. MAXIMUM EARTH COVER=5.0', HS-20 LOADING.

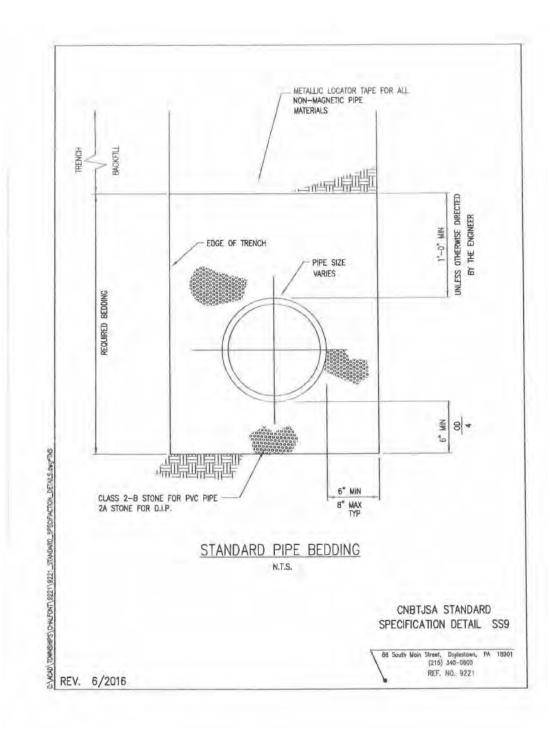
10) INLET AND OUTLET EQUIPPED WITH PIPE SEALS.

GREASE TRAP (1,500 GALLON - 2 COMPARTMENT) (NOT TO SCALE)

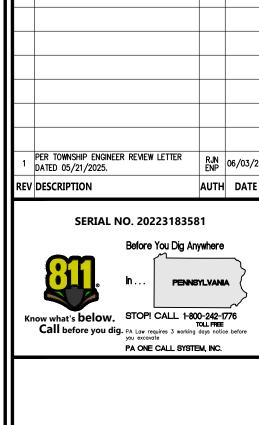


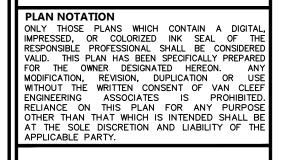












EAN DAVID McGRANAHAN



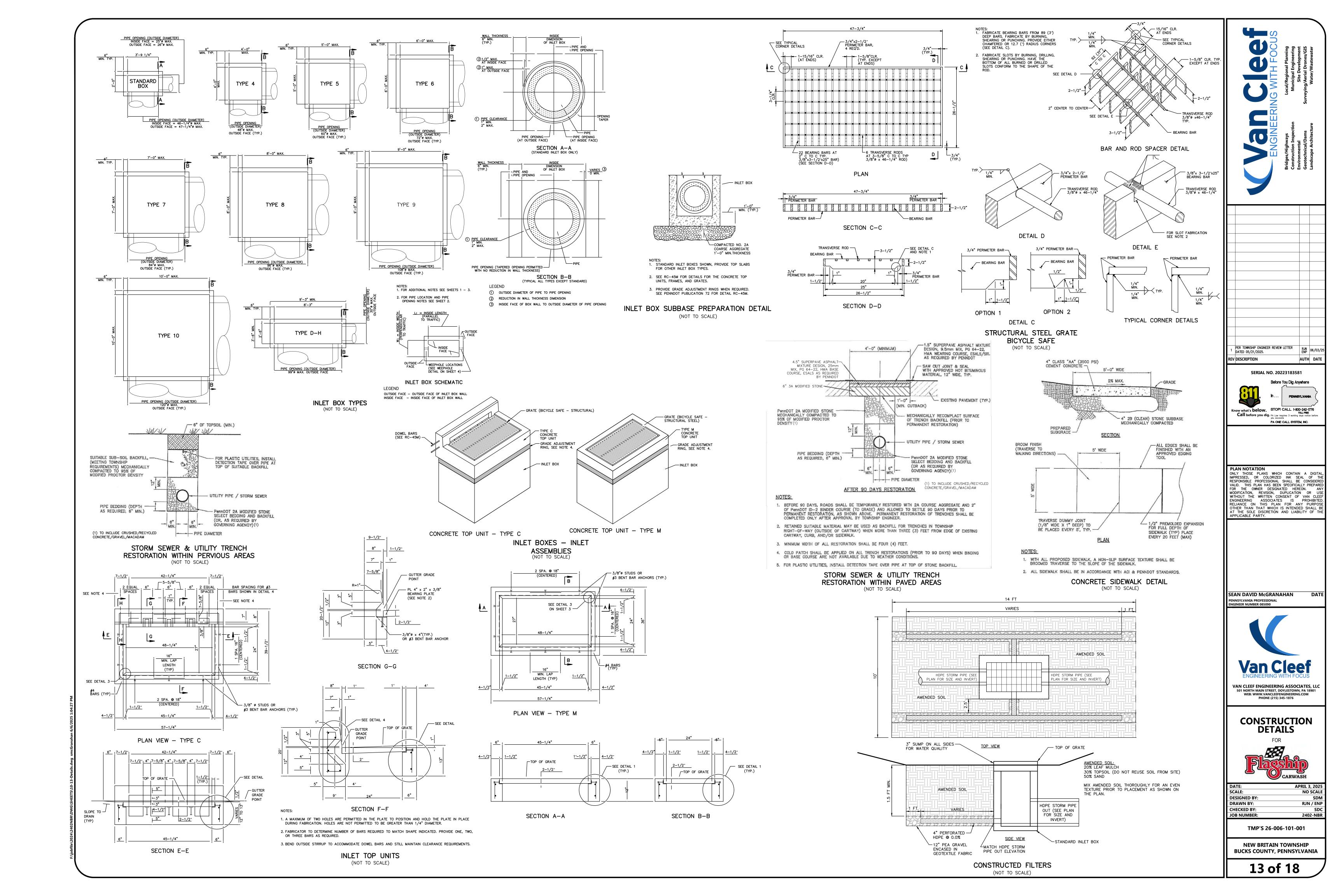
501 NORTH MAIN STREET, DOYLESTOWN, PA 18901 WEB: WWW.VANCLEEFENGINEERING.COM PHONE (215) 345-1876

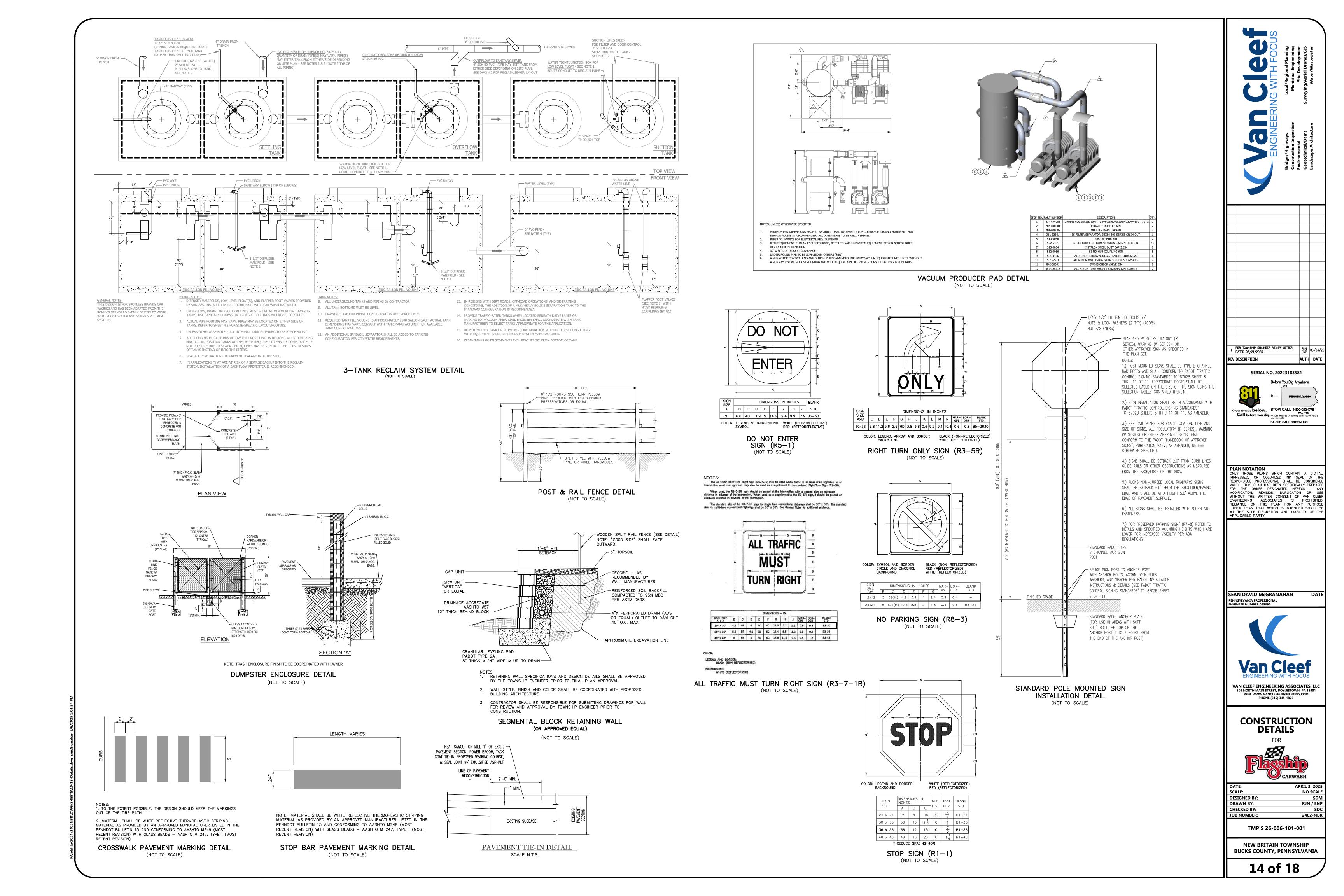


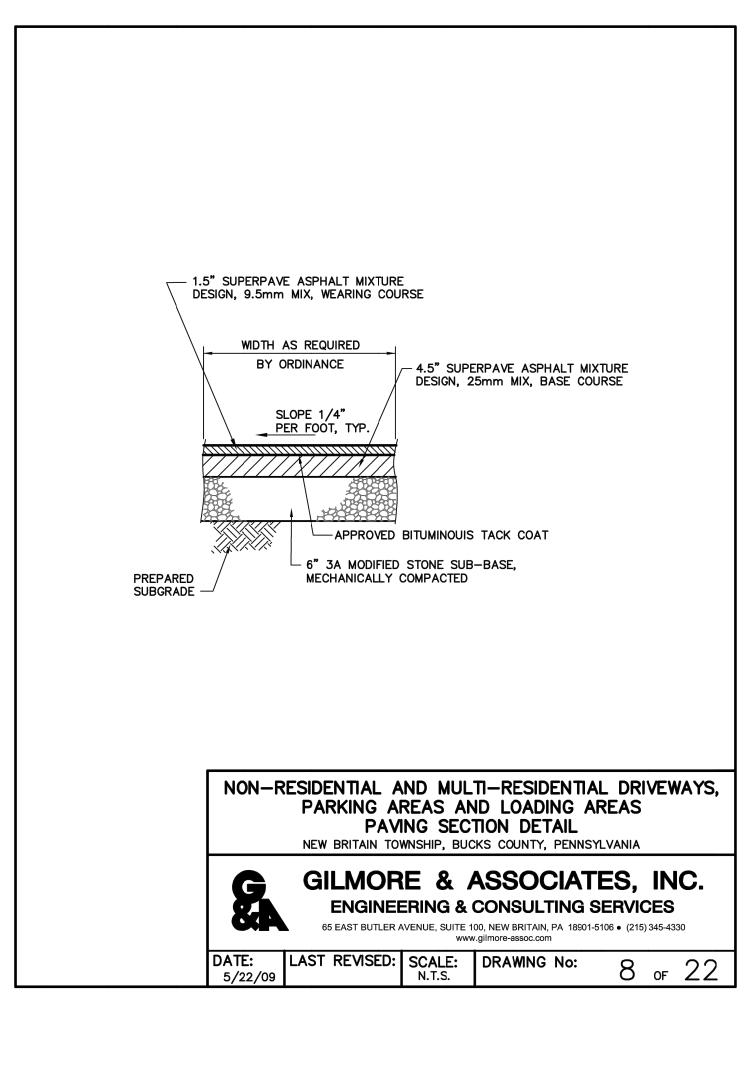
	CARWASH
DATE:	APRIL 3, 20
SCALE:	NO SCA
DESIGNED BY:	SE
DRAWN BY:	RJN / EI
CHECKED BY:	Si
JOB NUMBER:	2402-N

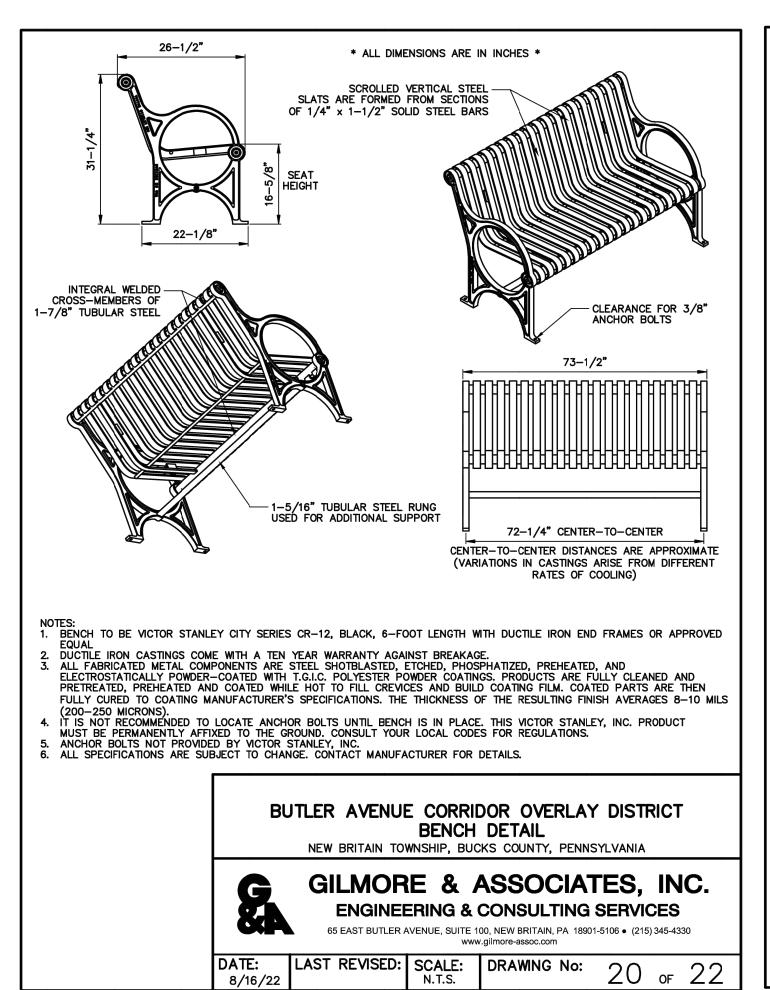
TMP'S 26-006-101-001

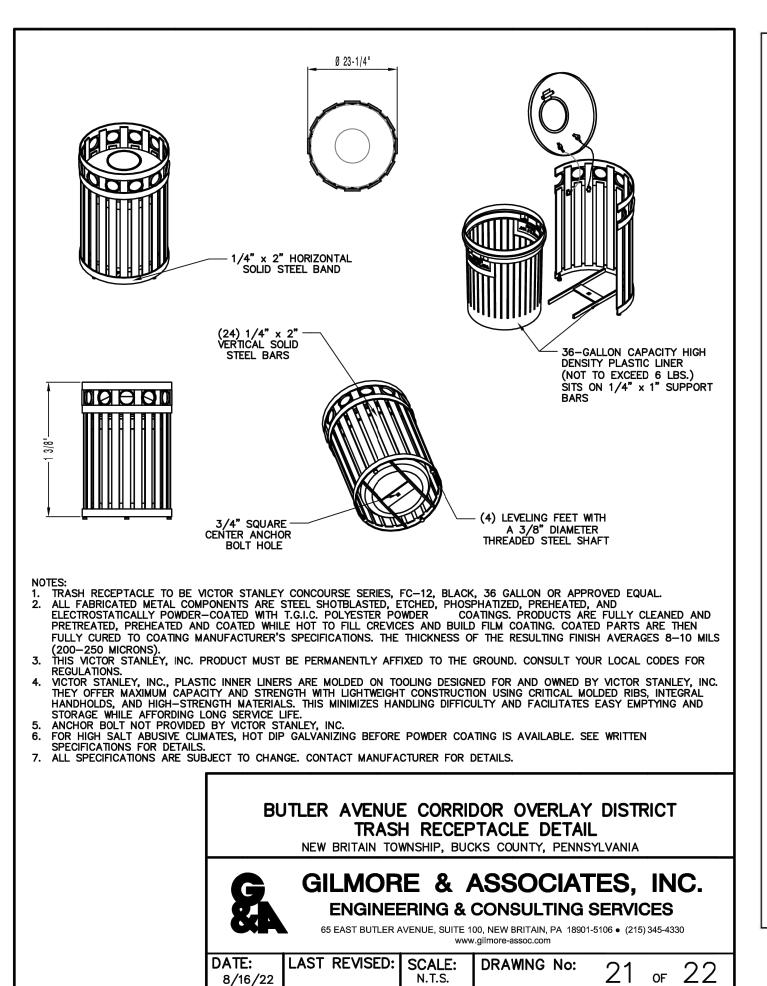
**NEW BRITAIN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA** 



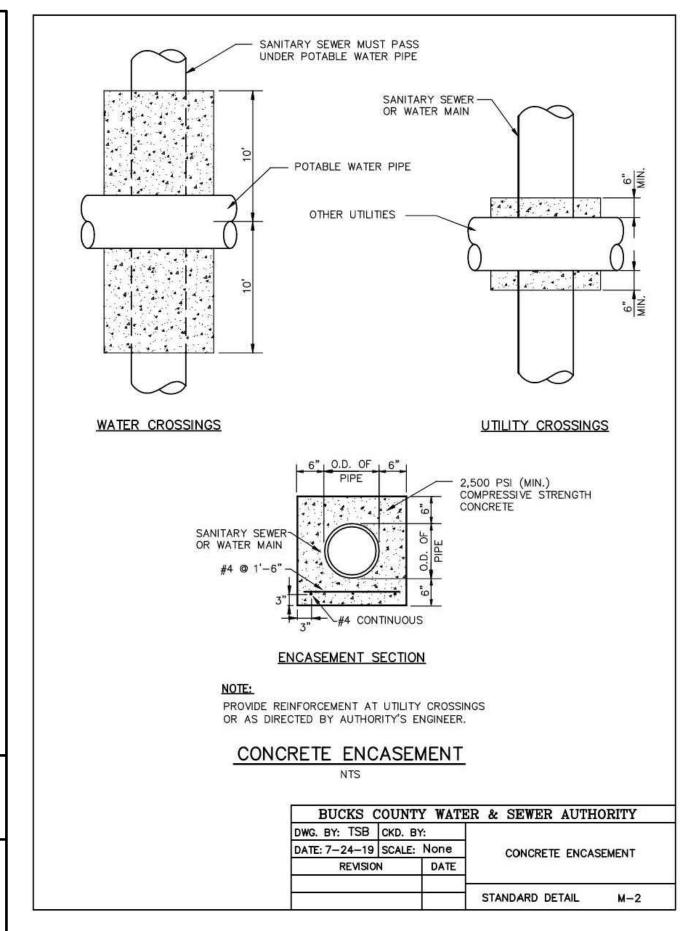








8/16/22



PER TOWNSHIP ENGINEER REVIEW LETTER

**SERIAL NO. 20223183581** 

Know what's below. STOP! CALL 1-800-242-1776 Call before you dig. PA Law requires 3 working days notice you excavate

ONLY THOSE PLANS WHICH CONTAIN A DIGITA MIPRESSED, OR COLORIZED INK SEAL OF THE RESPONSIBLE PROFESSIONAL SHALL BE CONSIDERED ALID. THIS PLAN HAS BEEN SPECIFICALLY PREPART OR THE OWNER DESIGNATED HEREON. AN

MODIFICATION, REVISION, DUPLICATION OR US
MITHOUT THE WRITTEN CONSENT OF VAN CLEE
ENGINEERING ASSOCIATES IS PROHIBITE
RELIANCE ON THIS PLAN FOR ANY PURPOS

THER THAN THAT WHICH IS INTENDED SHALL

AT THE SOLE DISCRETION AND LIABILITY OF APPLICABLE PARTY.

EAN DAVID McGRANAHAN

INSYLVANIA PROFESSIONAL NEER NUMBER 085090

Before You Dig Anywhere

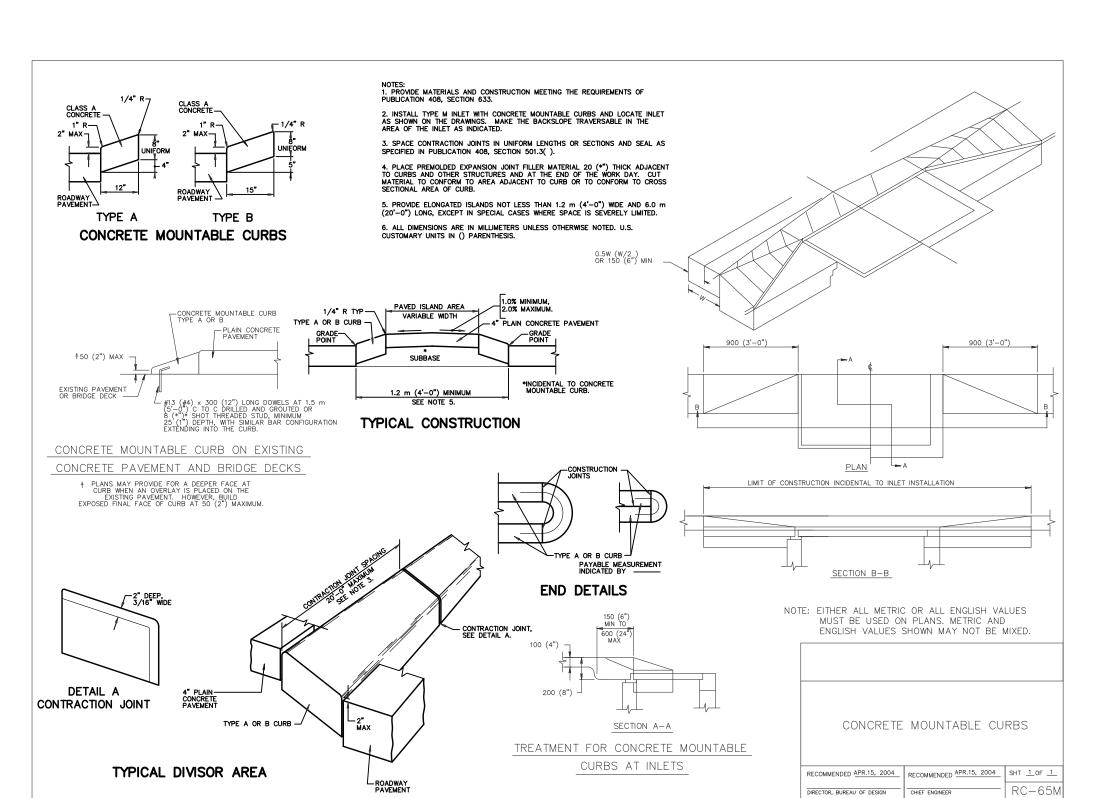
PA ONE CALL SYSTEM, INC.

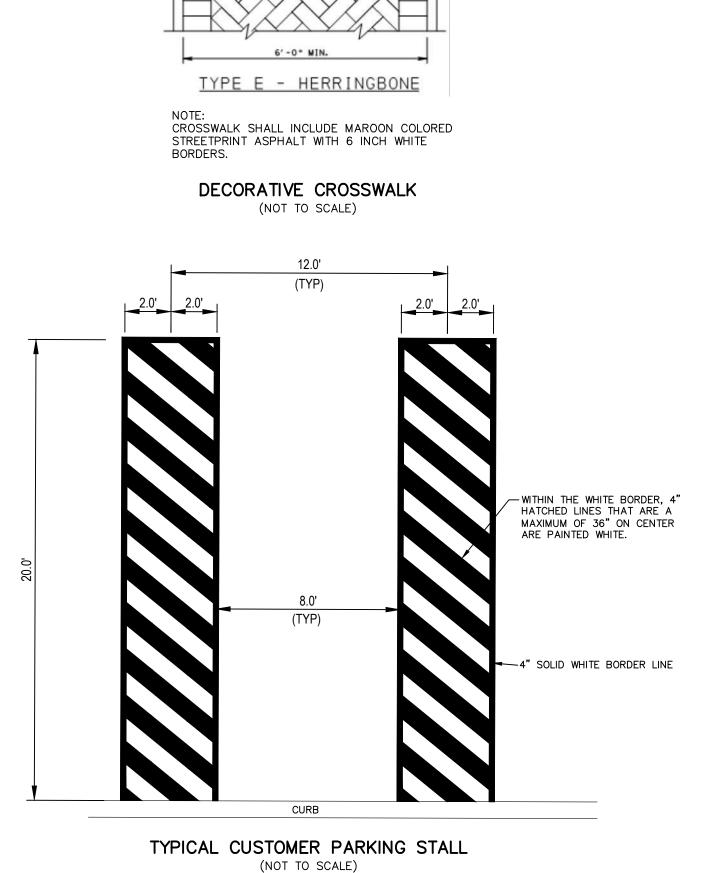
PENNSYLVANI

AUTH DATE

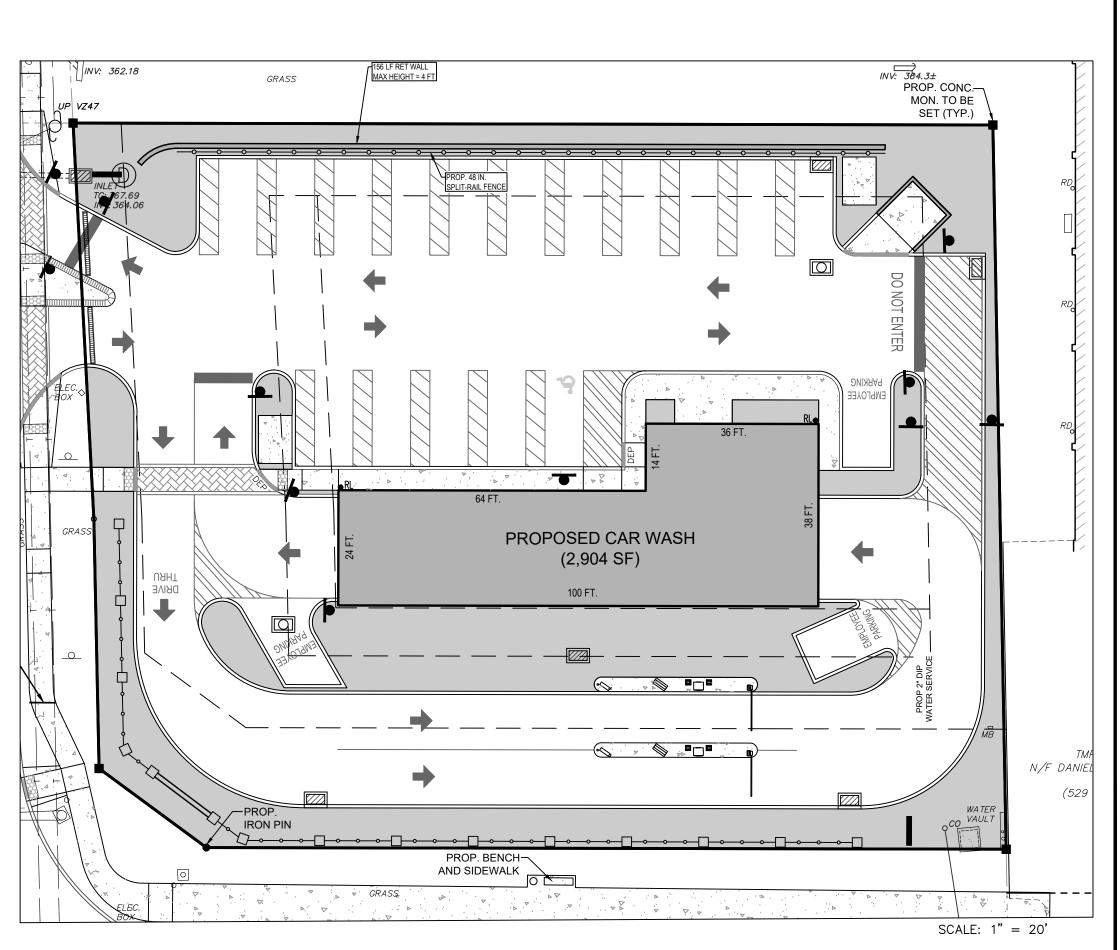
EV DESCRIPTION

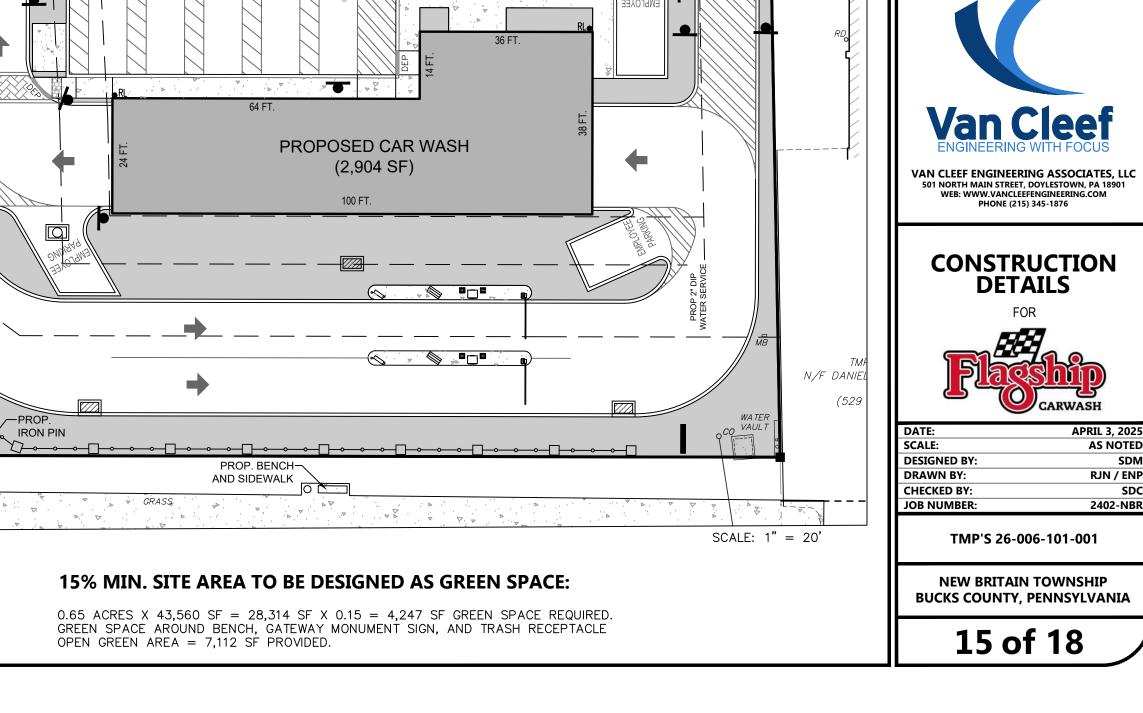
PLAN NOTATION

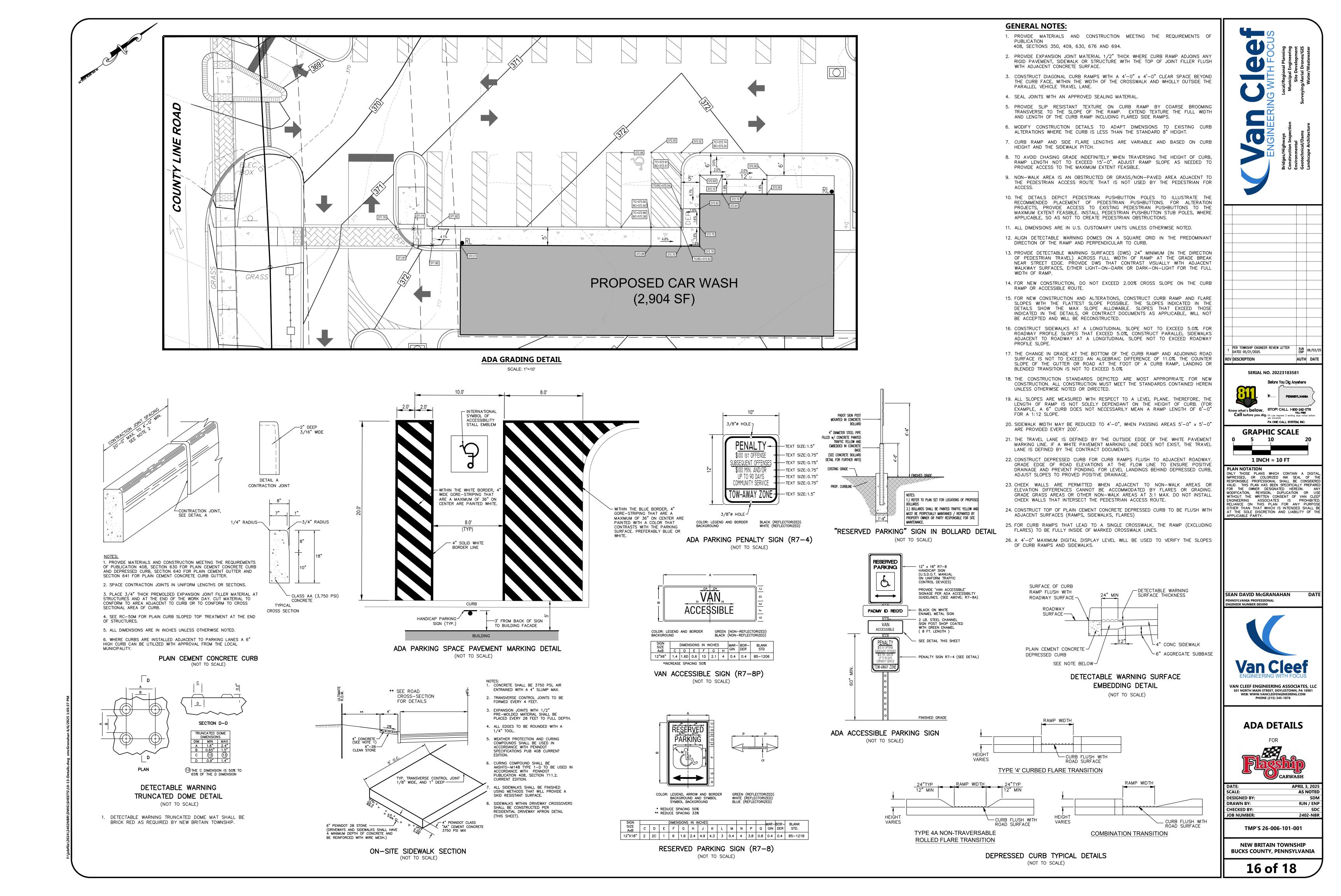


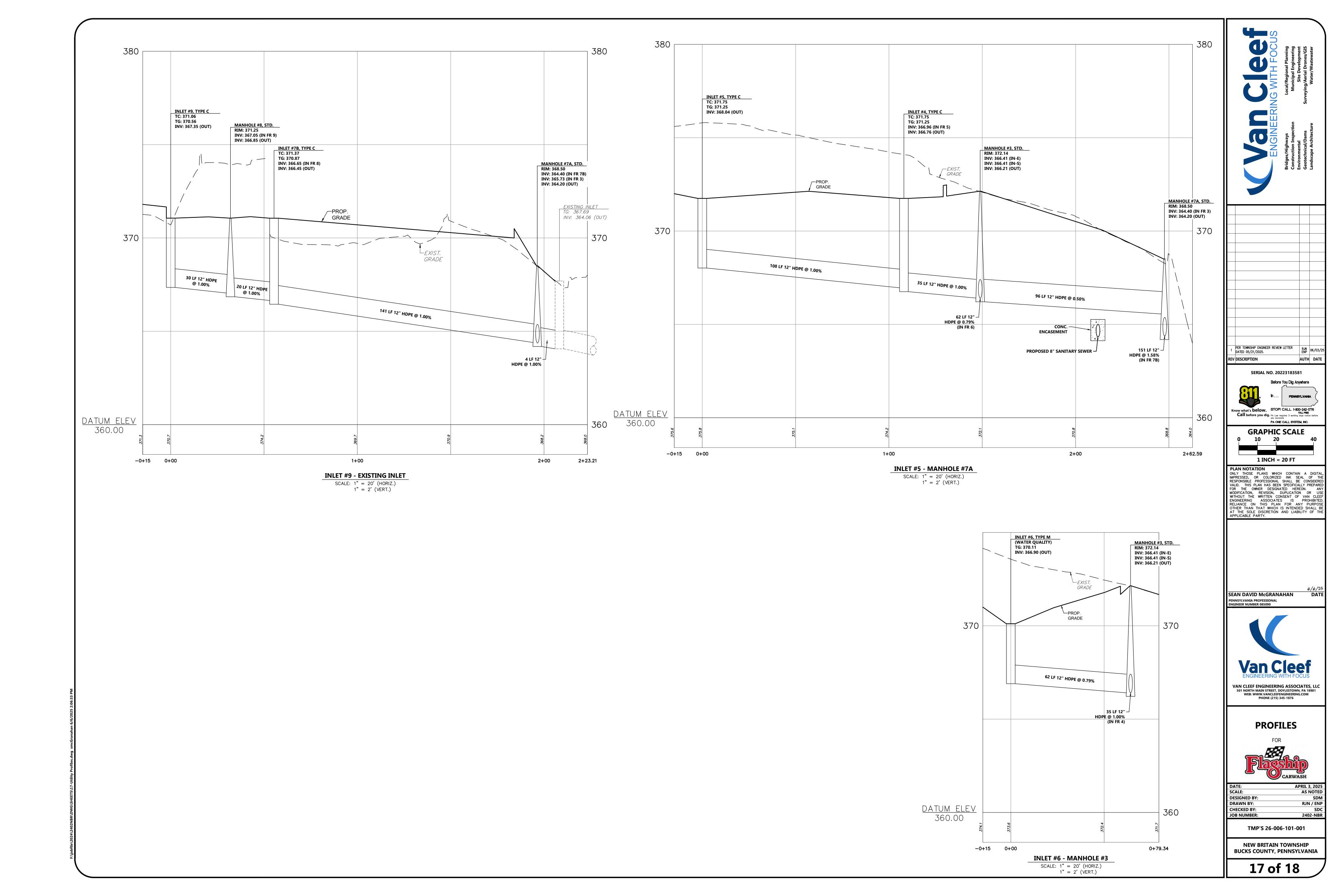


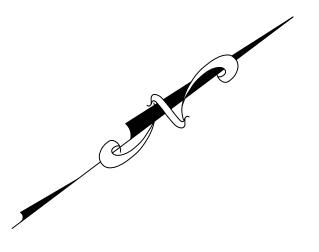
6" CROSSWALK BORDER (REQUIRED) EACH SIDE.

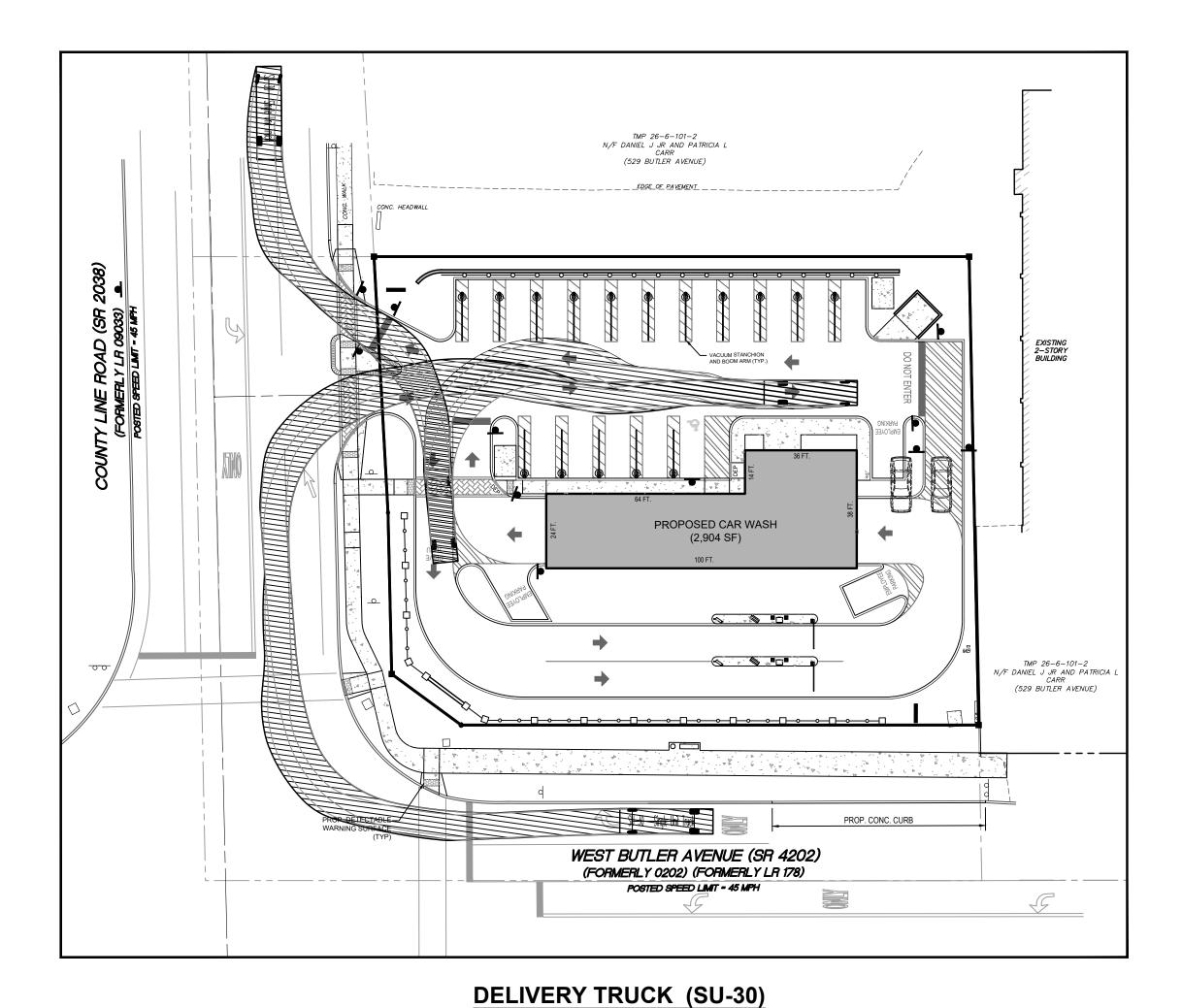


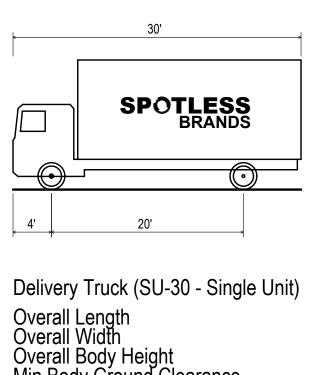




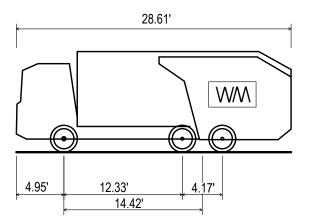






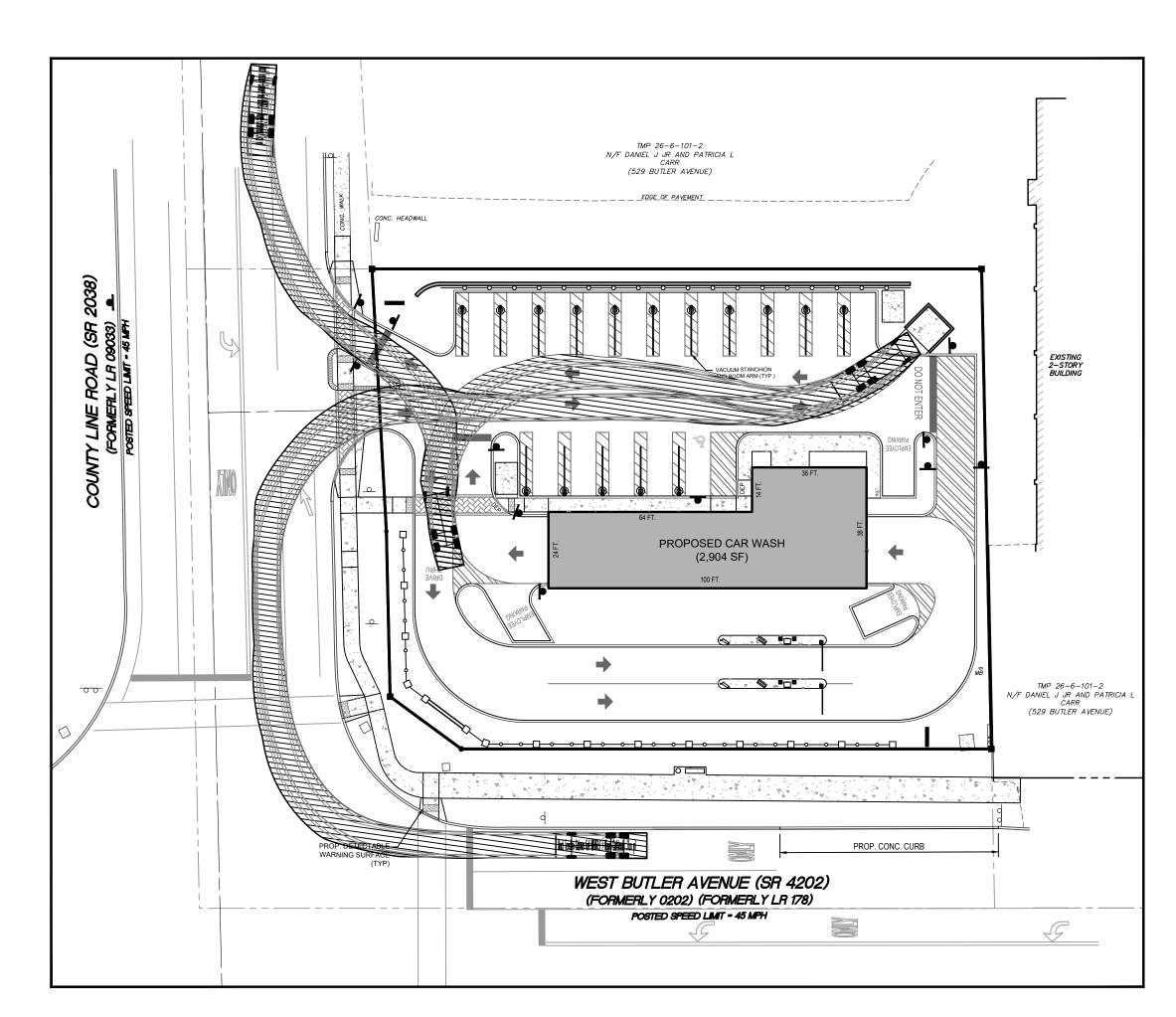


Overall Length
Overall Width
Overall Body Height
Min Body Ground Clearance
Track Width
Lock-to-lock time
Max Steering Angle (Virtual) 30.00 ft 8.00 ft 13.50 ft 1.37 ft 8.00 ft 5.00s 31.80°

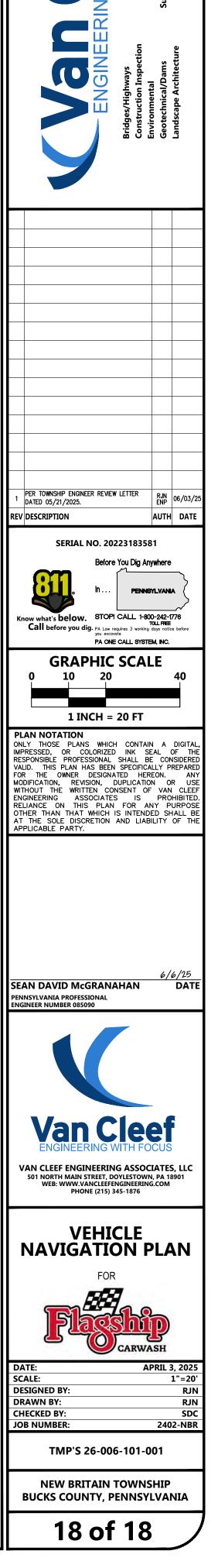


Mack TerraPro Cabover Trash Truck Overall Length
Overall Width
Overall Body Height
Min Body Ground Clearance
Track Width Lock-to-lock time Curb to Curb Turning Radius

28.61 ft 8.00 ft 10.48 ft 1.31 ft 8.00 ft 6.00s 33.50 ft



TRASH TRUCK (MACK TERRAPRO CABOVER)





ENGINEERING & CONSULTING SERVICES

June 17, 2025

File No. 220705201

Doreen Curtin, Zoning Officer New Britain Township 207 Park Avenue Chalfont, PA 18914

Reference: 545 W. Butler Avenue (Spotless Carwash) – Preliminary/ Final Plan Review 2

TMP #26-006-101-001

Dear Doreen:

Pursuant to your request, Gilmore & Associates, Inc. has reviewed the revised Preliminary Land Development Plan for the above-referenced project. Accordingly, we offer the following comments for consideration:

#### I. Submission

- A. Preliminary/Final Land Development Plans for Flagship Carwash, as prepared by Van Cleef Engineering Associates, LLC, consisting of sixteen (16) sheets, dated April 3, 2025, last revised June 3, 3025.
- B. Post Construction Stormwater Conveyance Calculation for Flagship Carwash, as prepared by Van Cleef Engineering Associates, LLC, dated April 3, 2025, last revised June 3, 2025.
- C. Supplemental Traffic data received June 12, 2025 from Albert Federico Consulting, LLCWaiver Request letter for Flagship Car Wash, as prepared by Van Cleef Engineering Associates, LLC, dated April 3, 2025, revised June 6, 2025.

#### II. Reference Documentation

A. Zoning Hearing Board Decision of Spotless Brands, LLC, dated March 4, 2025.

#### III. General Description

The 0.65-acre parcel is located at the intersection of West Butler Avenue (S.R. 4202) and County Line Road (S.R. 2038) within New Britain Township's C-1 Commercial Zoning District and Butler Corridor Overlay District. The site previously contained Wawa and is now vacant with a 24-space parking lot and full access from both West Butler Avenue and County Line Road. The site is currently at maximum 75% permitted impervious area for a lot in the C-1 Zoning District.

The Applicant, Flagship Pennsylvania Propco LLC, proposes to remove the existing building and construct a 2,904-sf drive-through car wash, a J19 Use, which is permitted by-right in the C-1 Zoning District. The plan shows a drive-through, 16-space parking area with 15 vacuums and 3 additional employee parking spaces. A double lane is proposed for queuing and a bypass lane provided as requested by the Township. The existing access from County Line Road is proposed to be modified to a right-in/ right-out access while the West Butler Avenue access is proposed to be eliminated. The plan includes a possible gateway sign along with Butler Corridor Overlay features. The project is exempt from stormwater management due to the amount of impervious surface area not being increased.

#### IV. Review Comments

#### A. Subdivision and Land Development Ordinance Waivers

The following waivers have been formally requested by the Applicant:

- 1. §22-403 From providing separate preliminary and final submissions.
- 2. §22-706.1.D & §22-708.6.I From the requirement that all curbs have a minimum 5-foot radius, which we support.
- 3. §22-708.2.F From the requirement to provide 6-foot-wide sidewalk adjacent to parking stalls. The sidewalk along the building has a width of 4.33 feet. We have no objection to this waiver request conditioned upon the Applicant providing wheel stops for these spaces to prevent overhang.
- 4. §22-711.4.C From the requirement that the top and bottom of slopes be a minimum of five (5) feet from the property line. The proposed grading extends to the northeastern and northwestern property lines. We have no objection to this waiver.
- 5. §22-712.5.E From the requirement that storm sewers be reinforced concrete pipe with a minimum diameter of 15 inches. The plan proposes 12-inch HDPE pipe within the site. We have no objection to this waiver as the submitted calculations indicate that the 12-inch storm pipe is adequate to convey the anticipated flows.
- 6. §22-722.4.C.(1)(a)&(b) and (2) From the Butler Avenue Corridor Overlay District 25-foot setback requirement for parking areas and interior drives from Butler Avenue and 10-foot setback from other street lines. The closest drive-through lane is 8 feet from the Butler Avenue legal right-ofway, where the existing parking lot is 20 feet, and 8 feet from County Line Road. We have no objection to this waiver.
- 7. §22-722.4.C.(7)(b) From the requirement that all street trees be planted in the planting strip between the curbline and the sidewalk. The plan proposes to install the street trees between the sidewalk and the Butler Avenue Corridor Overlay District buffer fence as requested by the Township. We have no objection to this waiver.
- 8. §22-722.4.D.(6) From the requirement to provide bicycle parking, which we support.

#### B. Subdivision and Land Development Ordinance

We offer the following comments with respect to the current New Britain Township Subdivision and Land Development Ordinance:

- 1. §22-401.8 The Township shall confirm that the applicant sent out the project and Planning Commission Meeting notification to properties within 1,000 feet of the proposed development.
- 2. §22-502.1 A review of the plan standards and technical information will be provided directly to the Design Engineer under separate cover.
- 3. §22-704 The Existing Features/ Demolition Plan notes that a portion of the existing driveway pavement for TMP 26-6-101-2 that extends onto the subject tract is to be removed. In addition, the business sign for this adjacent property appears to be on the subject property as well. The Applicant should coordinate with the adjacent owner on the removal or relocation of these features, or an easement should be provided for these features to remain.
- 4. §22-705 All plans that require access to a street or highway under the jurisdiction of PennDOT shall require a Highway Occupancy Permit. Any documentation submitted as part of the HOP process shall be simultaneously submitted to the municipality for review purposes.
- 5. §22-710.2.D. The water supply for fire protection shall be approved by the Fire Marshall. We note that the domestic and fire services are required as the building is required to be sprinklered.

- 6. §22-711.4 A retaining wall is proposed along the northwestern property line with an exposed wall height over 4 feet at the middle. The following comments should be addressed:
  - a. The Site Plan shows the proposed fence 1.5' from the face of curb. The proposed split rail fence does not provide a sufficient barrier at the edge of the retaining wall. We recommend a guiderail, concrete wheel stops, or other edge protection be considered to protect vehicles.
  - b. The retaining wall has a maximum height of 4 feet and building code requires fall protection for an elevation change exceeding 30 inches. Appropriate fall protection shall be provided.
  - c. Retaining wall specifications and design details shall be approved by the Township Engineer prior to final plan approval. The wall design should consider curb cross-section and paving sections if they overlap the geogrid, and other features such as vacuums to verify that the wall can be constructed as designed. Similarly, the proposed storm pipe between Manhole MH#7A and Inlet INL#7B is directly behind the retaining wall and may conflict with the wall reinforcement. We recommend the design engineer consider realigning the pipe connections to avoid this conflict.
- 7. §22-714.7 Medium activity nonresidential parking areas shall have a minimum footcandle coverage of 0.4 and an 4:1 avg:min ratio. The Calculation Summary on the Lighting Plan notes a minimum footcandle coverage of 0.1 and an avg:min ratio of 21 for the parking circulation area. In addition, two parking spaces on the northwestern side of the parking lot appear to have minimal coverage. The lighting should be revised to meet the minimum and uniformity requirements.
- 8. §§22-715.2.C.(1) & G(2) The amount of land dedicated for park and recreation areas for nonresidential subdivisions shall be 2,500 square feet per 4,000 square feet of building area unless a fee-in-lieu of land is offered. Based on a proposed building area of 2,904 square feet at a rate of \$2,500 per 4,000 s.f., a fee of \$1,815.00 is required.
- 9. §22-720 A PADEP Sewage Facilities Planning Module shall be submitted for review.
- 10. §22-722.4.B.(2)&(4) The subject property is within the Township's Butler Avenue Corridor Overlay District (COD) and shall conform to this section as required by Zoning §27-2118. The proposed building materials shall be stone, brick, slate, etc. We recommend the preliminary renderings for the building, gateway sign, retaining wall, buffer fence pillars, lighting, landscaping, fencing, etc. be presented to the Supervisors and Planning Commission for discussion.
- 11. §22-905.1.A The Applicant is responsible for any other required approvals, permits, etc. (i.e., BCPC, PennDOT, BCCD, Fire Marshal, NWWA, CNBJSA, etc.) as applicable.

#### C. Traffic Comments

- §27-2501 Though PennDOT has previously reviewed the Traffic Impact Analysis: (TIA) and has no additional comments, we request the collision data analysis be updated to include a summary of the crash data and indicate any accident clusters that should reviewed for corrective measures at the study area intersections.
- 2. The Applicant should determine if the signal equipment on the northeast corner of County Line Road and West Butler Avenue needs to be modified due to the relocation and modifications to the sidewalk. The Applicant will need to revise the Traffic Signal Permit plan to incorporate the modifications to any signal equipment and sidewalk modifications.
- 3. The plans show the curb ramps at the intersection of County Line Road and West Butler Avenue are being replaced. The pedestrian push button poles should be shown on the plans.
- 4. At the intersection of County Line Road and West Butler Avenue, we recommend the signal backplates be updated to louvered backplates with reflective yellow tape.
- 5. There may be conflicts between customers maneuvering the site and trash pickup based on the truck turning movements, therefore, we recommend trash pickup hours be limited to outside of the hours of operation.

#### D. Stormwater Management Ordinance

- 1. §26-121.8.C We recommend green infrastructure and/or low impact development practices be considered and documented for the site to offset the impacts of the impervious on the PennDOT MS4 storm sewer system and downslope properties receiving stormwater from the site.
- 2. §26-132.2.E, -164.1, & -165.2 Inspection, operations, and maintenance responsibilities for the proposed storm sewer facilities shall be provided on the plans for the stormwater conveyance systems and the "Constructed Filter" at Inlet INL#6. The Applicant shall sign an Operation and Maintenance (O&M) Agreement prepared by the Township covering all privately owned storm sewer facilities.
- 3. §26-151.A A Storm Sewer Maintenance Fee applies to all existing and proposed storm sewer located in public rights-of-way within the Township at a rate of \$2.50 per linear foot of existing/ proposed roadway. Based on 362 feet of frontage, a fee of \$905.00 is required.

If you have any questions regarding the above, please contact this office.

Sincerely,

Janene Marchand, P.E.

Gilmore & Associates, Inc.

Sanura Manchamal

Township Engineers & Assistant Township Zoning Officer

JM/tw

CC: Daniel Fox, Township Manager
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